



City of Oakland & Metropolitan Transportation Commission

DOWNTOWN OAKLAND PARKING STUDY

Technical Memorandum #2: Existing Conditions ***FINAL***

January 2016

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1 INTRODUCTION

This report provides a comprehensive assessment of downtown Oakland's parking supply and utilization. It presents the results of three major data collection efforts:

- A comprehensive inventory of the study area's on-street and off-street public parking. This includes privately-owned, City-owned, and State, County, or other public agency-owned facilities.
- Parking occupancy counts of all on-street spaces and all City-owned off-street parking facilities in the study area, conducted hourly on a Thursday and a Saturday in March 2015 between 8 a.m. and 9 p.m.
- A follow-up survey assessing the use of Disabled Person Parking Placards and License Plates at on-street parking meters, conducted hourly on several weekdays in October 2015 between 8 a.m. and 6 p.m.

More than a dozen surveyors, driving video-equipped vehicles, were deployed in the field simultaneously during the parking occupancy counts, in order to complete hourly surveys of the more than 10,000 City-owned parking spaces.¹ For the follow-up Disabled Person Parking Placard and License Plate² surveys, which assessed both the share of on-street parking meters occupied by vehicles with disabled placards and their length of stay, surveyors collected data by hand, walking their routes hourly in order to identify vehicles displaying disabled placards and then record the last three digits of these vehicles' license plates.

This report, *Technical Memorandum #2: Existing Conditions*, is the second deliverable of the Downtown Oakland Parking Study. The first deliverable, *Technical Memorandum #1: Context Analysis*, reviewed and documented the City's parking policies; parking prices and regulations for City-owned parking facilities; significant conclusions from previous studies; and other background information communicated during interviews and correspondence with City staff. Future deliverables will recommend parking management strategies and an implementation plan, based upon all of the data collected, public input gathered, and feedback received throughout the study.

¹ Parking occupancy data for non-City owned parking lots and garages was not available from the owners of these facilities. Due to both budget limitations and the difficulty of obtaining permission to conduct occupancy counts in private facilities, non-City-owned facilities were not included in the occupancy surveys.

² For brevity's sake, Disabled Person Parking Placards and License Plates and Disabled Veterans License Plates are collectively referred to as "disabled parking placards" or "disabled placards" throughout the remainder of this report. "Parking spaces designated for disabled persons and disabled veterans" established pursuant to California Vehicle Code Section 22511 and/or California Building Standards Code (see Title 24, Part 2, Section 1129B) are referred to as "accessible parking spaces". This nomenclature (e.g., using the term "Disabled Person" parking placard, rather than the often preferred phrase is "person with a disability", and the term "accessible parking space" rather than "disabled parking space") was chosen to maintain consistency with the terminology currently used (a) by the California Department of Motor Vehicles, (b) in the California Vehicle Code and (c) in the California Building Standards Code.

KEY FINDINGS

Nelson\Nygaard's mapping and analysis of the parking inventory and occupancy data yielded several key findings. These include:

- **In total, more than 20,000 parking spaces exist in the study area.** This includes 6,330 on-street spaces, 4,036 City-owned off-street spaces, 446 off-street spaces owned by other public agencies, and more than 9,656 privately-owned off-street spaces.
- **When the City-owned downtown parking spaces are considered as a whole, a parking surplus exists.** Overall parking occupancy for both on- and off-street City-owned spaces reached 79% at the peak hour of demand during the parking survey (Thursday, 12 p.m. to 1 p.m.). At this hour, more than 2,000 parking spaces remained vacant in the City-owned supply.
- **However, hot spots of high parking demand and localized parking shortages exist, while other lots and garages simultaneously remain underutilized.** In core business areas such as Chinatown and City Center, finding available curb parking spots can be difficult during much of the day, both on weekdays and Saturdays. The occupancy survey results appear to confirm the findings of previous studies such as the 2014 Lake Merritt Station Area Plan, which noted frequent curb parking problems in the core of Chinatown, including merchants using curbside parking spaces for storage throughout the day; illegal parking in loading zones and no parking zones; and double parking and street loading.
- **At peak hour on Thursday, three City lots and garages (Telegraph Plaza, the 18th Street Uptown Lot, and the Franklin Plaza Garage) are either nearly or entirely full.** In two other City facilities (the Dalziel Garage and the Clay Street Garage), while the “reserved” parking spaces may remain mostly vacant, “regular” parking spaces are full at the peak hour, making these facilities effectively full for the average member of the public.
- **By contrast, parking occupancy on Saturday is far lower, with overall parking occupancy reaching just 49% at the busiest hour (12 p.m. to 1 p.m.).** At this time, more than 5,000 parking spaces remain vacant in the City-owned downtown parking supply, and all of the City's off-street lots and garages have substantial excess capacity.
- **Disabled parking placard use at metered curb spaces is a significant issue.** Surveys of disabled placard use found that on numerous blocks in downtown Oakland, vehicles with disabled placards occupy most of the metered curb parking spaces most of the time. On some blocks, vehicles with disabled placards occupy more than 80% of metered curb parking spaces at the peak hours of the day. Approximately 23% of disabled placard vehicles remained parked at a meter for seven or more hours.

MAPPING WITH CITY POLICY GOALS IN MIND

On October 15, 2013, the Oakland City Council unanimously adopted, as official City policy, a set of goals and objectives entitled “Parking Principles for City of Oakland Commercial Districts.” These goals and objectives provide the overarching policy framework within which the City manages parking.³ Among other things, these parking principles established several clear principles for managing parking, including the following:

³ Brooke A. Levin, Interim Director, PWA. *Agenda Report re: Ordinance Supporting a Flexible Parking District Program*, August 23, 2013. <https://oakland.legistar.com/View.ashx?M=F&ID=2638143&GUID=B82816CE-EF18-4D2D-87D4-6017CA050209>.

- Parking should be managed to achieve an approximate 85% maximum occupancy per block so that there will always be some parking available to shoppers and visitors;
- Parking should be priced to achieve usage goals (“market pricing”); market prices may vary by area; by time of day and may be adjusted occasionally to reflect current use;
- Pricing and policies should encourage use of off-street parking lots where they are available;
- If possible, and where appropriate, time limits should be avoided in favor of market pricing.

While these principles (e.g., varying parking meter rates to achieve an approximate 85% maximum occupancy on each block) are currently being piloted in the Montclair Village Flexible Parking Pricing Pilot Project area, they have not yet been implemented in the Downtown Parking Study area. To assist with the potential future implementation of these adopted City policies in the study area, the parking occupancy maps in this memorandum are color-coded in a simple three-color format:

- Block faces of curb parking with an occupancy rate of 85% or greater are shown in red;
- Block faces of curb parking with an occupancy rate of 66-85% are shown in yellow;
- Block faces of curb parking with an occupancy rate of 65% or less are shown in green.

The occupancy maps for the City’s off-street garages and lots are also color-coded in this manner. If the City chooses to implement performance-based parking prices (a.k.a. market rate prices) in the downtown parking study area, then:

- Block faces of curb parking shown in red (85%+ occupancy) are over the City’s occupancy goal, and are therefore strong candidates for a price increase;
- Block faces of curb parking shown in yellow (66-85% occupancy) are well-used but still have parking readily available, and therefore are good candidates for leaving prices unchanged;
- Block faces of curb parking shown in green (occupancy rate of 65% or less) are underutilized, and are therefore good candidates for a price decrease.

Separate maps are provided for each parking survey period, to assist in analyzing parking demand patterns throughout the day. As might be expected, many blocks of curb parking which are full or nearly full at the peak hour are underutilized at other times. The hour-by-hour parking occupancy maps provided in this memorandum can be used to help determine how parking prices on individual blocks might be varied throughout the day. For example, Oakland may wish to implement three or four “time bands” (e.g., morning, lunchtime, afternoon and evening) for parking meter rates, with different parking rates for each of these time bands. This approach, used by San Francisco, helps set appropriate prices for blocks where parking demand varies substantially throughout the course of the day. In San Francisco, rates for the morning time band (when many shops and restaurants have not yet opened) are often low, while lunchtime rates are substantially higher.

A similar approach can be used to help determine how to adjust parking prices (including monthly, daily and hourly rates) for the City’s off-street lots and garages.

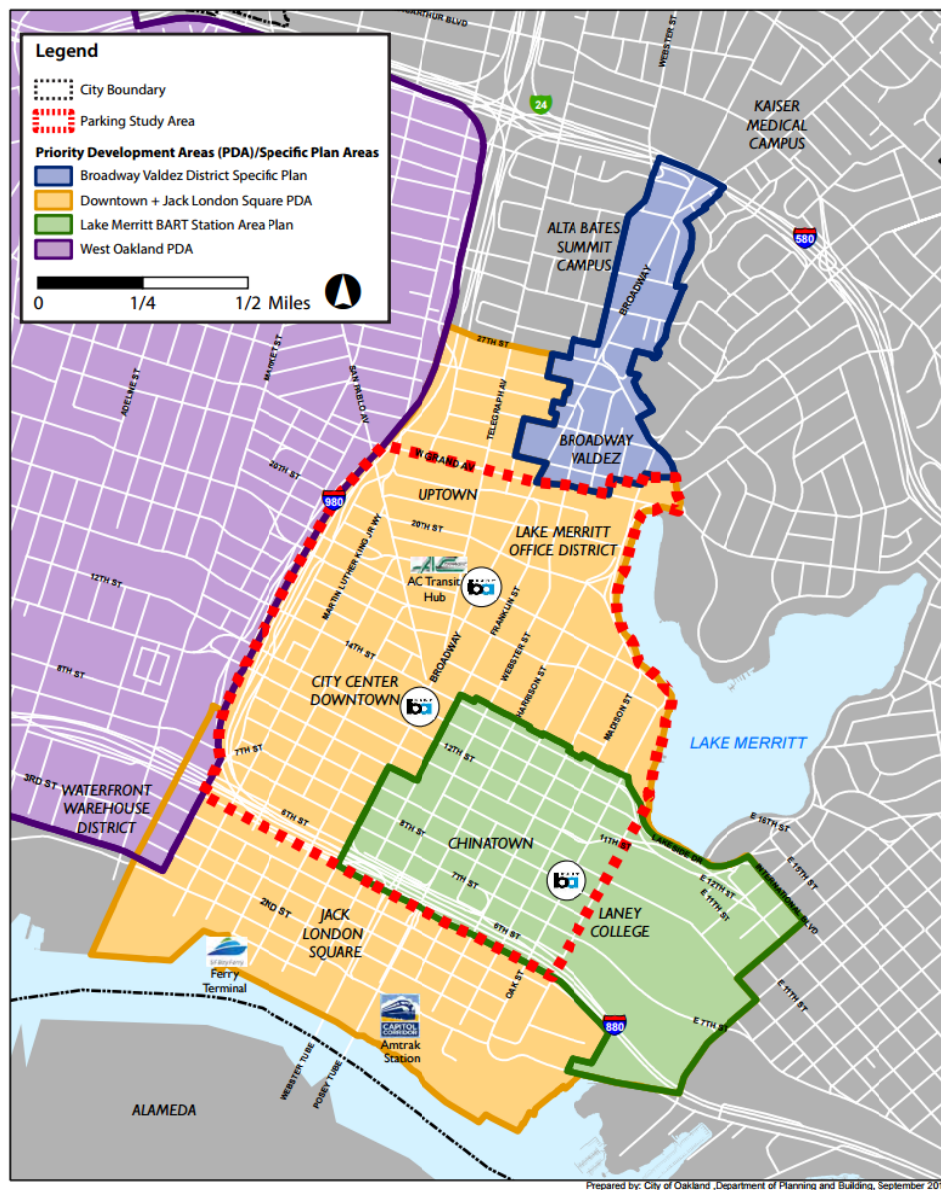
Of course, parking prices are only one lever – albeit an important one – available to help the City achieve its policy goals. Many other levers – reallocating types of parking spaces, changing time limits, improving enforcement, providing better way finding, and so on – have been identified in previous parking studies. In future deliverables, all of these strategies, as well as new ideas that may be suggested by the community, will be evaluated for their potential to (a) help alleviate the localized parking shortages identified in this memorandum, and (b) help Oakland achieve its broader economic, environmental, social equity and quality of life goals.

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2 PARKING INVENTORY

The Nelson\Nygaard team conducted an inventory of parking facilities in the study area in March 2015. The boundaries of this study area are shown in Figure 2-1.

Figure 2-1 Study Area



Methodology

Parking inventory and regulations were determined through field observations, including counting all available-to-the-public parking, including on-street spaces, public lots and garages, and parking facilities that are privately owned but open to the public. A total of 96 off-street facilities in the study area were identified from a combination of aerial photographs, city records, and observations by our surveyors. The data collection team was able to gain access to 83 of these facilities. The other 13 were either closed for construction or otherwise inaccessible to our surveyors, and are therefore not included in the inventory results.

The data collection team's work included documenting the number of parking spaces in each lot and garage, and on each blockface; noting curb parking regulations (e.g., loading zones, bus stops, no parking zones, and parking rules); marking the presence of accessible, compact, carpool spaces or other markings; documenting posted time restrictions and limits; recording posted parking fees; and noting any additional wayfinding or regulatory signage related to parking. At off-street facilities, inventory technicians noted the presence of permit parking or other restrictions posted in the immediate vicinity of specific parking spaces.

PARKING INVENTORY

Figure 2-2 provides a summary of the downtown parking inventory. In total, the survey identified 20,468 spaces, including 6,330 on-street spaces, 4,036 City-owned off-street spaces, 446 off-street spaces owned by other public agencies (Alameda County, the State of California, or BART), and 9,656 privately-owned off-street spaces.

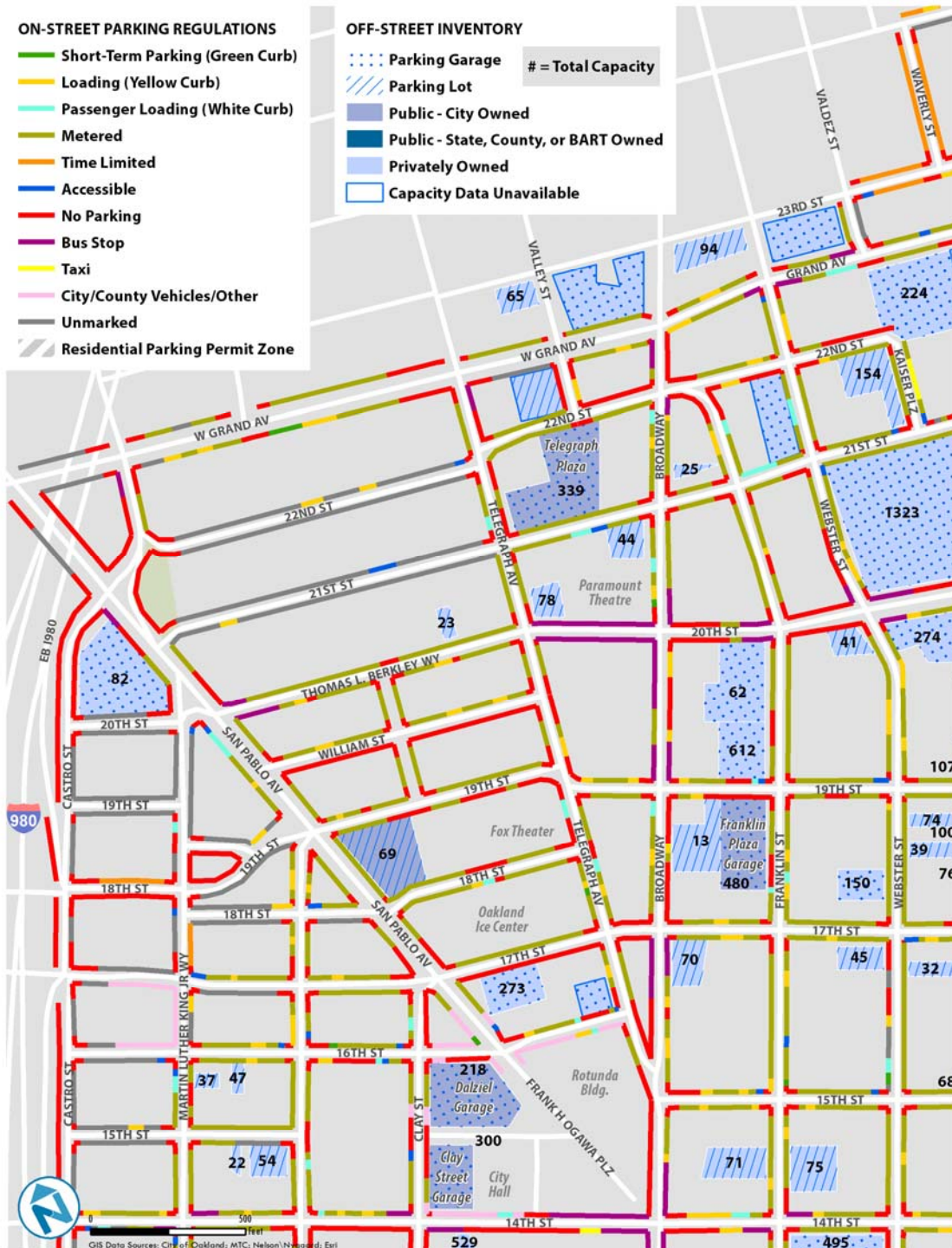
Figure 2-2 Downtown Parking Inventory, By Type of Space

	Number of Spaces	% of Total
On-Street	6,330	31%
Off-Street (City Owned)	4,036	20%
Off-Street (State, County, or BART Owned)	446	2%
Off-Street (Private)	9,656	47%
Total	20,468	100%

Figure 2-3 through Figure 2-6 depict the downtown Oakland parking inventory in more detail. On-street parking is available on most streets within the study area and comprises just over 30% of all publicly-available parking in downtown. Due to the size of the study area and the detail required to depict each space type, the inventory has been mapped in four quadrants – Northwest, Northeast, Southwest, and Southeast. A one-page version of this inventory map is provided in the Appendix.

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Figure 2-3 On- and Off-Street Parking Supply and Restrictions (Northwest Quadrant)



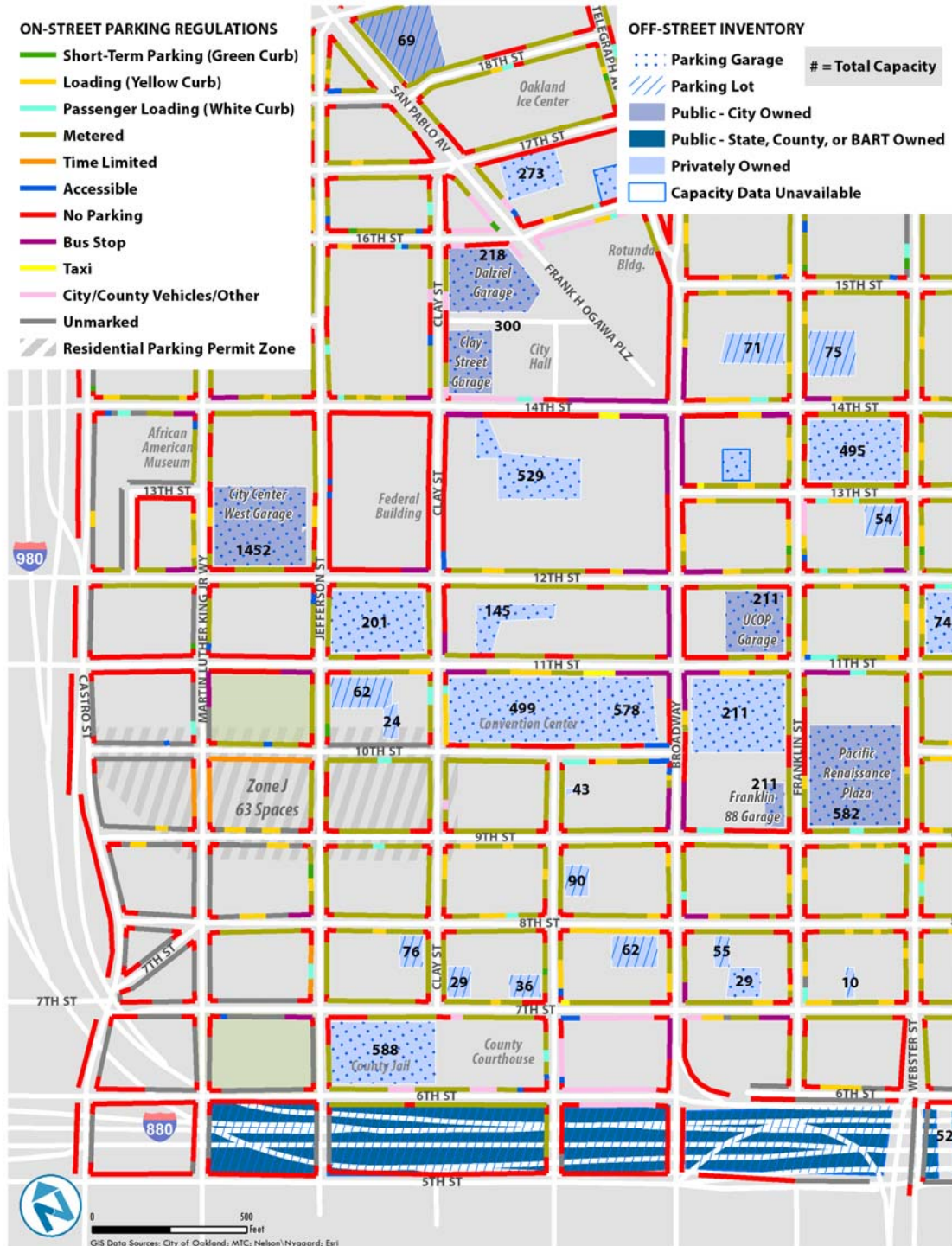
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Figure 2-4 On- and Off-Street Parking Supply and Restrictions (Northeast Quadrant)



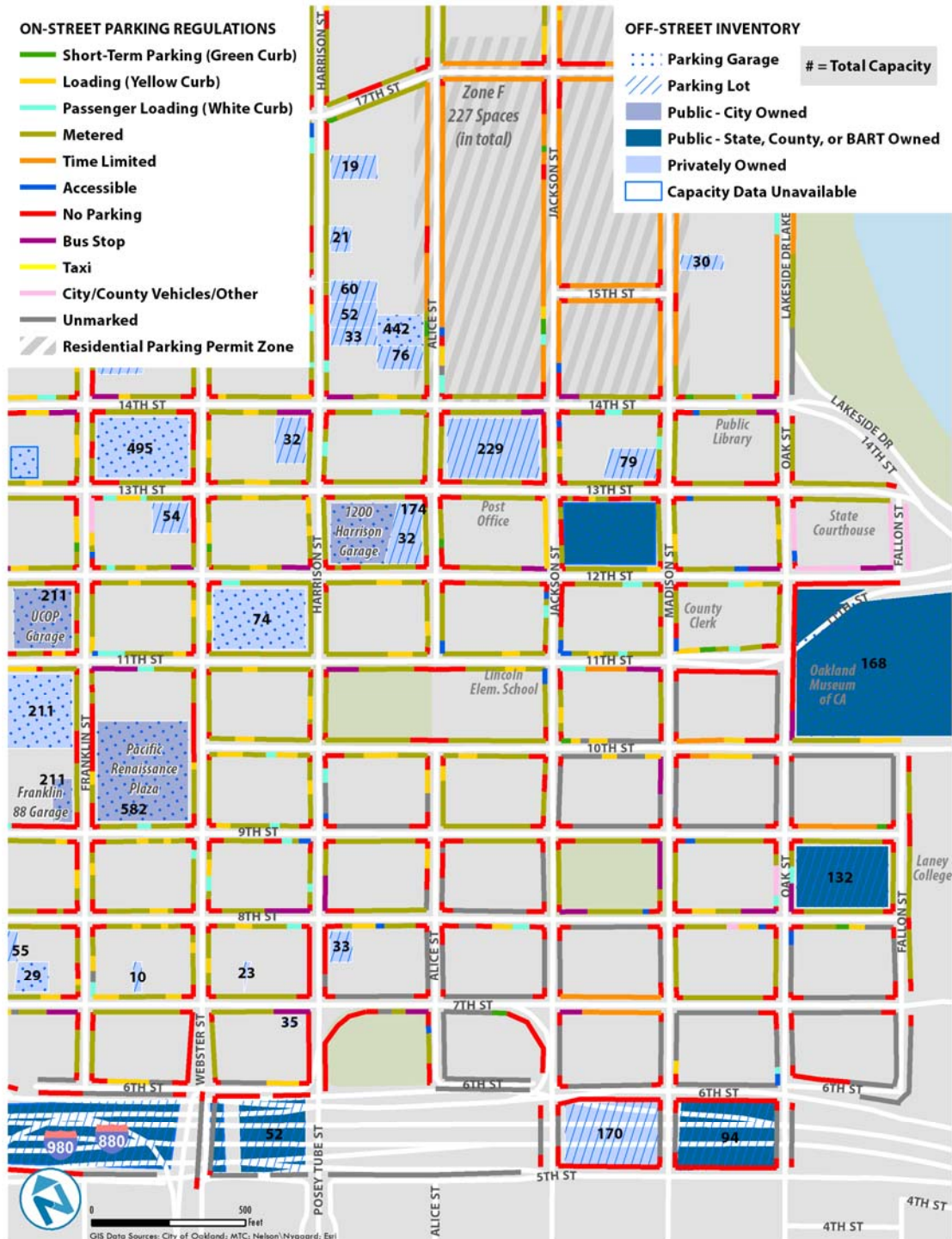
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Figure 2-5 On- and Off-Street Parking Supply and Restrictions (Southwest Quadrant)



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Figure 2-6 On- and Off-Street Parking Supply and Restrictions (Southeast Quadrant)



On-Street Parking Supply

Of the 6,330 total on-street spaces in the study area, the vast majority are regulated with either (a) time limits of up to two hours, or (b) both time limits and pricing, using meters that allow up to two hours of parking. Over half (56%) are metered. All curb parking meters in the study area charge a flat rate of \$2.00 per hour and operate from 8 AM to 6 PM, Monday through Saturday, with the exception of certain holidays.⁴

Time-limited, but not priced, parking is available on certain blocks, with most of these spaces subject to a time restriction of two hours. Loading zones account for less than 7% of on-street spaces. On-street spaces with meters and/or posted time limits are concentrated in the downtown core, while blocks on the periphery, particularly adjacent to I-980, I-880, and northwest of Uptown, are generally unmarked and therefore allow parking for up to 72 hours.

Figure 2-7 On-Street Parking Inventory, by Type of Regulation

Type	Number of Spaces	Percent
Metered	3,565	56.3%
Unmarked	1,327	21.0%
Time Limited, 30 minutes or longer	582	9.2%
Loading (Yellow Curb)*	426	6.7%
Passenger Loading (White Curb)	154	2.4%
City/County Vehicles/Other**	148	2.3%
Accessible	77	1.2%
12 minute Parking (Green Curb)	32	0.5%
Taxi	19	0.3%
Total	6,330	100%

* Includes four (4) signed Truck Loading spaces.

** Includes spaces signed and reserved for specialized uses, including: Alameda County Placards Only, BART Vehicle Only, City Car Share, City Council Staff, City Officials, City Vehicles Only, Fire Department Only, Fire Marshal Only, Mayor's Vehicle Only, OCIS Vehicles Only, Official Cars Only, Official Vehicles, Paratransit, Police, Press and City Vehicles Only, Transportation Vehicles Only.

Posted Time Limits

Time limits vary among on-street parking spaces, ranging from 3 minutes (passenger loading at white curbs) to 5 hours (a small number of metered parking spaces near Laney College), to 72 hours (all unmarked spaces). Most metered spaces (85%) are subject to a two-hour time limit. Figure 2-8 provides a breakdown of posted time limits for metered and time-limited but not metered curb parking spaces in downtown Oakland. Loading zones (yellow curbs and white curbs), Accessible and unmarked spaces are not included in this table.

⁴ All curb parking meters in the City of Oakland, with the exception of those in the Montclair Village Flexible Parking Pricing Pilot Project area, have these same rates and hours of enforcement. Technical Memorandum #1, Context Analysis, provides more detail on these policies and on the Montclair Village Flexible Pricing Pilot Project.

Figure 2-8 On-Street Parking Posted Time Limits (Excluding Loading, Accessible & Unmarked Spaces)

Time Limit	Metered (Number of Spaces)	Percent of Metered Spaces	Posted Time Limit but Unmetered (Number of Spaces)	Percent of Spaces with Posted Time Limit but No Meter
12 minute	0	0.0%	32	5.2%
30 Minute	34	1.0%	0	0.0%
1 Hour	491	13.8%	7	1.1%
2 Hour	3,009	84.4%	575	93.6%
5 Hour	31	0.9%	0	0.0%
Total	3,565	100%	614	100%

Loading Zones, 12-minute and Accessible Spaces

As discussed in Technical Memorandum #1: Context Analysis, the City relies on five distinct colors for marking curbs, each signifying a particular use designation. In addition to **red curbs**, which signify no parking zones, there are four primary colors used for marking curbs in downtown Oakland.⁵

Yellow curbs (Loading): Loading and unloading of passengers and materials between 7 a.m. and 6 p.m. Loading or unloading of passengers should not exceed three (3) minutes, and loading or unloading of materials from a vehicle with commercial plates should not exceed 30 minutes. Regulations are not enforced on Sundays or Meter Holidays.⁶

White curbs (Passenger Loading): Loading and unloading of passengers, not to exceed three (3) minutes. Most white curb restrictions apply 7 a.m. to 6 p.m., except:

- White curbs in front of a hotel or hospital, which are applicable 24/7.
- White curbs in front of a theater, hall, or other place of public assembly apply at all times except when the building is closed; in this case, vehicles may not park for longer than one hour.

Green curbs (12 Minute Parking): Short-term parking; no standing or parking for longer than 12 minutes at any time between 8 a.m. and 6 p.m., excluding Sundays and Parking Holidays.⁷

Blue curbs (Accessible): Reserved for vehicles displaying valid Disabled Person Parking Placards or Plates.

⁵ City of Oakland. "Parking ~ City of Oakland, California." Accessed March 3, 2015. <http://www2.oaklandnet.com/Parking/index.htm#CurbColor>.

⁶ Note: This count includes four (4) parking spaces signed for Truck Loading.

⁷ Note: Inventory data collection technicians noted two anomalies: one green curb space with a posted 30-minute time limit, and another with a posted 20-minute time limit, in effect Monday through Friday only.

Other Parking Regulations Downtown

In the course of the inventory, data collection technicians recorded the presence of a number of special parking regulations for very specific uses. These included parking zones for City and County staff (including signed spaces for City Council members and Oakland Police and Fire Department staff/vehicles), taxis, and City Car Share vehicles. Together, spaces with these special regulations comprise less than 3% of the total on-street supply.

Lastly, a total of 290 on-street parking spaces are signed as Residential Parking Permit zones, with 63 spaces signed for Permit area J, and 227 signed for Permit area F.

Off-Street Parking Facilities

Off-street parking is provided in 96 off-street lots and garages in the study area. Of these, 83 facilities were readily accessible to the data collection team, totaling 14,133 spaces.⁸ Thirteen off-street parking facilities were either closed for construction or otherwise unavailable to be counted at the time of the inventory. Although these 83 facilities are open to the public, not all of the spaces contained within them are available for public use; approximately 10% of the off-street parking spaces inventoried are reserved for a particular person or type of user.

Additionally, the study area contains numerous additional privately-owned parking lots and garages, such as gated residential parking garages, to which our surveyors were unable to gain access. These private parking facilities therefore went uncounted and are not included in this inventory.

Figure 2-9 presents an overview of the types of parking regulations at off-street facilities in the study area. The definitions are as follows:

- **Regular:** Unmarked spaces (i.e., not designated for monthly parkers, residents only, or indicated with any other special markings or signage).
- **Accessible:** Parking spaces designated as reserved for disabled persons and disabled veterans pursuant to California Vehicle Code Section 22511 and/or the California Building Standards Code Title 24, Part 2, Section 1129B.
- **Reserved:** Any parking space that had a sign or plaque identifying it as reserved for a particular person or particular type of user. In other words, any space that was not open and available to the general public. This category includes several types of spaces not typically available for use by the general public, such as Security, Hertz Rent-a-Car, and reserved Accessible spaces.
- **Other:** Any other type of regulation governing publicly-available parking. Types of parking covered by this designation include: 2-Minute Limit, 5-Minute Limit, and 30-Minute Limit.

Figure 2-9 Off-Street Parking Regulations by Owner Type

Totals by Type	Regular	Disabled	Reserved	Other	Total
City Owned	3,507	68	459	2	4,036
Other Public Agency (State, County, or BART)	403	16	9	18	446
Privately Owned	8,439	203	891	123	9,656

⁸ Additionally, a portion of the City Center Garage is open to residents only. Inventory data collectors were unable to gain access to this area to complete a count of the number of spaces in this section of the garage.

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Grand Total	12,349	287	1,359	143	14,138
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Parking pricing varies in City-owned off-street parking facilities. Current rates for City-owned off-street parking facilities are provided in Appendix B.

3 PARKING UTILIZATION

This section provides an overview of the results from the parking occupancy data collection. It includes a summary of the count methodology, as well as the key findings. This chapter focuses on parking demand at peak hours. A full set of maps depicting hour-by-hour occupancy rates over the course of the entire parking survey period is provided as Appendix A.

METHODOLOGY

Occupancy counts were conducted on the following days⁹:

- Thursday, March 5th: On-street weekday occupancy.
- Saturday, March 7th: On-street Saturday occupancy.
- Thursday, March 12th: Off-street weekday occupancy.
- Saturday, March 14th: Off-street Saturday occupancy.

On each of these days, occupancy data was collected in hourly intervals from 8 a.m. to 9 p.m. Occupancy data was collected for all on-street spaces and all City-owned off-street facilities in the study area.¹⁰ Unless otherwise noted, parking occupancy figures depicted in the following graphs and maps describe total parking occupancy (i.e., the total number of parked vehicles divided by the total number of parking spaces) in the spaces surveyed.

PARKING OCCUPANCY RATES

As shown in Figure 3-1, the peak hour of parking occupancy in the study area occurred on Thursday between noon and 1 PM, when overall parking occupancy reached 79%.¹¹ Generally, parking occupancy on Thursday followed a typical workday pattern, peaking at the noon hour and then gradually falling in the afternoon and early evening. On Saturday, overall parking occupancy peaked at 49% at noon.

⁹ On-street and off-street occupancy counts were conducted on two different non-holiday weeks, with more than a dozen surveyors, driving video-equipped vehicles, in the field each week. This was done in order to break the logistical problem of surveying more than 10,000 parking spaces on an hourly basis into two data collection efforts of a manageable size.

¹⁰ Parking occupancy data for non-City owned facilities was not available from the owners of these facilities. Due to both budget limitations and the difficulty of obtaining permission to conduct occupancy counts in private facilities, non-City-owned facilities were not included in the occupancy surveys.

¹¹ On Thursday, a few spaces (23 in total) were closed for construction. On Saturday, these spaces were available for parking. These spaces were counted as occupied on Thursday. These closed spaces have a nominal effect (boosting Thursday on-street occupancy by 0.3%), and therefore did not substantially affect the overall conclusions of this report.

Figure 3-1 Overall Parking Occupancy, Weekday versus Saturday

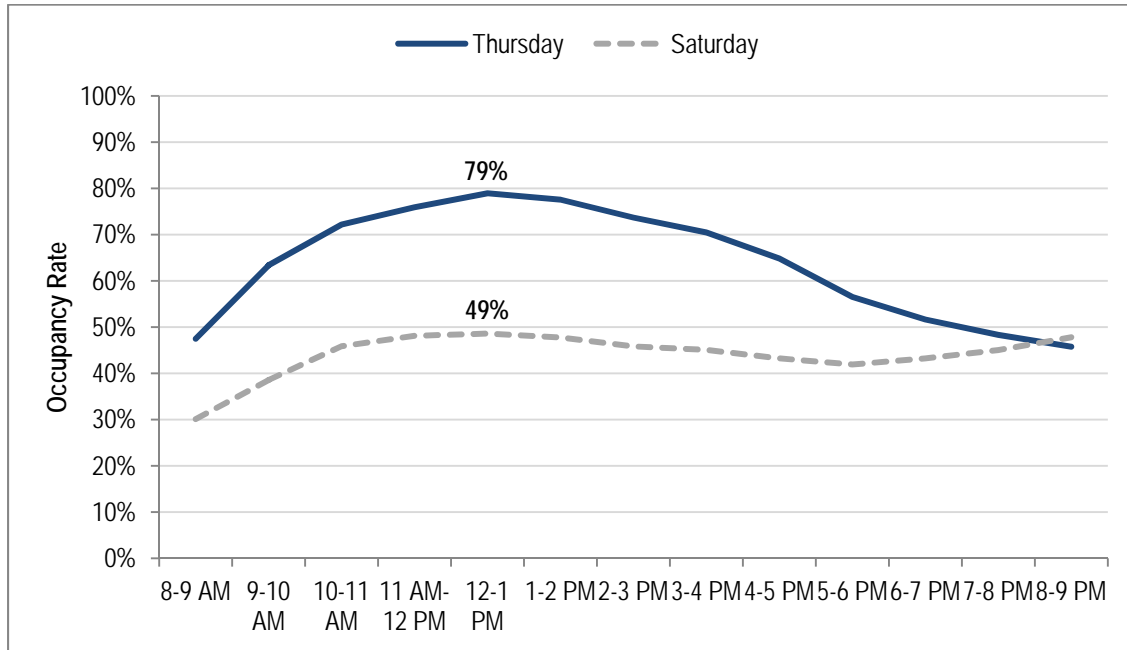


Figure 3-2 and Figure 3-3 compare on-street versus off-street parking occupancy for Thursday and Saturday, respectively.

On Thursday, on-street occupancy peaked at 84% (12-1 p.m.), and decreased gradually thereafter. Occupancy remained at about 70% during the dinner period and into the evening (5-9 p.m.). Off-street occupancy followed a much different pattern, peaking at 71% during the lunch hour (12-1 p.m.), and then steadily declining during the late afternoon and early evening to a low of just 11%.

On Saturday, on-street parking occupancy was more consistent throughout the day, increasing to 68% at noon, then falling slightly in the afternoon before rising to a peak of 70% from 8-9 p.m. Off-street occupancy was much lower. Parking occupancy at off-street facilities gradually increased to a peak of 22% from 3-4 p.m., falling again through the dinner period. Occupancy increased again during the 8-9 p.m. survey period.

Note that several major off-street facilities are closed on Saturday – including the UCOP, 1200 Harrison, and City Center West facilities – due to the low level of parking demand and the substantial cost of staffing garages. Even with this temporary reduction of 1,837 off-street parking spaces, parking occupancy is still dramatically lower than on-street occupancy, with a peak of 41% from 3 -4 p.m. Regardless, all calculations of off-street parking include these facilities in the overall supply in order to consistently compare Thursday and Saturday parking utilization.

Figure 3-2 On-Street Versus Off-Street Parking Occupancy, Thursday

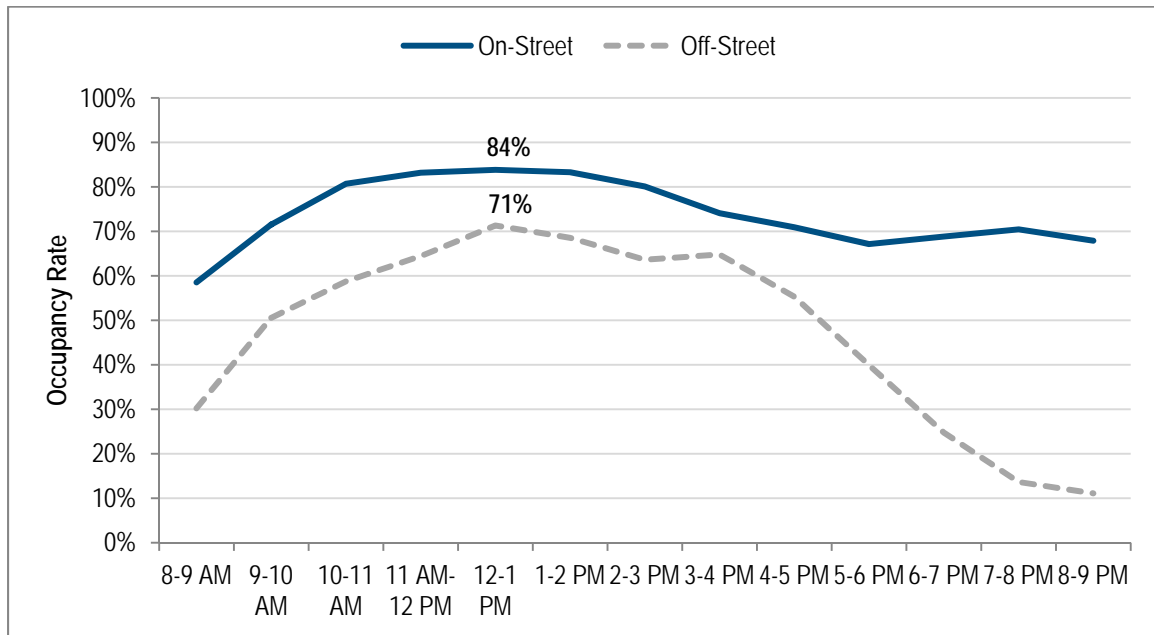


Figure 3-3 On-Street Versus Off-Street Parking Occupancy, Saturday

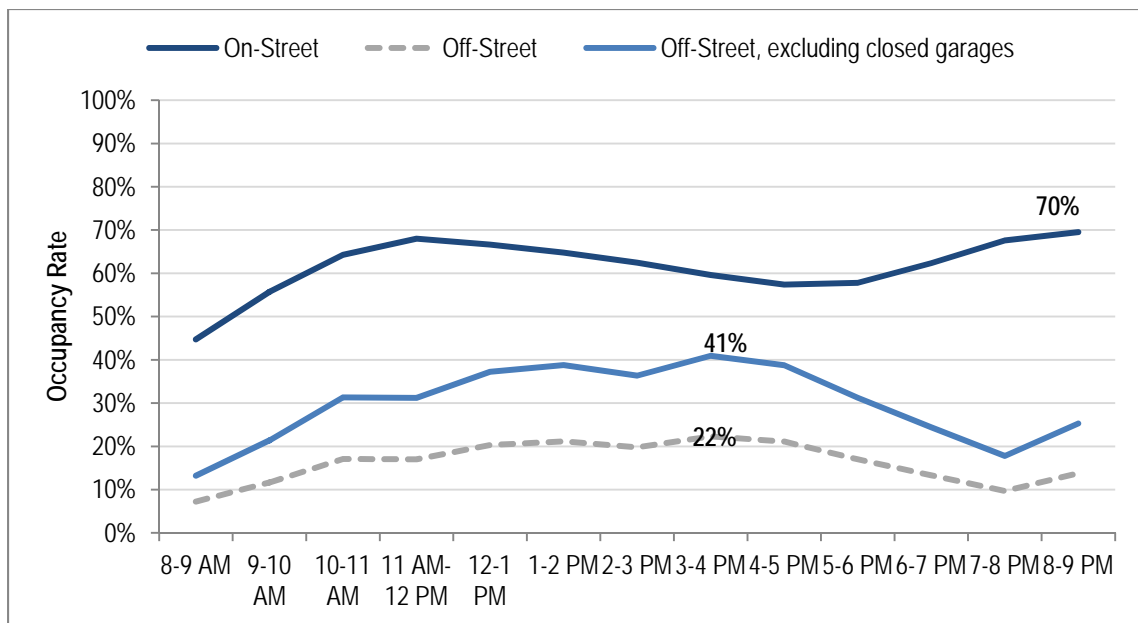


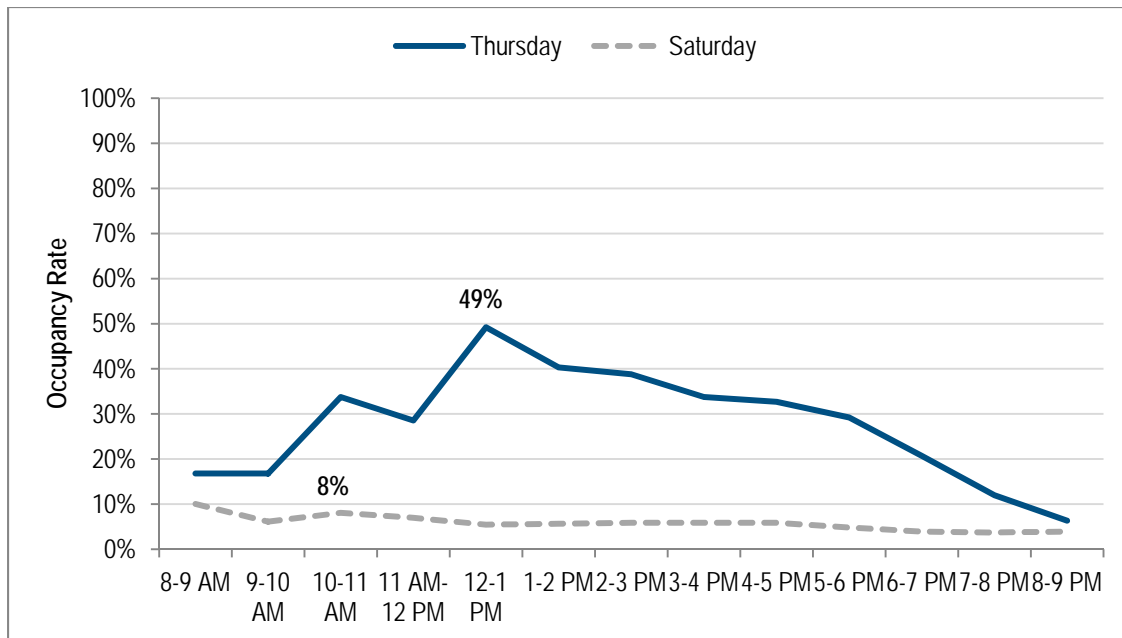
Figure 3-5 through Figure 3-8 map peak-hour occupancy in downtown Oakland. These maps show the occupancy level for each block face of curb parking and each off-street facility during the peak hours of parking demand.

On Thursday, the overall parking supply reached peak occupancy during the 12-1 p.m. survey period. At this hour, overall parking occupancy reached 79%. Figure 3-5 depicts total parking occupancy during this peak hour. At this time, over half of on-street blockfaces exceeded 85% occupancy. Curb parking demand was spread throughout the study area, with on-street spaces in Chinatown and other business areas around City Hall and near Franklin Plaza consistently exceeding 85% occupancy. At just three of the City's off-street facilities (Telegraph Plaza, the 18th Street Uptown Lot, and the Franklin Plaza Garage), total parking occupancy exceeded 85%.

However, at some City garages, although *total* parking occupancy remained below 85%, the garages' "regular" spaces (i.e., spaces that are available to the general public, and not reserved for monthly parkers, residents, or other special users) were full or nearly full. Therefore, for the average member of the public, these garages were effectively full.

The reserved spaces in these same garages remained underutilized at the peak hour. As shown in Figure 3-4 "Reserved" occupancy at all City-owned off-street facilities peaked at just under 50% on Thursday, with 233 reserved spaces unused at this time.

Figure 3-4 Off-Street "Reserved" Parking Occupancy Rates, Thursday and Saturday



To illustrate this situation in more detail, Figure 3-6 illustrates parking occupancy rates at the Thursday peak hour (12 p.m. to 1 p.m.) for "regular" parking spaces. At this hour, the parking occupancy rate for "regular" spaces exceeded 85% at five parking facilities: Telegraph Plaza, the 18th Street Uptown Lot, the Franklin Plaza Garage, the Dalziel Garage, and the Clay Street Garage. Three parking facilities met the City's 65-85% target rate for "regular" parking spaces (the City Center West, Franklin 88, and 1200 Harrison Garages), and two facilities (the UCOP and Pacific Renaissance Plaza garages) had occupancy rates below 65%. The close proximity of the

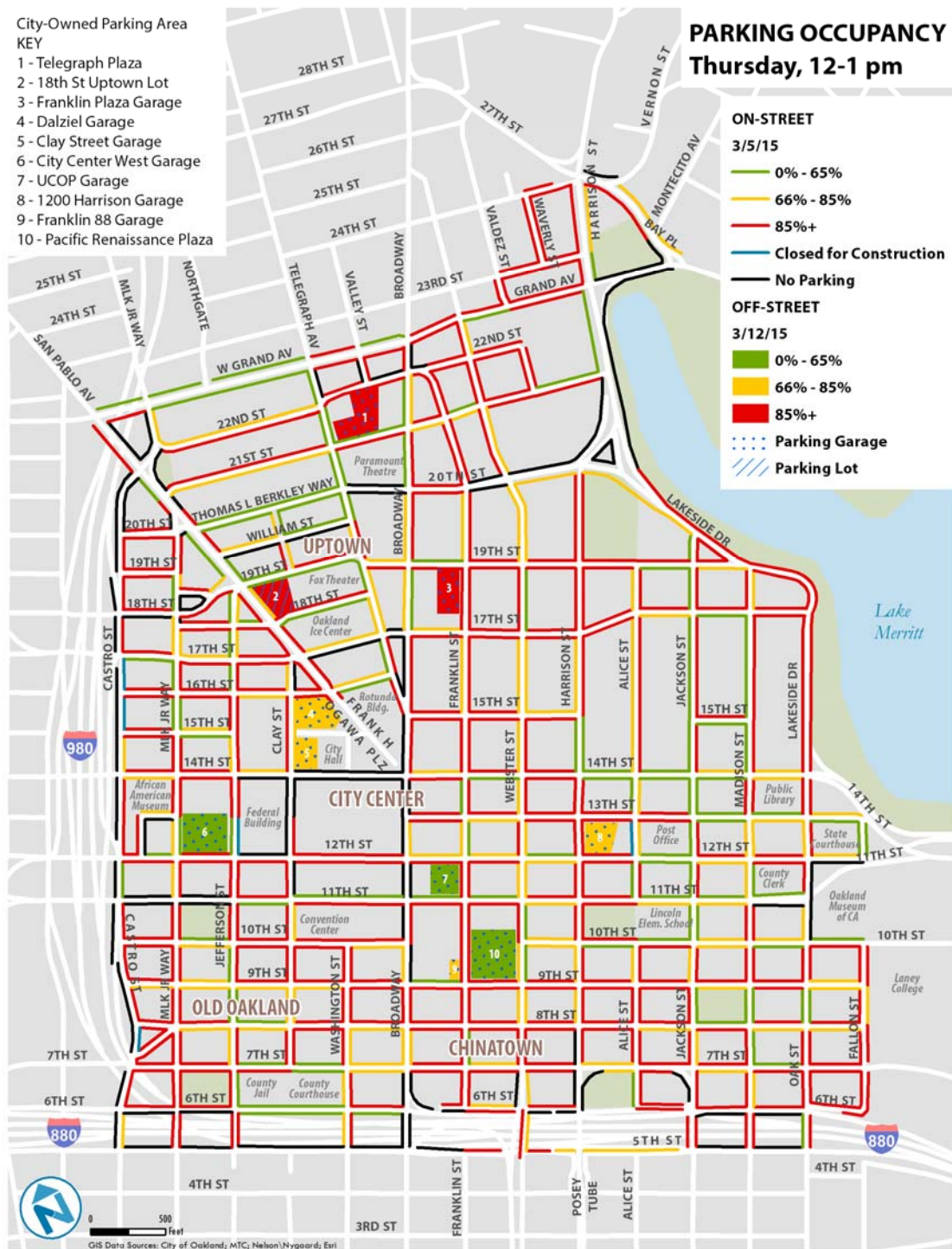
underutilized Pacific Renaissance Plaza garage to oversubscribed blockfaces in Chinatown is particularly striking.

On Saturday, the overall parking supply also reached peak occupancy during the 12-1 p.m. survey period. At this hour, overall parking occupancy reached 49%. Figure 3-7 depicts total parking occupancy during this peak hour. At this time, 32% of all on-street blockfaces exceeded an 85% occupancy rate. On-street demand patterns were more easily discernible on Saturday than Thursday, with curb parking in residential areas near Lake Merritt and blocks throughout Chinatown consistently exceeding 85% occupancy.

At the peak hour on Saturday, occupancy rates for “regular” spaces at City-owned lots and garages (shown in Figure 3-8) exceeded the target occupancy rate at just two facilities: Clay Street, and the 18th Street Uptown Lot. Note that four City garages are closed on Saturday (due to Saturdays’ low levels of parking demand and the substantial costs of staffing garages).

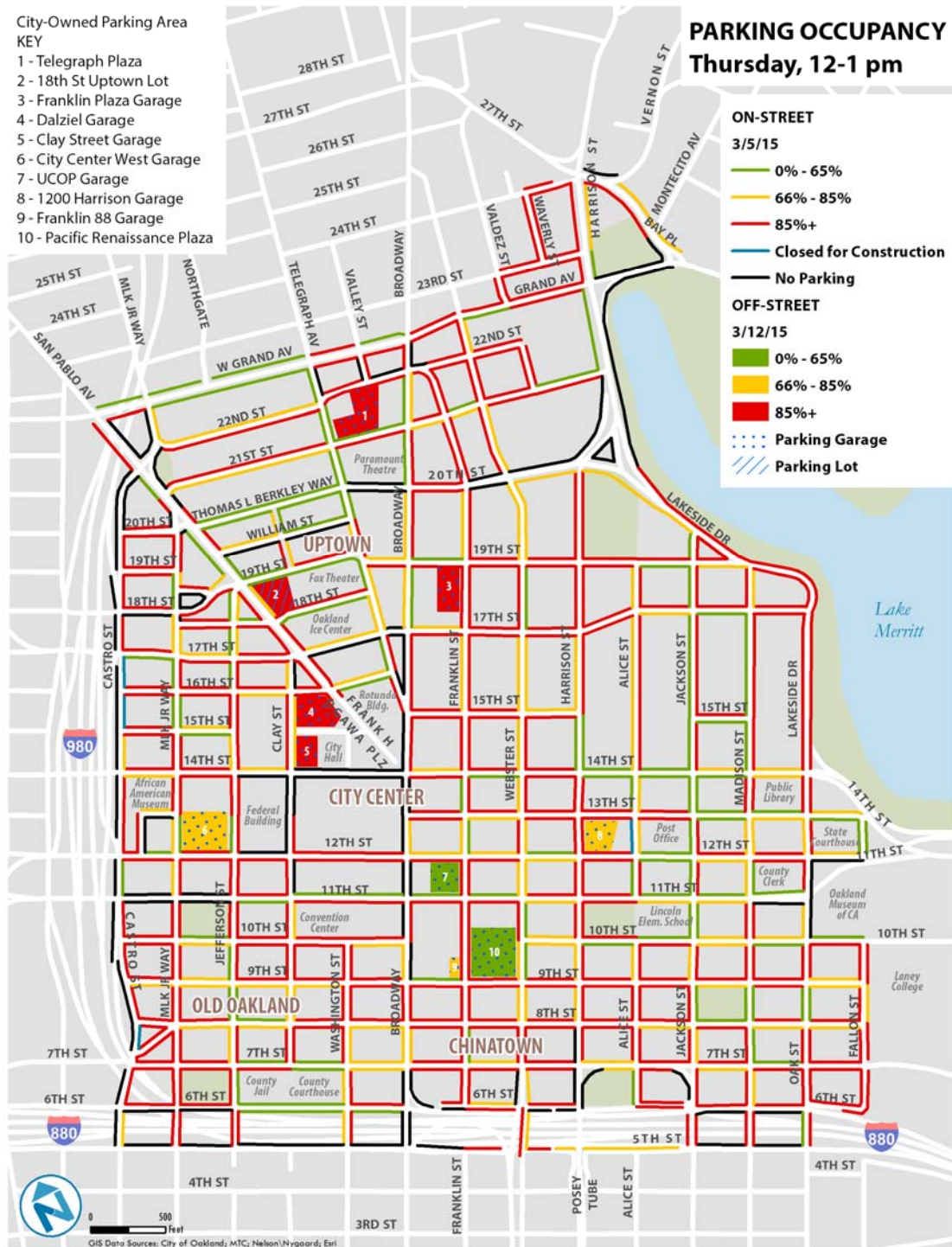
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Figure 3-5 Overall Parking Occupancy at Peak Hour (Thursday 12 p.m. to 1 p.m.)



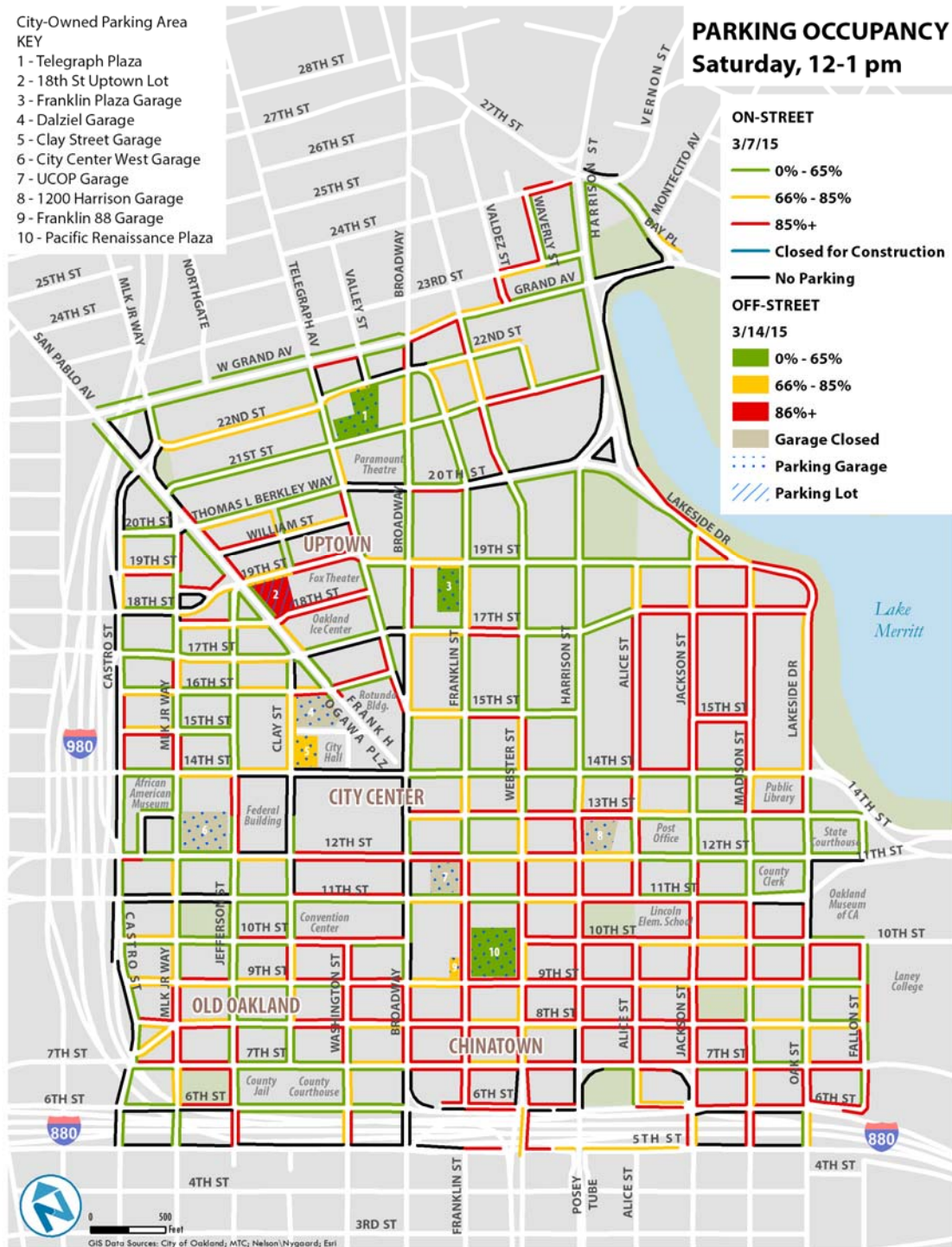
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Figure 3-6 Parking Occupancy of “Regular” Spaces at Peak Hour (Thursday 12 p.m. to 1 p.m.)



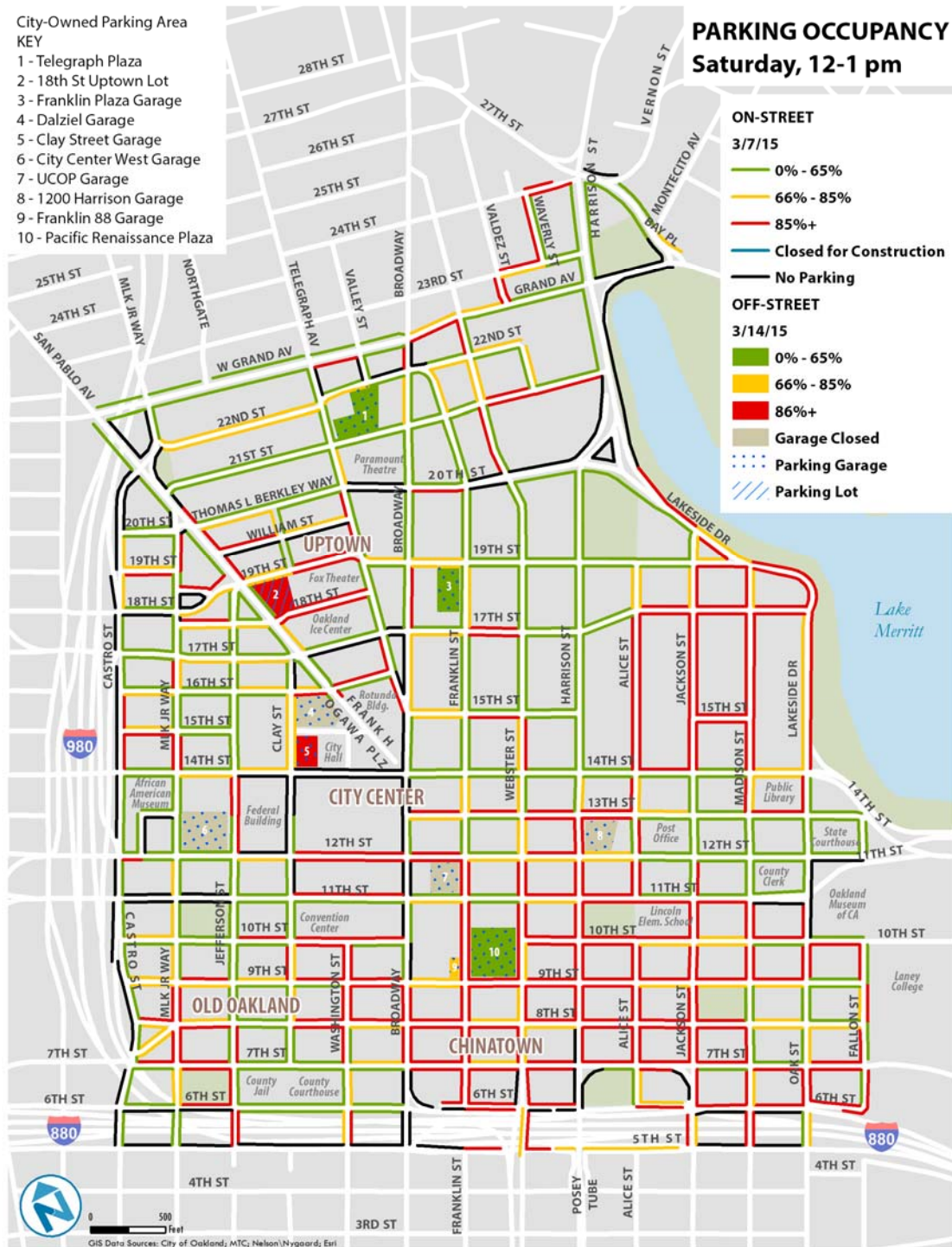
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Figure 3-7 Overall Parking Occupancy at Peak Hour (Saturday 12 p.m. to 1 p.m.)



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Figure 3-8 Parking Occupancy of “Regular” Spaces at Peak Hour (Saturday 12 p.m. to 1 p.m.)



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4 USE OF DISABLED PERSON PARKING PLACARDS AND PLATES

This section describes the results of the surveys of parked vehicles displaying Disabled Person Parking Placards and Plates¹² at the metered curb parking spaces in the study area. It includes a summary of the survey methodology, as well as the key findings. A full set of maps depicting hour-by-hour occupancy rates of parked vehicles displaying disabled placards over the course of the entire survey period is provided in Appendix A.

The survey work focused on determining both the share of metered curb parking spaces occupied by vehicles displaying disabled placards and on those vehicles' duration of stay. Metered curb parking spaces were chosen as the focus of these surveys in order to investigate the perception, voiced repeatedly during public meetings for both this study and previous downtown planning efforts, that (a) there is widespread use and abuse of disabled placards, with many drivers using them to park for free at metered curb parking spaces all day long, and (b) as a result, curb parking is frequently entirely full at peak hours, which hinders discourages shoppers and hampers businesses.

As previously mentioned in this report, the City of Oakland's Parking Principles state that curb parking should be managed to achieve an approximate 85% maximum occupancy per block, to ensure that there will always be some available on-street parking for shoppers and visitors. For people with disabilities that make travel on foot for any significant distance slow, difficult, painful, or all of the above, it is particularly important to ensure that at least some curb parking spaces are available on each blockface, since curb parking spaces are frequently closest and most convenient to their destinations.

Therefore, these disabled placard surveys, together with the overall parking occupancy surveys provided in previous chapters of this report, aim to help answer two questions:

1. Is downtown Oakland currently achieving the City's goal of achieving an approximate 85% maximum occupancy per block, so that there are always some convenient curb parking spaces available for shoppers and visitors, including those with disabilities?
2. If this goal is not being achieved (i.e., if the curb parking on many blockfaces is entirely full at peak hours), to what extent are the metered curb parking spaces being filled up by vehicles with disabled placards?

¹² For brevity's sake, Disabled Person Parking Placards and License Plates and Disabled Veterans License Plates are collectively referred to as "disabled parking placards" or "disabled placards" throughout the remainder of this chapter. "Parking spaces designated for disabled persons and disabled veterans" established pursuant to California Vehicle Code Section 22511 and/or California Building Standards Code (see Title 24, Part 2, Section 1129B) are referred to as "accessible parking spaces". This nomenclature (e.g., using the term "Disabled Person" parking placard, rather than the often preferred phrase is "person with a disability", and the term "accessible parking space" rather than "disabled" parking space) was chosen to maintain consistency with the terminology currently used (a) by the California Department of Motor Vehicles, (b) in the California Vehicle Code and (c) in the California Building Standards Code.

BACKGROUND

California Vehicle Code Section 22511.5 establishes that a “disabled person or disabled veteran displaying special license plates issued under Section 5007 or a distinguishing placard issued under Section 22511.55 or 22511.59 is allowed to park for unlimited periods” in any parking zone “that is restricted as to the length of time parking is permitted as indicated by a sign erected pursuant to a local ordinance”. Disabled placard holders are also “allowed to park in any metered parking space without being required to pay parking meter fees”.

However, this state law exempting disabled placard users from paying parking meter fees and allowing them to park for unlimited periods applies only to metered *curb* parking spaces, and not to *off-street* parking spaces.

In California, Disabled Person Parking Placards and License Plates are issued by the Department of Motor Vehicles, upon receiving a letter from any of a variety of healthcare practitioners declaring that the person applying for the placard or plate has a disability. Vehicle Code Section 22511.55 specifies that prior to issuing a disabled parking placard, “the department shall require the submission of a certificate” substantiating the disability and signed by a physician, surgeon, nurse practitioner, certified nurse midwife, or physician assistant, “unless the applicant’s disability is readily observable and uncontested”. The disability of a person “who has lost, or has lost use of, one or more lower extremities or one hand, for a disabled veteran, or both hands, for a disabled person, or who has significant limitation in the use of lower extremities” may also be certified by a licensed chiropractor. The blindness of an applicant shall be certified by “a licensed physician and surgeon who specializes in diseases of the eye or a licensed optometrist.”

Concerns about Disabled Placard Usage Raised during Public Outreach

During public meetings held to gain input on this study, members of the public recommended that the study address what they perceived as widespread abuse of disabled placards, and emphasized their perception that people use disabled placards to park for free at metered curb parking spaces all day long. This, they suggested, hinders parking availability, which is important for businesses.

Similar concerns were expressed by downtown merchants participating in the merchant surveys conducted for this study. In that survey, business owners and managers were not asked a specific question about disabled placards, but were invited to give feedback about parking and transportation generally in one open-ended question.¹³ One theme that emerged was disabled parking. Many survey respondents shared the perception that vehicles with Disabled Person Parking Placards occupy a large share of the metered on-street parking and frequently stay all day; some respondents also felt that disabled placards are often used by people without any apparent disabilities. Nine of 78 total respondents noted what they described as “rampant” and “likely illegal” use of disabled placards on vehicles that take up curb parking spaces all day.¹⁴

¹³ In response to the question, “Are there any other issues that you would like to share with us? Who should we talk to about transportation issues?”

¹⁴ Nelson\Nygaard Consulting Associates, *Downtown Oakland Parking Study Technical Memorandum #3: Public Outreach Summary*, November 2015.

Concerns about Disabled Placard Use Raised by Previous Studies

The City of Oakland has previously examined the issue of disabled placard use in the downtown area. Limited parking surveys conducted in 2012 and 2013 in the western half of downtown Oakland found that approximately 60% of all the vehicles parked at the metered curb parking spaces surveyed displayed a disabled placard.¹⁵

Concerns about Disabled Placard Use Raised by the Experience of Peer Cities

Several of the peer cities whose parking programs were reviewed as background for this parking study had investigated the use (and abuse) of disabled placards at curb parking meters in their downtowns and other commercial districts. Their experiences with this issue were documented at length in Technical Memorandum #1: Context Analysis.¹⁶ In summary, the peer cities' experiences included the following.

Berkeley

Recent statements by Berkeley city officials indicate that the fraudulent use of disabled placards and plates has been a significant problem in Berkeley. According to Berkeley Police Chief Michael K. Meehan, "It's an unfortunate truth, but the abuse of disabled placards is not unusual in Berkeley." As in Los Angeles and San Francisco (described below), disabled placard users occupy most metered curb parking spaces on some blocks. In one example highlighted by City staff, a total of 21 vehicles with disabled placards were observed on Dwight Way between Milvia Street and Shattuck Avenue, a block of metered parking adjacent to the Alta Bates Summit Medical Center's Herrick Campus medical complex. City staff explained that the valet parking attendants for the Herrick Campus' remote parking lot on Shattuck regularly park vehicles with disabled placards at the on-street parking meters, without paying, to make more room in the remote parking lot. While this practice is illegal, staff said that it persists despite repeated stings.^{17 18}

Los Angeles

Recent news stories indicate that fraudulent use of disabled parking placards is a frequent problem in Los Angeles, and curbing disabled placard abuse has proved challenging. According to UCLA Professor Donald Shoup, "In 2010, a UCLA survey of placard use on several blocks in downtown Los Angeles found that cars with disabled placards occupied most of the curb spaces most of the time. For five hours of the day, cars with placards occupied every space on one block. The meter rate was \$4 an hour, but the meters earned only 32 cents an hour because cars with

¹⁵ City Of Oakland DPW Memo on the impact of disabled placard parking policy on parking revenues and parking space availability in downtown Oakland, August 12, 2013.

¹⁶ Nelson\Nygaard Consulting Associates, Downtown Oakland Parking Study Technical Memorandum #1: Context Analysis, April 2015.

¹⁷ Ng, Willa. Interview with City of Berkeley, April 2nd, 2015.

¹⁸ If a valet parking attendant drives and parks a vehicle with a disabled placard at an on-street parking meter without paying, and is neither the valid owner of the placard nor transporting a person with disabilities, this is a violation of state laws governing placard use.

placards consumed 80% of the meter time. Drivers with disabled placards were often seen carrying heavy loads between their cars and the adjacent businesses.”¹⁹

Since then, the abuse problem has apparently persisted, with media stories about targeted enforcement stings appearing on a regular basis. Within the past several years, both the Los Angeles Department of Transportation (in collaboration with the Los Angeles Police Department) and the California Department of Motor Vehicles (DMV) have conducted enforcement and/or special stings of disabled placard use in Los Angeles.^{20 21}

San Francisco

According to the San Francisco Municipal Transportation Authority’s (SFMTA’s) *SFpark: Pilot Project Evaluation* report, published in June 2014, City surveys found that disabled placard users account for approximately 20% of the vehicles parked at the curb on metered blocks, and nearly half of all vehicles marked at metered spaces in downtown San Francisco. According to the report, “In general, a relatively high number of vehicles parked at metered spaces in San Francisco display a disabled placard. As one example, a 2008 SFMTA survey found that 45% of metered spaces in a downtown study area were occupied by vehicles displaying placards. Of the vehicles using placards, 57% were registered outside of San Francisco. Another SFMTA survey of many neighborhoods in 2006 showed that in neighborhood commercial districts (e.g., Fillmore, Marina, etc.), about 10-20% of occupied parking spaces were cars displaying placards, with some areas between 20-40% and two areas over 50%.”²²

The SFMTA also conducted detailed surveys, using a combination of real-time parking data from parking occupancy sensors embedded in the street, real-time parking payment data gathered from the wirelessly networked meters, and visual surveys by parking enforcement officers, to identify which metered parking spaces were occupied by vehicles but not paid for, and why. These surveys found that it was common for vehicles to occupy parking meters without paying. On the average weekday, only 45% of vehicles occupying a metered space paid the meter at the beginning of the *SFpark* pilot project, with this figure increasing to 54% by the end of the pilot project.²³ Use of disabled placards was by far the most common reason for a vehicle to be occupying a metered parking space without paying. For example, according to data gathered by SFMTA in Fisherman’s Wharf in 2013, “at least 48% of spaces occupied but not paid were cars displaying placards.”²⁴

In 2013, the SFMTA took steps towards improving enforcement to prevent the abuse of disabled placards. As of July 2013, the SFMTA “increased the number of PCOs [Parking Control Officers]

¹⁹ Shoup, Donald. “Curbing the Parking Crunch by Cutting Disabled Placard Abuse.” *Los Angeles Times*, October 22, 2012. <http://articles.latimes.com/2012/oct/22/opinion/la-oe-shoup-disabled-parking-abuse-20121022>.

²⁰ Steve Lopez. “Cracking down on Los Angeles Parking Meter Cheaters - Latimes.” *Los Angeles Times*, February 15, 2012. <http://articles.latimes.com/2012/feb/15/local/la-me-0215-lopez-placardsting-20120213>.

²¹ Joel Grover. “DMV Sting Operation Is Response to I-Team Report,” August 27, 2014. <http://www.nbcalosangeles.com/news/local/DMV-Sting-Operation-Is-Response-to-I-Team-Report-272962471.html>.

²² *SFpark: Pilot Project Evaluation*. San Francisco Municipal Transportation Agency, June 2014, p. 80-81.

²³ Ibid. The percentage of vehicles paying to park at meters is likely to have increased as a result of the *SFpark* project because the project introduced several options which make it easier for drivers to pay and park legally, including introducing new meters that accept payment by credit card as well as coins, introducing the option to pay-by-phone, and extending time limits at meters from one or two hours to at least four hours or, in some areas, eliminating time limits entirely.

²⁴ *SFpark: Pilot Project Evaluation*. San Francisco Municipal Transportation Agency, June 2014, p. 85.

serving on the Disabled Placard Detail by over 25 percent, so now the SFMTA has 14 [full-time] PCOs dedicated to enforcing placard misuse.”²⁵

Seattle

Seattle, like the other peer review cities, has also studied the use of disabled parking placards at metered curb parking spaces, with a statewide working group on the issue concluding that placard abuse in Washington is a significant problem. In the state of Washington, as in California, valid use of a disabled parking placard allows the user to park for free at meters. A statewide workgroup led by the Washington State Department of Licensing recently concluded that “the disabled parking privilege is easily abused and the verifiable data does show there is abuse of the system”.²⁶ According to SDOT, “recent studies have shown that between 30% and 40% of downtown, First Hill, Pioneer Square, and Cherry Hill parking are typically used by vehicles with disabled parking placards.”²⁷

In summary, the peer cities reviewed for this study concluded overall that fraudulent use of disabled parking placards is a frequent problem in their cities, and curbing disabled placard abuse has proven challenging. The cities also concluded that widespread use (and abuse) of disabled placards, particularly in their downtowns and highly populated blocks, often results in the metered curb parking spaces on numerous blocks filling up entirely. Surveys often found that placard holders parking for free are a significant contributor to this problem: in Seattle, service found that between 30% and 40% of the curb parking in popular districts was typically occupied by vehicles with disabled parking placards; in San Francisco, 45% of metered spaces in a downtown study area were occupied by vehicles with placards; and in downtown Los Angeles, a survey found that cars with disabled placards occupied most of the curb spaces most of the time.

METHODOLOGY

In order to gather a representative sample of the Downtown Oakland Parking Study area, over 25% of the curb parking meters in the study area were surveyed to observe disabled placard use. The metered blocks surveyed are indicated in Figure 4-1. Occupancy and turnover counts of vehicles displaying disabled placards were conducted on the following days²⁸:

- Tuesday, October 13
- Wednesday, October 14
- Thursday, October 15
- Tuesday, October 20
- Tuesday, October 27

On each of these days, disabled placard occupancy data was collected in hourly intervals from 8 a.m. to 6 p.m., the hours of operation for meters in downtown Oakland. When a vehicle with a

²⁵ Ibid.

²⁶ *Reducing Fraudulent Use of Disabled Parking Placards and Plates - Disabled Parking Work Group Recommendations*. Washington State Department of Licensing. Accessed March 26, 2015. <http://www.seattle.gov/transportation/parking/docs/2013dolreport.pdf>, p.1.

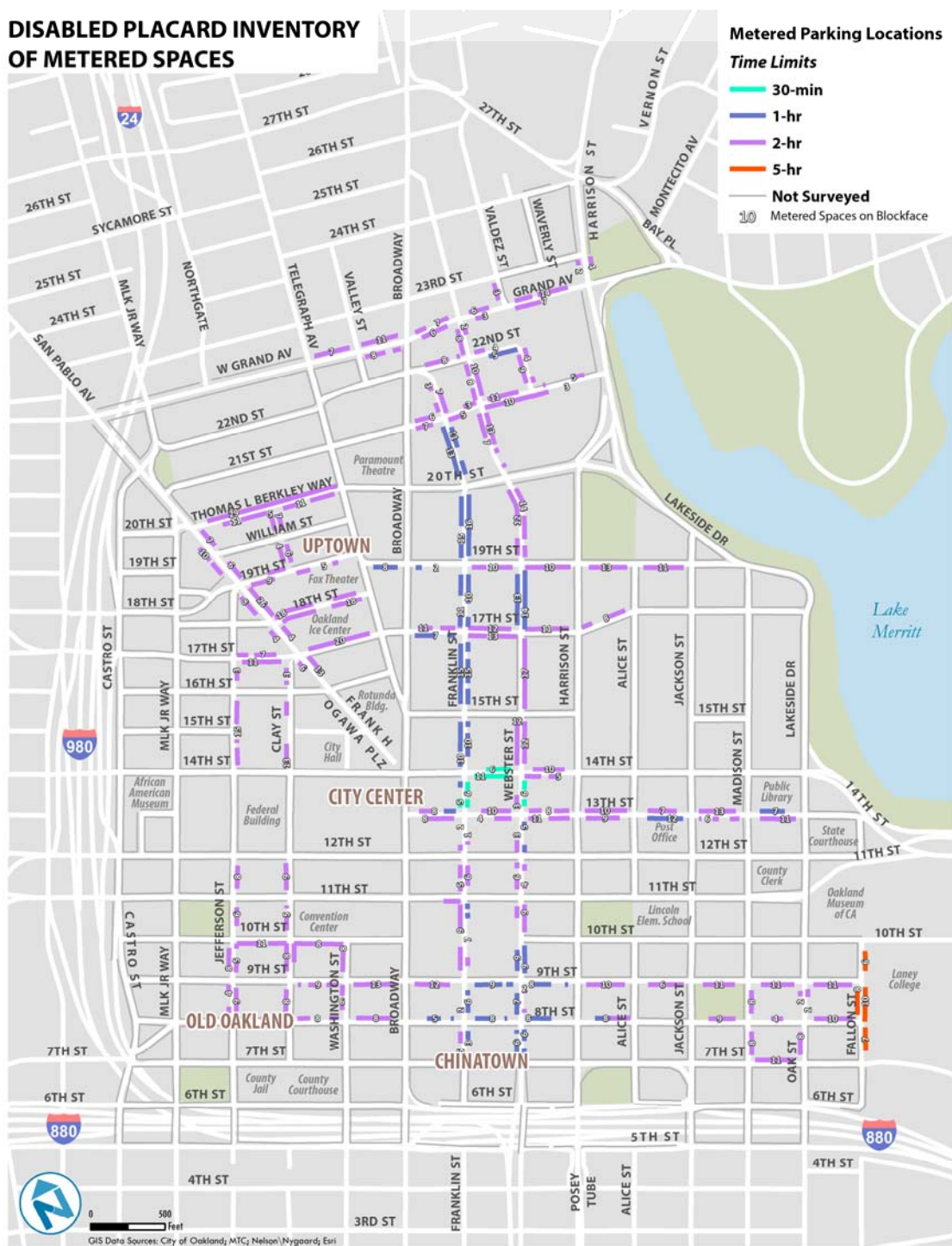
²⁷ “SDOT - Disabled Parking in the City of Seattle.” Accessed March 26, 2015. <http://www.seattle.gov/transportation/parking/disabledparking.htm>.

²⁸ Although these on-street occupancy counts for the entire study area were conducted over multiple days, data for each blockface was gathered on the same day. Spreading out the data collection over multiple days was done in order to break the task of walking these blocks and manually recording license plate information into more manageable sizes.

disabled parking placard or distinguishing plates was observed, the surveyors recorded the last three digits of its license plate in order to document length of stay. Occupancy figures depicted in the following graphs and maps describe only the percentage of metered curb parking spaces occupied by vehicles displaying disabled placards. To understand the effect that vehicles with placards have on *overall* curb parking availability on each block, these maps should be compared to the overall parking occupancy maps provided in previous sections of this report.

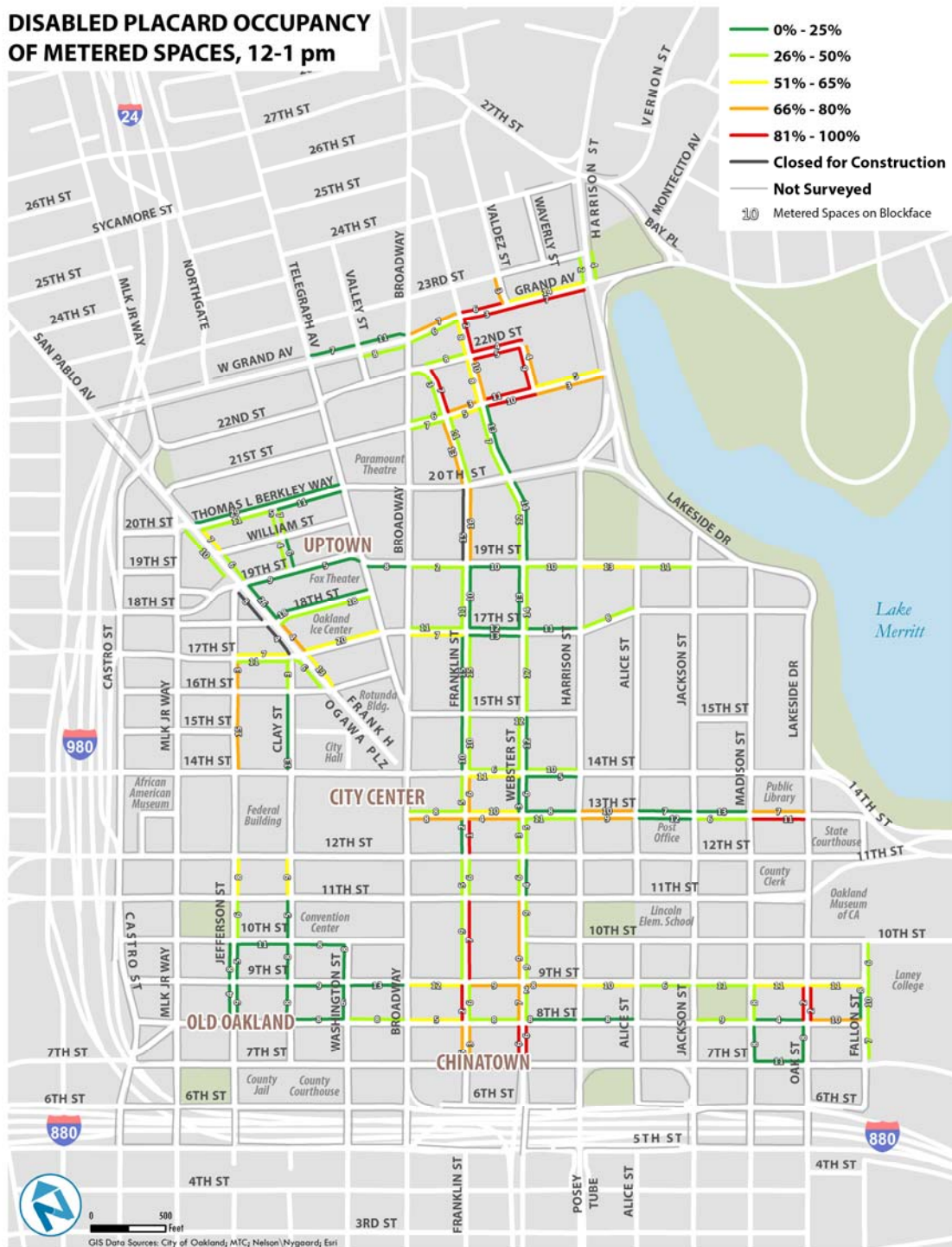
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Figure 4-1 Inventory of Metered Spaces Surveyed for Disabled Placard Use



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Figure 4-2 Metered Spaces Occupied by Vehicles with Disabled Placards at Peak Hour (12 p.m. to 1 p.m.)



DISABLED PLACARD SURVEY RESULTS

As illustrated in Figure 4-2 (showing peak hour occupancy) and in the additional occupancy maps provided in Appendix A, the surveys of placard use found that on numerous blocks, vehicles with disabled placards occupied most of the metered curb parking spaces most of the time. Partially due to this high percentage of curb spaces occupied by vehicles with disabled placards, the overall parking occupancy surveys (described in previous chapters of this report) found that on many of the same blocks, curb parking was entirely full during peak hours.

As shown in Figure 4-2, on some block faces, such as on Grand Avenue between Webster and Harrison, and on 21st and 22nd Streets between Webster and Valdez, vehicles with disabled placards occupy more than 80% of metered curb parking spaces at the peak hours of the day.

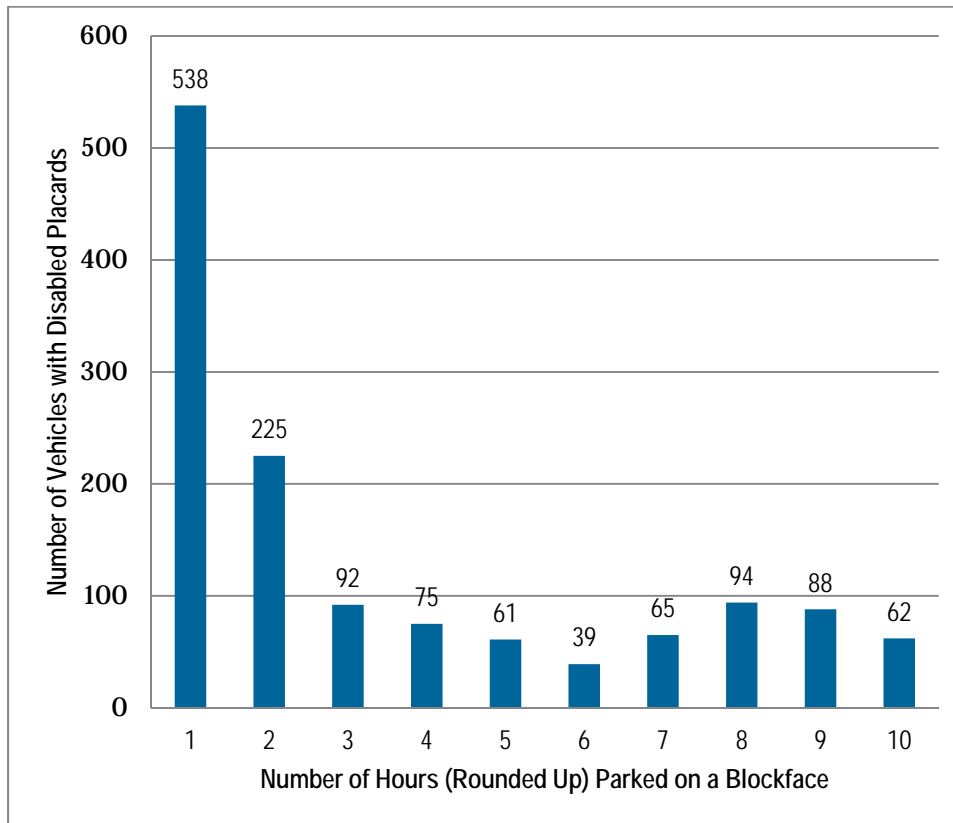
Figure 4-3 illustrates the duration of stay for vehicles displaying disabled placards at the metered spaces surveyed. The surveyors observed 1339 unique vehicles displaying disabled placards at the metered curb parking spaces surveyed.²⁹ The average duration of stay for these vehicles with disabled placards was approximately three and a half hours, with a median stay of two hours.

Approximately 40% of these vehicles with disabled placards occupied a metered space for one hour or less, and 17% stayed for two hours or less. While these 763 vehicles (538 staying for one hour or less, 225 staying for two hours or less) accounted for the majority of the vehicles displaying disabled placards, they used a relatively small share of metered curb parking supply. Altogether, most of the drivers using disabled placards to park for free at metered curb parking spaces stayed for less than two hours each, and these drivers used less than 988 hours of metered curb parking time combined.

By contrast, a relatively small number of vehicles with disabled placards were left parked at the curb for more than six hours, yet their vehicles occupied a much greater share of the metered curb parking supply. 62 vehicles with disabled placards were left parked at meters for nine to 10 hours: that is, they were left for all or nearly all of the entire period (8 a.m. to 6 p.m.) during which downtown meters operate. Another 88 vehicles were left parked for eight to nine hours, 94 for seven to eight hours, and 65 for six to seven hours. Altogether, these 309 vehicles each left parked at the curb for more than six hours used up more than 2310 hours of metered curb parking time combined.

²⁹ By "unique vehicles", we mean vehicles with different license plate numbers.

Figure 4-3 Duration of Stay at Metered Curb Spaces by Vehicles with Disabled Placards (n=1339)



In other words, most vehicles observed using disabled placards (57% of placard holders) to park for free at metered curb parking spaces stayed for just one or two hours, and together used up 988 hours of metered curb parking time. However, a small share (23% of placard holders) of vehicles were parked at metered curb parking spaces for more than six hours each, and these vehicles collectively used up more than 2310 hours of metered curb parking time.

SUMMARY

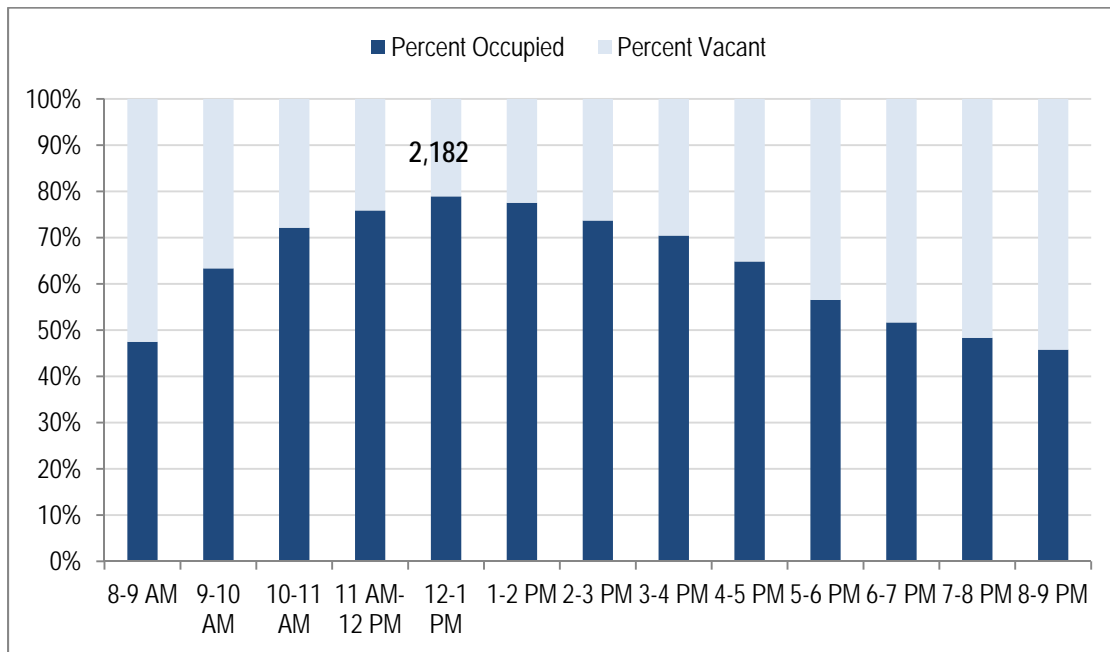
In summary, the surveys of disabled placard use found that on numerous blocks in downtown Oakland, vehicles with disabled placards occupy most of the metered curb parking spaces most of the time. On some blocks, vehicles with disabled placards occupy more than 80% of metered curb parking spaces at the peak hours of the day.

5 CONCLUSIONS

As a rule of thumb, a parking system is considered to be effectively full when curb parking spaces (which are generally intended for short-term parking) reach an occupancy rate of 85%, and off-street parking lots and garages (generally intended for longer-term parking) reach an occupancy rate of 90%. An 85% occupancy rate for each block of curb parking typically leaves one or two spaces available on the block, meaning that parking is well-used but readily available. A slightly higher (90%) occupancy rate is generally considered acceptable for off-street parking, since many users (e.g., office workers parking for the day) are staying for longer periods. These occupancy rates leave a cushion of spaces available to accommodate incidents (such as spaces which are unavailable due to construction), and mean that a driver need not search every aisle in the parking system to find the last available space.

By these measures, when the City's downtown parking facilities are considered *as a whole*, a parking surplus exists. Figure 5-1 and Figure 5-2 compare the total inventory of City-owned parking with the total number of vehicles parked at these facilities at the peak hour. Overall parking occupancy for City-owned facilities reaches only 79% at the peak hour (Thursday, 12 p.m. to 1 p.m.) with more than 2,000 parking spaces remaining vacant, as shown in Figure 4-1.

Figure 5-1 Total Parking Occupancy, Thursday

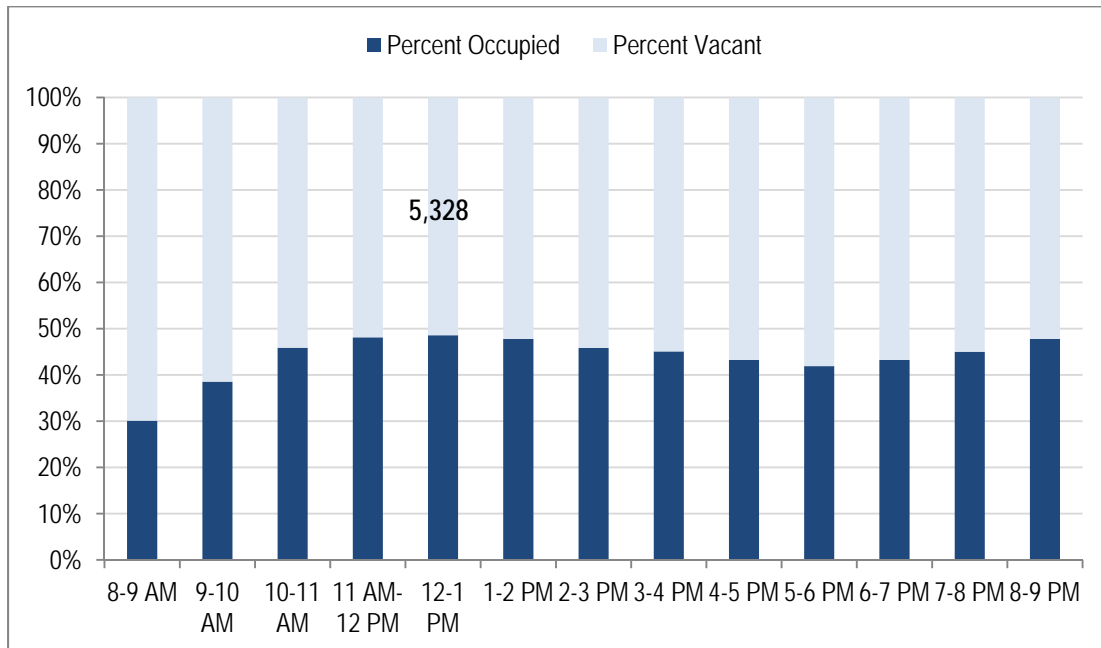


However, hot spots of high parking demand do exist. In core business areas such as Chinatown and City Center, finding available curb parking spots can be difficult during much of the day, both on weekdays and Saturdays. The survey results appear to confirm the findings of previous studies such as the 2014 Lake Merritt Station Area Plan, which noted frequent curb parking problems in the core of Chinatown, including merchants using curbside parking spaces for storage throughout the day; illegal parking in loading zones and no parking zones; and double parking and street loading.

At peak hour on Thursday, three City lots and garages (Telegraph Plaza, the 18th Street Uptown Lot, and the Franklin Plaza Garage) are also either nearly or entirely full. In two other City facilities (the Dalziel Garage and the Clay Street Garage), while the “reserved” parking spaces may remain mostly vacant, “regular” parking spaces are full at the peak hour, making these facilities effectively full for the average member of the public.

By contrast, parking occupancy on Saturday is far lower, with overall parking occupancy reaching just 49% at the busiest hour (12 p.m. to 1 p.m.). At this time, more than 5,000 parking spaces remain vacant in the City’s downtown parking facilities, as shown in Figure 5-2, and all of the City’s off-street lots and garages have substantial excess capacity.

Figure 5-2 Total Parking Occupancy, Saturday



In addition, disabled placard use and abuse at on-street parking meters appears to be a significant issue. Surveys of disabled placard use found that on numerous blocks, vehicles with disabled placards occupy most of the metered curb parking spaces most of the time. Almost one in five (18.2%) vehicles displaying a disabled parking placard remained at a parking space for eight hours or longer. On some blocks, vehicles with disabled placards occupy more than 80% of metered curb parking spaces at the peak hours of the day. In part due to the high percentage of curb spaces on many blocks occupied by vehicles with disabled placards, curb parking on these same blocks is often entirely full during peak hours. This leaves no convenient curb parking available when many shoppers and visitors arrive – including those with genuine disabilities, for whom walking long distances to less convenient spaces can be slow, difficult, and painful.

Appendix A Complete Parking Occupancy Survey Results

This appendix includes maps depicting parking occupancy rates in downtown Oakland for all surveyed hours and days, including the peak hours depicted in Chapter 3.

For each day, a full set of maps depicting Overall Parking Occupancy are provided first, followed by a set of maps depicting Parking Occupancy at “Regular” parking spaces.

It also includes maps depicting metered parking occupancy by cars displaying disabled person placards for the hours surveyed.

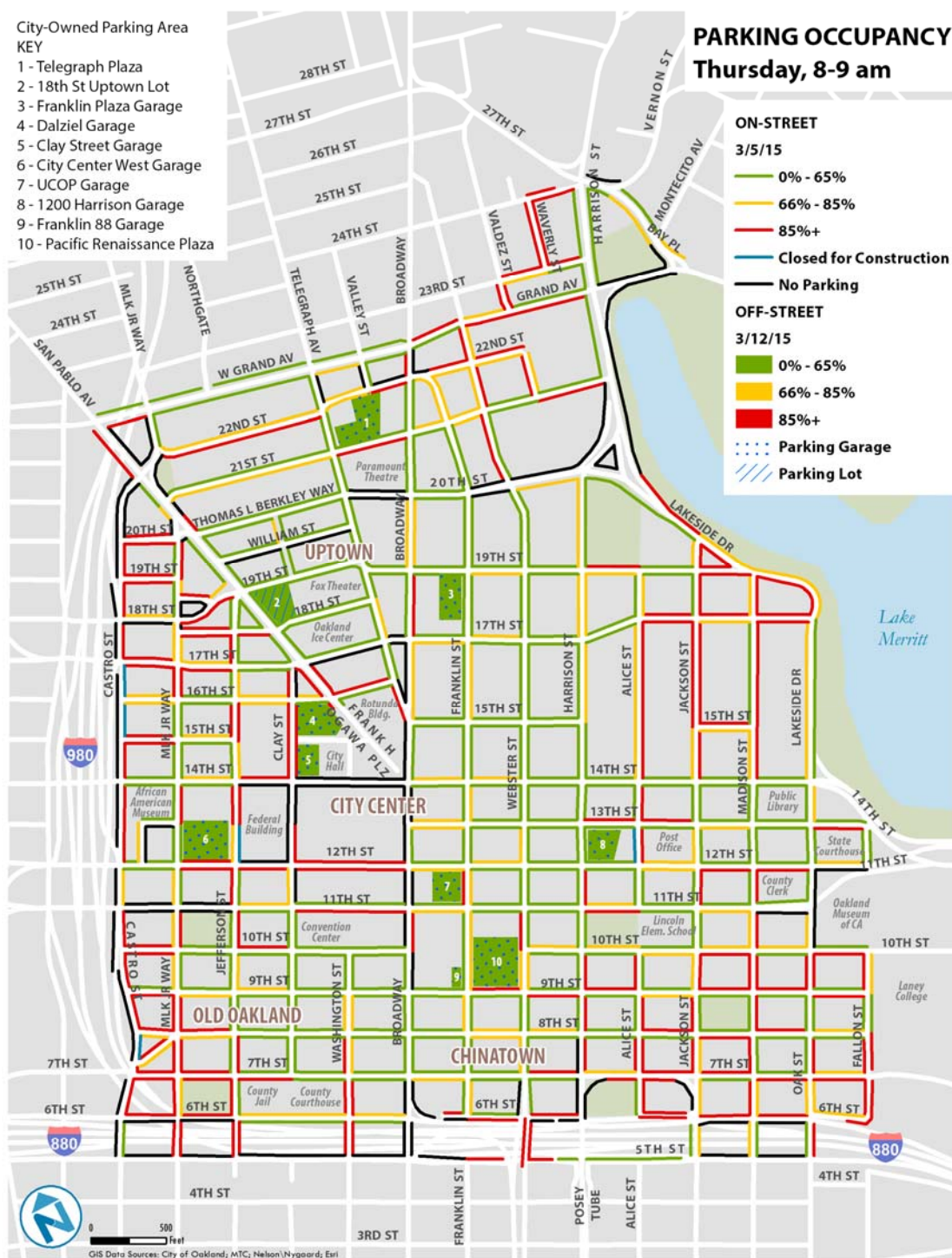
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-1 On- and Off-Street Parking Supply and Restrictions



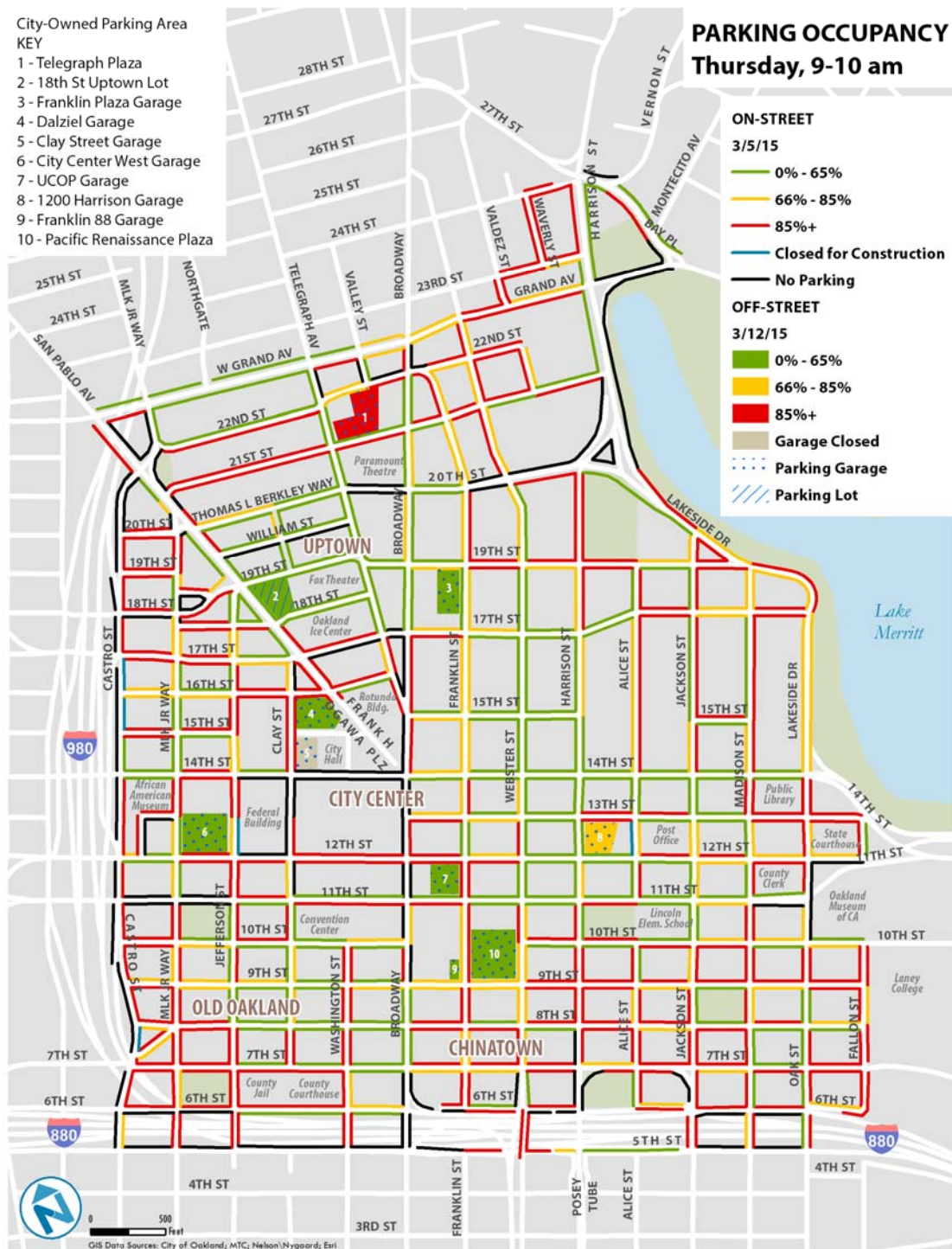
City of Oakland/Metropolitan Transportation Commission

Figure A-2 Overall Parking Occupancy, Thursday 8 a.m. to 9 a.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-3 Overall Parking Occupancy, Thursday 9 a.m. to 10 a.m.



City of Oakland/Metropolitan Transportation Commission

Figure A-4 Overall Parking Occupancy, Thursday 10 a.m. to 11 a.m.



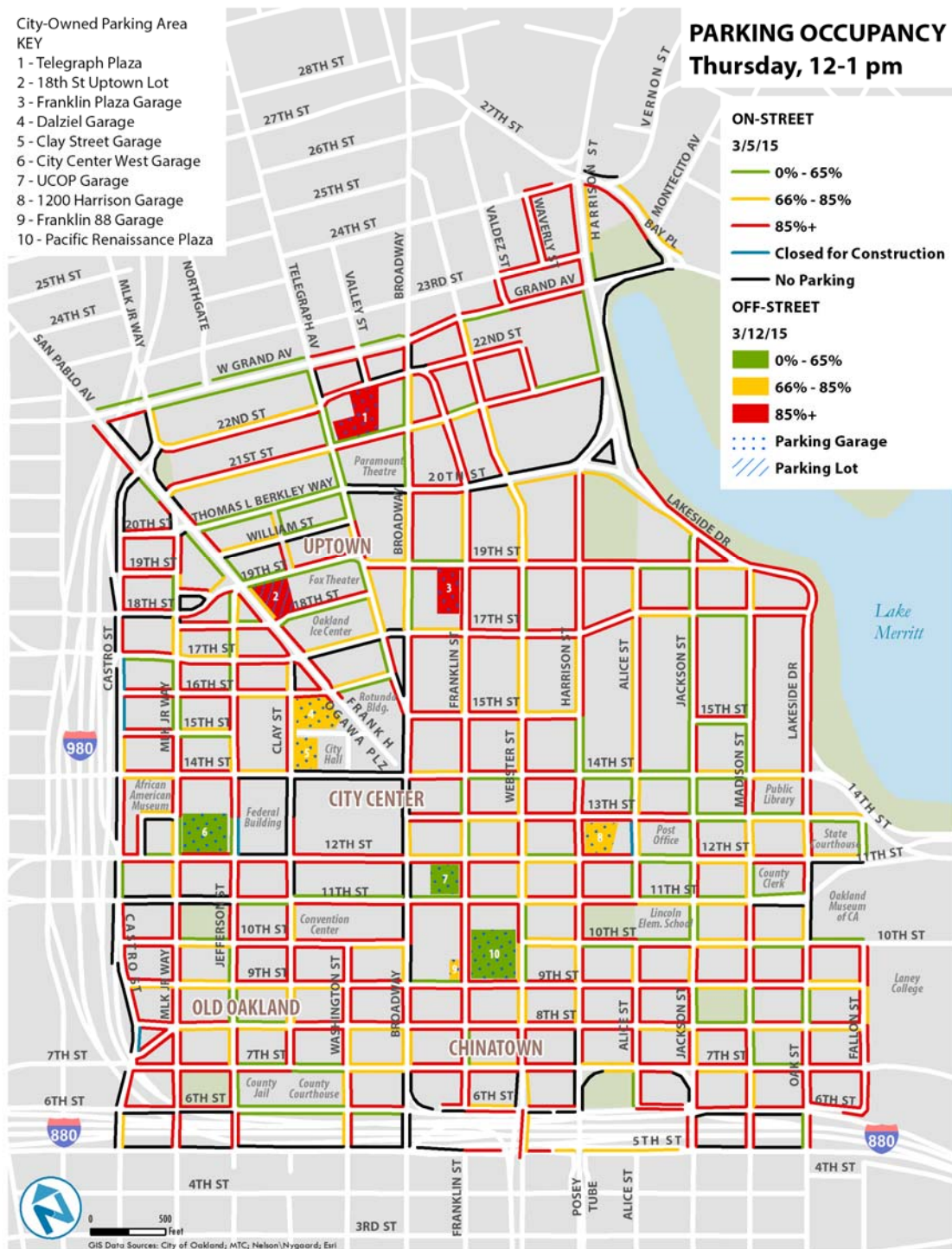
City of Oakland/Metropolitan Transportation Commission

Figure A-5 Overall Parking Occupancy, Thursday 11 a.m. to 12 p.m.



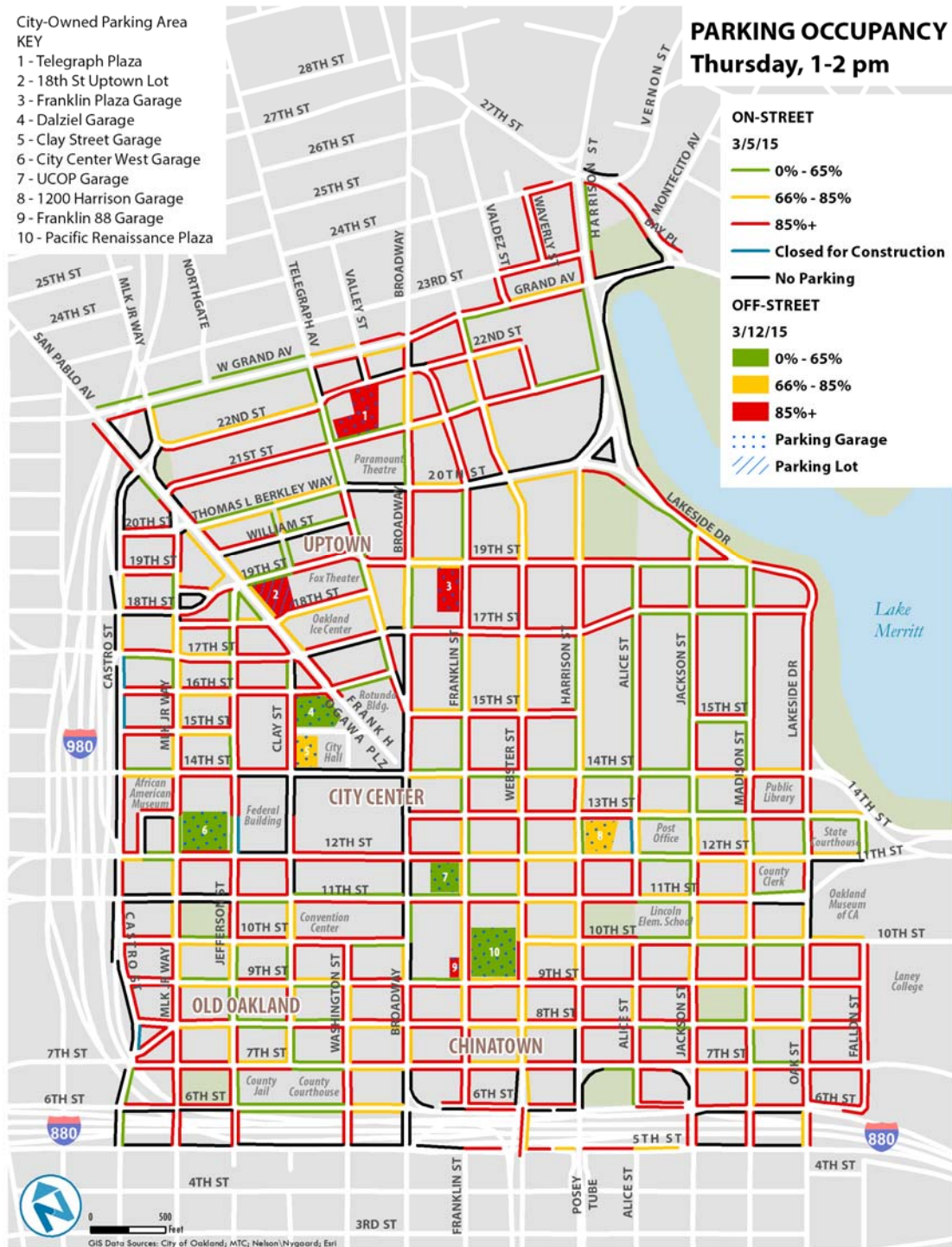
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-6 Overall Parking Occupancy, Thursday 12 p.m. to 1 p.m.



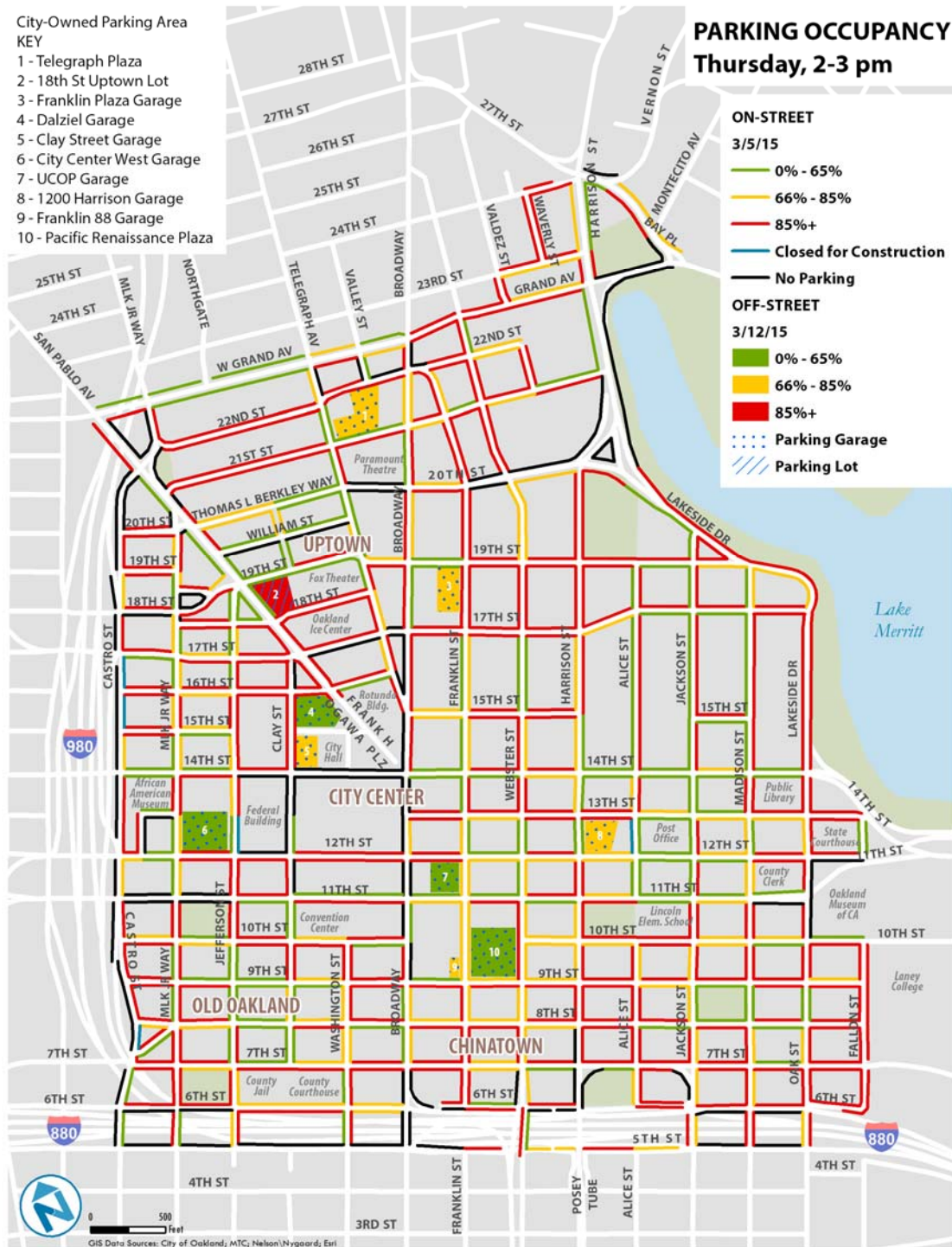
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-7 Overall Parking Occupancy, Thursday 1 p.m. to 2 p.m.



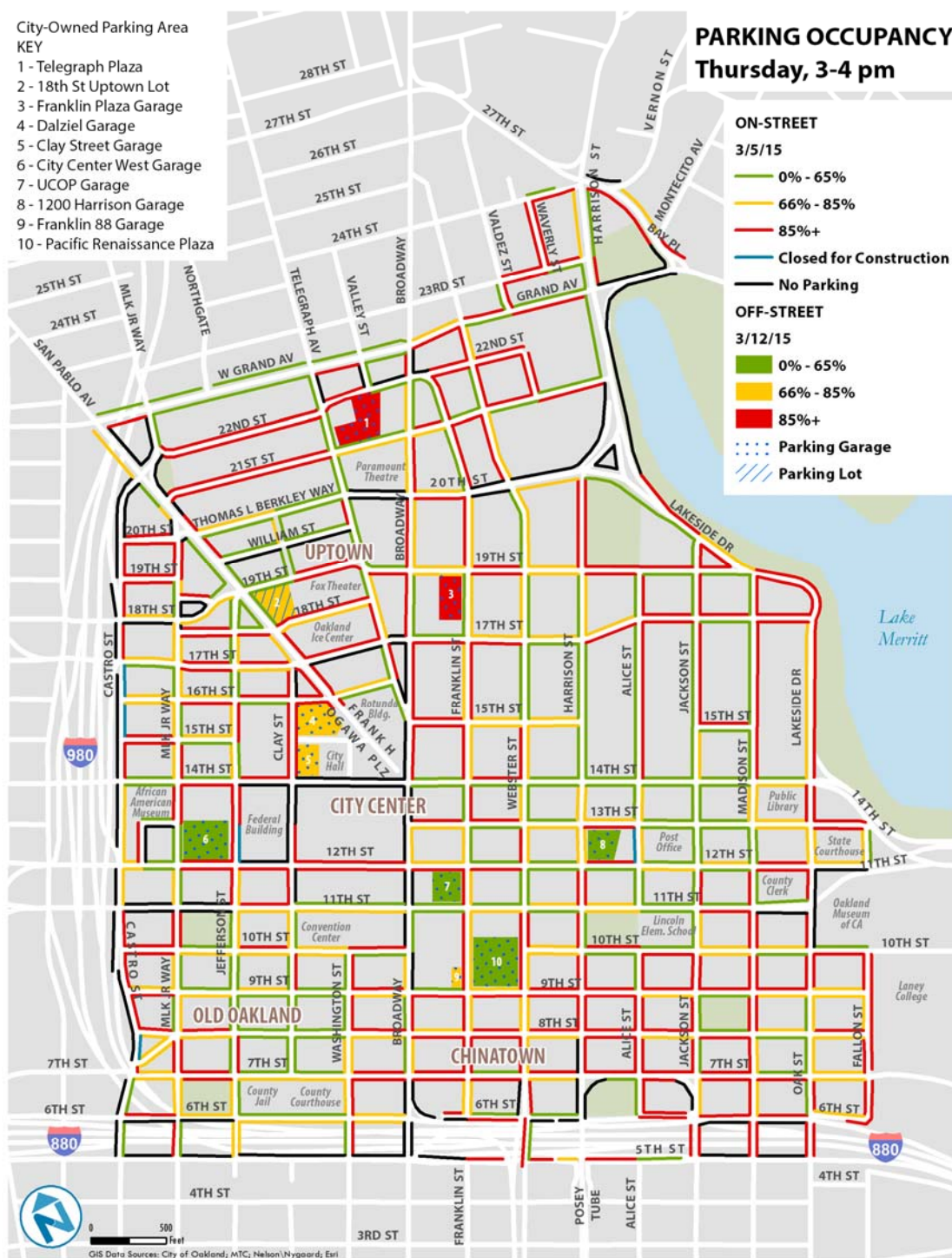
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-8 Overall Parking Occupancy, Thursday 2 p.m. to 3 p.m.



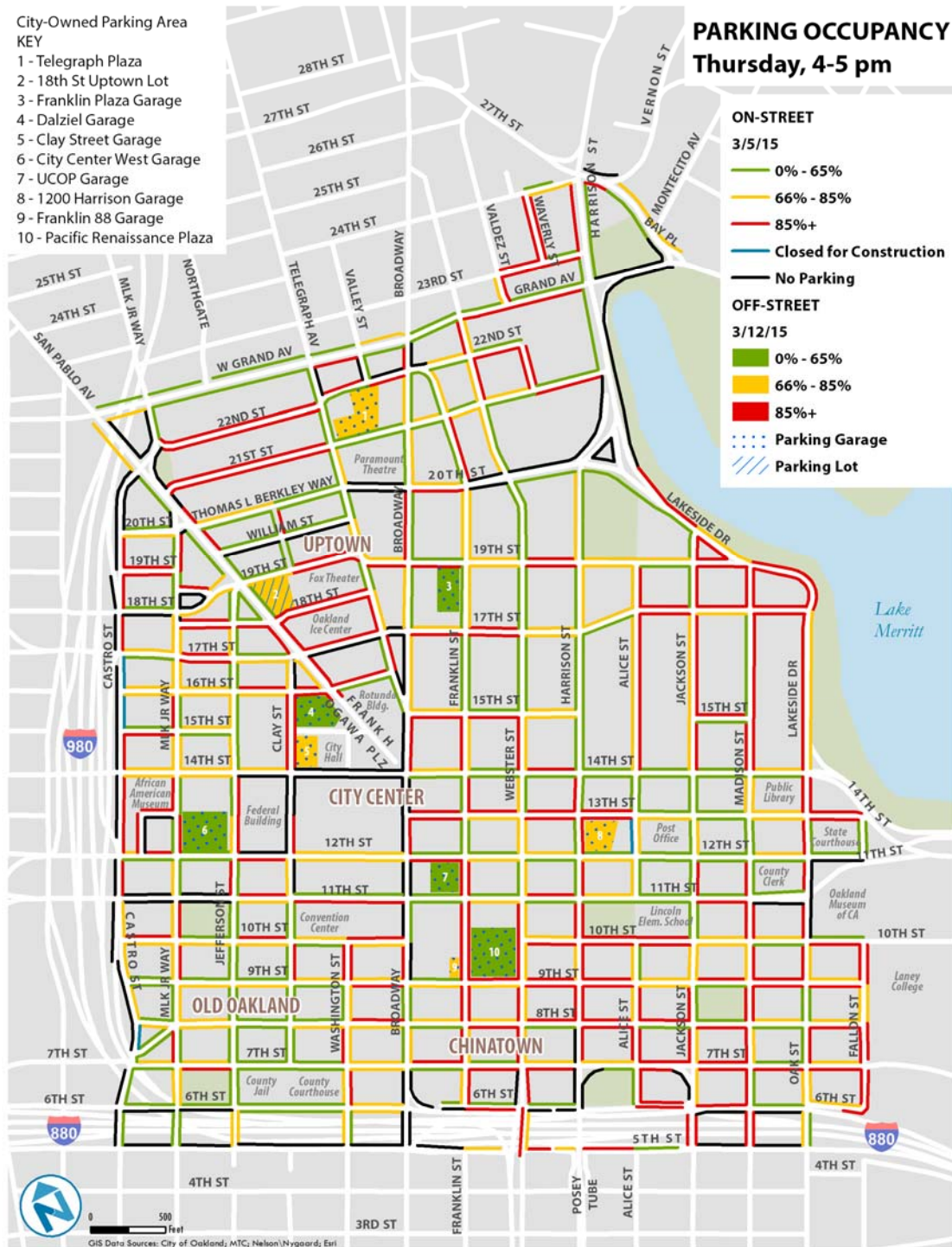
City of Oakland/Metropolitan Transportation Commission

Figure A-9 Overall Parking Occupancy, Thursday 3 p.m. to 4 p.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-10 Overall Parking Occupancy, Thursday 4 p.m. to 5 p.m.



City of Oakland/Metropolitan Transportation Commission

Figure A-11 Overall Parking Occupancy, Thursday 5 p.m. to 6 p.m.



City of Oakland/Metropolitan Transportation Commission

Figure A-12 Overall Parking Occupancy, Thursday 6 p.m. to 7 p.m.



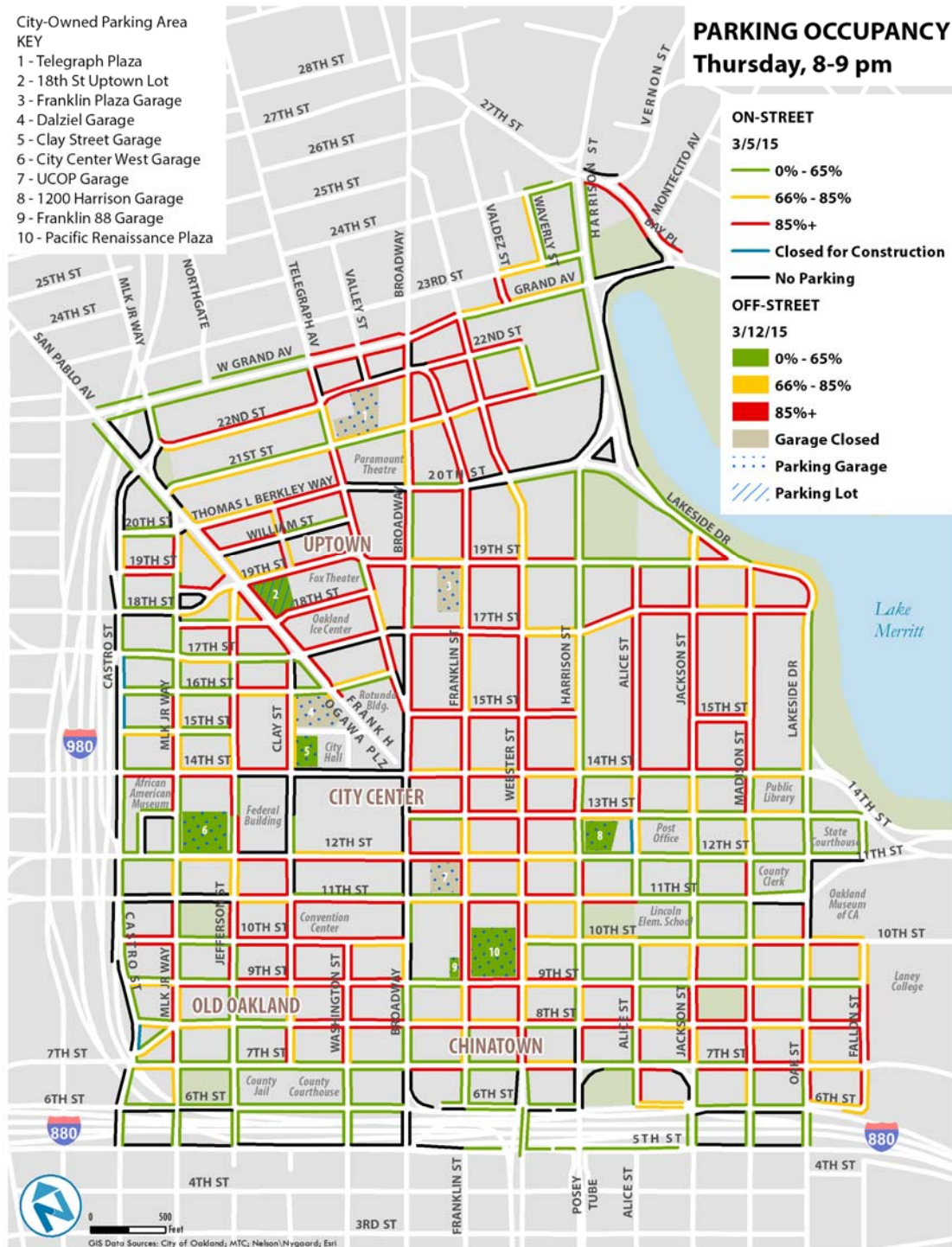
City of Oakland/Metropolitan Transportation Commission

Figure A-13 Overall Parking Occupancy, Thursday 7 p.m. to 8 p.m.



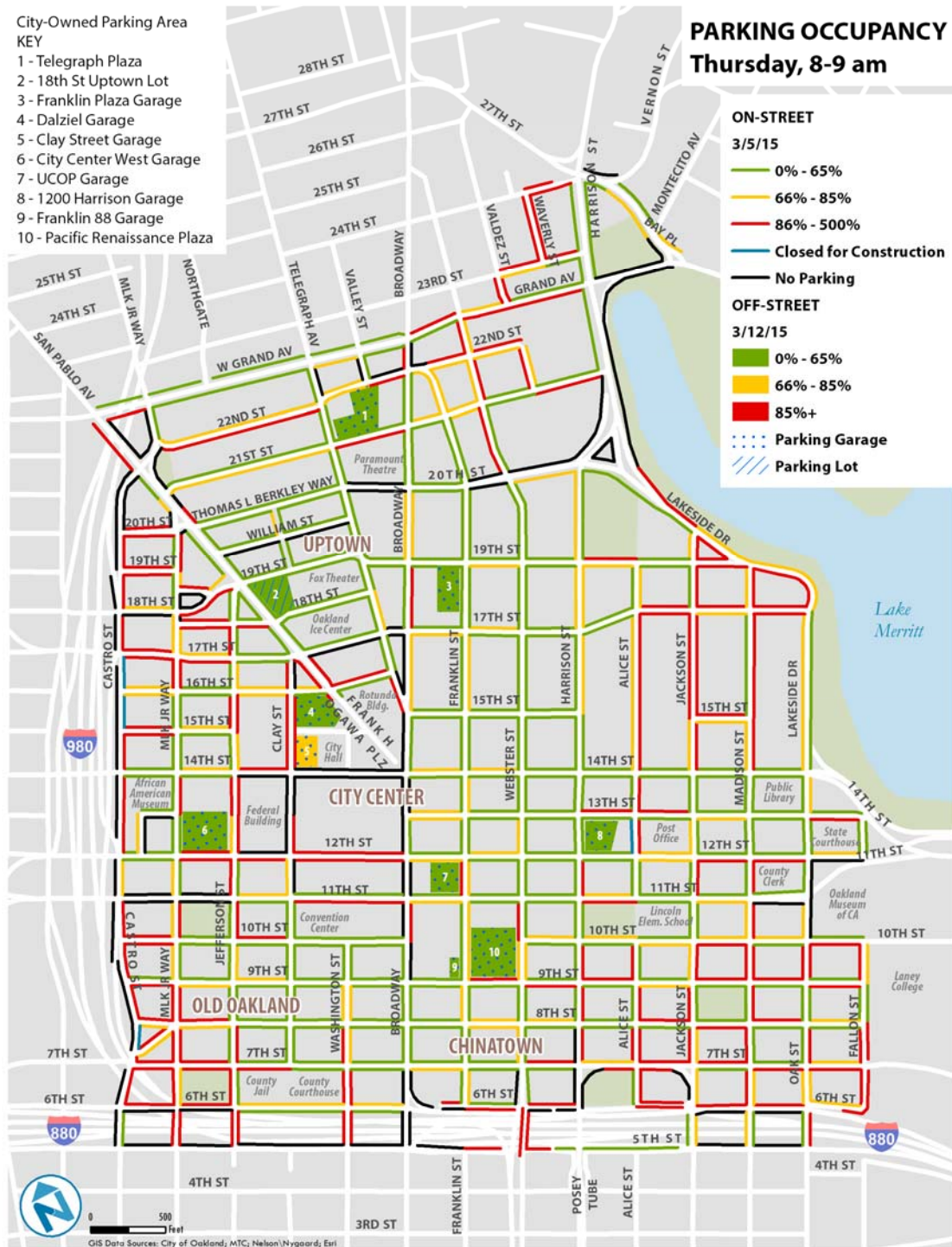
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-14 Overall Parking Occupancy, Thursday 8 p.m. to 9 p.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-15 Parking Occupancy of “Regular” Spaces, Thursday 8 a.m. to 9 a.m.



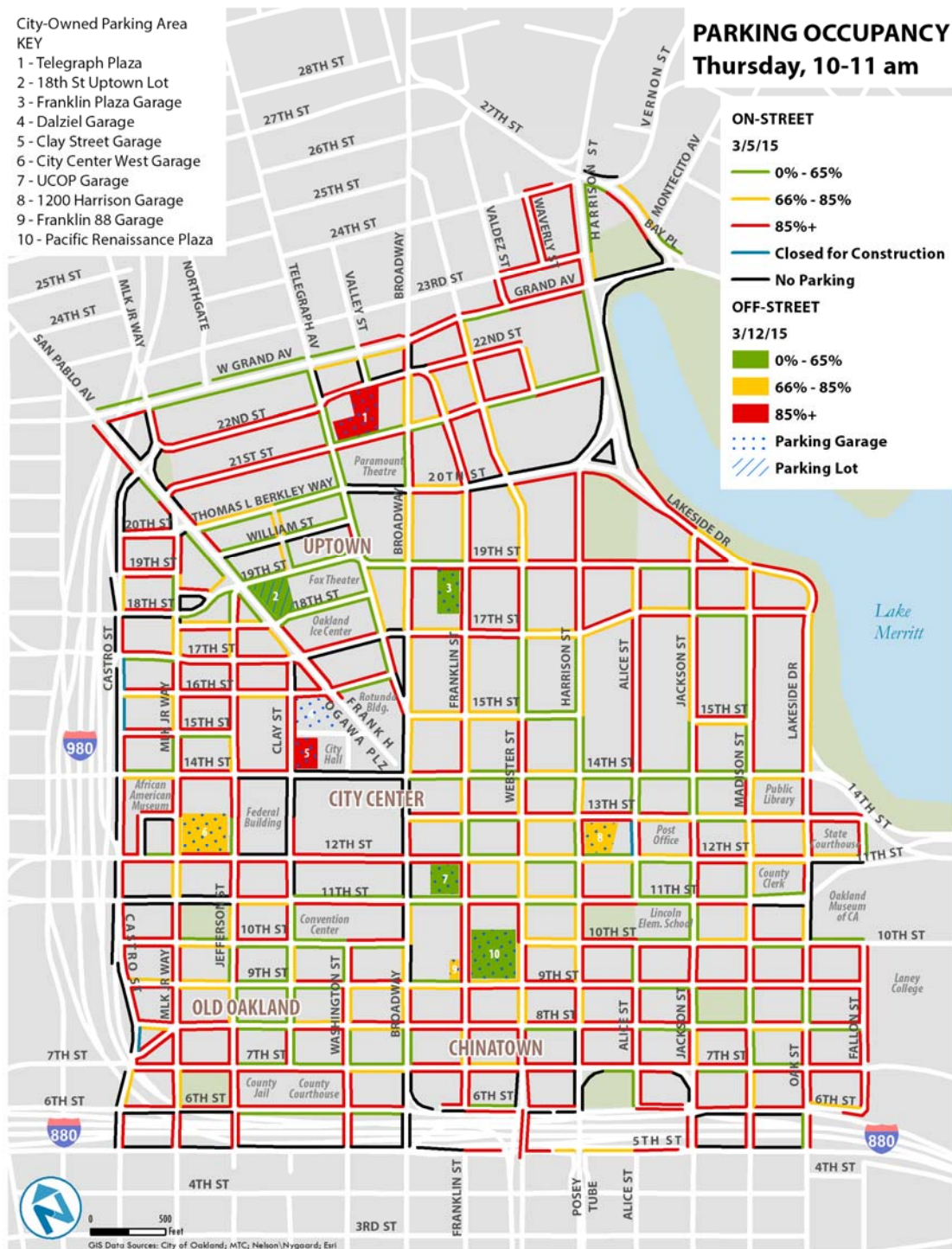
City of Oakland/Metropolitan Transportation Commission

Figure A-16 Parking Occupancy of "Regular" Spaces, Thursday 9 a.m. to 10 a.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-17 Parking Occupancy of “Regular” Spaces, Thursday 10 a.m. to 11 a.m.



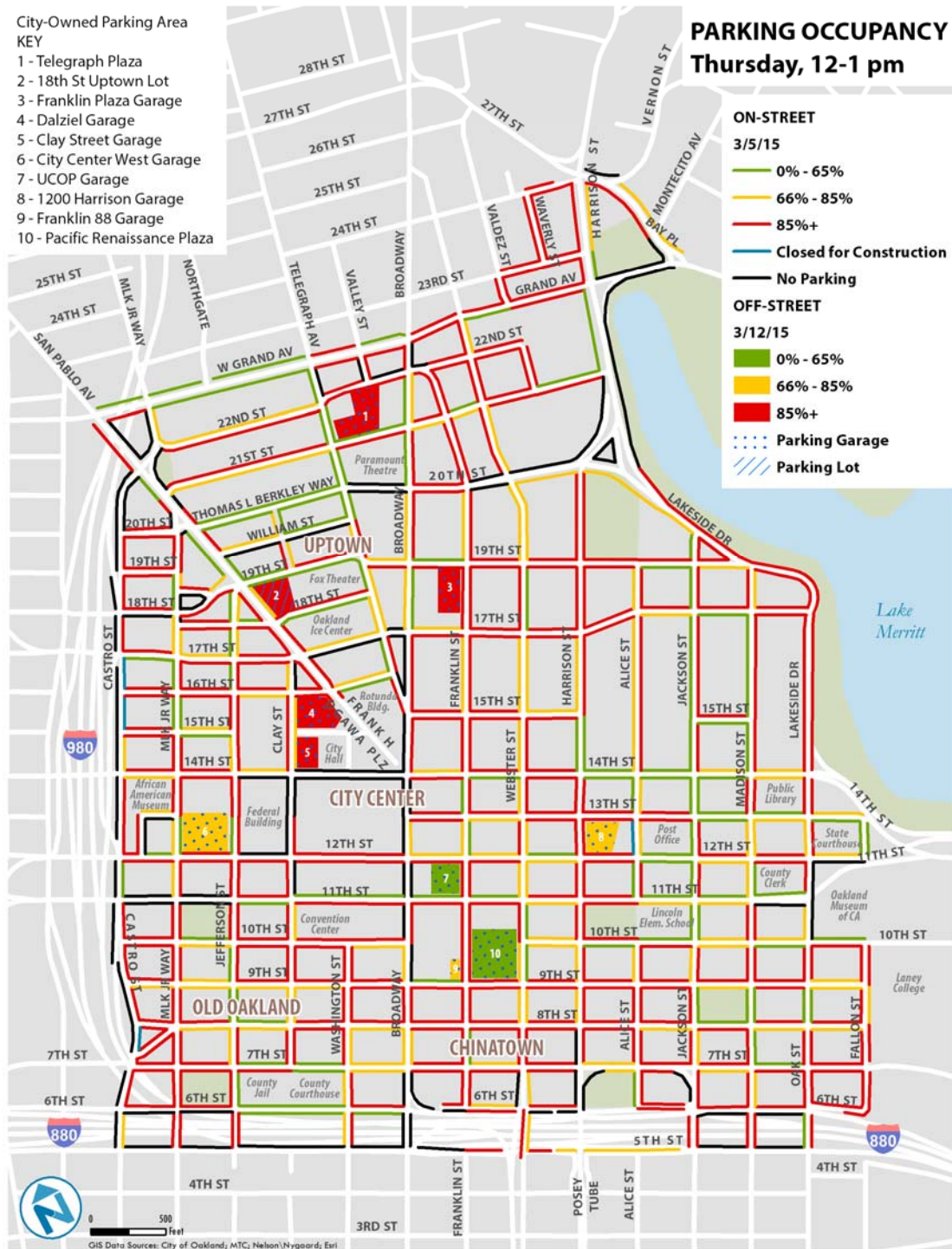
City of Oakland/Metropolitan Transportation Commission

Figure A-18 Parking Occupancy of "Regular" Spaces, Thursday 11 a.m. to 12 p.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
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Figure A-19 Parking Occupancy of “Regular” Spaces, Thursday 12 p.m. to 1 p.m.



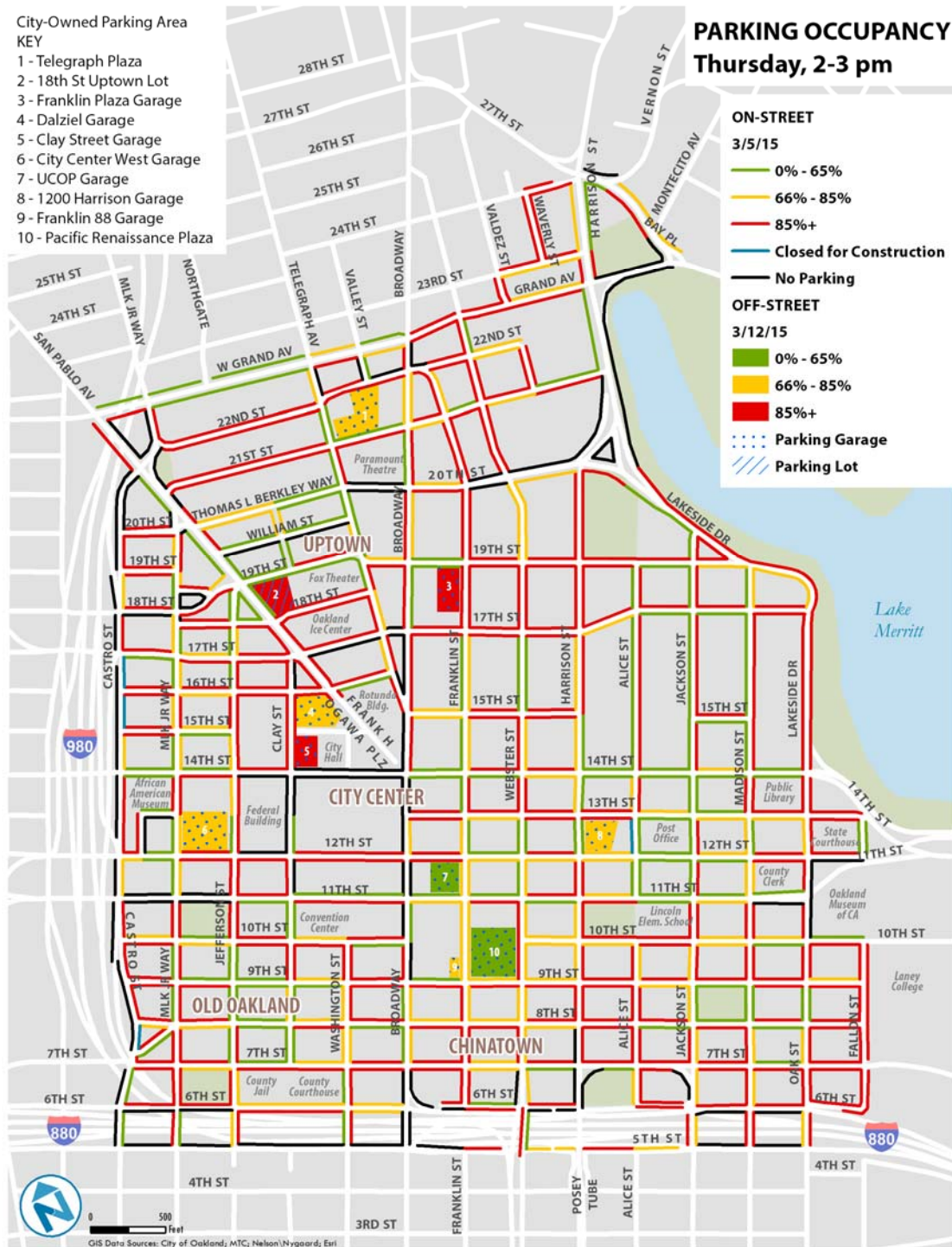
City of Oakland/Metropolitan Transportation Commission

Figure A-20 Parking Occupancy of "Regular" Spaces, Thursday 1 p.m. to 2 p.m.



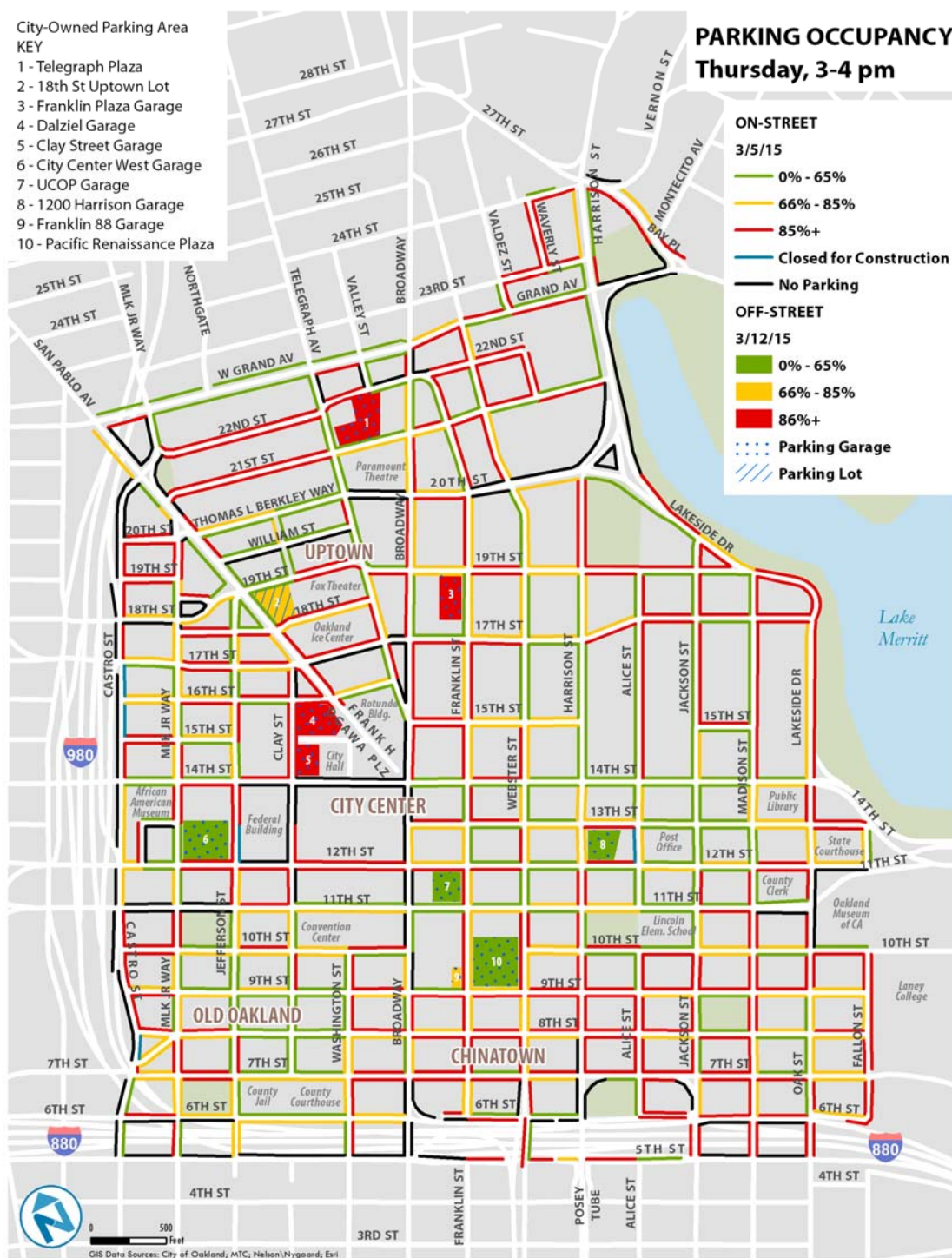
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-21 Parking Occupancy of “Regular” Spaces, Thursday 2 p.m. to 3 p.m.



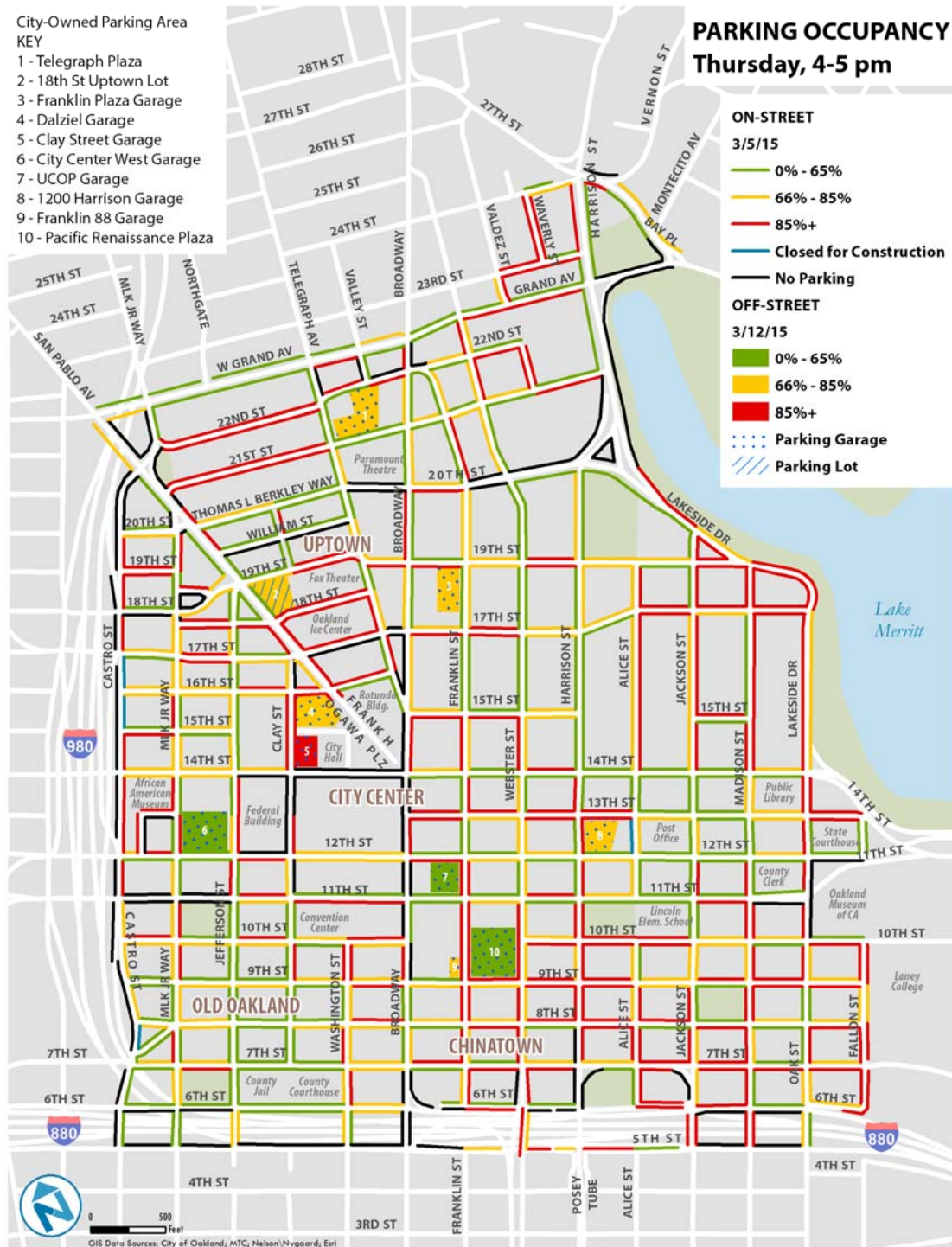
City of Oakland/Metropolitan Transportation Commission

Figure A-22 Parking Occupancy of "Regular" Spaces, Thursday 3 p.m. to 4 p.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-23 Parking Occupancy of “Regular” Spaces, Thursday 4 p.m. to 5 p.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-24 Parking Occupancy of “Regular” Spaces, Thursday 5 p.m. to 6 p.m.



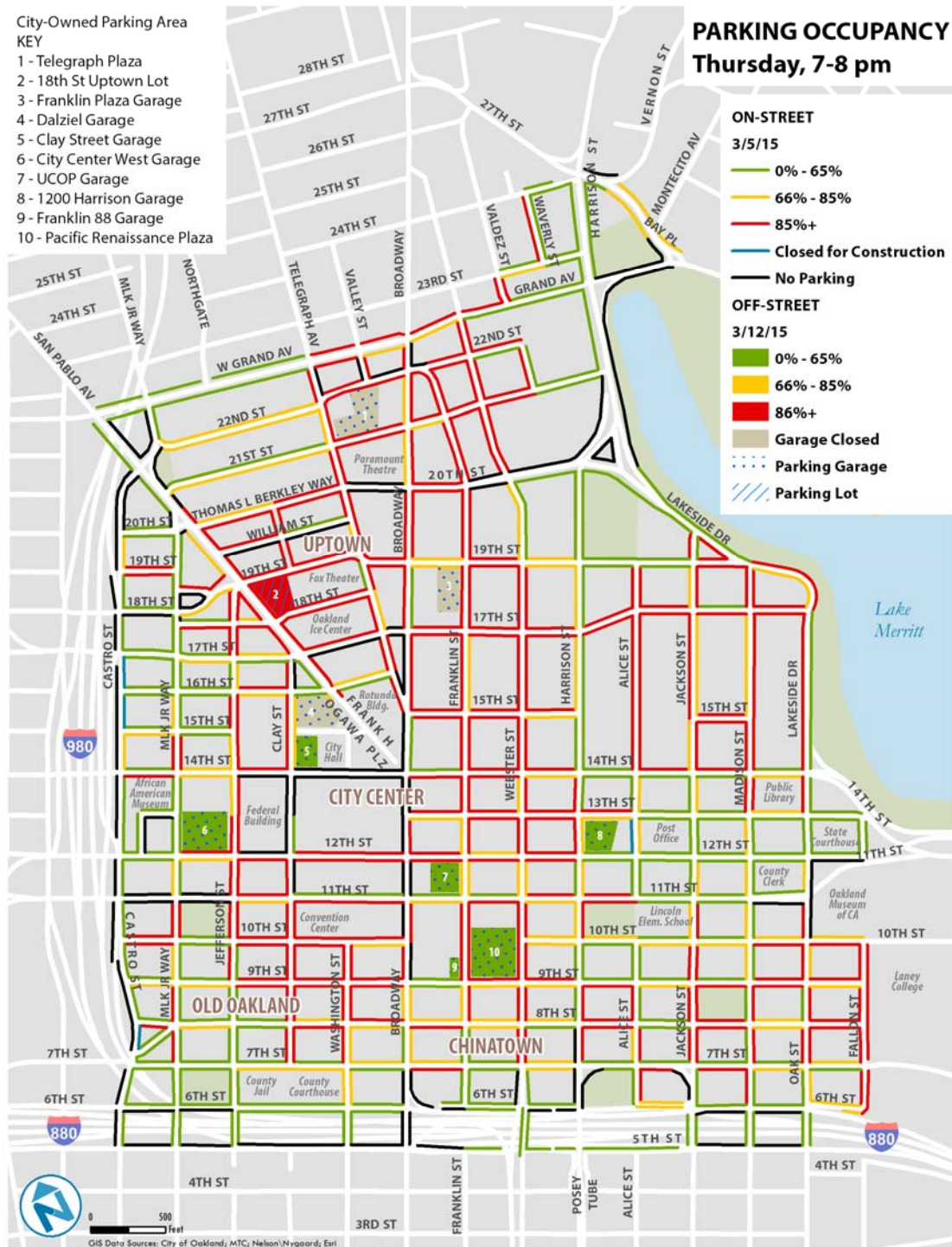
City of Oakland/Metropolitan Transportation Commission

Figure A-25 Parking Occupancy of "Regular" Spaces, Thursday 6 p.m. to 7 p.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-26 Parking Occupancy of “Regular” Spaces, Thursday 7 p.m. to 8 p.m.



City of Oakland/Metropolitan Transportation Commission

Figure A-27 Parking Occupancy of "Regular" Spaces, Thursday 8 p.m. to 9 p.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-28 Overall Parking Occupancy, Saturday 8 a.m. to 9 a.m.



City of Oakland/Metropolitan Transportation Commission

Figure A-29 Overall Parking Occupancy, Saturday 9 a.m. to 10 a.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-30 Overall Parking Occupancy, Saturday 10 a.m. to 11 a.m.



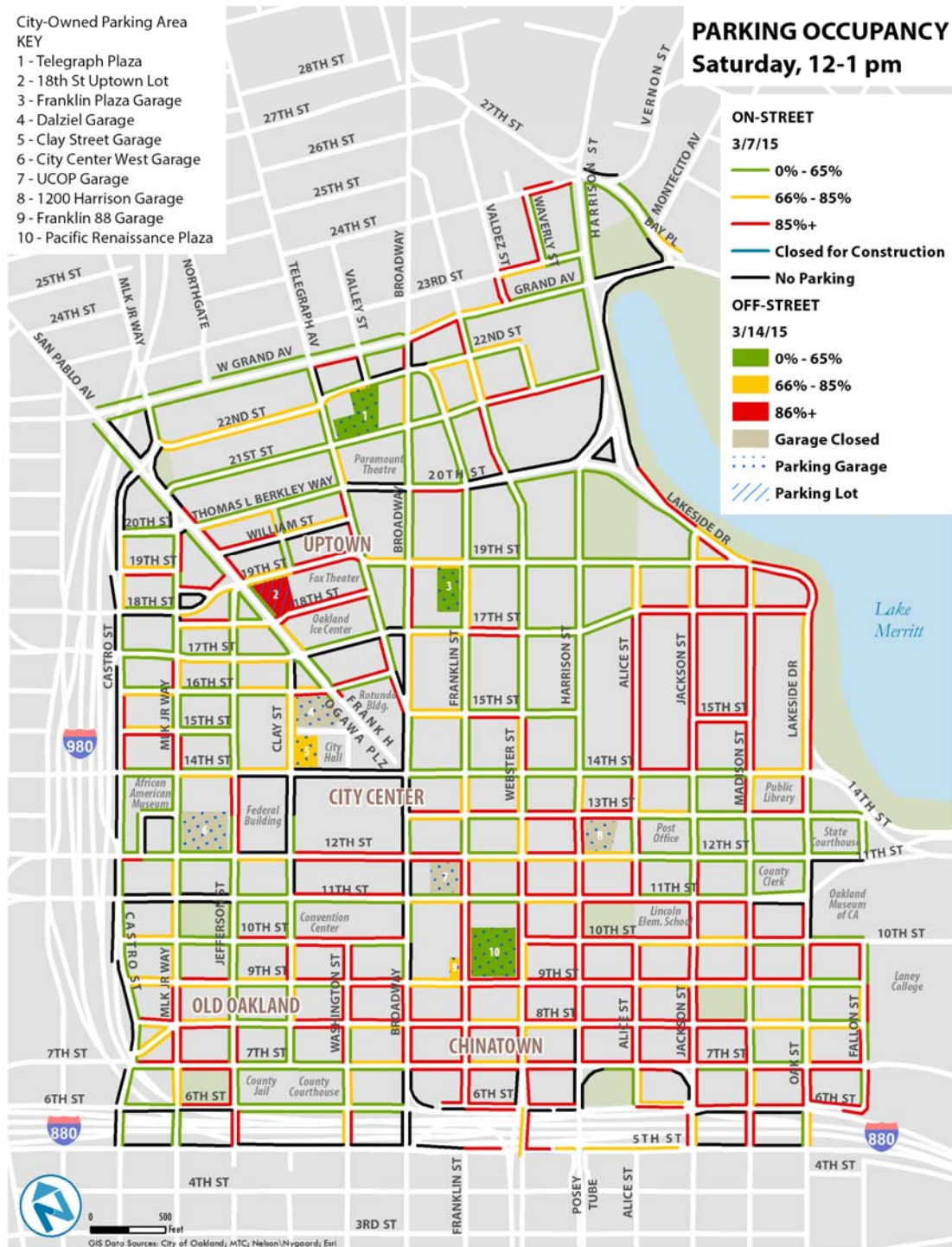
City of Oakland/Metropolitan Transportation Commission

Figure A-31 Overall Parking Occupancy, Saturday 11 a.m. to 12 p.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-32 Overall Parking Occupancy, Saturday 12 p.m. to 1 p.m.



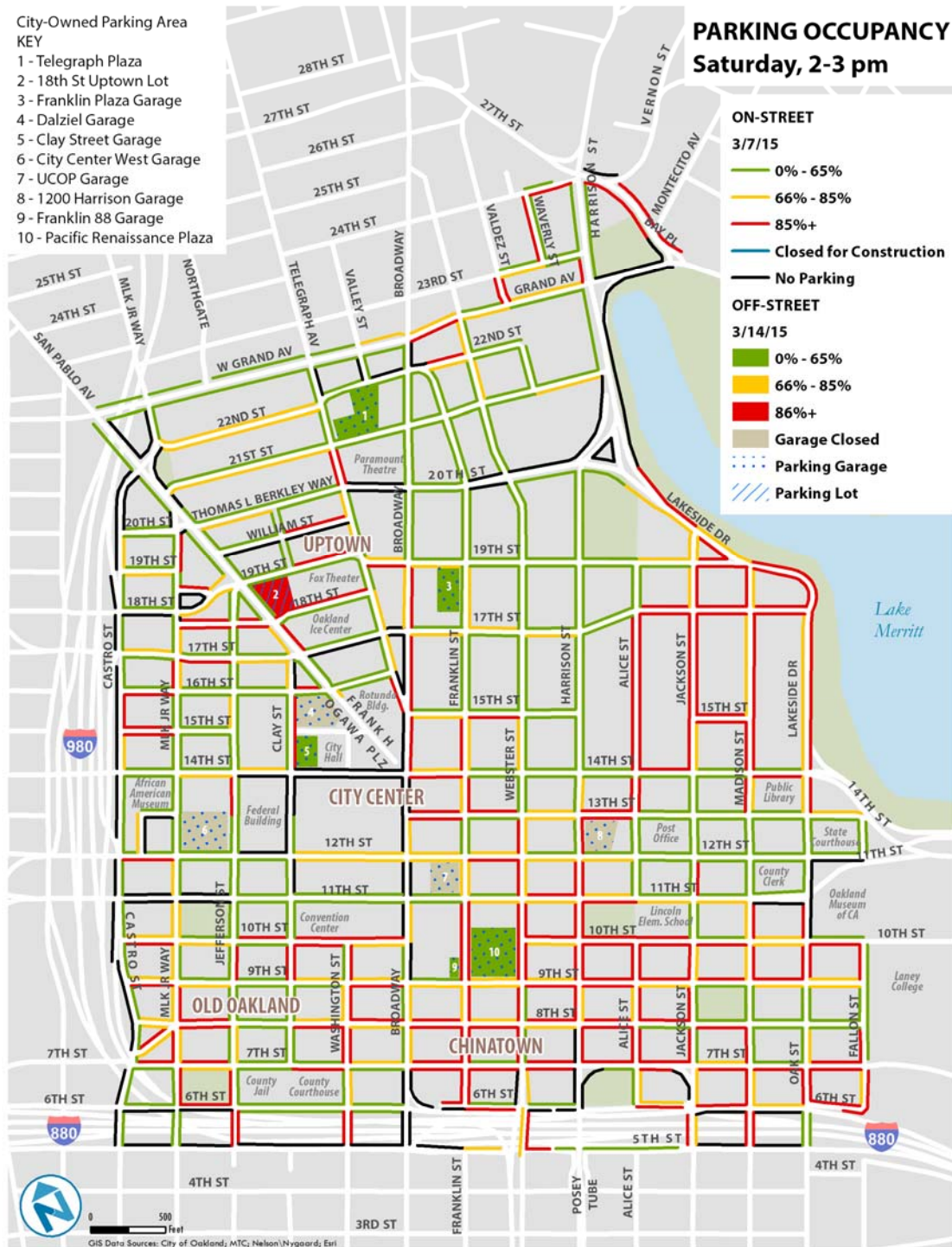
City of Oakland/Metropolitan Transportation Commission

Figure A-33 Overall Parking Occupancy, Saturday 1 p.m. to 2 p.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-34 Overall Parking Occupancy, Saturday 2 p.m. to 3 p.m.



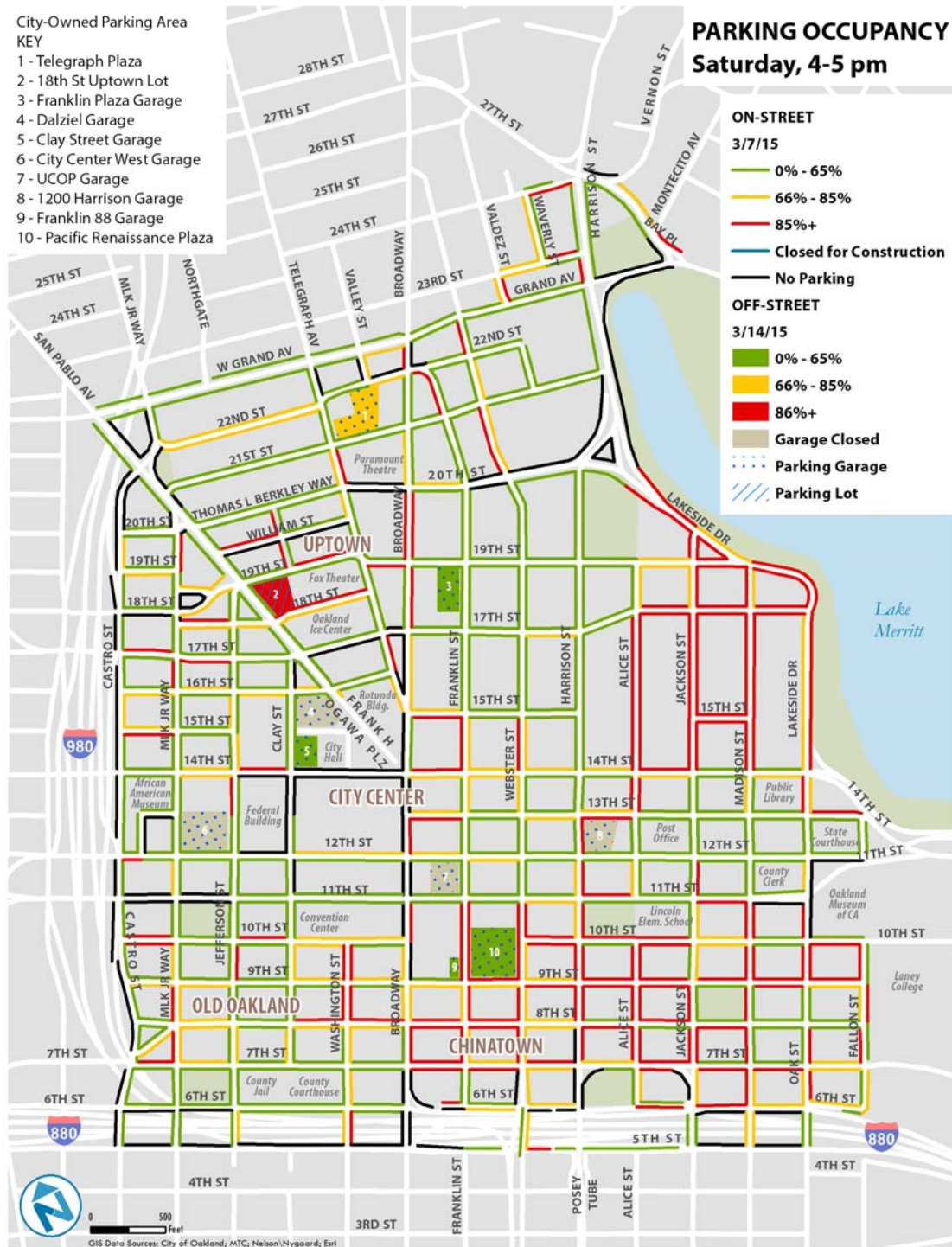
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-35 Overall Parking Occupancy, Saturday 3 p.m. to 4 p.m.



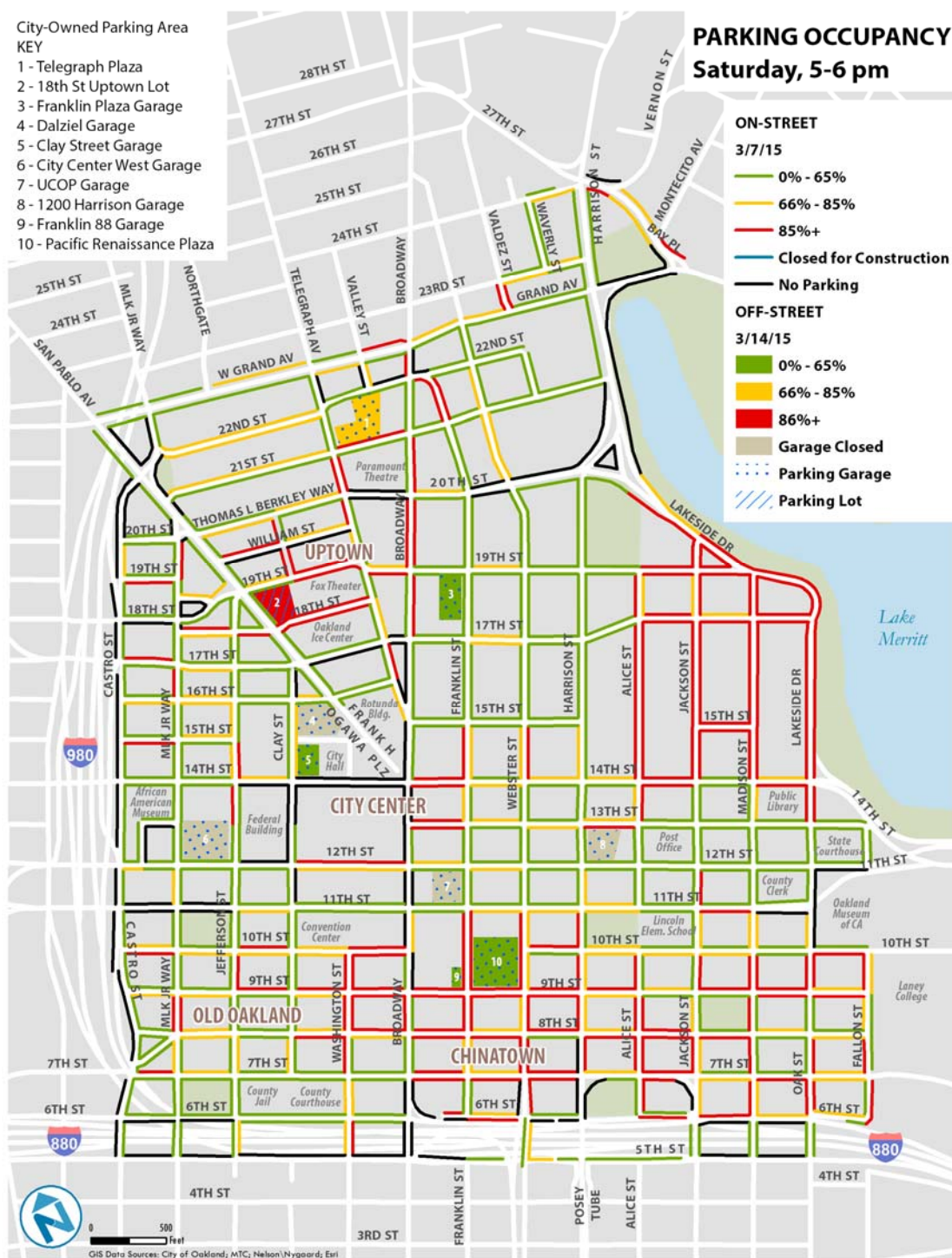
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-36 Overall Parking Occupancy, Saturday 4 p.m. to 5 p.m.



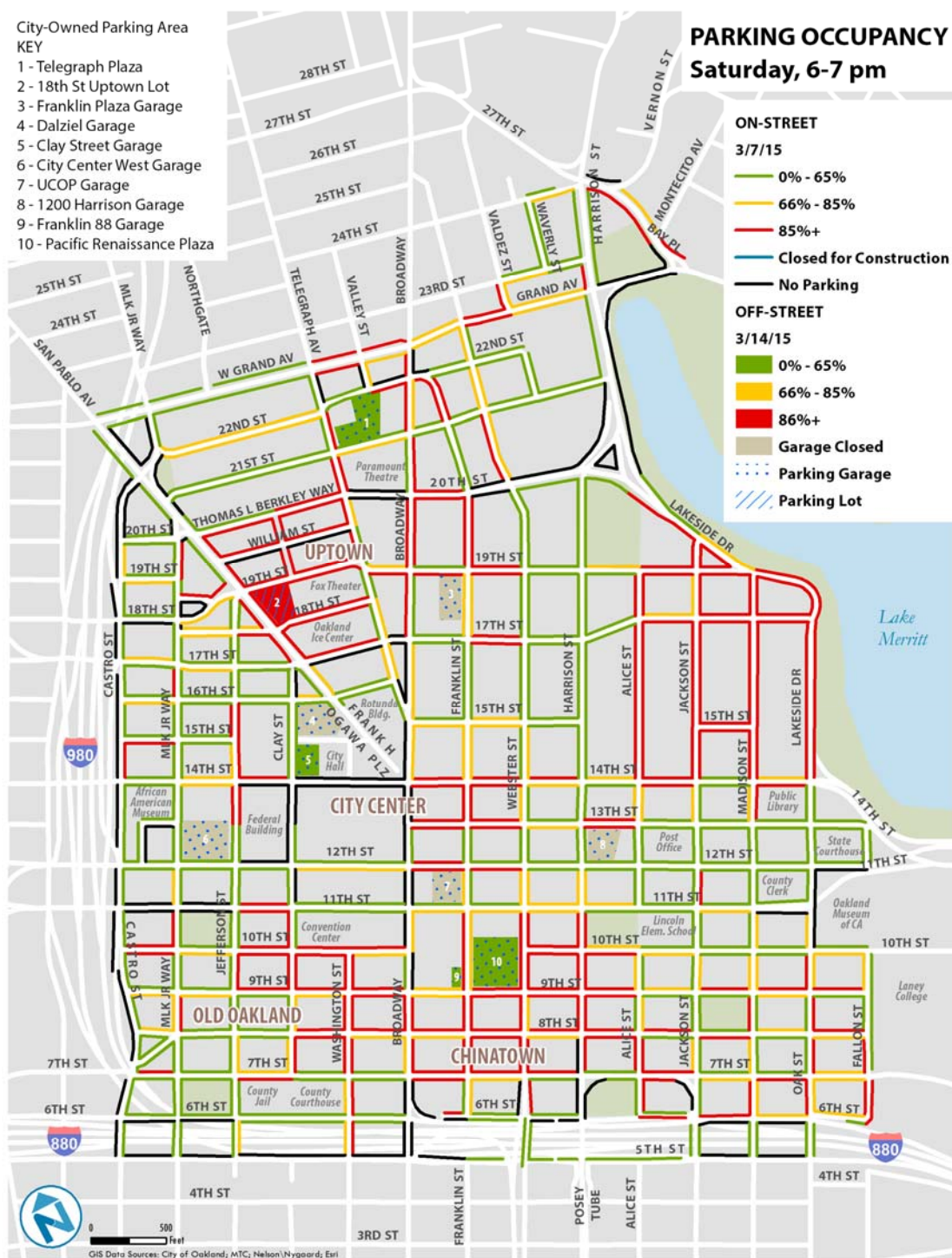
City of Oakland/Metropolitan Transportation Commission

Figure A-37 Overall Parking Occupancy, Saturday 5 p.m. to 6 p.m.



City of Oakland/Metropolitan Transportation Commission

Figure A-38 Overall Parking Occupancy, Saturday 6 p.m. to 7 p.m.



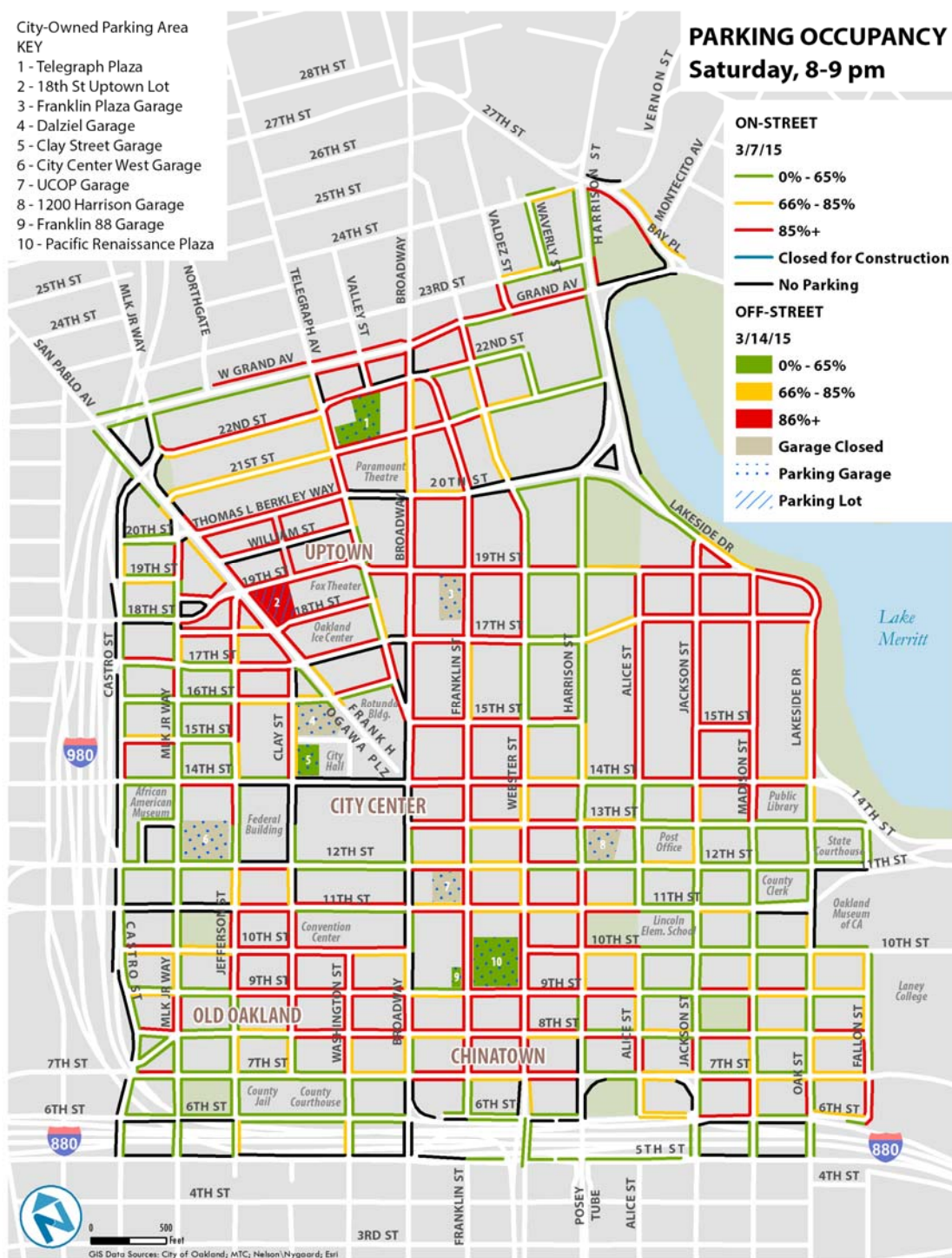
City of Oakland/Metropolitan Transportation Commission

Figure A-39 Overall Parking Occupancy, Saturday 7 p.m. to 8 p.m.



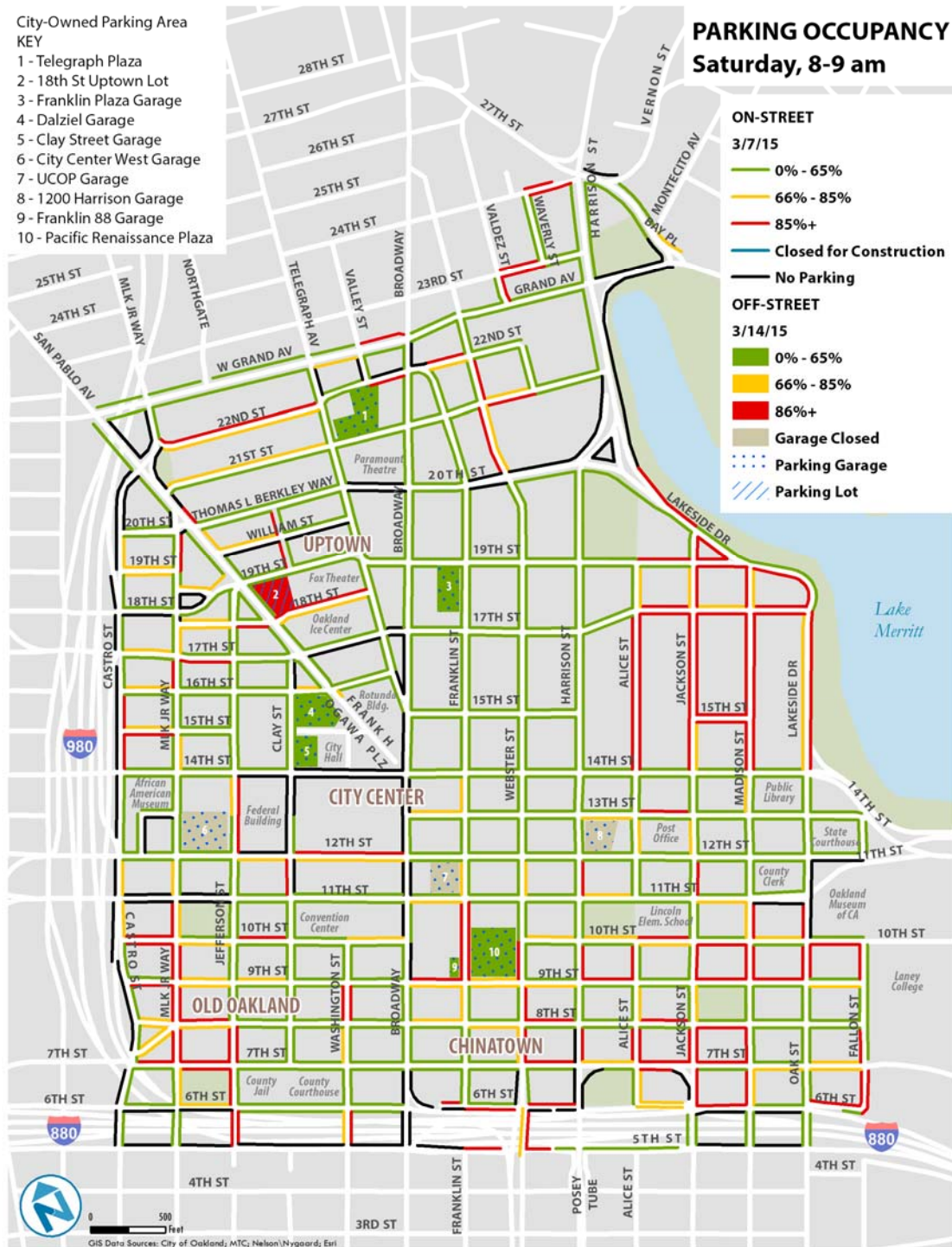
City of Oakland/Metropolitan Transportation Commission

Figure A-40 Overall Parking Occupancy, Saturday 8 p.m. to 9 p.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-41 Parking Occupancy of “Regular” Spaces, Saturday 8 a.m. to 9 a.m.



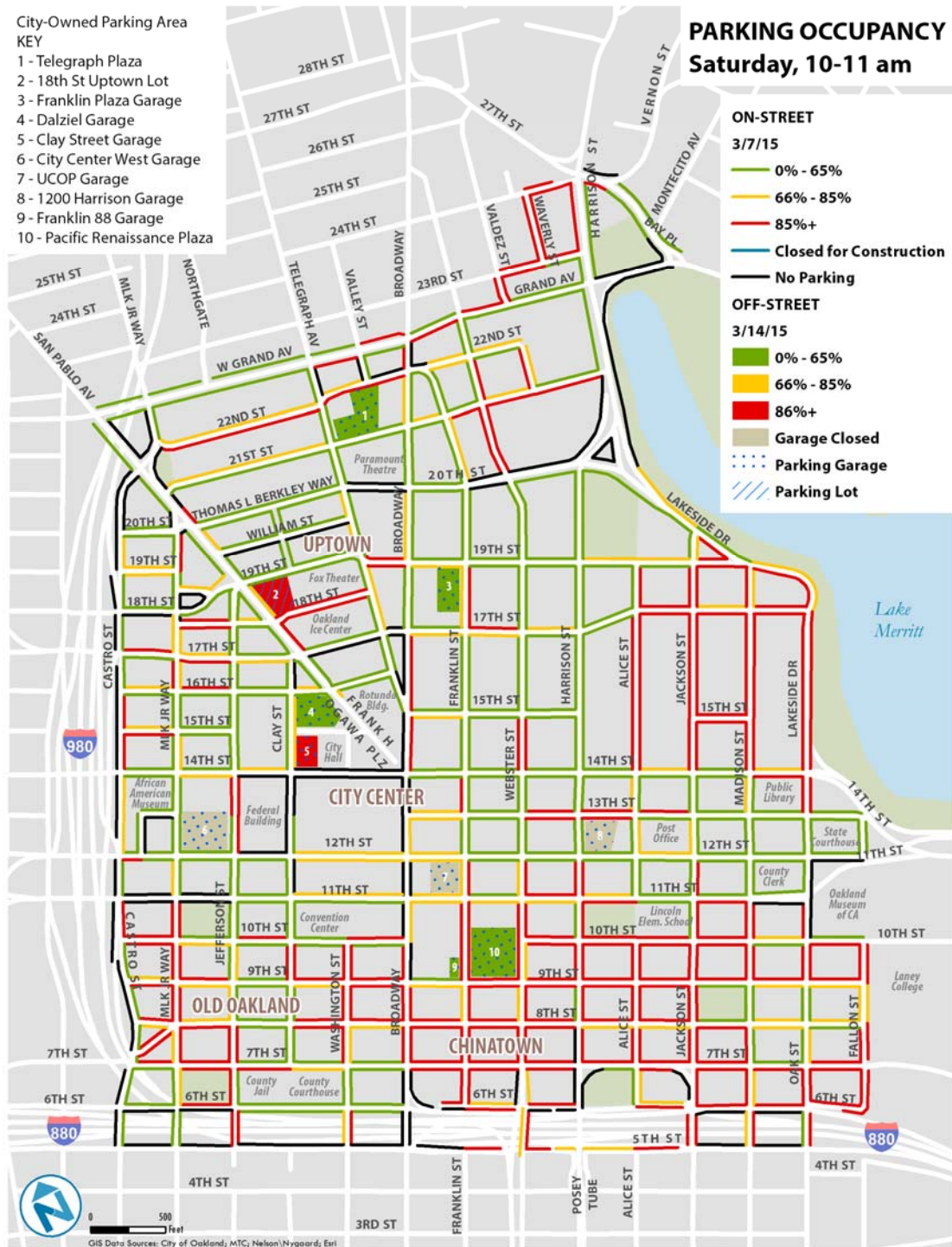
City of Oakland/Metropolitan Transportation Commission

Figure A-42 Parking Occupancy of "Regular" Spaces, Saturday 9 a.m. to 10 a.m.



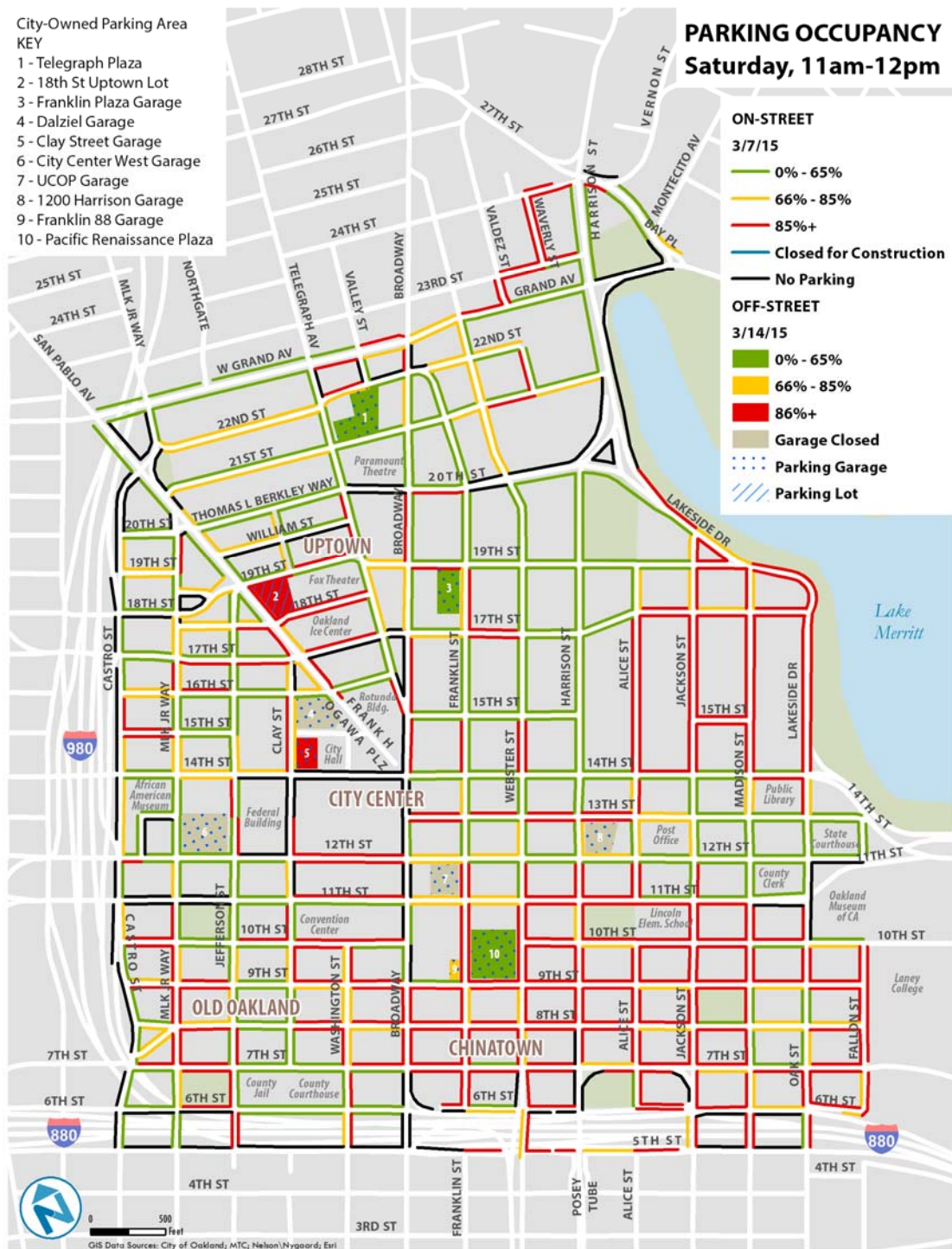
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-43 Parking Occupancy of “Regular” Spaces, Saturday 10 a.m. to 11 a.m.



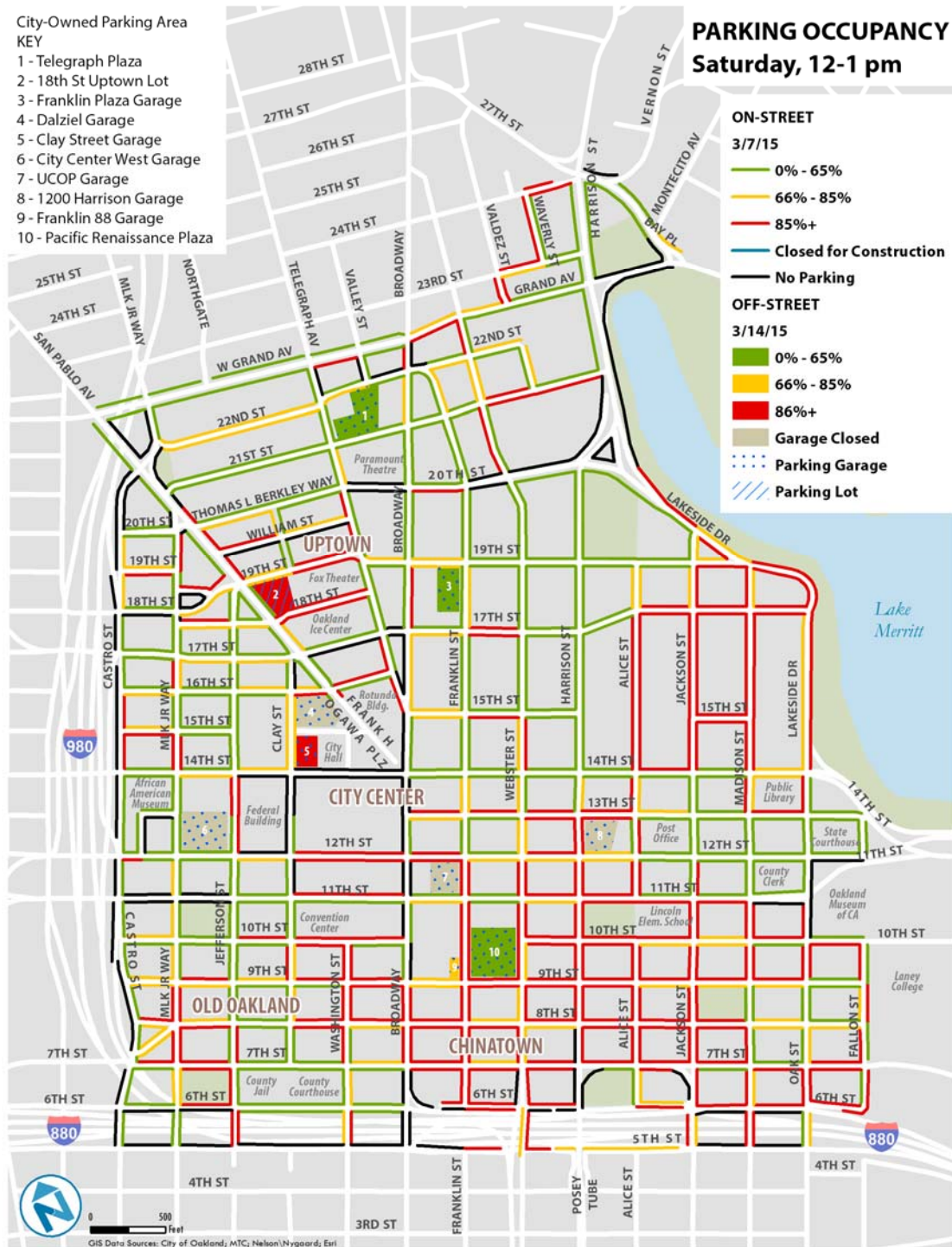
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-44 Parking Occupancy of “Regular” Spaces, Saturday 11 a.m. to 12 p.m.



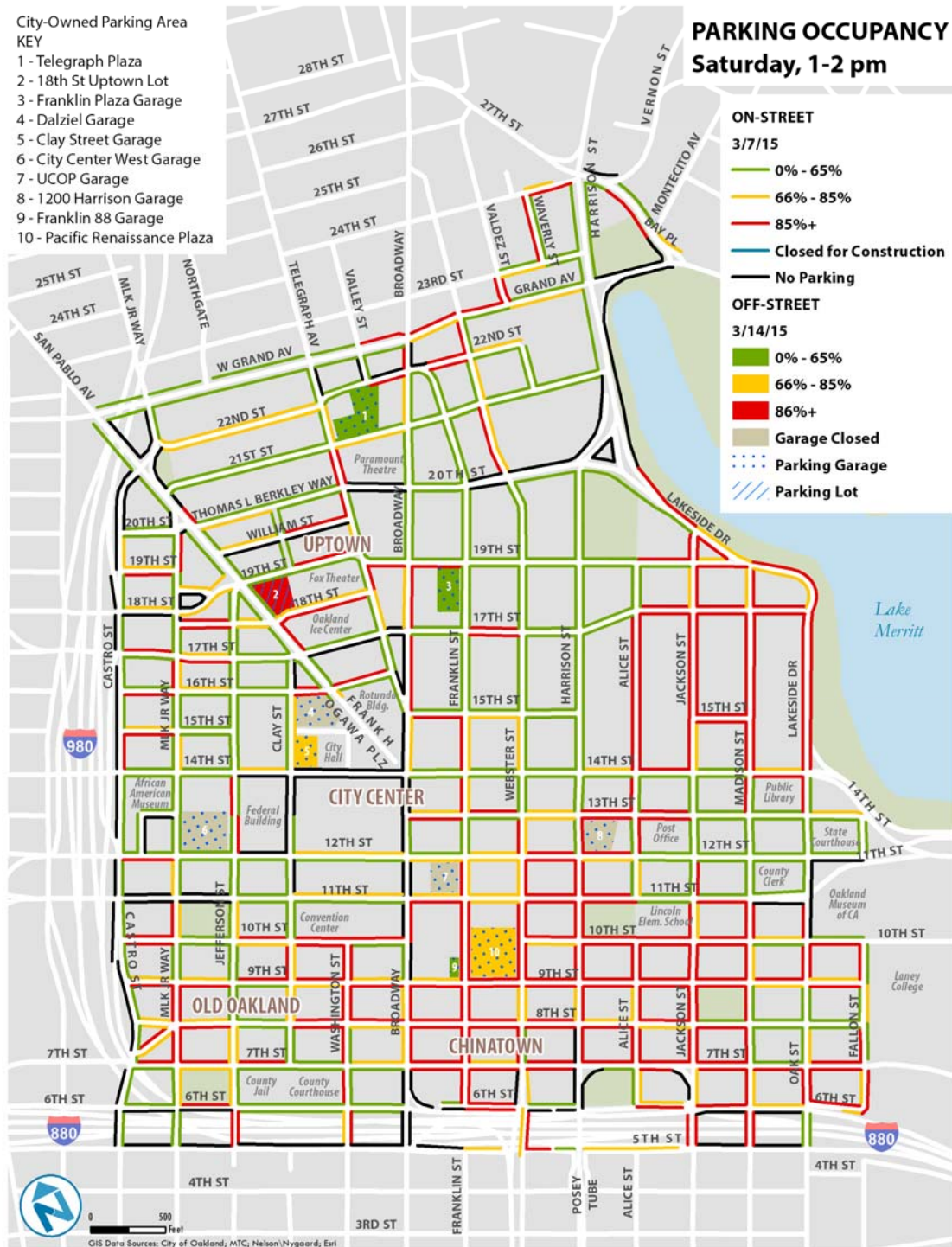
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-45 Parking Occupancy of “Regular” Spaces, Saturday 12 p.m. to 1 p.m.



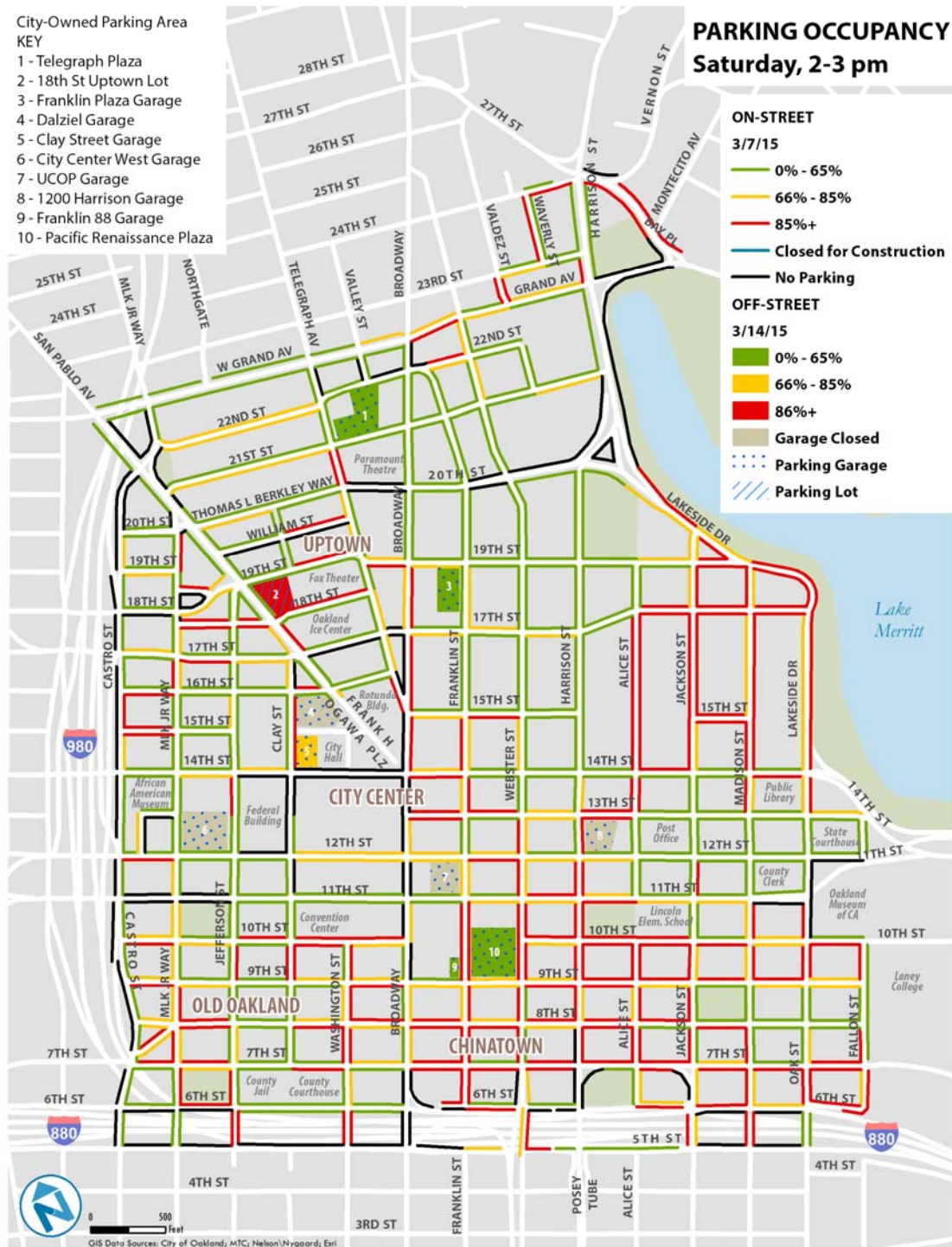
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-46 Parking Occupancy of “Regular” Spaces, Saturday 1 p.m. to 2 p.m.



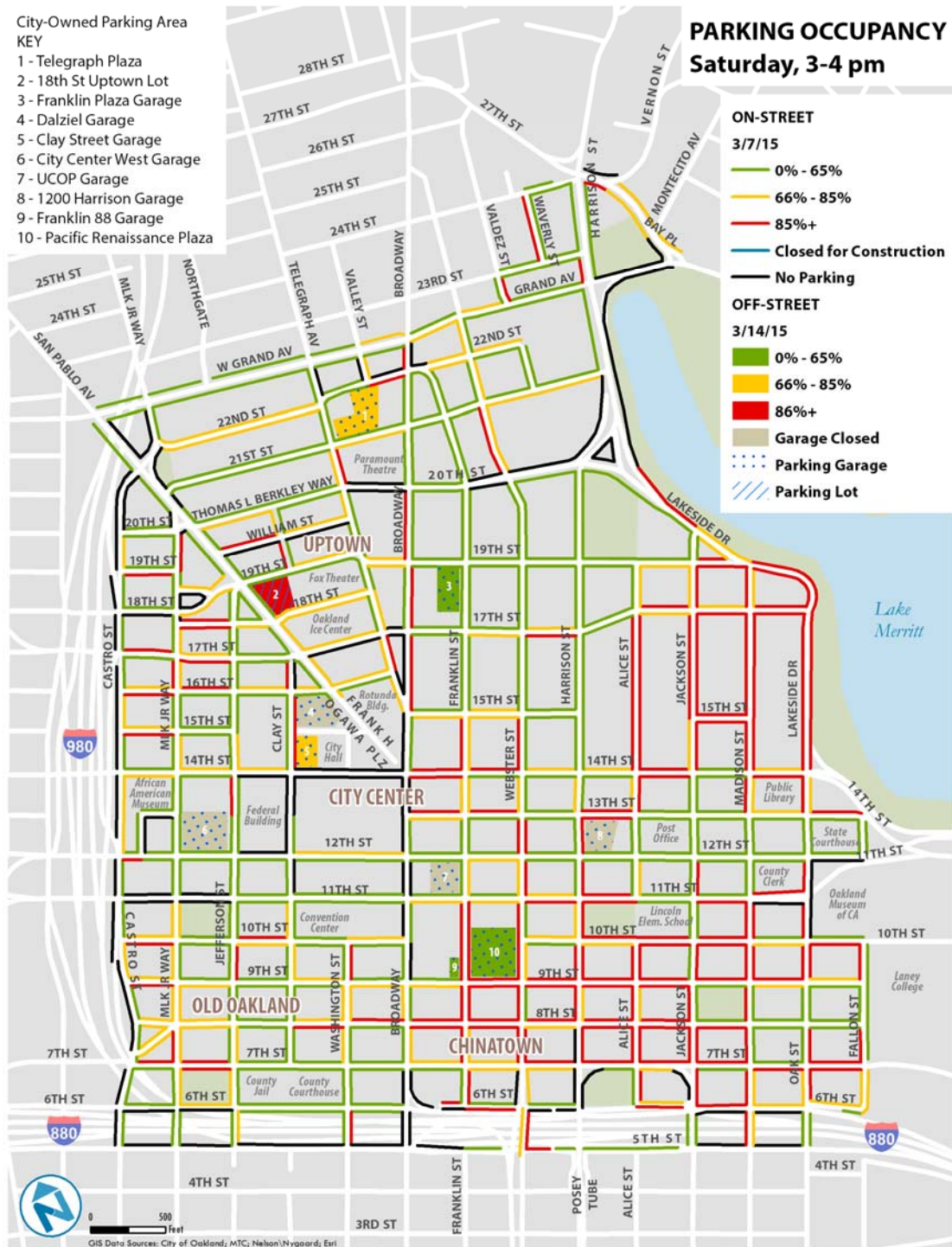
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-47 Parking Occupancy of “Regular” Spaces, Saturday 2 p.m. to 3 p.m.



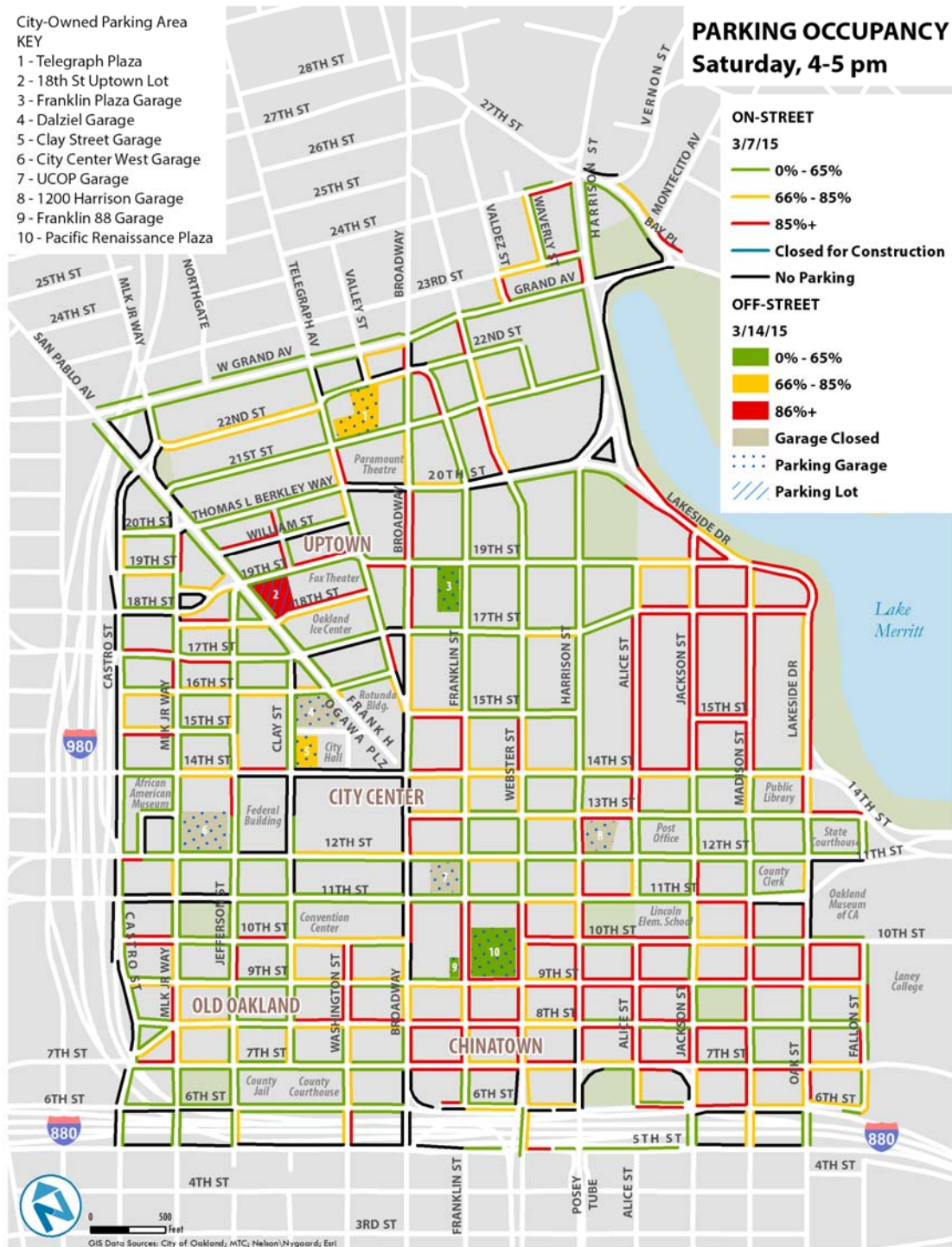
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-48 Parking Occupancy of “Regular” Spaces, Saturday 3 p.m. to 4 p.m.



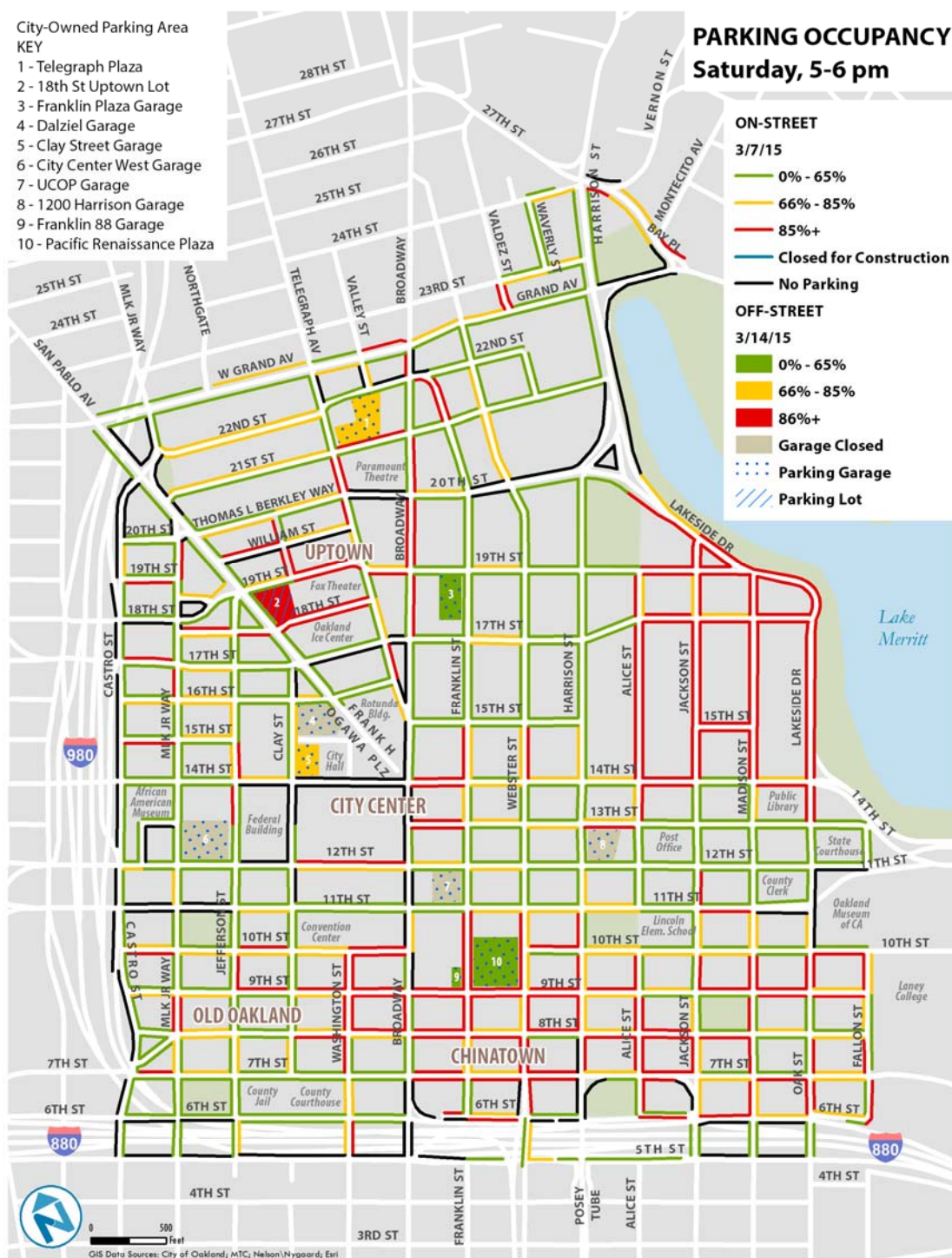
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-49 Parking Occupancy of “Regular” Spaces, Saturday 4 p.m. to 5 p.m.



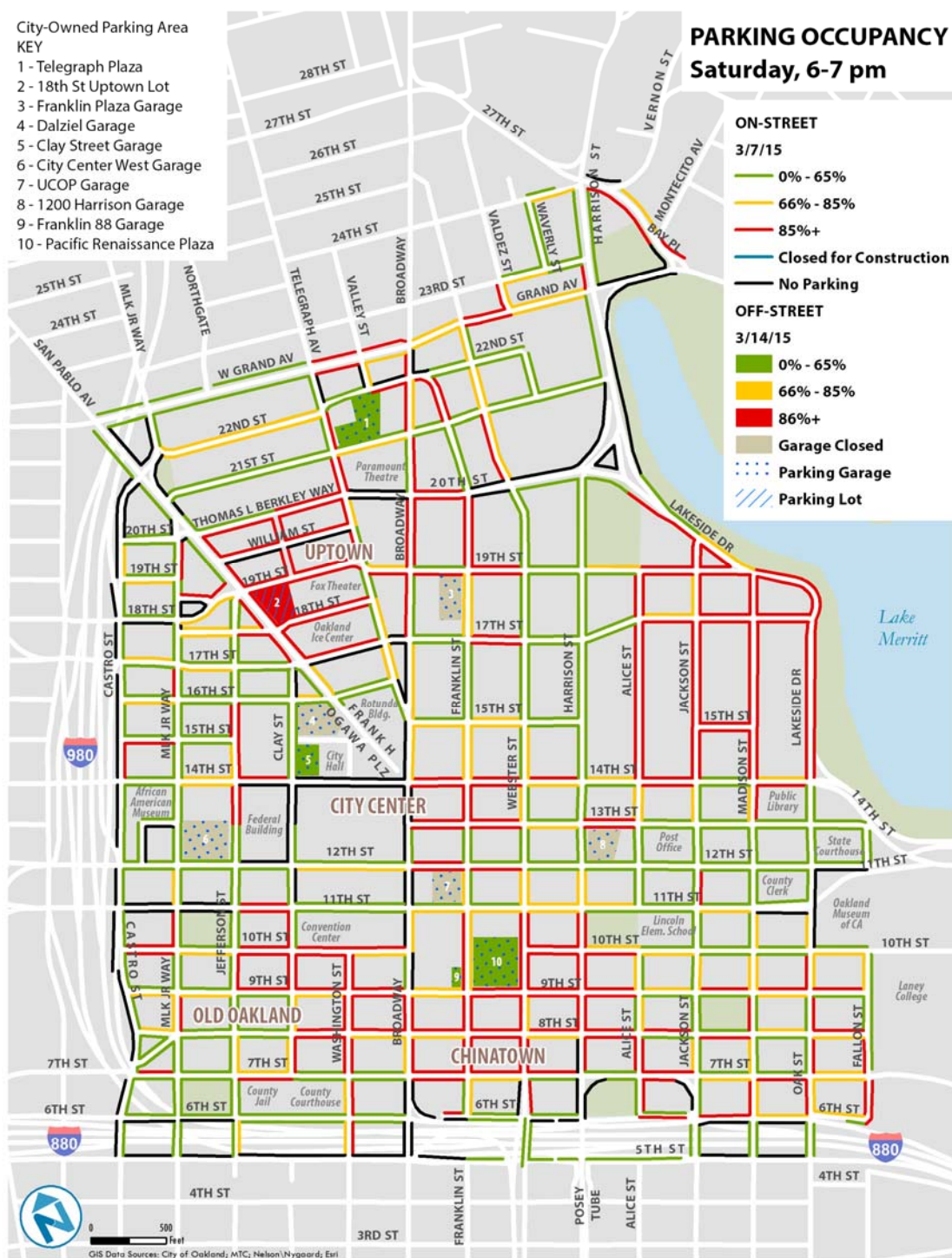
City of Oakland/Metropolitan Transportation Commission

Figure A-50 Parking Occupancy of "Regular" Spaces, Saturday 5 p.m. to 6 p.m.



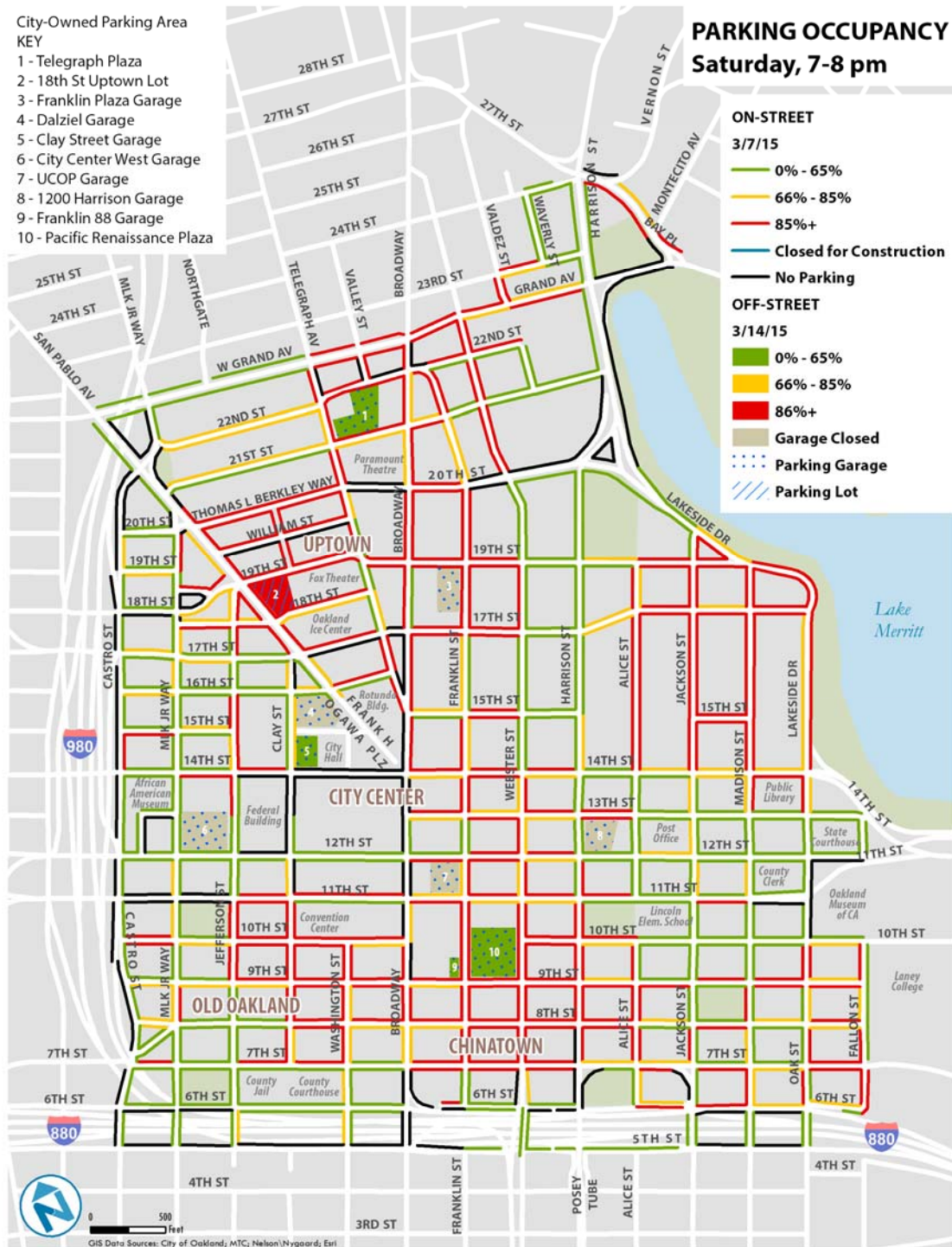
City of Oakland/Metropolitan Transportation Commission

Figure A-51 Parking Occupancy of "Regular" Spaces, Saturday 6 p.m. to 7 p.m.



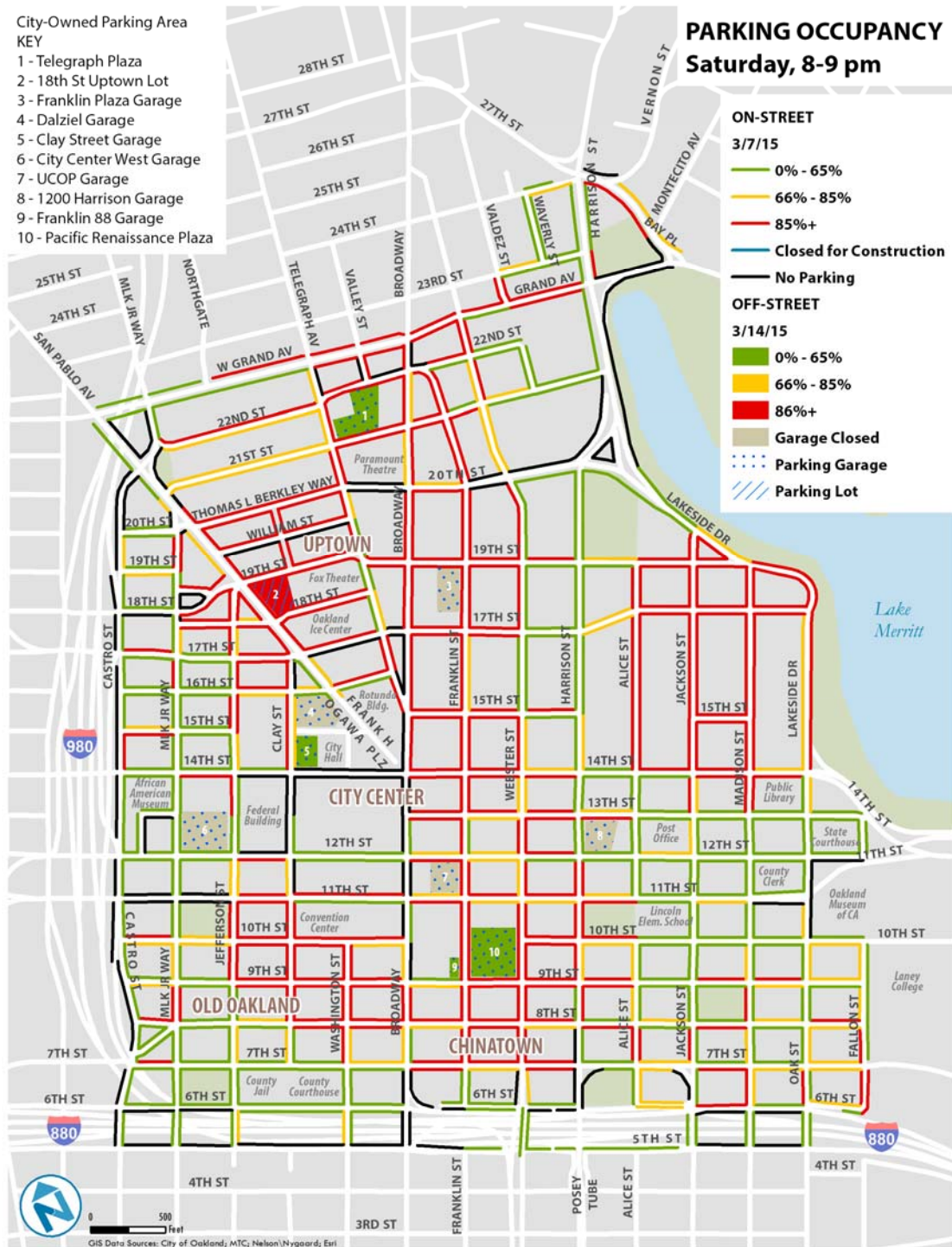
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-52 Parking Occupancy of “Regular” Spaces, Saturday 7 p.m. to 8 p.m.



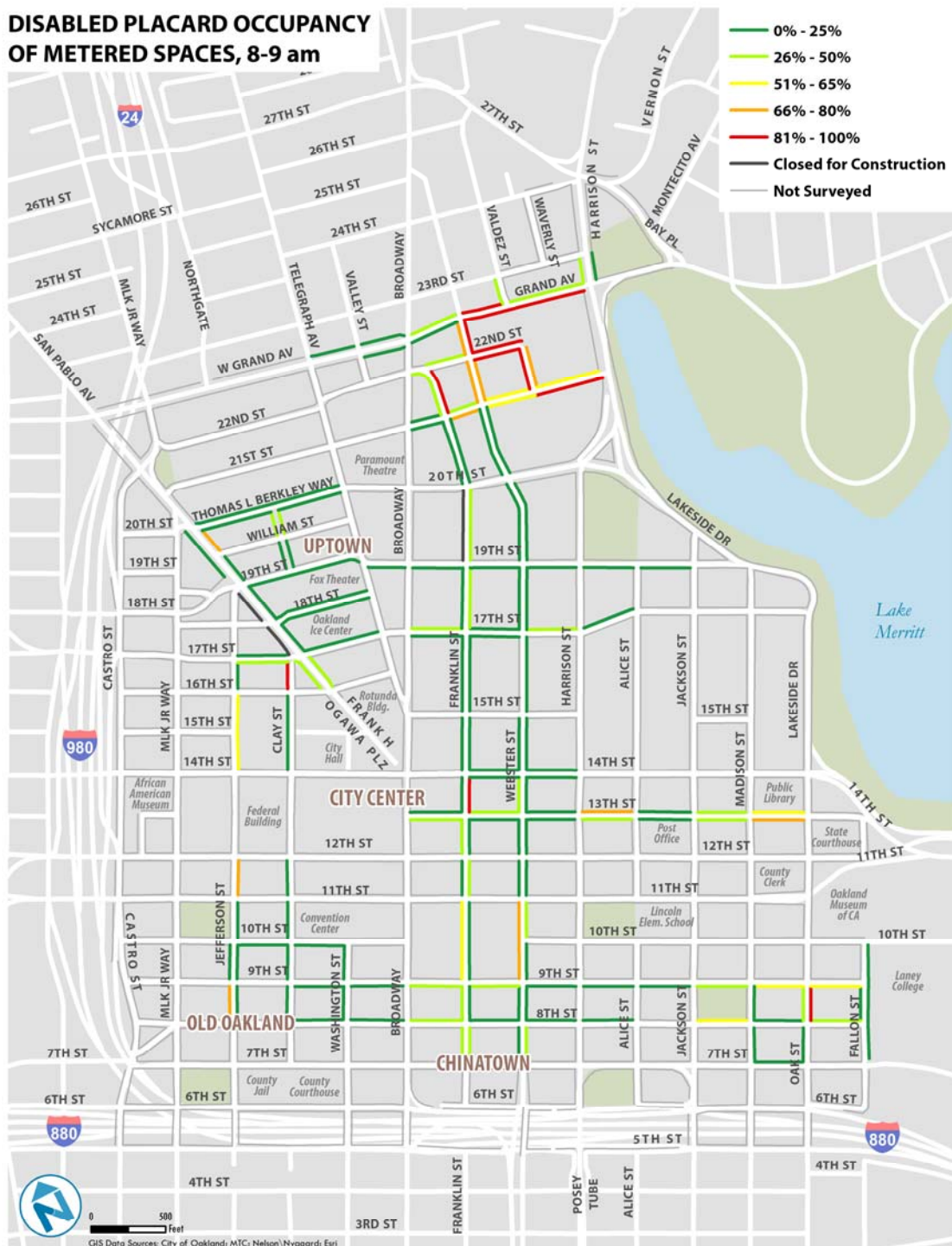
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-53 Parking Occupancy of “Regular” Spaces, Saturday 8 p.m. to 9 p.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-54 Disabled Placard Parking Occupancy of Metered Spaces, 8 a.m. to 9 a.m.



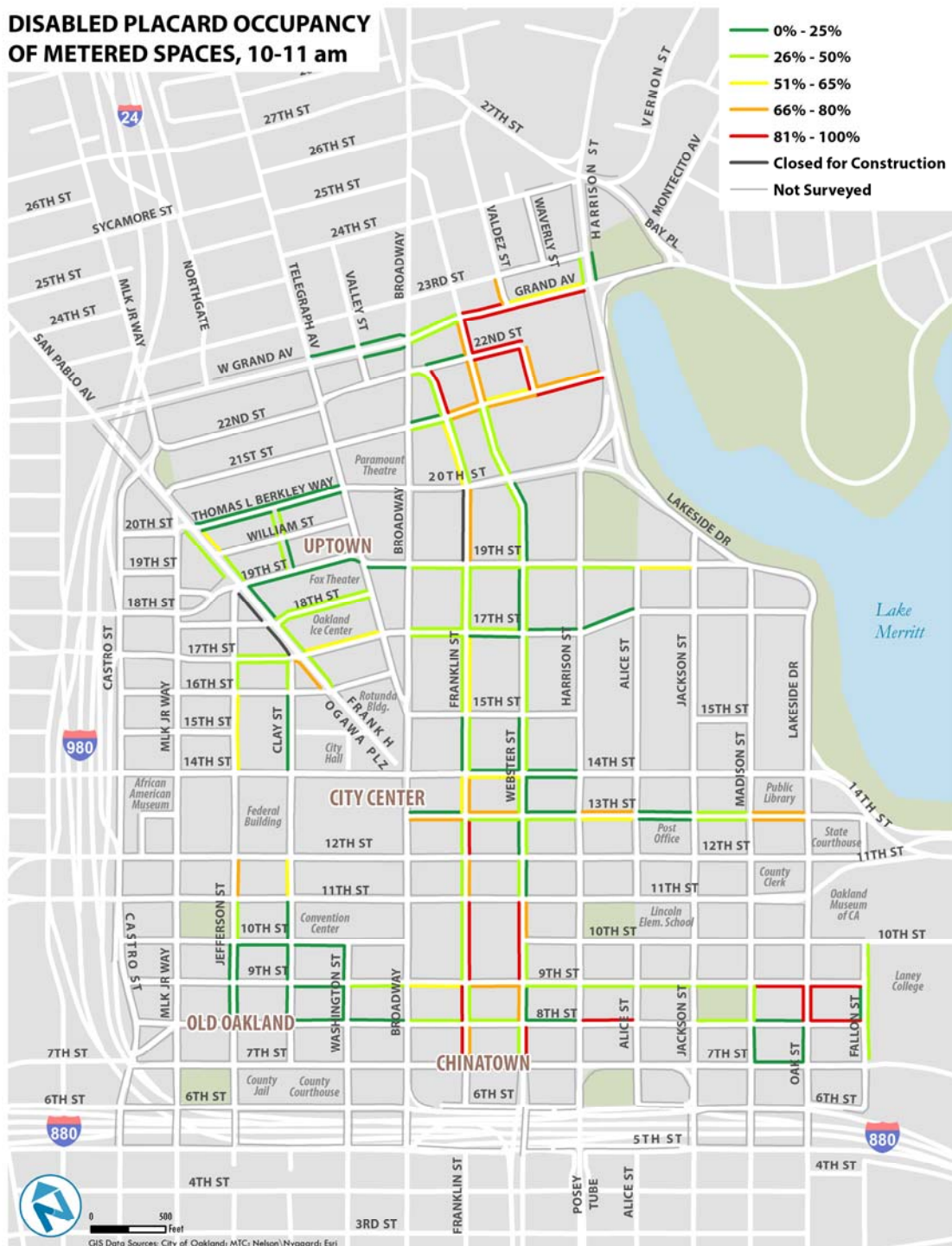
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-55 Disabled Placard Parking Occupancy of Metered Spaces, 9 a.m. to 10 a.m.



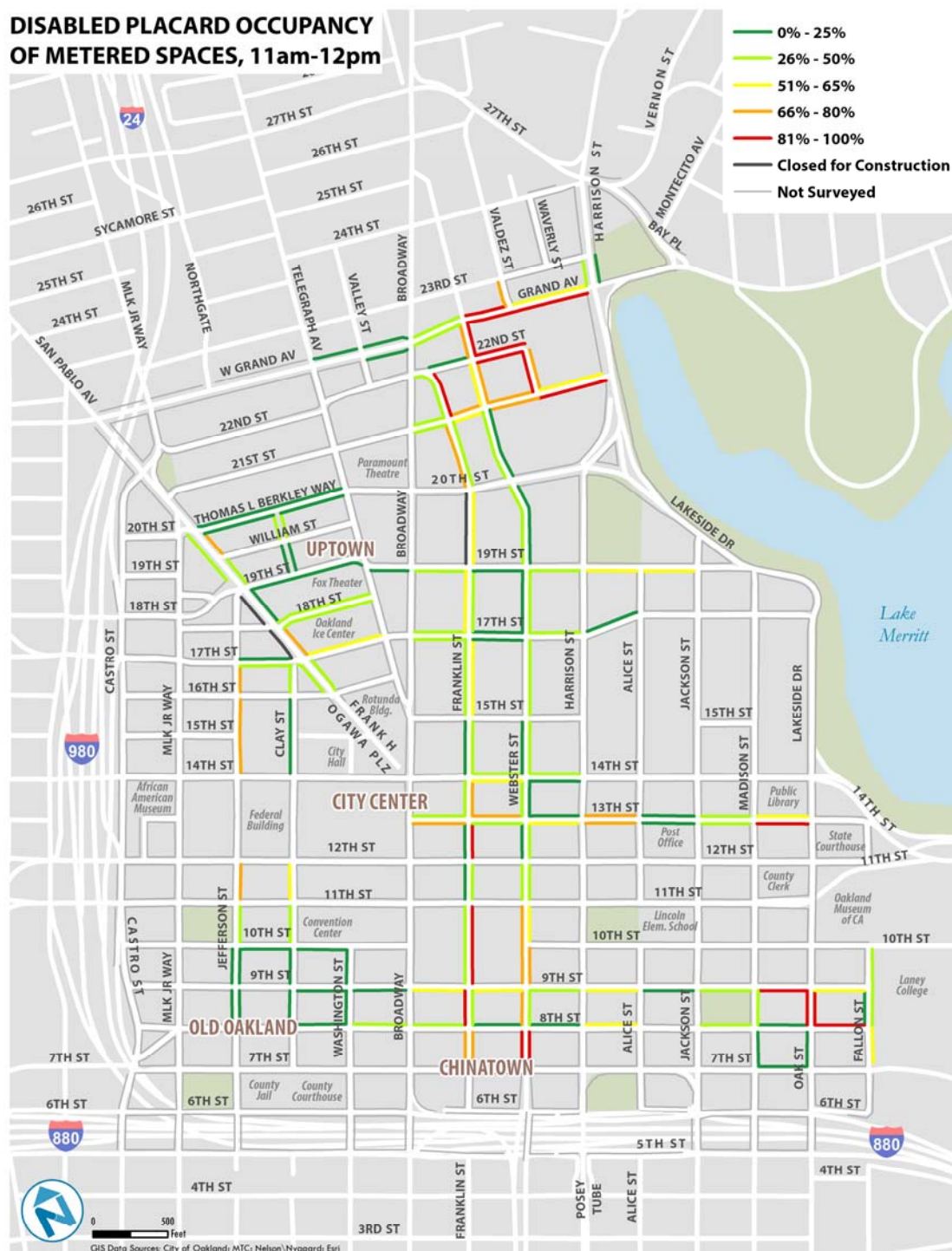
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-56 Disabled Placard Parking Occupancy of Metered Spaces, 10 a.m. to 11 a.m.



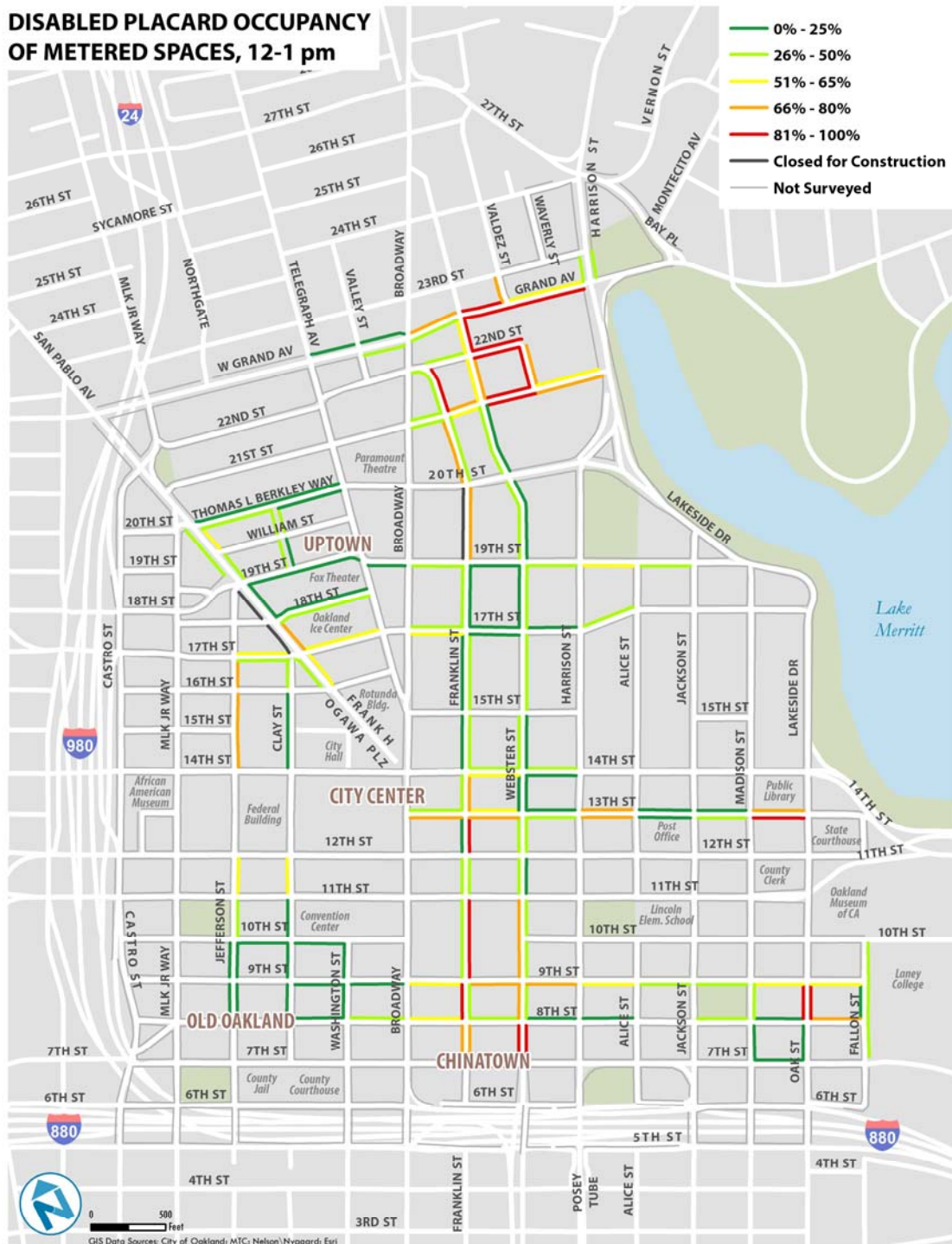
City of Oakland/Metropolitan Transportation Commission

Figure A-57 Disabled Placard Parking Occupancy of Metered Spaces, 11 a.m. to 12 p.m.



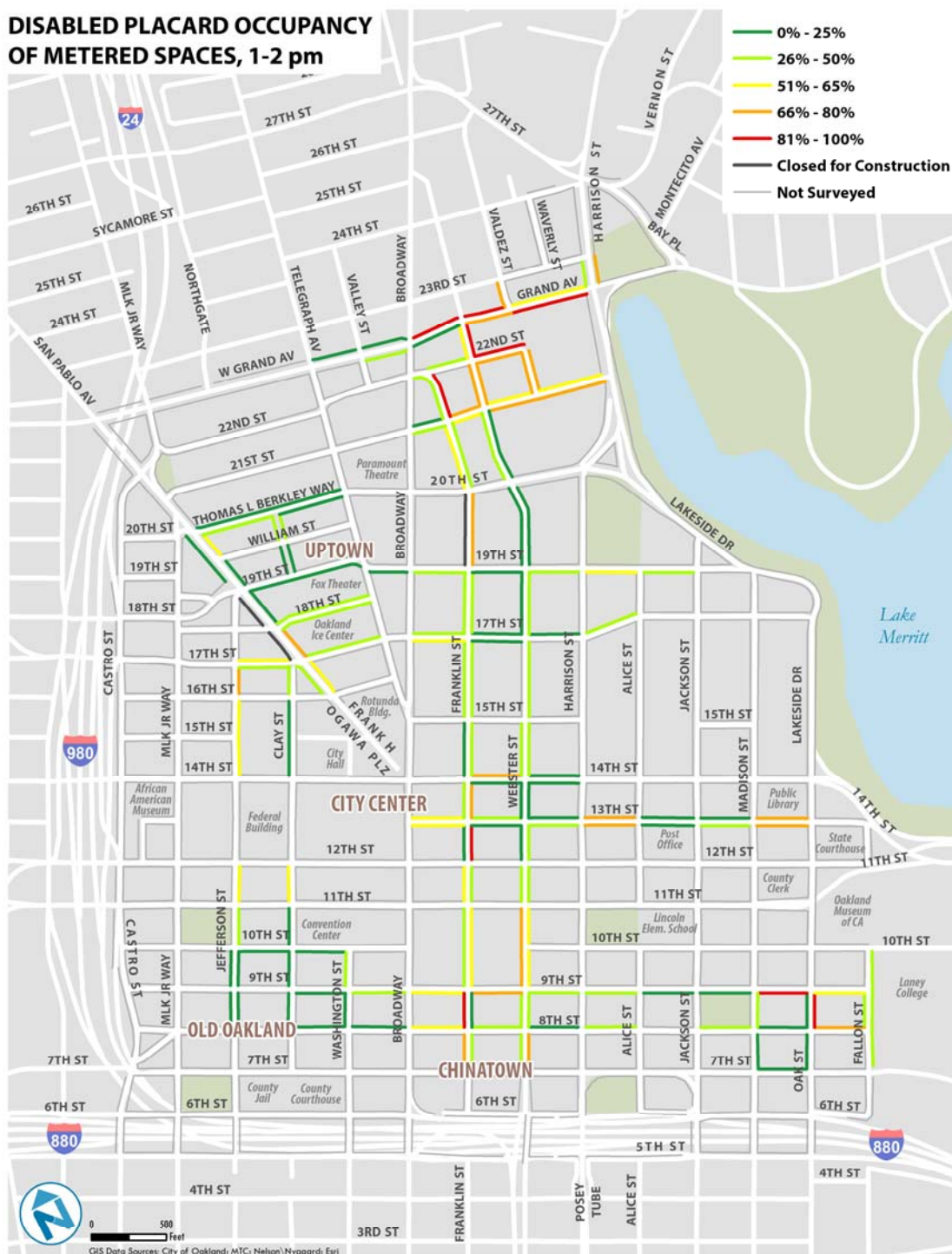
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-58 Disabled Placard Parking Occupancy of Metered Spaces, 12 p.m. to 1 p.m.



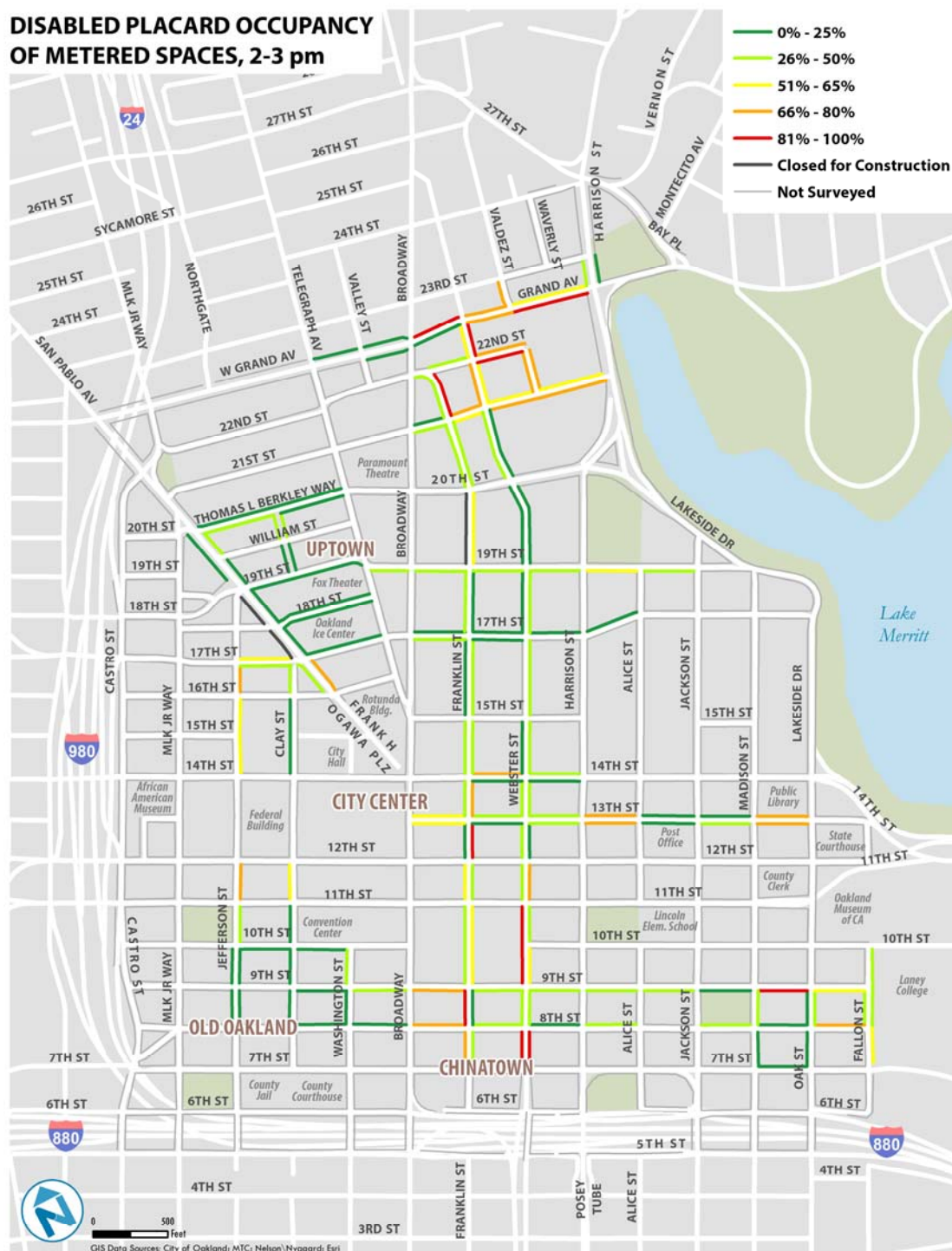
City of Oakland/Metropolitan Transportation Commission

Figure A-59 Disabled Placard Parking Occupancy of Metered Spaces, 1 p.m. to 2 p.m.



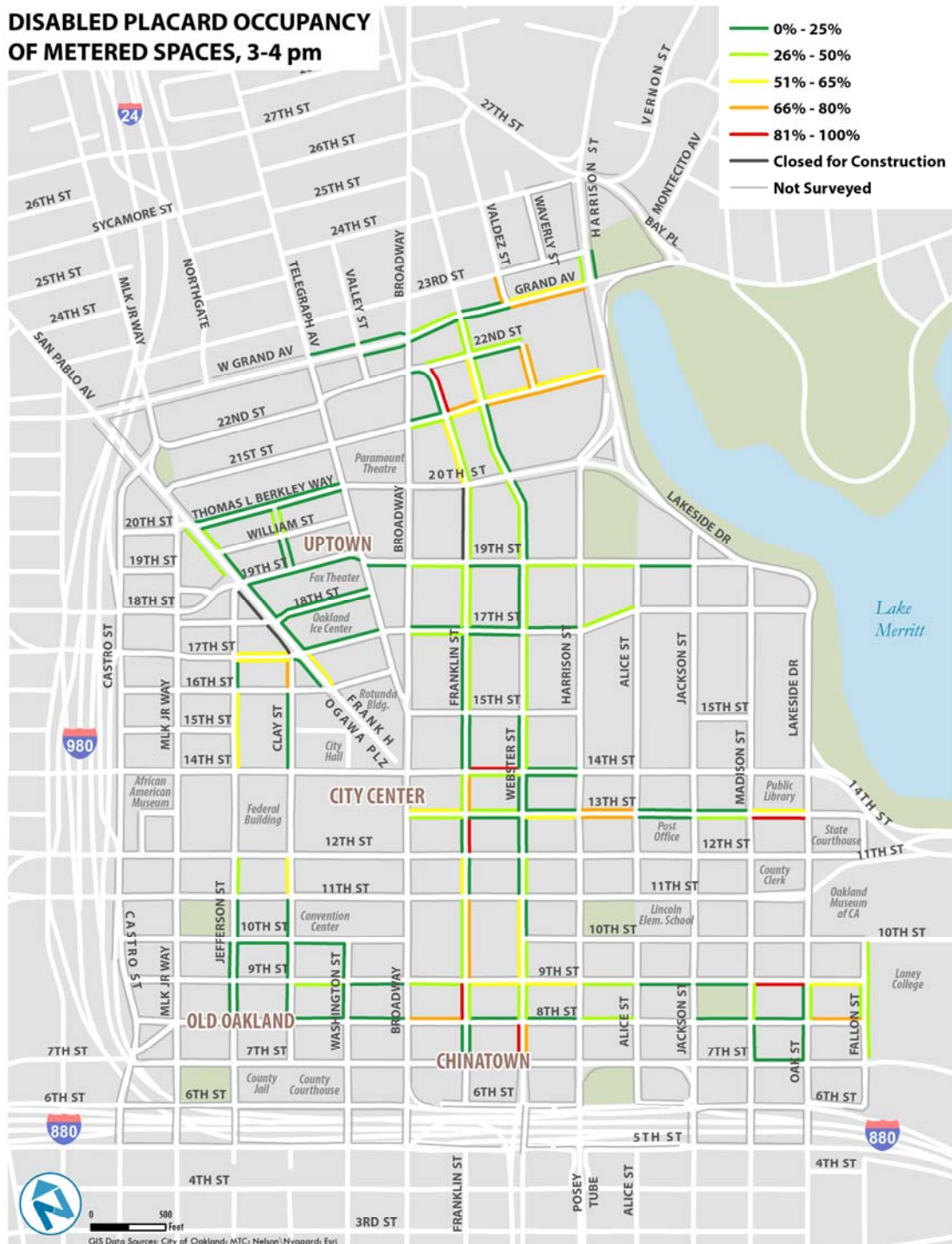
City of Oakland/Metropolitan Transportation Commission

Figure A-60 Disabled Placard Parking Occupancy of Metered Spaces, 2 p.m. to 3 p.m.



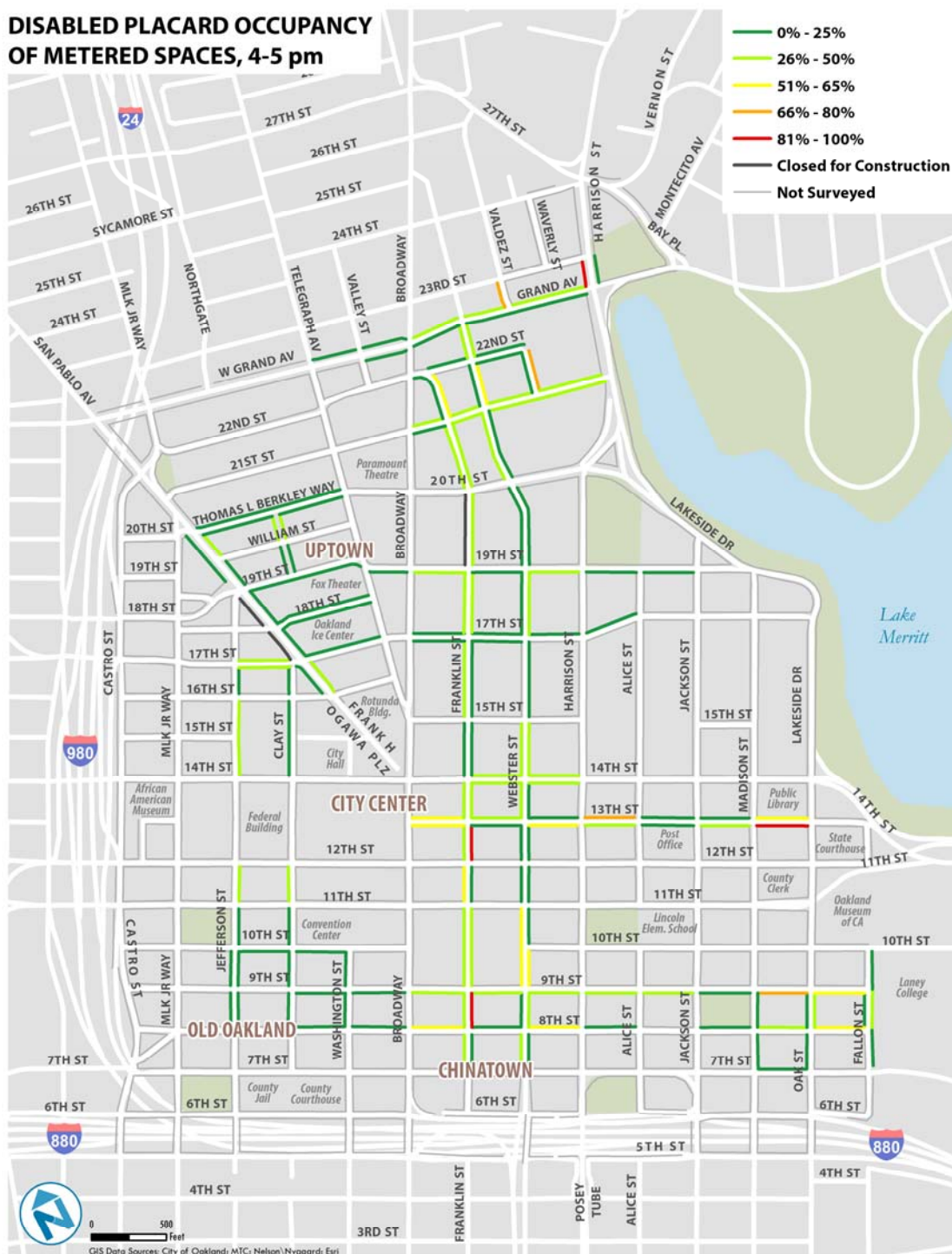
DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-61 Disabled Placard Parking Occupancy of Metered Spaces, 3 p.m. to 4 p.m.



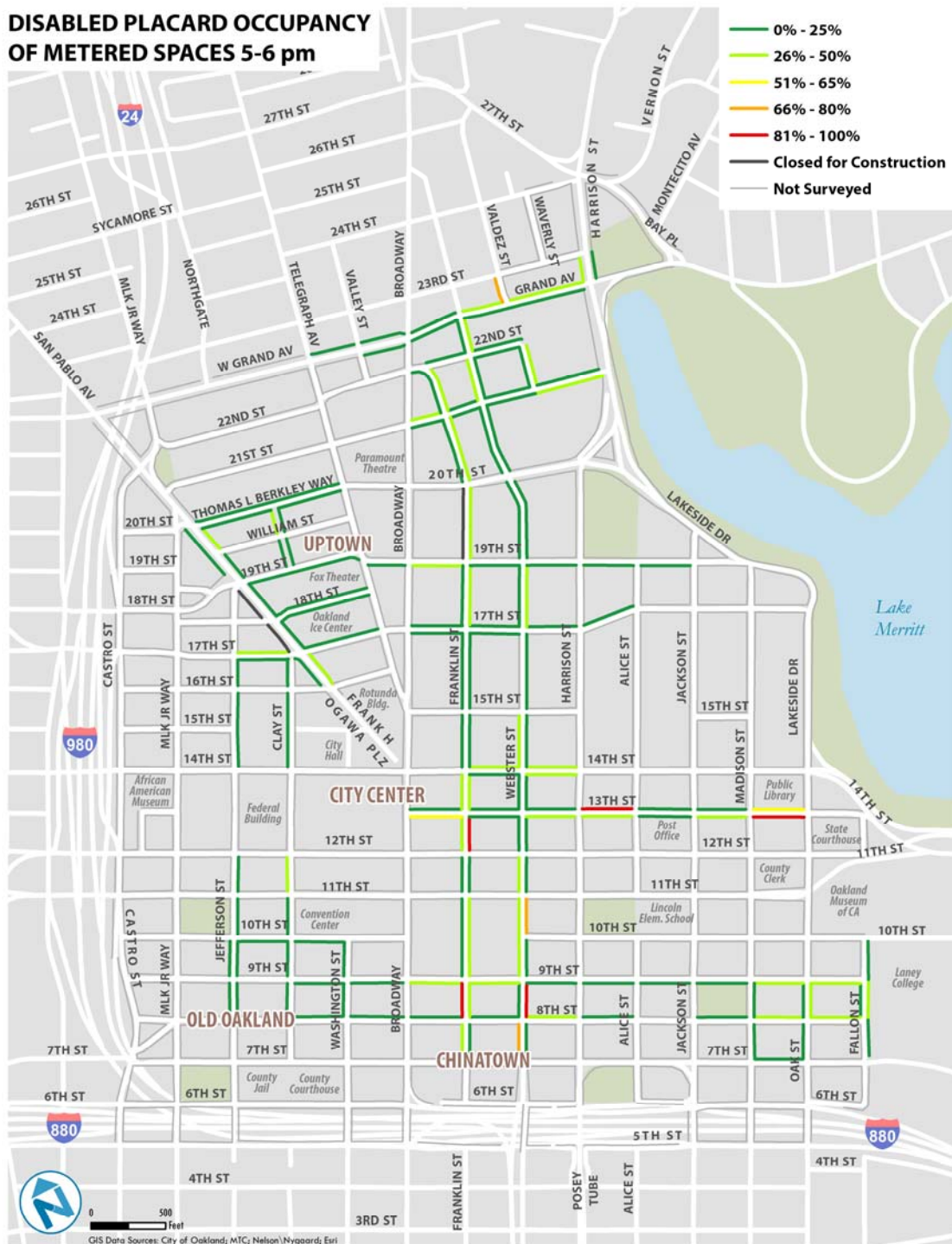
City of Oakland/Metropolitan Transportation Commission

Figure A-62 Disabled Placard Parking Occupancy of Metered Spaces, 4 p.m. to 5 p.m.



DOWNTOWN OAKLAND PARKING STUDY | TM #2: EXISTING CONDITIONS – FINAL
City of Oakland/Metropolitan Transportation Commission

Figure A-63 Disabled Placard Parking Occupancy of Metered Spaces, 5 p.m. to 6 p.m.



Appendix B Pricing at City-Owned Off-Street Parking Facilities

This section includes excerpts from the City's FY 2015 *Master Fee Schedule*, pages F-4 through F-11, which establish prices at the City's public off-street lots and garages.



City of Oakland Master Fee Schedule

Effective July 1, 2015

FINANCE DEPARTMENT

FEE DESCRIPTION	FEE	UNIT
7 Penalty for Failure to Pay Excess Litter Fee Invoice beyond 120 Days from Due Date	50%	of Invoice
8 Interest, Inclusive of Penalties, for Delinquent Excess Litter Fees Due	1%	Per Month
9 Filing Fee for an Appeal for Litter Ordinance	67.50	Appeal
AA. RE-ISSUE OF W2 FORM	5.00	Form

PARKING MANAGEMENT

A. ON-STREET PARKING METER

1 General	2.00	Space/Hour
2 Flexible Parking Zone	0.50 - 3.00	Space/Hour

B. OFF-STREET PARKING FACILITIES

1 Franklin Parking Plaza

a. Basic Fees

1 Transient Parking (Automobiles) MAX	1.00	Space/ 20 Min
2 Daily Maximum (Automobiles) MAX	18.00	Space/ Day
3 Monthly Parking (Reserved) MAX	200.00	Space/ Month
4 Monthly Parking (Unreserved) MAX	150.00	Space/ Month
5 Early Bird: in by 9:30 am MAX	10.00	Space / Day
6 Flat Rate After 4:00 pm to Closing Time MAX	4.00	Space
7 Overnight Parking (Close to Open) Max	4.00	Space
8 Motorcycles Monthly Parking (Unreserved) MAX	75.00	Monthly
9 Bicycles	Free	

b. Special Fees

1 Lost Ticket MAX	20.00	Ticket
2 Monthly Access Card Set up and Purchase MAX	15.00	Card
3 Replacement Card MAX	15.00	Card
4 Penalty for Monthly Parking Paid After the 7th of the Month MAX	15.00	Card
5 Special Event Parking MAX	20.00	Space / Day

2 Clay Street Garage

a. Basic Fees

1 Transient Parking (Automobiles) MAX	1.00	Space / 15 Min
2 Daily (Automobiles) MAX	15.00	Space / Day
3 Monthly Parking (Reserved) MAX	180.00	Space / Month
4 Monthly Parking (Unreserved) MAX	160.00	Space / Month
5 Motorcycles Monthly (Unreserved) MAX	80.00	Space / Month



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FEE DESCRIPTION	FEE	UNIT
6 Evening Rate After 4:00 pm to Closing Time MAX	6.00	Space
7 Overnight Parking (Close to Open) MAX	4.00	Space / Night
8 Bicycles	Free	
b. Special Fees		
1 Lost Ticket MAX	20.00	Ticket
2 Monthly Access Card Set up and Purchase MAX	15.00	Card
3 Replacement Card MAX	15.00	Card
4 Penalty for Monthly Parking Paid After the 7th of the Month MAX	15.00	Card
5 Special Event Parking MAX	20.00	Space / Day
3 Pacific Renaissance Plaza Garage		
a. Basic Fees		
1 Transient Parking (Automobiles) MAX	1.00	Space / 30 Min
2 Daily (Automobiles) MAX	24.00	Space / Day
3 Monthly Parking (Unreserved) Monday through	170.00	Space / Month
4 Monthly Parking, Monday through Friday MAX	155.00	Space / Month
5 Evening Rate After 5:00 pm to Closing Time MAX	5.00	Space
6 Overnight Parking (Close to Open) MAX	6.00	Space / Night
7 Special Event MAX	20.00	Space
8 Bicycles	Free	
b. Special Fees		
1 Lost Ticket MAX	30.00	Ticket
2 Monthly Access Card Set up and Purchase MAX	15.00	Card
3 Replacement Card MAX	15.00	Card
4 Penalty for Monthly Parking Paid After the 7th of the Month MAX	15.00	Card
4 Dalziel Garage		
a. Basic Fees		
1 Transient Parking (Automobiles) MAX	1.00	Space / 15 Min
2 Daily (Automobiles) MAX	15.00	Space / Day
3 Overnight Parking (Close to Open) MAX	4.00	Night
4 Monthly Parking (Reserved) MAX	180.00	Space / Month
5 Monthly Parking (Unreserved) MAX	160.00	Space / Month
6 Motorcycle Monthly Parking (Unreserved) Max	80.00	Monthly
7 Bicycles	Free	
b. Special Fees		
1 Lost Ticket MAX	20.00	Ticket



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FEE DESCRIPTION	FEE	UNIT
2 Monthly Access Card Set up and Purchase MAX	15.00	Card
3 Replacement Card MAX	15.00	Card
4 Penalty for Monthly Parking Paid After the 7th of the Month MAX	15.00	Card
5 Special Event Parking MAX	20.00	Space
5 Medical Hill Site No. 1 (Medical Hill Garage)		
a. Basic Fees		
1 Transient Parking (Automobiles) MAX	2.00	Space / 20 Min
2 Daily Maximum (Automobiles)	18.00	Space / Day
3 Monthly Parking (Unreserved) MAX	115.00	Space / Month
4 Motorcycles	3.00	Space / Day
5 Bicycles	Free	
b. Special Fees		
1 Lost Ticket MAX	20.00	Card
2 Monthly Access Card Set up and Purchase MAX	15.00	Card
3 Replacement Card MAX	15.00	Card
4 Penalty for Monthly Parking Paid After the 7th of the Month MAX	15.00	Space
5 Special Event Parking MAX	20.00	Space
6 Telegraph Parking Plaza		
a. Basic Fees		
1 Transient Parking (Automobiles) MAX	1.00	Space / 20 Min
2 Daily Maximum (Automobiles) MAX	12.00	Space / Day
3 Monthly Parking (Unreserved) MAX	125.00	Space / Month
4 Motorcycle Monthly Parking (Unreserved) Max	65.00	Space / Month
5 Early Bird in by 9:30 am MAX	8.00	Space / Day
6 Flat Rate After 4:00 pm till Closing Time MAX	4.00	Space
7 Overnight Parking (Close to Open) MAX	4.00	Space / Night
b. Special Fees		
1 Lost Ticket MAX	17.00	Ticket
2 Monthly Access Card Set up and Purchase MAX	15.00	Card
3 Replacement Card MAX	15.00	Card
4 Penalty for Monthly Parking Paid After the 7th of the Month MAX	15.00	Card
5 Special Event Parking MAX	20.00	Space
7 1200 Harrison Frank Mar Garage		
a. Basic Fees		
1 Transient Parking (Automobiles) MAX	1.00	Space / 20 Min



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FEE DESCRIPTION	FEE	UNIT
2 Daily Maximum (Automobiles) MAX	12.00	Space / Day
3 Monthly Parking (Reserved) MAX	140.00	Space / Month
4 Monthly Parking (Unreserved) MAX	120.00	Space / Month
5 Early Bird in by 9:30 am MAX	8.00	Space / Day
6 Flat Rate After 4:00 pm till Closing Time MAX	4.00	Space
7 Overnight Parking (Close to Open) MAX	4.00	Space / Night
8 Bicycles	Free	
b. Special Fees		
1 Lost Ticket MAX	17.00	Ticket
2 Monthly Access Card Set up and Purchase MAX	15.00	Card
3 Replacement Card MAX	15.00	Card
4 Penalty for Monthly Parking Paid After the 7th of the Month MAX	15.00	Card
5 Special Event Parking MAX	20.00	Space
8 Montclair Parking Garage		
a. Basic Fees		
1 Hourly Parking (Automobiles) MAX	2.00	Space / Hour
2 Daily Maximum (Automobiles) MAX	10.00	Space / Day
3 Overnight Parking (Close to Open) MAX	4.00	Space / Night
4 Monthly Parking (Unreserved) MAX	100.00	Space / Month
5 Motorcycles Daily MAX	4.00	Space
6 Bicycles	Free	
7 Early Bird in by 9:30 am MAX	8.00	Space / Day
8 Evening after 4:00 pm till Closing Time MAX	4.00	Space / Day
b. Validations Tickets (Max. 2 hrs per ticket)	100.00	Book of 100
c. Special Fees		
1 Lost Ticket MAX	12.00	Ticket
2 Monthly Access Card Set up and Purchase MAX	15.00	Card
3 Replacement Card MAX	15.00	Card
4 Penalty for Monthly Parking Paid After the 7th of the Month MAX	15.00	Card
5 Special Event Parking MAX	20.00	Space
9 UCOP Garage		
a. Basic Fees		
1 Transient Parking (Automobiles) MAX	1.00	Space / 20 Min
2 Daily (Automobiles) MAX	14.00	Space / Day
3 Monthly Parking (Unreserved) MAX	145.00	Space / Month



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FEE DESCRIPTION	FEE	UNIT
4 Monthly Motorcycle Parking (Unreserved) MAX	90.00	Space / Month
5 Early Bird in by 9:30 am MAX	11.00	Space / Day
6 Overnight Parking (Close to Open) MAX	4.00	Space / Night
7 Bicycles	Free	
b. Special Fees		
1 Lost Ticket MAX	20.00	Ticket
2 Monthly Access Card Set up and Purchase MAX	15.00	Card
3 Replacement Card MAX	15.00	Card
4 Penalty for Monthly Parking Paid After the 7th of the Month MAX	15.00	Card
5 Special Event Parking MAX	20.00	Space
10 City Center West Garage		
a. Basic Fees		
1 Transient Parking (Automobiles) MAX	2.00	Space / 30 Min
2 Daily (Automobiles) MAX	40.00	Space / Day
3 Monthly Parking (Reserved) MAX	250.00	Space / Month
4 Monthly Parking (Unreserved) MAX	195.00	Space / Month
5 Monthly Motorcycle Parking (Unreserved) MAX	90.00	Space / Month
6 Early Bird in by 9:30 am MAX	12.00	Space / Day
7 Overnight Parking (Close to Open) MAX	6.00	Space / Night
8 Bicycles	Free	
b. Special Fees		
1 Lost Ticket MAX	40.00	Ticket
2 Monthly Access Card Set up and Purchase MAX	15.00	Card
3 Replacement Card MAX	15.00	Card
4 Penalty for Monthly Parking Paid After the 7th of the Month MAX	15.00	Card
5 Special Event Parking MAX	20.00	Space
11 Franklin 88 Garage		
a. Basic Fees		
1 Transit Parking (Automobiles) MAX	1.50	Space / 30 Min
2 Daily (Automobiles) MAX	14.00	Space / Day
3 Early bird in before 9:30 am (Automobiles) MAX	10.00	Space / Day
4 Evening rate after 5:00 pm (Automobiles) MAX	6.00	Space / Night
5 Overnight Parking (Close to Open) MAX	4.00	Space / Night
6 Monthly Parking (Unreserved) MAX	175.00	Space / Month
7 Monthly Parking Mon-Fri (Unreserved) MAX	155.00	Space / Month



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FEE DESCRIPTION	FEE	UNIT
8 Monthly Parking Tandem (Unreserved) MAX	262.50	Space / Month
9 Motorcycles Daily MAX	7.00	Space / Day
10 Bicycles	Free	
b. Special Fees		
1 Lost Ticket MAX	20.00	Ticket
2 Monthly Access Card Set up and Purchase MAX	15.00	Card
3 Replacement Card MAX	15.00	Card
4 Penalty for Monthly Parking Paid After the 7th of the Month MAX	15.00	Card
5 Special Event Parking MAX	20.00	Space
12 Grand Avenue District Municipal Parking Lot as Described by Section 23.08 of Oakland City Council Resolution No.1989 C.M.S. Hourly Parking (Automobiles)		
a. Hourly Parking MAX	2.00	Space / Hour
b. Monthly Parking MAX	80.00	Space / Month
c. Special Event MAX	10.00	Space
d. Replacement of Monthly Parking Tag	80.00	Tag
13 Parkway District Municipal Parking Lot as Described by Section 23.09 of Oakland City Council Resolution No.1989 C.M.S.		
a. Hourly Parking MAX	2.00	Space / Hour
b. Monthly Parking MAX	80.00	Space / Month
c. Special Event MAX	10.00	Space
d. Replacement of Monthly Parking Tag	80.00	Tag
14 Piedmont Avenue Municipal Parking Lot as Described by Section 23.05 of Oakland City Council Resolution No. 1987 C.M.S.		
a. Hourly Parking MAX	2.00	Space / Hour
b. Monthly Parking MAX	80.00	Space / Month
c. Special Event MAX	10.00	Space
d. Replacement for Monthly Parking Tag	80.00	Tag
15 Scout Road & Mountain Blvd. Parking Lot		
a. Monthly Parking (Unreserved) MAX	80.00	Space / Month
b. Special Event MAX	10.00	Space
c. Replacement for Monthly Parking Tag	80.00	Tag
16 Lake Park Avenue Parking Lot		
a. Hourly Parking MAX	2.00	Space / Hour
b. Monthly Parking MAX	80.00	Space / Month



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FEE DESCRIPTION	FEE	UNIT
c. Special Event MAX	10.00	Space
d. Replacement for Monthly Parking Tag	80.00	Tag
17 Henry J Kaiser Convention Center Parking Lot		
a. Hourly Parking MAX	2.00	Space / Hour
b. Monthly Parking MAX	90.00	Space / Month
c. Special Event MAX	10.00	Space
d. Replacement of Monthly Parking Tag	80.00	Tag
18 Lakeshore Ave Parking Garage		
a. Monthly Parking, Mon-Fri (Automobile) Max	120.00	Space / Month
b. Replacement for Monthly Parking Tag	120.00	Tag
19 1800 San Pablo Lot		
a. Hourly Parking MAX	3.00	Space / Hour
b. Monthly Parking MAX	125.00	Space / Month
c. Special Event MAX	15.00	Space
d. Replacement of Monthly Parking Tag	150.00	Tag
20 Webster-Valdez (Labor Temple) Lot		
a. Hourly Parking MAX	2.00	Space / Hour
b. Daily Minimum MAX	10.00	Space / Day
c. Early bird special before 9:30 am MAX	8.00	Space / Day
d. Monthly Parking MAX	150.00	Space / Month
e. Special Event MAX	10.00	Space
21 Damage to Parking Facility	Actual Cost	
22 After Hours Access to Parking Facility MAX	25.00	Occurrence
23 Validation Terminal Deposit MAX	800.00	
24 Validation Terminal Programming Fee MAX	100.00	
C. ON-STREET PARKING METER		
1 Registered Vanpools (11 or More Passengers), Maximum of 40 Spaces	10.00	Space / Month
D. RESIDENTIAL PERMIT PARKING FEE		
Permit for Eligible Vehicles of Residents		
1 Registered Permit Address		
a. Annual	82.00	Per Lic. Plate
b. Renewal	59.00	Per Lic. Plate
c. Prorated (less than six months)	57.00	Per Lic. Plate
2 Permit for Eligible Vehicles of Owners or Employees of Businesses with the Vehicle Not Registered at the Business Address		
a. Annual	96.00	Per Lic. Plate



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FEE DESCRIPTION	FEE	UNIT
b. Renewal	96.00	Per Lic. Plate
c. Prorated (less than six months & Except Area M)	67.00	Per Lic. Plate
3 Area M Permits		
a. Annual	160.00	Per Lic. Plate
b. Renewal	160.00	Per Lic. Plate
c. Prorated (less than six months)	115.00	Per Lic. Plate
d. One-day	10.00	Per Permit
e. 14-day	50.00	Per Permit
3 Replacement of Lost or Damaged Permit	10.00	Per Lic. Plate
4 Visitor		
a. One-day	9.00	Per Permit
b. 14-day	25.00	Per Permit
E. SPECIAL COST OF COLLECTING PARKING VIOLATION PENALTIES	30%	Ticket Value
F. VEHICLE IMMOBILIZER "BOOT"		
1 Daily Fee for Unreturned Paylock Book	25.00	Per Day
2 Boot Replacement Fee	500.00	Per Boot
3 Damaged Boot Fee	250.00	Per Boot
4 Parking Boot Administration Fee	25.00	Per Boot
5 Vehicle Immobilizer Removal Fee	140.00	Per Boot
G. CHAPTER 8.44.040, SECURITY FOR EVENTS AT THE OAKLAND ALAMEDA COUNTY COLISEUM COMPLEX		
G. 1 - No person shall park or stand a vehicle in more than one parking space. If the vehicle exceeds twenty (20) feet in length, the driver thereof shall park said vehicle in parking spaces for standard size vehicles and pay for the additional space and display evidence of such payment. O.M.C 8.44.040. G1	58.00	Per Citation
G. 1 - No person shall utilize in any manner more than the parking space that his or her vehicle is entitled to occupy under the provisions of this chapter. Roadways and fire lanes shall remain clear of vehicles and objects to maximize use for traffic circulation. O.M.C. 8.44.040. G2	58.00	Per Citation
H. ADMINISTRATIVE FEE IN LIEU OF FINE FOR NON-DISPLAYED DISABLED PLACARD (DP) -The \$25.00 processing fee for cancellation of a citation for non-display of DP will be available only as a one-time courtesy to the registered owner of the cited vehicle with a valid DP.	25.00	Per Violation