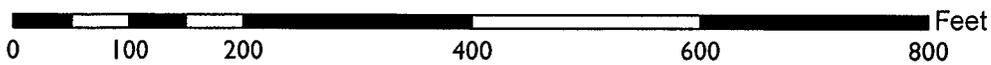
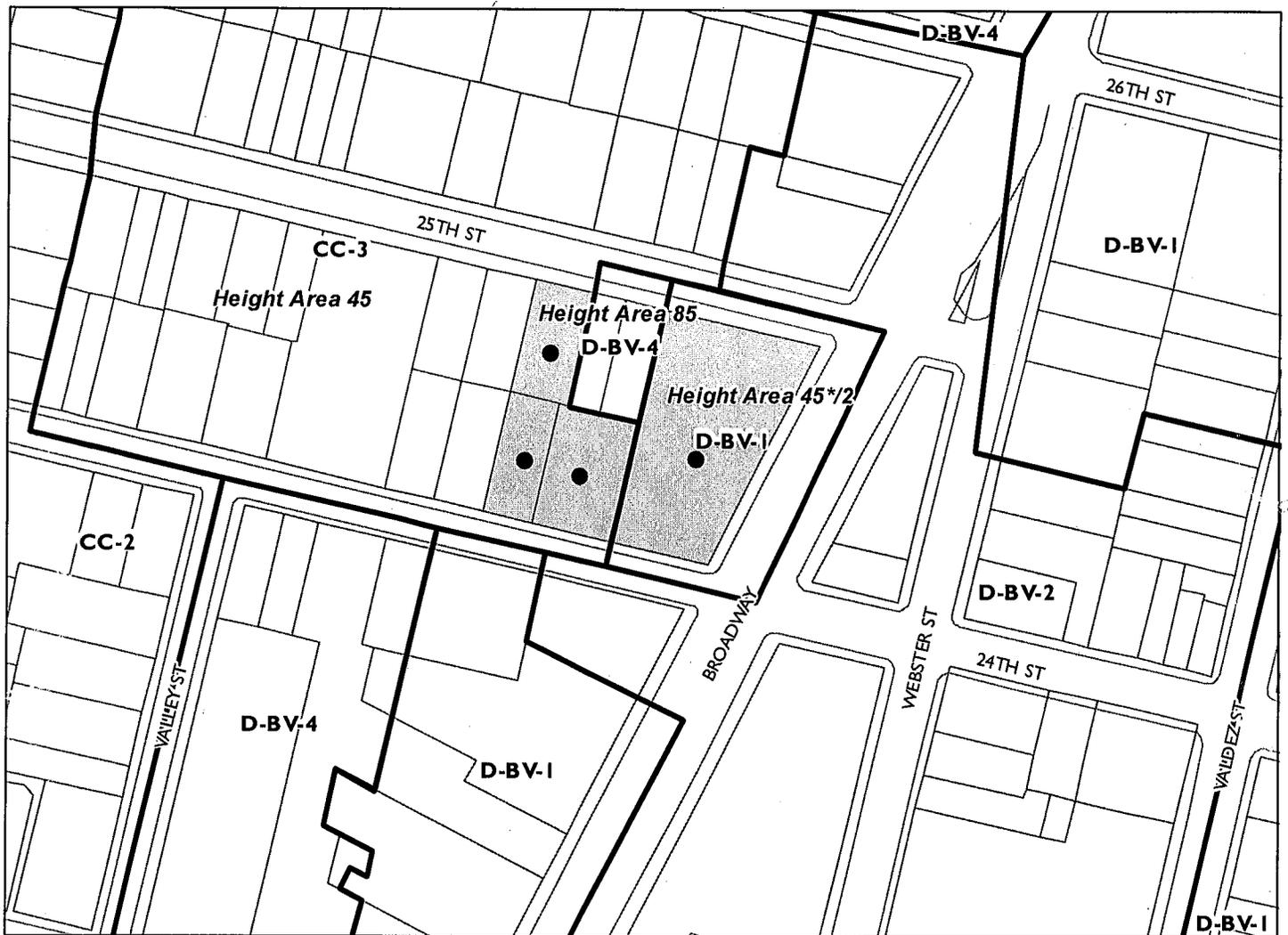


Location:	2401 Broadway (See map on reverse)
Assessors Parcel Numbers:	008-0674-003-01; -004-00; -005-00; & -036-00
Proposal:	Proposal to construct a new six story mixed use building containing 72 dwelling units, a 159-room hotel and approximately 17,000 square feet of ground floor retail. The project site is Retail Priority Site 2 in the Broadway Valdez District Specific Plan.
Applicant:	Signature Land Advisors Inc. Jamie Choy, (510) 251-9276
Owner:	2401 Broadway Investors LLC
Planning Permits Required:	Regular Design Review for new construction and to allow a D-BV height boundary shift of 30 feet into adjacent CC Zone; Major Conditional Use Permits to allow D-BV-1 Zone Bonuses, a Transient Habitation Commercial Activity, an Alcohol Beverage Sales Activity, expansion of D-BV uses into adjacent CC Zone, and shared parking; and Tentative Parcel Map for lot merger and new condominiums.
General Plan:	Central Business District
Zoning:	D-BV-1 CC-3
Environmental Determination:	A detailed CEQA Analysis was prepared for this project which concluded that the proposed project satisfies each of the following CEQA provisions: 15183 - Projects consistent with a community plan, general plan, or zoning; 15183.3 – Streamlining for in-fill projects; and/or 15164 – Addendum to the 2014 certified Broadway Valdez District Specific Plan EIR; Each of which provides a separate and independent basis for CEQA compliance.
Historic Status:	Potentially Designated Historic Property (PDHP), 2401 Broadway – Eb-1* (API Non-contributor) 437 25 th Street – C1+ (API Contributor)
City Council District:	3
Action to be Taken:	Decision on Application
Staff Recommendation:	Approve with the attached conditions.
Finality of Decision:	Appealable to City Council within 10 days
For Further Information:	Contact case planner Peterson Z. Vollmann at 510-238-6167 or by e-mail at pvollmann@oaklandnet.com .

CITY OF OAKLAND PLANNING COMMISSION



Case File: PLN16246

Applicant: Signature Land Advisors Inc. – Jamie Choy

Address: 2401 Broadway

Zone: D-BV-1; CC-3

SUMMARY

The applicant Signature Land Advisors has filed an application with the Bureau of Planning to develop a mixed-use project at 2401 Broadway that would consist of a new six story building containing approximately 17,000 square feet of retail at the ground floor, a 159-room hotel and 72 dwelling units. The project site is located within the Broadway Valdez District Specific Plan (BVDSF) area and is Retail Priority Site 2 in the plan.

On August 9, 2017, the proposal appeared before the Design Review Committee, during which the Committee recommended the item move forward to the full Planning Commission for consideration.

Staff recommends approval, subject to the attached findings and conditions of approval.

PROPERTY DESCRIPTION

The subject property consists of four lots consisting of approximately 1.21 acres on the western side of Broadway between 24th and 25th Streets. The site includes the entire Broadway street frontage of the western side of the block, which includes the building at 2401 Broadway and the vacant parking lot to the north. The site also includes a vacant lot to the west of 2401 Broadway on 24th Street and a property on 25th Street that includes the existing brick auto garage building at 437 25th Street.

The project site is located partially within the 25th Street Garage District API and includes one vacant lot within the district, a contributing building at 437 25th Street and a non-contributing building at 2401 Broadway.

PROJECT DESCRIPTION

The proposed project includes a three to six story mixed use building that will include 72 residential dwelling units and a new 159 room hotel along with ground floor retail and a new bar located at 437 25th Street. The proposal would be approximately 85 feet in height along the Broadway frontage and then step down to three stories as it backs into the 25th Street Garage District. The Broadway street frontage would be occupied with the ground floor retail as well as the hotel and residential lobbies. Parking will be provided in the rear portion of the ground floor with access off 24th and 25th Streets.

The proposal would include the demolition of 2401 Broadway while retaining and restoring the façade of the building to incorporate it into the new project. Alterations will also be made to 437 25th Street that will include an addition to the upper rear of the building for a new bar and lower level parking that will tie the building into the development project.

GENERAL PLAN ANALYSIS

The General Plan's Land Use and Transportation Element (LUTE) classifies the project site as primarily being located in the Central Business District (CBD) General Plan area. This land use classification is intended to encourage, support, and enhance the downtown area as a high density mixed use urban center of regional importance and a primary hub for business, communications, office, government, high technology, retail, entertainment, community facilities, and visitor uses. The CBD classification includes a mix of large-scale offices, commercial, urban high rise residential, institutional, open-space, cultural, educational, arts, entertainment, service, community facilities, and visitor uses. The rear portion of the site is also located within the Community Commercial General Plan land use area. This land use classification is intended to create, maintain, and enhance areas suitable for a wide variety of commercial and institutional operations along the City's major corridors and in shopping districts and centers.

Among the General Plan Land Use and Transportation policies and objectives applicable to the proposed Project, and which the Project conforms with, are the following:

Policy D2.1 - Enhancing the Downtown – Downtown development should be visually interesting, harmonize with its surroundings, respect and enhance important views in and of the downtown, respect the character, history and pedestrian orientation of the downtown, and contribute to an attractive skyline.

Policy D10.1 – Encouraging Housing – Housing in the downtown should be encouraged as a vital component of a 24-hour community.

Policy D10.2 – Locating Housing – Housing in the downtown should be encouraged in identifiable districts, within walking distance of the 12th Street, 19th Street, City Center, and Lake Merritt BART stations to encourage transit use, and in other locations where compatible with surrounding uses.

Policy N1.7 - Locating Hotels and Motels – Hotels and motels should be encouraged to locate downtown, along the waterfront, near the airport, or along the I-880 corridor.

Policy N3.1 – Facilitating Housing Construction – Facilitating the construction of housing units should be considered a high priority for the City of Oakland.

Policy N3.2 – Encourage In-fill Development – In order to facilitate the construction of needed housing units, in-fill development that is consistent with the General Plan should take place throughout the City of Oakland.

The proposed Project is consistent/conforms with the above referenced policies and objectives and the general intent of the Central Business District and Community Commercial land use designations by constructing a new mixed use building containing a new downtown hotel, viable ground floor retail and dense housing units on a major commercial street and transit corridor within walking distance to the 19th Street BART station.

Broadway Valdez District Specific Plan

The BVDSPP provides a vision and planning framework for future growth and development in the approximately 95-acre area along Oakland's Broadway corridor between Grand Avenue and I-580. The Specific Plan, which has been developed with a thorough analysis of the area's economic and environmental conditions and input from City decision-makers, landowners, developers, real estate experts, and the community at large, provides a comprehensive vision for the Plan Area along with goals, policies, and development regulations to guide future public and private actions relating to the area's development. The Plan also serves as the mechanism for insuring that future development will be coordinated and occur in an orderly and well-planned manner.

Among the Specific Plan goals and policies applicable to the proposed Project, and which the Project conforms with, are the following:

BVDSP Goal LU-1—A destination retail district that addresses the City's deficiency in comparison goods shopping and significantly reduces sales tax leakage.

BVDSP Goal LU-2 - A "complete" mixed-use neighborhood that is economically and socially sustainable—providing an appealing mix of retail, dining and entertainment uses as well as quality jobs and diverse housing opportunities.

BVDSP-Policy LU-1.1—Prioritize development and tenanting of comparison goods retailers in the Broadway Valdez District.

BVDSP-Policy LU-1.2—Enhance the identity and function of the Broadway Valdez District as a retail destination for Oakland and the East Bay.

BVDSP-Policy LU-1.3—Balance retail uses with a mix of residential, office, and service uses that complement and support the economic viability of the commercial core, and contribute to the creation of a new "24-hour" neighborhood with around-the-clock vitality.

BVDSP-Policy LU-2.1 – Establish the Broadway Valdez District as an attractive pedestrian and transit oriented, mixed use neighborhood with a core of retail and complementary commercial uses.

BVDSP-Policy LU-4.1—Encourage the gradual transition of the Plan Area toward uses that will contribute to the creation of a vibrant, pedestrian-oriented, mixed-use district.

BVDSP Policy LU-4.3 - Encourage infill development along Broadway that will improve the corridor's economic vitality, enhance the definition and character of the corridor, and create better pedestrian scale and orientation.

BVDSP-Policy LU-8.3—Broadway, Valdez Street, 24th Street, 23rd Street, and 27th Street will be the primary shopping streets that give structure to the retail district and physically integrate the

Triangle with adjacent areas by providing active retail frontages and pedestrian-friendly streetscapes that extend along both sides of these key streets.

BVDSP-Policy LU-9.2—The intent is to promote a complementary mix of retail, office, entertainment, and residential uses that creates a vibrant urban corridor that is active both day and night, and on weekdays and weekends.

BVDSP Policy LU-9.6 - Emphasis is placed on the renovation and repurposing of historic garage and auto showroom buildings primarily along Broadway to preserve a link to the corridor's past and enrich its character.

BVDSP Policy CD-3.10 - Ensure that development on the Retail Priority Site on the west side of Broadway between 24th and 25th streets creates an active, ground-level facade that supports pedestrian activity and further contributes to the creation of a continuous retail frontage along Broadway.

The Project is consistent/conforms with the above mentioned goals and policies by creating a new, mixed use development located at a retail priority site of the Plan Area. The proposal will contain an active ground floor commercial presence with more than 17,000 square feet of new retail space that will promote a vibrant, pedestrian-oriented environment for Broadway as well as a new hotel use to bring visitors to the area. The proposal will retain and restore the existing façade of 2401 Broadway to provide a connection to the past while repurposing it from an auto use to an active retail use. The Project also will create high density, upper level residential uses that will be in close proximity to transit access and help to create a 24-hour neighborhood.

ZONING ANALYSIS

The subject property is located within the D-BV-1, Broadway Valdez District Retail Priority Sites Commercial Zone, and is within a 45' Height/Intensity Area. The intent of the D-BV-1 zone is to establish Retail Priority Sites in the Plan area in order to encourage a core of comparison goods retail with a combination of small, medium, and large scale retail stores. Priority Sites 3 and 5 are further divided into subareas A, B, and C and Priority Site 4 into subareas A and B, as shown in the Height Area Map. Each Retail Priority Site and subarea has a specified minimum square footage of retail required prior to residential or transient habitation activities and facilities being permitted.

Density & Height Bonuses

The Project site is located within Priority Site 2, which calls for a minimum retail of 15,572 square feet prior to the allowance of any residential units or height bonus above 45 feet. A proposal with 15,572 square feet (50% of the site area) of retail may permit a maximum of one dwelling unit per 125 square feet of retail provided, which would allow for 124 units. A proposal that includes retail square footage of 18,686 or more (60% of the site area) would allow for dwelling units at a ratio of one dwelling per 100 square feet of retail provided. The proposed project includes 17,439 square feet of retail, which would more than allow for the proposed 72

dwelling units. Meeting the minimum retail square footage requirement also allows an increase in height above the 45-foot height limit to 200 feet. The proposed project would not exceed a height of 85 feet. The granting of the D-BV-1 Zone bonuses is allowed through a Conditional Use Permit process, which in this instance would be considered by the Planning Commission.

Density and Height Bonus			
Bonus	Permitted	50% Retail Bonus	Proposed
Height	45 feet	200 feet	85 feet
Dwelling Units	0	139 units	72 units

Extension of D-BV-1 Zoning into CC-3 Zone

The Planning Code includes provisions to allow regulations of one zone to encroach into adjacent zones when part of a new development proposal. Pursuant to Section 17.154.060C of the Planning Code the height limit within one boundary may extend up to 30 feet into an adjacent zone with a lower height limit as part of the Design Review process. The proposed project would include use of this provision to extend the 85-foot portion of the building allowed in the D-BV-1 Zone back to the lower 45-foot height limit in the CC-3 Zone to the west.

Planning Code Section 17.102.110 allows uses permitted or conditionally permitted in one zone to encroach up to 150 feet into an adjacent zone where such uses are otherwise prohibited by the base zoning. The applicant has requested a Conditional Use Permit to utilize this provision to allow the Transient Habitation (Hotel) Activity to encroach into the CC-3 Zoned portion of the site.

Transient Habitation Commercial (Hotel)

In addition to the residential and retail components of the project, the largest component is the proposed hotel operation that would include a large ground floor lobby, related restaurant and bar and mezzanine level meeting rooms as well as 159 rooms on the upper floors. The identified operator is West Elm Hospitality, which initially a successful lifestyle furniture retailer, is expanding as a new hotel operator that is beginning with the opening of six locations around the US including this proposed location in Oakland. The proposed hotel activity requires the granting of a Conditional Use Permit. Staff feels that the proposed hotel meets the required findings for a hotel operation and the location is ideal as it is both downtown as well as within the growing Broadway Valdez retail district, and is direct proximity to the arts district that brings many locals and tourists to Oakland and this neighborhood in particular.

Alcohol Sales Commercial (Bar)

The proposed development project includes a new bar that would be located at the back side of the development site with street access from 25th Street. The use would be within an addition to the existing building at 437 25th Street and have an internal connection to the larger overall development, specifically from the proposed hotel. The Planning Code requires a Major

Conditional Use Permit approval for any new Alcohol Beverage Sales Activities. Staff feels that the proposal is consistent with the required findings and is appropriate in this location as it will function as a bar for the hotel and its patrons with an internal connection as well as have direct access off 25th Street, which is a part of the growing arts and entertainment area.

Parking

Auto and Bike parking is required and proposed as set forth in the following tables below.

Auto Parking			
Use	Amount	Required Auto Parking	Stalls Provided
Residential	72 units	0.5: unit = 36 stalls	36
Ground Floor Commercial	40,357 sq.ft.	1: 600 sq.ft. = 68 stalls	93*
Upper Level Commercial	79,984 sq.ft.	1:1,000 sq.ft. = 80 stalls	
TOTAL		184	129*

* The applicant has requested a Conditional Use Permit to allow shared parking pursuant to Section 17.116.110.D.

Bike Parking Long Term			
Use	Amount	Required Bike Parking	Provided
Residential	72 units	1: 2 units = 36	36
Rest/Retail	27,169 sq.ft.	1:8,000 sq.ft. = 4	6
Hotel	159 rooms	1:20 rooms = 8	8
TOTAL		48	50

Bike Parking Short Term			
Use	Amount	Required Bike Parking	Provided
Residential	72 units	1:15 units = 5	*
Restaurant	12,553 sq.ft.	1:2,000 sq.ft. = 6	*
Retail	14,616 sq.ft.	1:3,000 sq.ft. = 5	*
Hotel	159 rooms	1:20 rooms = 8	*
TOTAL		24	*

* Short Term Commercial Bike Parking varies by use and can't exactly be determined until a tenant is identified. A high-end estimate is provided based upon the submittal. The proposal will be required to accommodate the short-term bike parking by installation of at least 12 bike racks in the sidewalk around the site.

Shared Parking

Pursuant to Planning Code Section 17.116.110.D an applicant of a development project that contains a mix of uses may request a Conditional Use Permit to reduce the required parking by 50% by developing a shared parking arrangement. The applicant proposes that the residents of the building would have access of up to 36 designated parking stalls as requested by tenants since the parking will be unbundled from any residential leases. The remaining 93 parking spaces will

be provided for the various retail visitors and hotel guests on a valet service basis where they would enter on 25th Street where their cars would be taken and parked within a mechanical parking system.

Design Review

The proposed project occupies the entire western block frontage of Broadway between 24th and 25th Streets. The design creates two building masses at the corners of Broadway and joins them by an accent mass in the center of the block, which also acts as a central focal point to the main hotel lobby. The building mass at the northern corner at 25th Street is entirely new construction and is made up of a tall ground floor retail base with the upper residential levels being clad in a brick veneer with alternating punched window patterns that results in a simplified base middle and top design that is anchored at the corner with the retail base a three-level window wall at the middle and a two-level window wall at the top. The southern corner at 24th Street will retain and restore the façade of the existing building at 2401 Broadway by recreating the arched storefronts to match those that remain along the back portion of the 24th Street façade. The upper hotel levels will be setback and contain a very simplified high quality design that reflects on the more industrial character of the 25th Street Garage District. The central mid-block building mass that will contain a similar architectural vocabulary as the southern massing while incorporating tall vertical glass sunscreens that will add a lot of visual interest to the building.

As the building extends down 24th Street the upper level design of the hotel portion will continue above the restored building façade at the ground floor and then step down to a four-story height that eventually joins the ground floor level providing for the garage entries to the building.

On 25th Street there is a vacant property that is not included in the development project and the project picks back up at the property at 437 25th Street. The building at 437 25th will be retained and an upper level addition will be included that joins the structure into the new development. Two bays of the ground floor of the building will be used as garage entries that connect to the parking structure for the building, which is typical of buildings in the 25th Street Garage District, and the third bay will be used as a new lobby entry to a proposed bar that will be located on the upper level of the building.

The design proposal has evolved since the initial submittal. At the time of the initial application submittal zoning staff and OCHS staff met with the applicant and provided a lot of input on the proposal. In response, the applicant restarted the design process and altered the entire design concept to address staff comments. Staff had provided comments on the overall design concept particularly focusing in on the design of the building as it related to the two structures within the 25th Street Garage District API. The requests were to simplify the design of the building above the restored storefront at 2401 Broadway and retain the structure at 437 25th Street and only incorporate an addition that is setback from the existing street wall (the original design would have only retained the façade of the building).

Design Review Committee

As previously mentioned, this item appeared before the Design Review Committee (DRC) on August 9, 2017. Comments from the public and commissioners were generally supportive but there were some recommendations that were made. At the meeting, public comments were raised about the way the restored façade abruptly ended and didn't return to the adjacent building wall, which the applicant has addressed. However, the parapet and cornice do not return to the building because they would conflict with glazing in the wall at the upper story.

Committee comments recommended that the applicant should look into bringing the brick façade on the new building down to the ground level as pilasters, which has been provided into a revised design option. Comments were also a recommendation that they may want to look at doing something more interesting at the corner of 25th & Broadway, but there wasn't a desire to hold the project up from proceeding. The applicant provided a few different alternatives for staff to review that enlarged the bay at the corner and or expanded the metal and window wall system. In staff's opinion none of the modifications expanding the form of the bay was more successful than the design presented to the DRC as all of them seemed over done for the location of the site. The applicant has opted for a redesigned corner bay that only includes the expansion of the metal panel and window wall system instead of breaking it up with the brick band in the middle as previously shown. This approach does help to differentiate the bay and pronounce it more as a stand out corner feature without being overdone and out of scale with the rest of the project.

Commissioners also had differing points of view on the overall façade program and how the three different elements of the Broadway façade come together. There were also differencing opinions on the color scheme being provided on the renovated façade of 2401 Broadway and the wall of the building beyond. The applicant has provided alternative color schemes in the plan set that show the variation that was being discussed. Staff still feels that the prior version with the light façade on the renovated façade with the darker faced on the new building rising behind is the superior design as it allow the historic façade to really stand out.

Demolition Findings

Pursuant to Planning Code Section 17.136.075.C, the proposed project is also subject to the Category II Demolition Findings for non-contributing buildings in an API. Staff feels that the proposed project is consistent with the required findings because while the building is technically being demolished all street facing elevations are being retained and restored and incorporated into the new project. In addition, the new building facades within the district utilize high quality materials with window patterns that are similar to those in the historic district but incorporated in a modern design for different uses than those of the auto garage uses of the historic district.

ENVIRONMENTAL DETERMINATION

The *Broadway Valdez District Specific Plan Environmental Impact Report* (EIR) analyzed the environmental impacts of adoption and implementation of the BVDSP and, where the level of

detail available was sufficient to adequately analyze the potential environmental effects, provided a project-level CEQA review for reasonably foreseeable development. This project-level analysis allows the use of CEQA streamlining and/or tiering provisions for projects developed under the BVDSP.

Applicable CEQA streamlining and/or tiering code sections are described below, each of which, separately and independently, provide a basis for CEQA compliance.

1. **Community Plan Exemption.** Public Resources Code Section 21083.3 and CEQA Guidelines Section 15183 allow streamlined environmental review for projects that are “consistent with the development density established by existing zoning, community plan or general plan policies for which an EIR was certified, except as might be necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site.” Section 15183(c) specifies that “if an impact is not peculiar to the parcel or to the proposed project, has been addressed as a significant effect in the prior EIR, or can be substantially mitigated by the imposition of uniformly applied development policies or standards..., then an EIR need not be prepared for the project solely on the basis of that impact.”
2. **Qualified Infill Exemption.** Public Resources Code Section 21094.5 and CEQA Guidelines Section 15183.3 allow streamlining for certain qualified infill projects by limiting the topics subject to review at the project level, if the effects of infill development have been addressed in a planning level decision, or by uniformly applying development policies or standards. Infill projects are eligible if they are located in an urban area on a site that either has been previously developed or that adjoins existing qualified urban uses on at least 75 percent of the site’s perimeter; satisfy the performance standards provided in CEQA Guidelines Appendix M; and are consistent with the general use designation, density, building intensity, and applicable policies specified for the project area in either a sustainable communities strategy or an alternative planning strategy. No additional environmental review is required if the infill project would not cause any new specific effects or more significant effects, or if uniformly applicable development policies or standards would substantially mitigate such effects.
3. **Addendum.** Public Resources Code Section 21166 and CEQA Guidelines Section 15164 state that an addendum to a certified EIR is allowed when minor changes or additions are necessary and none of the conditions for preparation of a subsequent EIR or Negative Declaration pursuant to Section 15162 are satisfied.

Note:

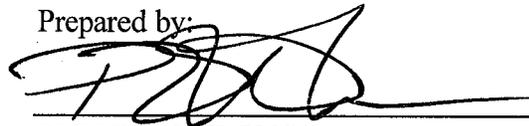
A detailed CEQA Analysis was prepared for the project and was provided under separate cover for review and consideration by the Planning Commission, and is available to the public at the Planning Department office at 250 Frank H. Ogawa Plaza, 2nd Floor, Oakland, CA 94612 and on the City’s website at: <http://www2.oaklandnet.com/Government/o/PBN/OurServices/Application/DOWD009157>

CONCLUSION

Staff believes that the proposed project is well designed and helps to implement the vision of the BVDSP by providing a large retail tenant space as well as other active commercial uses that will enhance the area as a destination for shopping as well as dining and entertainment uses. The proposal also retains and restores important historic resources. The proposed hotel along with the additional commercial activities mixed with the residential housing will help to make the area a 24/7 neighborhood.

- RECOMMENDATIONS:**
1. Affirm staff's environmental determination and adopt the attached CEQA Findings.
 2. Approve the Major Conditional Use Permits, Design Review, and Vesting Tentative Parcel Map subject to the attached findings and conditions.

Prepared by:



PETERSON Z. VOLLMANN
Planner IV

Reviewed by:



ROBERT MERKAMP
Development Projects Manager
Bureau of Planning

Approved for Forwarding to the
City Planning Commission:



DARIN RANELETTI
Deputy Director
Bureau of Planning

ATTACHMENTS:

- A. Findings for Approval
- B. Conditions of Approval
- C. SCA/MMRP from the 2401 Broadway CEQA Analysis Checklist
- D. Plans of the Project Site

ATTACHMENT A

FINDINGS FOR APPROVAL

This proposal meets all the required Design Review Criteria (Sections 17.136.050, 17.136.075 & 17.154.060), and Conditional Use Permit Criteria (Sections 17.134.050, 17.102.110, 17.103.030, 17.103.050, & 17.116.110) as set forth below and which are required to approve your application. This proposal does not contain characteristics that require denial pursuant to the Tentative Map Findings (Section 16.08.030) and is consistent with the Lot Design Standards (Section 16.24.040) of the Oakland Subdivision Regulations. Required findings are shown in **bold** type; reasons your proposal satisfies them are shown in normal type. (Note: The Project's conformance with the following findings is not limited to the discussion below, but is also included in all discussions in this report and elsewhere in the record).

17.136.050(A) - RESIDENTIAL DESIGN REVIEW CRITERIA:

- 1. The proposed design will create a building or set of buildings that are well related to the surrounding area in their setting, scale, bulk, height, materials, and textures.**

The proposed project is located on a Retail Priority Site of the Broadway Valdez Specific Plan that envisions a high density mixed use project. The proposed ground floor along Broadway provides for a double height retail space with a large footprint that can accommodate an anchor tenant in the area. The southern end of the site will restore and reincorporate the façade of the building at 2401 Broadway with a new building setback and rising beyond that will contain a simplified façade largely made of up industrial looking window sashes. The northern end of the site will use a brick façade that will be modern in design but its materiality is compatible with the many brick buildings within the 25th Street Garage District. The height of the building will be mostly up towards Broadway at approximately 85 feet which is consistent with the vision in the BVDSP and consistent with the heights of other buildings that are currently under construction. The building then steps down to a height of 45 feet as it enters the 25th Street garage district that is largely made up of buildings of about 20-30 feet in height.

- 2. The proposed design will protect, preserve, or enhance desirable neighborhood characteristics.**

The proposed design will enhance the desirable neighborhood characteristics by filling in a largely underdeveloped site with a new mixed use building with 17,000 square feet of new ground floor retail on a priority site within the Broadway Valdez Specific Plan, as well as provide for a dense residential environment in close proximity to downtown jobs, local and regional transit and open space. The proposal will also include a new hotel that will further enhance the area as a destination location.

3. The proposed design will be sensitive to the topography and landscape.

The subject site is relatively flat and void of landscape other than a few cypress trees that will be removed and replaced with a comprehensive street tree planting scheme consistent with the BVDSP.

4. If situated on a hill, the design and massing of the proposed building relates to the grade of the hill.

The site is not situated on a hill.

5. The proposed design conforms in all significant respects with the Oakland General Plan and with any applicable design review guidelines or criteria, district plan or development control map which has been adopted by the Planning Commission or City Council.

As detailed earlier in the report, and hereby incorporated by reference, the General Plan's Land Use and Transportation Element (LUTE) classifies the project site as being located in the Central Business District (CBD) General Plan area. This land use classification is intended encourage, support, and enhance the downtown area as a high density mixed use urban center of regional importance and a primary hub for business, communications, office, government, high technology, retail, entertainment, community facilities, and visitor uses. The CBD classification includes a mix of large-scale offices, commercial, urban high rise residential, institutional, open-space, cultural, educational, arts, entertainment, service, community facilities, and visitor uses.

The Broadway Valdez District Specific Plan provides a vision and planning framework for future growth and development in the approximately 95 acre area along Oakland's Broadway corridor between Grand Avenue and I-580. The Specific Plan, which has been developed with a thorough analysis of the area's economic and environmental conditions and input from City decision-makers, landowners, developers, real estate experts, and the community at large, provides a comprehensive vision for the Plan Area along with goals, policies, and development regulations to guide future public and private actions relating to the area's development. The Plan also serves as the mechanism for insuring that future development will be coordinated and occur in an orderly and well-planned manner.

The Project is consistent/conforms with the previously mentioned goals and policies by creating a new, mixed use development located at a retail priority site of the Plan Area. The proposal will contain an active ground floor commercial presence with more than 17,000 square feet of new retail space that will promote a vibrant, pedestrian-oriented environment for Broadway as well as a new hotel use to bring visitors to the area. The proposal will retain and restore the existing façade of 2401 Broadway to provide a connection to the past while repurposing it from an auto use to an active retail use. The Project also will create high density, upper level residential uses that will be in close proximity to transit access and help to create a 24-hour neighborhood.

17.136.050(C) - LOCAL REGISTER DESIGN REVIEW CRITERIA:

1. **That for additions or alterations, the proposal will not substantially impair the visual, architectural, or historic value of the affected site or facility. Consideration shall be given to design, form, scale, materials, texture, lighting, landscaping, Signs, and any other relevant design element or effect, and, where applicable, the relation of the above to the original design of the affected facility.**

The proposal involves alterations to two buildings that are considered Local Register Properties, one at 437 25th Street and one at 2401 Broadway. The only changes to 437 25th Street is an addition to the rear portion of the building that will tie the existing building into the new development. The building as seen from the street will remain largely unchanged as the façade will be retained as is with the exception of new garage door and a new storefront for the entry into the upper level bar that is setback from the street. The roof truss system at the front portion of the building will also be retained and the building will continue to fit in seamlessly with the rest of the historic 25th Street Garage District. The other building being modified is 2401 Broadway, which while located within the API is not itself a contributor to the district due to the large number of changes to the building over time. Most of this building will be demolished, however the façade will be retained and restored to be incorporated into the project. The restoration of the façade will better connect this portion of the site with the API as the original façade features reminiscent of the old auto row and garage district will be brought back and have a better relationship with the district. The larger new building that will rise beyond will be a simplified design largely made up of windows that relate to the steel sash windows of the district. The simplified design allows for the renovated historic façade can stand out.

SECTION 17.136.075.C – CATEGORY II DEMOLITION FINDINGS (Non-contributor):

1. **For the demolition of structures in the CIX-1A Zone; or contributors to an S-7 Zone, S-20 Zone, or API:**
 - a. **The applicant demonstrates that: i) the existing property has no reasonable use or cannot generate a reasonable economic return and that the development replacing it will provide such use or generates such return, or ii) the applicant demonstrates that the structure constitutes a hazard and is economically infeasible to rehabilitate on its present site. For this criterion, a hazard constitutes a threat to health and safety that is not immediate; and**

Not Applicable– 2401 Broadway is not a contributor to the district.

FINDINGS

- b. It is economically, functionally, architecturally, or structurally infeasible to incorporate the historic structure, or existing structure in the CIX-1A Zone, into the proposed development.**

Not applicable – 2401 Broadway is not a contributor to the district.

- 2. For the demolition of noncontributors to an S-7 Zone, S-20 Zone, or API: The existing structure is either: i) seriously deteriorated or a hazard; or ii) the existing design is undistinguished and does not warrant retention. For this finding, a hazard constitutes a threat to health and safety that is not immediate;**

The current design is largely undistinguished due to past renovations that removed much of the character defining features of the façade such as the arched entry ways with divided lite, the cornice line, and cast medallions. The proposed project would not in fact remove these items but rather will restore these character defining features as part of the façade restoration. The other portions of the building beyond the façade will be removed and replaced with a new building in which the restored façade will tie into.

- 3. For the demolition of any structure in an S-7 Zone, S-20 Zone, or API:**
 - a. The design quality of the replacement structure is equal/superior to that of the existing structure; and**

The design of the replacement project would be superior in design than that of the existing building at 2401 Broadway. As previously stated above, the existing building is undistinguished due to past building renovations that stripped the building of its character defining features. The proposed project will renovate the façade and restore those features. The new building behind the restored façade will consist of quality materials but is purposely simplified as to not compete with or take away from the restored façade at the ground levels.

- b. The design of the replacement project is compatible with the character of the district, and there is no erosion of design quality at the replacement project site and in the surrounding area. This includes, but is not necessarily limited to, the following additional findings:**

- i. The replacement project is compatible with the district in terms of massing, siting, rhythm, composition, patterns of openings, quality of material, and intensity of detailing;**

The replacement project will be compatible with the district by restoring the façade of a building that is currently a non-contributor, but in time the restoration work to the façade on the building may allow it to be considered a contributor to the district. The new building that will be constructed behind the restored façade will be a simplified design that is heavily articulated with windows that are reminiscent of the steel sash windows seen throughout the district.

- ii. New street frontage includes forms that reflect the widths and rhythm of the facades on the street and entrances that reflect the patterns on the street;**

The new street frontage will restore the prior historic façade and will be compatible with the district.

- iii. The replacement project provides high visual interest that either reflects the level and quality of visual interest of the district contributors or otherwise enhances the visual interest of the district;**

The replacement project will provide high visual interest that reflects that of the district by restoring the existing façade and reinstalling the character defining elements of the building. The new construction at the upper levels is largely defined by the window pattern on the building which relates well to the steel ash windows of the industrial character of the 25th Garage District.

- iv. If the design contrasts the new to the historic character, the replacement project enriches the historic character of the district;**

The design will create a contrast between the new building by having a very simplified modern design that will contain window patterns that relate well to the industrial character of the district. The project will enrich the character of the district by restoring the façade of the building and bringing it back into character with the rest of the district.

- v. **The replacement project is consistent with the visual cohesiveness of the district. For the purpose of this item, visual cohesiveness is the architectural character, the sum of all visual aspects, features, and materials that defines the district. A new structure contributes to the visual cohesiveness of a district if it relates to the design characteristics of a historic district. New construction may do so by drawing upon some basic building features, such as the way in which a building is located on its site, the manner in which it relates to the street, its basic mass, form, direction or orientation (horizontal vs. vertical), recesses and projections, quality of materials, patterns of openings and level of detailing. When a combination of some of these design variables are arranged in a new building to relate to those seen traditionally in the area, but integral to the design and character of the proposed new construction, visual cohesiveness results; and**

The proposed project holds the street frontage consistent with the district, except at 2401 Broadway where the new structure's upper floors are set back from the existing street facade by approximately eight feet to clearly express old versus new and a literal layering of development over time. The existing opening widths and architectural detailing are maintained, as well as the old openings and architectural detailing that will be restored on the remaining portions of the façade. The new structure also relates to the characteristics of the district with its high-quality materials and minimal ornamentation.

- vi. **The replacement project will not cause the district to lose its current historic status.**

2401 Broadway is a non-contributor and the proposed work would not change its status or that of the district, though the restoration of the façade may possibly make the building a contributor to the district in the future.

SECTION 17.154.060.C –HEIGHT EXTENSION ACROSS ZONE BOUNDARIES:

- 1. **The height area line adjustment creates a more successful site plan in terms of open space, parking, or building location; and**

The extension of the D-BV height limit into the CC zone by 30 feet allows for the hotel component of the project to have an open interior courtyard as well as a more functional and attractive interior circulation plan which may have needed to be moved to the building exterior in order to retain the same number of hotel rooms. In addition, the 30-foot extension allows for

a more robust building at the corner location of Broadway and 24th Street consistent with the building across the street to the south.

- 2. Appropriate height transitions are incorporated into the building design and site plan to adjacent lower density residential properties that either share a parcel line or are across the street from the proposal.**

No adjacent residential properties exist to the subject property. However, a height transition is still provided where the building is broken down by a courtyard above the podium and a second building mass of only 45 feet is provided at the rear of the property to create the visual transition to the lower scale warehouses on 24th Street.

SECTION 17.134.050 –CONDITIONAL USE PERMIT FINDINGS:

- 1. That the location, size, design, and operating characteristics of the proposed development will be compatible with, and will not adversely affect, the livability or appropriate development of abutting properties and the surrounding neighborhood, with consideration to be given to harmony in scale, bulk, coverage, and density; to the availability of civic facilities and utilities; to harmful effect, if any upon desirable neighborhood character; to the generation of traffic and the capacity of surrounding streets; and to any other relevant impact of the development.**

The proposed mixed use development is consistent with the desired character for the area as set forth in the Broadway Valdez Specific Plan Area by helping to establish a new pedestrian oriented retail development on Broadway with approximately 17,000 square feet of new ground floor retail and providing for the density desired for a 24 hour neighborhood. The proposal will also include a new hotel for the Broadway Valdez District that will help to bring tourists to the area which will help to establish the area as a shopping destination. The proposed project establishes the desired mixed use character and density envisioned for the area. The proposed scale, bulk and coverage of the proposal is consistent with other projects within the Broadway Valdez District that are currently under construction and/or planned for the area. The project will not generate any more traffic than already considered under the BVDSP EIR and the project will pay it fair share contribution to proposed traffic improvements in the area.

- 2. That the location, design, and site planning of the proposed development will provide a convenient and functional living, working, shopping, or civic environment, and will be as attractive as the nature of the use and its location and setting warrant.**

The proposal will provide a functional mixed use environment with new ground floor retail opportunities and a new hotel on an important shopping street with upper level residential activities that are located in close proximity to local and regional transit and contains ample on-site open space.

- 3. That the proposed development will enhance the successful operation of the surrounding area in its basic community functions, or will provide an essential service to the community or region.**

The development will help to enhance the area as a neighborhood and regional shopping district by establishing new commercial and residential uses that will help to bring more activity to the area while creating an attractive pedestrian environment around the project site. The site will also contain a large retail space along the Broadway frontage that will be suitable for an anchor tenant in the area.

- 4. That the proposal conforms to all applicable design review criteria set forth in the DESIGN REVIEW PROCEDURE of Chapter 17.136 of the Oakland Planning Code.**

See Design Review findings above.

- 5. That the proposal conforms in all significant respects with the Oakland General Plan and with any other applicable plan or development control map which has been adopted by the City Council.**

The project site is consistent with the City's Corridor Design Guidelines and the Broadway Valdez Design Guidelines. The Project is consistent with the goals and policies of the LUTE and BVDSP as indicated in Findings in Sections 17.136.050 above and the City Planning Commission Report, hereby incorporated by reference.

17.102.110: CONDITIONAL USE PERMIT CRITERIA – EXPANSION OF USES INTO ADJACENT ZONE

- 1. That the location, size, design, and other characteristics of the entire use as proposed will substantially improve or provide superior environmental relationships among all uses in the immediate vicinity.**

The Conditional Use Permit will allow the proposed hotel activity to extend for the full distance into the BVDSP priority site, the back portion of which was not included in the D-BV zoning. The proposal will create a visual break in the building as it extends into the adjacent CC Zone to create a height transition to the lower scale warehouse buildings to the west of the site. The proposed hotel activity will not be detrimental to the existing enclosed warehouses that currently exist along the block within the CC-3 zone.

- 2. That the design and site planning of all buildings, open areas, parking, service areas, paths, stairways, accessways, corridors, and balconies will be so designed as to not adversely affect the privacy, safety, or environmental amenities of adjacent properties.**

The proposed hotel has been designed so that the amenity spaces are internalized and will not directly face the adjacent warehouse uses. In addition, the adjacent warehouse buildings are fully enclosed to even further limit incompatibility of the uses.

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- 3. That within the expansion area every reasonable effort will be undertaken to preserve natural grades, topographic features, watercourses, and significant landscape features.**

The project site is relatively flat and no topographic or landscape feature currently exist.

SECTION 17.103.030(A)- CONDITIONAL USE PERMIT CRITERIA –ALCOHOL SALES

- 1. That the proposal will not contribute to undue proliferation of such uses in an area where additional ones would be undesirable, with consideration to be given to the area's function and character, problems of crime and loitering, and traffic problems and capacity;**

The proposal includes a new bar along the 25th Street frontage of the project. The bar is designed in a manner where it will primarily serve as a component to serve the hotel use, but will be operated as a standalone bar with direct access open to the public from 25th Street. The 25th Street area is currently a mix of warehouses and auto garages and has been quickly transforming into an arts district with numerous art galleries and creative spaces. The proposal for a bar at this location would fit well into the mix of uses and will further enhance the area as an entertainment/nightlife destination. Given the desired character of the area as a destination for shopping nightlife and other cultural/arts activities as well as the fact that the bar will be closely associated with the hotel it would not be anticipated that there would be any issues associated with crime or unwanted loitering.

- 2. That the proposal will not adversely affect adjacent or nearby churches, temples, or synagogues; public, parochial, or private elementary, junior high, or high schools; public parks or recreation centers; or public or parochial playgrounds;**

The proposed bar is not adjacent to any of the activities cited. A nearby church located at Broadway and 27th Street, two blocks away, is the closest such activity which should not be impacted by a bar operating in conjunction with the proposed hotel use.

- 3. That the proposal will not interfere with the movement of people along an important pedestrian street;**

The proposal will not interfere with the movement of people along an important pedestrian street. The proposal will have frontage on 25th Street, which is becoming an important pedestrian street due to the new influx of art galleries and creative working spaces that draw large crowds. The proposed development will include improvements to the pedestrian environment with new sidewalks and street trees and will not add any more auto curb cuts than that of what already exists.

- 4. That the proposed development will be of an architectural and visual quality and character which harmonizes with, or where appropriate enhances, the surrounding area;**

The proposed bar will be located on the upper rear of the existing historic building at 437 25th Street, and as seen from the street the design of the existing building has very little change and as such will retain its compatibility with the character of the area.

- 5. That the design will avoid unduly large or obtrusive Signs, bleak unlandscaped parking areas, and an overall garish impression;**

Signage will be required to meet the sign regulations to avoid any obtrusive signs or will undergo an additional formal review under a future master sign program permit for the development site as a whole. The proposal will not contain any open parking lots.

- 6. That adequate litter receptacles will be provided where appropriate;**

Installation of litter receptacles will be reviewed in conjunction with a px permit (public improvements) as part of the building permit review process. In addition, there are currently early plans for installing new trash receptacles throughout the BVDSP area.

- 7. That where the proposed use is in close proximity to residential uses, and especially to bedroom windows, it will be limited in hours of operation, or designed or operated, so as to avoid disruption of residents' sleep between the hours of 10:00 p.m. and 7:00 a.m.;**

The proposal will not be in close proximity to residential activities other than those within the same proposed development, which given the site layout would be about 70 feet away.

SECTION 17.103.030(B)(3)- CRITERIA FOR PUBLIC CONVENIENCE OR NECESSITY

- A. That a community need for the project is clearly demonstrated. To demonstrate community need, the applicant shall document in writing, specifically how the project would serve an unmet or underserved need or population within the overall Oakland community or the community in which the project is located, and how the proposed project would enhance physical accessibility to needed goods or services that the project would provide, including, but not limited to alcohol; and**

The proposed bar will be built in conjunction and connected to a new hotel that is included within the same development proposal. At present Downtown Oakland lacks a sufficient number of hotel rooms to adequately take advantage of potential tourist dollars. The area around the proposed hotel development includes the thriving arts district that draws in large crowds every first Friday of the month. Additionally, the developing Broadway Valdez District that the site is located within will include large amounts of retail opportunities so Oakland can begin to close the gap of dollars spent outside of the City limits by residents for

comparison goods shopping. The inclusion of the alcohol sales within a bar is a typical component of a hotel, and by allowing the proposal to proceed would meet a large unmet need of hotel rooms within the area.

- B. That the overall project will have a positive influence on the quality of life for the community in which it is located, providing economic benefits that outweigh anticipated negative impacts, and that will not result in a significant increase in calls for police service; and**

The proposed bar will be developed along with the hotel within the same development proposal. The stated above, the inclusion of a bar within a hotel is typical and a new hotel within the downtown area in direct proximity to the developing Broadway Valdez shopping area and the emerging arts district events would allow for enhanced economic benefits to the City by taking advantage of additional tourist dollars and hotel room taxes. Given the nature of the proposal being tied to a new hotel activity, it would not be anticipated that the bar would result in a significant increase in call for police service.

- C. That alcohol sales are typically a part of this type of business in the City of Oakland.**

Alcohol sales as part of a bar operations within first class hotels are common within the City of Oakland as well as other cities where they are included as an amenity to the hotel guests. Other examples in Oakland are the Marriott on Broadway, the Waterfront Hotel in Jack London, or the Hilton on Hegenberger Road near the airport.

17.103.050: CONDITIONAL USE PERMIT CRITERIA – TRANSIENT HABITATION

- 1. That the proposal is consistent with the goal of attracting first-class, luxury hotels in downtown, along the waterfront, near the airport, along the I-880 freeway, in a specific plan area, and/or in an area with a concentration of amenities for hotel patrons, including but not limited to restaurant, retail, recreation, open space and exercise facilities, and is well-served by public transit;**

The proposal will include a West Elm Hotel which is one of six start up hotels that is being developed by the West Elm chain that is looking at creating a boutique hotel experience where each hotel is unique to its host City and neighborhood. The proposal will be located within downtown and within the Broadway Valdez District Specific Plan Area, and is well served by nearby amenities including numerous restaurants, existing and planned public open spaces, nearby gyms, and numerous public transit options.

- 2. That the proposal considers the impact of the employees of the hotel or motel on the demand in the City for housing, public transit, and social services;**

The proposal will create new jobs that would be available to existing Oakland residents. Available jobs can help to reduce the unemployment rate within Oakland that is still higher

than the California State average by and much higher than the local Bay Area average. The project site is located within close proximity to multiple public transit options that are readily available such as BART and major AC Transit trunk lines such as the 51 line and the 6 line, as well as other AC transit lines. It would not be anticipated that the creation of new jobs in a central location accessible to Oakland residents would create a significant impact upon social services offered by or within the City of Oakland, as the Hotel would be anticipated to create between 144 new jobs and the Broadway Valdez Specific Plan anticipates the creation of 4,500 new jobs in the area.

3. That the proposed development will be of an architectural and visual quality and character which harmonizes and enhances the surrounding area, and that such design includes:

- a. Site planning that insures appropriate access and circulation, locates building entries which face the primary street, provides a consistent development pattern along the primary street, and insures a design that promotes safety for its users;**

The proposal will include a primary entry to the hotel lobby on Broadway which is the primary street frontage for the site. The lobby will fit in as part of an overall active street frontage that will also include a large retail frontage as well as a renovated historic façade that will likely serve restaurant activities. The large amount of activity along the frontage will provide for a safe environment.

- b. Landscaping that creates a pleasant visual corridor along the primary streets with a variety of local species and high quality landscape materials;**

The subject is located within a downtown setting which precludes the desire for a setback situation with lush landscaping along the façade of the building. The project will be required to provide street trees and landscaped interior spaces, which is more appropriate for the setting of the project site.

- c. Signage that is integrated and consistent with the building design and promotes the building entry, is consistent with the desired character of the area, and does not detract from the overall streetscape;**

Signage will be required to meet the sign regulations to avoid any obtrusive signs or will undergo an additional formal review under a future master sign program permit for the development site as a whole.

- d. The majority of the parking is located either to the side or rear of the site, or where appropriate, within a structured parking facility that is consistent, compatible and integrated into the overall development;**

The proposed parking will be provided in an enclosed garage with access off the side streets.

- e. Appropriate design treatment for ventilation of room units as well as structured parking areas; and prominent entry features that may include attractive porte-cocheres;**

The room ventilation will be by way of a central ventilation system. Parking will be provided in an enclosed garage to the rear of the building with access off the side streets which will limit any visual impacts of ventilation. Given the desire for a pedestrian oriented development along Broadway, no porte-cochere will be provided.

- f. Building design that enhances the building's quality with strong architectural statements, high quality materials particularly at the pedestrian level and appropriate attention to detail;**

The proposed building provides a high quality design as outlined in the Design Review findings and pays close attention to the ground floor where the pedestrian environment will be the central focus of the design including the new ground floor retail, entry lobby, and the renovated historic façade with large arched openings that will open the interior food and beverage/uses at the location onto the street.

- g. Lighting standards for hotel buildings, grounds and parking lots that are not overly bright and direct the downward placement of light.**

Per the Standard Conditions of Approval all lighting shall have bulbs that are shielded and directed downward.

- 4. That the proposed development provides adequately buffered loading areas and to the extent possible, are located on secondary streets;**

The project site will contain one loading berth for the retail and one loading berth for the hotel. Both loading berths will be enclosed with access off the side streets.

- 5. The proposed operator of the facility shall be identified as part of the project description at the time of application.**

The proposed hotel operator has been identified as West Elm Hotel, this location being one of six initial start up locations.

17.116.110.D: CONDITIONAL USE PERMIT CRITERIA – SHARED PARKING

- 1. The typical utilization of the parking area would be staggered to such an extent that the reduced number of spaces would be adequate to serve all such activities.**

The residents of the building would have access of up to 36 designated parking stalls as requested by tenants since the parking will be unbundled from any residential leases. The remaining 93 parking spaces will be provided for the various retail visitors and hotel guests on a valet service basis where they would enter on 25th Street where their cars would be taken and parked within a mechanical parking system. The varying commercial uses such as retail, restaurant, and hotel would be staggered so that retail would be primarily occupied during weekend daytime hours, the restaurants would be late afternoon into the evening, and hotel parking would be overnight. Also, many of these uses would all be patronized by many of the same users.

16.08.030 - TENTATIVE MAP FINDINGS (Pursuant also to California Government Code §66474 (Chapter 4, Subdivision Map Act))

The Advisory Agency shall deny approval of a tentative map, or a parcel map for which a tentative map was not required, if it makes any of the following findings:

- A. That the proposed map is not consistent with applicable general and specific plans as specified in the State Government Code Section 65451.**

The proposal is consistent with the Central Business District General Plan designation and with the Broadway Valdez District Specific Plan by creating a mixed-use development with viable street fronting retail for an anchor tenant along Broadway and hotel and residential uses to create a 24-hour neighborhood. See additional General Plan Conformity findings above.

- B. That the design or improvement of the proposed subdivision is not consistent with applicable general and specific plans.**

The proposal is consistent with the Central Business District General Plan designation and with the Broadway Valdez District Specific Plan by creating a mixed-use development with viable street fronting retail for an anchor tenant along Broadway and hotel and residential uses to create a 24-hour neighborhood. See additional General Plan Conformity findings above.

- C. That the site is not physically suitable for the type of development.**

The site is suitable for the proposed development as it is located close to public utilities, transit, and other civic facilities, and fulfills the vision for the area as set forth in the Broadway Valdez District Specific Plan.

- D. That the site is not physically suitable for the proposed density of development.**

The proposed density is consistent with the General Plan and Specific Plan density envisioned for the area.

- E. That the design of the subdivision or the proposed improvements are likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.**

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This site has been previously developed and does not contain any wildlife habitat or waterways.

F. That the design of the subdivision or type of improvements is likely to cause serious public health problems.

There should be no adverse health effects. This is in a mixed-use development containing commercial, residential and retail uses located in the downtown area and it will introduce no new use classifications that are incompatible with the surrounding neighborhood.

G. That the design of the subdivision or the type of improvements will conflict with easements, acquired by the public at large, for access through or use of, property within the proposed subdivision. In this connection, the governing body may approve a map if it finds that alternate easements, for access or for use, will be provided, and that these will be substantially equivalent to ones previously acquired by the public. (This subsection shall apply only to easements of record or to easements established by judgment of a court of competent jurisdiction and no authority is hereby granted to a legislative body to determine that the public at large has acquired easements for access through or use of property within the proposed subdivision.)

There are no easements on this property at present to allow the public access to anything.

H. That the design of the subdivision does not provide to the extent feasible, for future passive or natural heating or cooling opportunities in the subdivision

The project could to be set up for solar panels on the rooftop.

SECTION 16.24.040 – LOT DESIGN STANDARDS

As a one lot subdivision for condominium purposes these standards are not applicable.

CEQA COMPLIANCE FINDINGS

I. Introduction These findings are made pursuant to the California Environmental Quality Act (Public Resources Code section 21000 et seq.; “CEQA”) and the CEQA Guidelines (Cal. Code Regs. title 14, section 15000 et seq.; “CEQA Guidelines”) by the City Planning Commission in connection with the environmental analysis of the effects of implementation of the 2401 Broadway project, as more fully described elsewhere in this Staff Report and City Of Oakland (“City”)-prepared CEQA Analysis document entitled “2401 Broadway Project CEQA Analysis” dated September 5, 2017 (“CEQA Analysis”) (the “Project”). The City is the lead agency for purposes of compliance with the requirements of CEQA. These CEQA findings are attached and incorporated by reference into each and every decision associated with approval of the Project and are based on substantial evidence in the entire administrative record.

II. Adoption of BVDSP and Certification of BVDSP EIR: The City finds and determines that (a) the Oakland City Council on June 17, 2014 adopted Resolution No. 85065 C.M.S. which adopted the Broadway Valdez District Specific Plan (“BVDSP”), made appropriate CEQA findings, including certification of the BVDSP Environmental Impact Report (“EIR”); and (b)

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the BVDSP satisfies the description of “Community Plan” set out in Public Resources Code section 21083.3(e) and in CEQA Guidelines section 15183 as well the description of “Planning Level Document” set out in Public Resources Code section 21094.5 and in CEQA Guidelines section 15183.3. The City Council, in adopting the BVDSP following a public hearing, approved as a part thereof Standard Conditions of Approval (“SCAs”) which constitute uniformly applied development policies or standards (together with other City development regulations) and determined that the uniformly applicable development policies or standards, together with the mitigation measures set out in the BVDSP EIR, would substantially mitigate the impacts of the BVDSP and future projects thereunder.

III. CEQA Analysis Document: The CEQA Analysis and all of its findings, determinations and information is hereby incorporated by reference as if fully set forth herein. The CEQA Analysis concluded that the Project satisfies each of the following CEQA provisions, qualifying the Project for two separate CEQA statutory exemptions and that the CEQA Analysis constitutes an addendum to the BVDSP EIR, as summarized below and provides substantial evidence to support the following findings.

The City hereby finds that, as set forth below and in the checklist attached as part of the CEQA Analysis, the Project is exempt from any additional CEQA Analysis under the “Community Plan Exemption” of Public Resources Code section 21083.3 (CEQA Guidelines §15183) and/or the “Qualified Infill Exemption” under Public Resources section 21094.5 (CEQA Guidelines §15183.3) and that the CEQA Analysis also constitutes an Addendum to the BVDSP EIR pursuant to Public Resources Code section 21166 (CEQA Guidelines §15162) and that such Addendum determines that none of the three events requiring subsequent or supplemental environmental analysis as stipulated in Public Resources Code section 21166 have occurred, thus no additional environmental analysis beyond the BVDSP EIR and the CEQA Analysis is necessary. The specific statutory exemptions and the status of the CEQA Analysis as an Addendum are discussed below in more detail.

A. Community Plan Exemption; Public Resources Code Section 21083.3 (CEQA Guidelines §15183): The City finds and determines that, for the reasons set out below and in the CEQA Analysis, the Community Plan Exemption applies to the Project. Therefore, no further environmental analysis is required because all of the Project’s effects on the environment were adequately analyzed and mitigation measures provided in the BVDSP EIR; there are no significant effects on the environment which are peculiar to the Project or to the parcel upon which it is located not addressed and mitigated in the BVDSP EIR; and there is no new information showing that any of the effects shall be more significant than described in the BVDSP EIR.

As set out in detail in Attachment B to the CEQA Analysis, the City finds that, pursuant to CEQA Guidelines section 15183 and Public Resources Code section 21083.3, the Project is consistent with the development density established by the BVDSP and analyzed in the BVDSP EIR and that there are no environmental effects of the Project peculiar to the Project or the Project Site which were not analyzed as significant effects in the BVDSP EIR: nor are there potentially significant off-site impacts and cumulative impacts not discussed in the BVDSP EIR; nor are any of the previously identified significant effects which, as a result of substantial

information not known at the time of certification of the BVDSP EIR, are now determined to present a more severe adverse impact than discussed in the BVDSP EIR. As such, no further analysis of the environmental effects of the Project is required.

B. Qualified Infill Exemption; Public Resources Code Section 21094.5 (CEQA Guidelines §15183.3): The City finds and determines that, for the reasons set forth below and in the CEQA Analysis, a Qualified Infill Exemption applies to the Project and no further environmental analysis is required since all the Project's effects on the environment were adequately analyzed and mitigation measures provided in the BVDSP EIR; the Project will cause no new specific effects not addressed in the BVDSP EIR that are specific to the Project or the Project Site; and there is no substantial new information showing that the adverse environmental effects of the Project are more significant than described in the BVDSP EIR.

The City finds that, pursuant to CEQA Guidelines section 15183.3, the CEQA Analysis contains in Attachment C a written analysis consistent with Appendix M to the CEQA Guidelines examining whether the Project will cause any effects that require additional review under CEQA. The contents of Attachment C documents that the Project is located in an urban area satisfying the requirements of CEQA Guidelines section 15183.3 and satisfies the applicable performance standards set forth in Appendix M to the CEQA Guidelines. It also explains how the effects of the Project were analyzed in the BVDSP EIR; and indicates that the Project incorporates all applicable mitigation measures and SCAs from the BVDSP EIR. Attachment C also determines that the Project will cause no new specific effects not analyzed in the BVDSP EIR; determines that there is no substantial new information showing that the adverse environmental effects of the Project are more significant than described in the BVDSP EIR, determines that the Project will not cause new specific effects or more significant effects, and documents how uniformly applicable development policies or standards (including, without limitation, the SCAs) will mitigate environmental effects of the Project. Based upon the CEQA Analysis and other substantial evidence in the record, the City finds and determines that no further environmental analysis of the effects of the Project is required.

C. CEQA Analysis Constitutes an Addendum; Public Resources Code Section 21166 (CEQA Guidelines §15164): The City finds and determines that the CEQA Analysis constitutes an Addendum to the BVDSP EIR and that no additional environmental analysis of the Project beyond that contained in the BVDSP EIR is necessary. The City further finds that no substantial changes are proposed in the Project that would require major revisions to the BVDSP EIR because of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; no substantial changes occur with respect to the circumstances under which the Project will be undertaken which will require major revisions of the BVDSP EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and there is no new information of substantial importance not known and which could not have been known with the exercise of reasonable diligence as of the time of certification of the BVDSP EIR showing that the Project will have one or more significant effects not discussed in the BVDSP EIR; significant effects previously examined will be substantially more severe than shown in the BVDSP EIR, mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project; or mitigation

measures or alternatives which are considerably different from those analyzed in the BVDSP EIR would substantially reduce one or more significant effects on the environment.

Based on these findings and determinations, the City further finds that no Subsequent or Supplemental EIR or additional environmental analysis shall be required because of the Project. The City has considered the CEQA Analysis along with the BVDSP EIR prior to making its decision on the Project and a discussion is set out in the CEQA Analysis explaining the City's decision not to prepare a Subsequent or Supplemental EIR pursuant to Guidelines sections 15162 and/or 15163.

IV. Severability: The City finds that all three CEQA provisions discussed and determined to be applicable in Section III above are separately and independently applicable to the consideration of the Project and should any of the three be determined not to be so applicable, such determinations shall have no effect on the validity of these findings and the approval of the Project on any of the other grounds.

V. Incorporation by Reference of Statement of Overriding Considerations: The BVDSP EIR identified seven areas of environmental effects of the BVDSP that presented significant and unavoidable impacts. Because the Project may contribute to some significant and unavoidable impacts identified in the BVDSP EIR, but a Subsequent and/or Supplemental EIR is not required in accordance with CEQA Guidelines sections 15162, 15163, 15164, 15183 and 15183.3, a Statement of Overriding Considerations is not legally required. Nevertheless, in the interest of being conservative, the Statement of Overriding Consideration for the BVDSP EIR, approved as Section XII of the CEQA Findings adopted by the City Council on June 17, 2104, via Resolution No. 86065 C.M.S., is hereby incorporated by reference as if fully set forth herein.

ATTACHMENT B

CONDITIONS OF APPROVAL

STANDARD ADMINISTRATIVE CONDITIONS:

1. Approved Use

The project shall be constructed and operated in accordance with the authorized use as described in the approved application materials, **staff report** and the approved plans **dated September 18, 2017**, as amended by the following conditions of approval and mitigation measures, if applicable (“Conditions of Approval” or “Conditions”).

2. Effective Date, Expiration, Extensions and Extinguishment

This Approval shall become effective immediately, unless the Approval is appealable, in which case the Approval shall become effective in ten calendar days unless an appeal is filed. Unless a different termination date is prescribed, this Approval shall expire **two years** from the Approval date, or from the date of the final decision in the event of an appeal, unless within such period all necessary permits for construction or alteration have been issued, or the authorized activities have commenced in the case of a permit not involving construction or alteration. Upon written request and payment of appropriate fees submitted no later than the expiration date of this Approval, the Director of City Planning or designee may grant a one-year extension of this date, with additional extensions subject to approval by the approving body. Expiration of any necessary building permit or other construction-related permit for this project may invalidate this Approval if said Approval has also expired. If litigation is filed challenging this Approval, or its implementation, then the time period stated above for obtaining necessary permits for construction or alteration and/or commencement of authorized activities is automatically extended for the duration of the litigation.

3. Compliance with Other Requirements

The project applicant shall comply with all other applicable federal, state, regional, and local laws/codes, requirements, regulations, and guidelines, including but not limited to those imposed by the City’s Bureau of Building, Fire Marshal, and Public Works Department. Compliance with other applicable requirements may require changes to the approved use and/or plans. These changes shall be processed in accordance with the procedures contained in Condition #4.

4. Minor and Major Changes

- a. Minor changes to the approved project, plans, Conditions, facilities, or use may be approved administratively by the Director of City Planning.
- b. Major changes to the approved project, plans, Conditions, facilities, or use shall be reviewed by the Director of City Planning to determine whether such changes require submittal and approval of a revision to the Approval by the original approving body or a new independent permit/approval. Major revisions shall be reviewed in accordance

CONDITIONS OF APPROVAL

with the procedures required for the original permit/approval. A new independent permit/approval shall be reviewed in accordance with the procedures required for the new permit/approval.

5. Compliance with Conditions of Approval

- a. The project applicant and property owner, including successors, (collectively referred to hereafter as the “project applicant” or “applicant”) shall be responsible for compliance with all the Conditions of Approval and any recommendations contained in any submitted and approved technical report at his/her sole cost and expense, subject to review and approval by the City of Oakland.
- b. The City of Oakland reserves the right at any time during construction to require certification by a licensed professional at the project applicant’s expense that the as-built project conforms to all applicable requirements, including but not limited to, approved maximum heights and minimum setbacks. Failure to construct the project in accordance with the Approval may result in remedial reconstruction, permit revocation, permit modification, stop work, permit suspension, or other corrective action.
- c. Violation of any term, Condition, or project description relating to the Approval is unlawful, prohibited, and a violation of the Oakland Municipal Code. The City of Oakland reserves the right to initiate civil and/or criminal enforcement and/or abatement proceedings, or after notice and public hearing, to revoke the Approval or alter these Conditions if it is found that there is violation of any of the Conditions or the provisions of the Planning Code or Municipal Code, or the project operates as or causes a public nuisance. This provision is not intended to, nor does it, limit in any manner whatsoever the ability of the City to take appropriate enforcement actions. The project applicant shall be responsible for paying fees in accordance with the City’s Master Fee Schedule for inspections conducted by the City or a City-designated third-party to investigate alleged violations of the Approval or Conditions.

6. Signed Copy of the Approval/Conditions

A copy of the Approval letter and Conditions shall be signed by the project applicant, attached to each set of permit plans submitted to the appropriate City agency for the project, and made available for review at the project job site at all times.

7. Blight/Nuisances

The project site shall be kept in a blight/nuisance-free condition. Any existing blight or nuisance shall be abated within 60 days of approval, unless an earlier date is specified elsewhere.

8. Indemnification

- a. To the maximum extent permitted by law, the project applicant shall defend (with counsel acceptable to the City), indemnify, and hold harmless the City of Oakland, the Oakland City Council, the Oakland Redevelopment Successor Agency, the Oakland City Planning Commission, and their respective agents, officers, employees, and volunteers (hereafter collectively called “City”) from any liability, damages, claim,

CONDITIONS OF APPROVAL

judgment, loss (direct or indirect), action, causes of action, or proceeding (including legal costs, attorneys' fees, expert witness or consultant fees, City Attorney or staff time, expenses or costs) (collectively called "Action") against the City to attack, set aside, void or annul this Approval or implementation of this Approval. The City may elect, in its sole discretion, to participate in the defense of said Action and the project applicant shall reimburse the City for its reasonable legal costs and attorneys' fees.

- b. Within ten (10) calendar days of the serving of any Action as specified in subsection (a) above on the City, the project applicant shall execute a Joint Defense Letter of Agreement with the City, acceptable to the Office of the City Attorney, which memorializes the above obligations. These obligations and the Joint Defense Letter of Agreement shall survive termination, extinguishment, or invalidation of the Approval. Failure to timely execute the Letter of Agreement does not relieve the project applicant of any of the obligations contained in this Condition or other requirements or Conditions of Approval that may be imposed by the City.

9. Severability

The Approval would not have been granted but for the applicability and validity of each and every one of the specified Conditions, and if one or more of such Conditions is found to be invalid by a court of competent jurisdiction this Approval would not have been granted without requiring other valid Conditions consistent with achieving the same purpose and intent of such Approval.

10. Special Inspector/Inspections, Independent Technical Review, Project Coordination and Monitoring

The project applicant may be required to cover the full costs of independent third-party technical review and City monitoring and inspection, including without limitation, special inspector(s)/inspection(s) during times of extensive or specialized plan-check review or construction, and inspections of potential violations of the Conditions of Approval. The project applicant shall establish a deposit with the Bureau of Building, if directed by the Building Official, Director of City Planning, or designee, prior to the issuance of a construction-related permit and on an ongoing as-needed basis.

11. Public Improvements

The project applicant shall obtain all necessary permits/approvals, such as encroachment permits, obstruction permits, curb/gutter/sidewalk permits, and public improvement ("p-job") permits from the City for work in the public right-of-way, including but not limited to, streets, curbs, gutters, sidewalks, utilities, and fire hydrants. Prior to any work in the public right-of-way, the applicant shall submit plans for review and approval by the Bureau of Planning, the Bureau of Building, and other City departments as required. Public improvements shall be designed and installed to the satisfaction of the City.

12. Compliance Matrix

The project applicant shall submit a Compliance Matrix, in both written and electronic form, for review and approval by the Bureau of Planning and the Bureau of Building that lists each Condition of Approval (including each mitigation measure if applicable) in a

CONDITIONS OF APPROVAL

sortable spreadsheet. The Compliance Matrix shall contain, at a minimum, each required Condition of Approval, when compliance with the Condition is required, and the status of compliance with each Condition. For multi-phased projects, the Compliance Matrix shall indicate which Condition applies to each phase. The project applicant shall submit the initial Compliance Matrix prior to the issuance of the first construction-related permit and shall submit an updated matrix upon request by the City.

13. Construction Management Plan

Prior to the issuance of the first construction-related permit, the project applicant and his/her general contractor shall submit a Construction Management Plan (CMP) for review and approval by the Bureau of Planning, Bureau of Building, and other relevant City departments such as the Fire Department and the Public Works Department as directed. The CMP shall contain measures to minimize potential construction impacts including measures to comply with all construction-related Conditions of Approval (and mitigation measures if applicable) such as dust control, construction emissions, hazardous materials, construction days/hours, construction traffic control, waste reduction and recycling, stormwater pollution prevention, noise control, complaint management, and cultural resource management (see applicable Conditions below). The CMP shall provide project-specific information including descriptive procedures, approval documentation, and drawings (such as a site logistics plan, fire safety plan, construction phasing plan, proposed truck routes, traffic control plan, complaint management plan, construction worker parking plan, and litter/debris clean-up plan) that specify how potential construction impacts will be minimized and how each construction-related requirement will be satisfied throughout construction of the project.

14. Standard Conditions of Approval / Mitigation Monitoring and Reporting Program (SCAMMRP)

- a. All mitigation measures identified in the 2401 Broadway CEQA Analysis Document are included in the Standard Condition of Approval / Mitigation Monitoring and Reporting Program (SCAMMRP) which is included in these Conditions of Approval and are incorporated herein by reference, as Attachment C, as Conditions of Approval of the project. The Standard Conditions of Approval identified in the 2401 Broadway CEQA Analysis Document are also included in the SCAMMRP, and are, therefore, incorporated into these Conditions by reference but are not repeated in these Conditions. To the extent that there is any inconsistency between the SCAMMRP and these Conditions, the more restrictive Conditions shall govern. In the event a Standard Condition of Approval or mitigation measure recommended in the 2401 Broadway CEQA Analysis Document has been inadvertently omitted from the SCAMMRP, that Standard Condition of Approval or mitigation measure is adopted and incorporated from the 2401 Broadway CEQA Analysis Document into the SCAMMRP by reference, and adopted as a Condition of Approval. The project applicant and property owner shall be responsible for compliance with the requirements of any submitted and approved technical reports, all applicable mitigation measures adopted, and with all Conditions of Approval set forth herein at his/her sole cost and expense, unless otherwise expressly provided in a specific mitigation measure or Condition of Approval, and subject to the review and approval by the City of Oakland. The SCAMMRP identifies the timeframe

CONDITIONS OF APPROVAL

and responsible party for implementation and monitoring for each Standard Condition of Approval and mitigation measure. Monitoring of compliance with the Standard Conditions of Approval and mitigation measures will be the responsibility of the Bureau of Planning and the Bureau of Building, with overall authority concerning compliance residing with the Environmental Review Officer. Adoption of the SCAMMRP will constitute fulfillment of the CEQA monitoring and/or reporting requirement set forth in section 21081.6 of CEQA.

- b. Prior to the issuance of the first construction-related permit, the project applicant shall pay the applicable mitigation and monitoring fee to the City in accordance with the City's Master Fee Schedule.

PROJECT SPECIFIC CONDITIONS:

15. Public Improvements Consistent with the BVDSP

Requirement: Plans shall be submitted for review and approval that include public right of way improvements that are consistent with the Broadway Valdez District Specific Plan. This shall apply to all project frontages.

When Required: Prior to issuance of Building Permit

Initial Approval: Bureau of Planning; Public Works

Monitoring/Inspection: Bureau of Building

16. Master Sign Program required

Requirement: The applicant shall prepare a Master Sign Program for the proposed project if the proposed signage shall exceed 200 square feet, which shall include all commercial signage and residential signage.

When Required: Prior to issuance of a Sign Permit

Initial Approval: Bureau of Planning

17. Exterior Finishes

Requirement: The final building permit plan set shall contain detailed information on all proposed exterior finishes for city approval. If requested by the Bureau of Planning sample materials shall be submitted and are subject to final approval by the Zoning Manager.

When Required: Prior to issuance of a Building Permit

Initial Approval: Bureau of Planning

Monitoring/Inspection: Bureau of Planning

18. Promotion of Local Hiring

Requirement: The applicant shall submit a proposal for review and approval by the Economic Development Manager and/or the Zoning Manager that demonstrates efforts to hold a "job fair" that will advertise job openings to local Oakland residents or otherwise participate in another local job fair if timely for hotel hiring. This may be accomplished by

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methods including but not limited by posting of a “job fair” at locations such as local Oakland libraries, community centers or employment centers.

When Required: Plan required prior to issuance of building permit

Requirement: Hold “job fair” event as advertised prior to hiring employment staff

When Required: Prior to Operation of Transient Habitation (Hotel) Activity

Monitoring/Inspection: Bureau of Planning

19. Public Art for Private Development Condition of Approval

Requirement: The project is subject to the City’s Public Art Requirements for Private Development, adopted by Ordinance No. 13275 C.M.S. (“Ordinance”). The public art contribution requirements are equivalent to one-half percent (0.5%) for the “residential” building development costs, and one percent (1.0%) for the “non-residential” building development costs. The contribution requirement can be met through the commission or acquisition and installation of publicly accessible art fund, or satisfaction of alternative compliance methods described in the Ordinance. The applicant shall provide proof of full payment of the in-lieu contribution, or provide proof of installation of artwork on the development site prior to the City’s issuance of a final certificate of occupancy for each phase unless a separate, legal binding instrument is executed ensuring compliance within a timely manner subject to City approval. On-site art installation shall be designed by independent artists, or artists working in conjunction with arts or community organizations that are verified by the City to either hold a valid Oakland business license and/or be an Oakland-based 501(c) (3) tax designated organization in good standing.

When Required: Prior to issuance of Final Certificate of Occupancy and Ongoing

Initial Approval: Bureau of Planning

20. Covenants, Conditions and Restrictions & Homeowner’s Association

Requirement: When the condominium units created are offered for sale, the Covenants, Conditions and Restrictions (CC&Rs) for the approved units shall be submitted to the Planning and Zoning Division for review. The CC&Rs shall provide for the establishment of a non-profit homeowners association to maintenance and operation of all common landscaping, driveways, and other facilities, in accordance with approved plans. Membership in the association shall be made a condition of ownership. The developer shall be a member of such association until all units are sold.

When Required: If the condominium units are offered for immediate sale, within one year after issuance of the first certificate of occupancy. If not, prior to the first sale of a condominium unit.

21. Miscellaneous Transportation Improvement Measures

Requirement: Ensure that the project driveway on 24th and 25th Streets would provide adequate sight distance between motorists exiting the driveway and pedestrians on the adjacent sidewalks. This may require redesigning and/or widening the driveway. If adequate sight distance cannot be provided, consider providing audio and/or visual warning devices at the driveway.

When Required: Prior to Certificate of Occupancy

Attachment C

Standard Conditions of Approval and Mitigation Monitoring and Reporting Program

This Standard Conditions of Approval and Mitigation Monitoring and Reporting Program (SCAMMRP) is based on the CEQA Analysis prepared for the 2401 Broadway Project.

This SCAMMRP is in compliance with Section 15097 of the CEQA Guidelines, which requires that the Lead Agency "adopt a program for monitoring or reporting on the revisions which it has required in the project and the measures it has imposed to mitigate or avoid significant environmental effects." The SCAMMRP lists mitigation measures ("MM") recommended in the BVDSP EIR and identifies mitigation monitoring requirements. The SCAMMRP also lists other City's Standard Conditions of Approval ("SCA") that apply to the proposed project, most of which were identified in the BVDSP EIR and some of which have been subsequently updated or otherwise modified by the City. Specifically, on July 22, 2015, the City of Oakland released a revised set of all City of Oakland SCAs, which largely still include SCAs adopted by the City in 2008, along with supplemental, modified, and new SCAs. SCAs are measures that would minimize potential adverse effects that could result from implementation of the proposed project, to ensure the conditions are implemented and monitored. The revised set of the City of Oakland SCAs includes new, modified, and reorganized SCAs; however, none of the revisions diminish or negate the ability of the SCAs considered "environmental protection measures" to minimize potential adverse environmental effects. As such, the SCAs identified in the SCAMMRP reflect the current SCAs only. Although the SCA numbers listed below may not correspond to the SCA numbers in the BVDSP EIR, all of the environmental topics and potential effects addressed by the SCAs in the BVDSP EIR are included in this SCAMMRP (as applicable to the proposed project). This SCAMMRP also identifies the mitigation monitoring requirements for each mitigation measure and SCA.

All MMs and SCAs identified in the CEQA Analysis, which is consistent with the measures and conditions presented in the BVDSP EIR, are included herein. To the extent that there is any inconsistency between the SCA and MM, the more restrictive conditions shall govern; to the extent any MM and/or SCA identified in the CEQA Analysis were inadvertently omitted, they are automatically incorporated herein by reference.

- The first column identifies the SCA and MM applicable to that topic in the CEQA Analysis.
- The second column identifies the monitoring schedule or timing applicable to the Project.

- The third column names the party responsible for monitoring the required action for the Project.

The project applicant is responsible for compliance with any recommendations in approved technical reports, with all applicable mitigation measures adopted and with all conditions of approval set forth herein at its sole cost and expense, unless otherwise expressly provided in a specific mitigation measure or condition of approval, and subject to the review and approval of the City of Oakland. Overall monitoring and compliance with the mitigation measures will be the responsibility of the Planning and Zoning Division. Prior to the issuance of a demolition, grading, and/or construction permit, the project applicant shall pay the applicable mitigation and monitoring fee to the City in accordance with the City's Master Fee Schedule.

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
		Schedule	Responsibility
Aesthetics, Shadow, and Wind			
SCA AES-1 (Standard Condition of Approval 16): Graffiti Control			
<p>a. During construction and operation of the project, the project applicant shall incorporate best management practices reasonably related to the control of graffiti and/or the mitigation of the impacts of graffiti. Such best management practices may include, without limitation:</p> <ul style="list-style-type: none"> i. Installation and maintenance of landscaping to discourage defacement of and/or protect likely graffiti-attracting surfaces. ii. Installation and maintenance of lighting to protect likely graffiti-attracting surfaces. iii. Use of paint with anti-graffiti coating. iv. Incorporation of architectural or design elements or features to discourage graffiti defacement in accordance with the principles of Crime Prevention Through Environmental Design (CPTED). <p>b. The project applicant shall remove graffiti by appropriate means within seventy-two (72) hours. Appropriate means include the following:</p> <ul style="list-style-type: none"> i. Removal through scrubbing, washing, sanding, and/or scraping (or similar method) without damaging the surface and without discharging wash water or cleaning detergents into the City storm drain system. ii. Covering with new paint to match the color of the surrounding surface. iii. Replacing with new surfacing (with City permits if required). 		Ongoing.	City of Oakland Bureau of Building Services Division, Zoning Inspections
SCA AES-2 (Standard Condition of Approval 17): Landscape Plan			
<p>a. Landscape Plan Required</p> <p>The project applicant shall submit a final Landscape Plan for City review and approval that is consistent with the approved Landscape Plan. The Landscape Plan shall be included with the set of drawings submitted for the construction-related permit and shall comply with the landscape requirements of chapter 17.124 of the Planning Code.</p> <p>b. Landscape Installation</p> <p>The project applicant shall implement the approved Landscape Plan unless a bond, cash deposit, letter of credit, or other equivalent instrument acceptable to the Director of City Planning, is provided. The financial instrument shall equal the greater of \$2,500 or the estimated cost of implementing the Landscape Plan based on a licensed contractor's bid.</p> <p>c. Landscape Maintenance</p> <p>All required planting shall be permanently maintained in good growing condition and, whenever necessary, replaced with new plant materials to ensure continued compliance with applicable landscaping requirements. The property owner shall be responsible for maintaining planting in adjacent public rights-of-way. All required fences, walls, and irrigation systems shall be permanently maintained in good condition and, whenever necessary, repaired or replaced.</p>		<ul style="list-style-type: none"> a. Prior to approval of construction-related permit. b. Prior to building permit final. c. Ongoing 	<ul style="list-style-type: none"> a. City of Oakland Bureau of Planning and Building b. City of Oakland Bureau of Building Services Division, Zoning Inspections c. City of Oakland Bureau of Building Services Division, Zoning Inspections

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring
		Schedule
		Responsibility
Aesthetics, Shadow, and Wind (cont.)		
SCA AES-3 (Standard Condition of Approval 18): Lighting		
Proposed new exterior lighting fixtures shall be adequately shielded to a point below the light bulb and reflector and that prevent unnecessary glare onto adjacent properties.		Prior to building permit final.
Air Quality		
SCA AIR-1 (Standard Condition of Approval 19): Construction-Related Air Pollution Controls (Dust and Equipment Emissions)		
<p>The project applicant shall implement all of the following applicable air pollution control measures during construction of the project:</p> <ul style="list-style-type: none"> a. Water all exposed surfaces of active construction areas at least twice daily (using reclaimed water if possible). Watering should be sufficient to prevent airborne dust from leaving the site. Increased watering frequency may be necessary whenever wind speeds exceed 15 miles per hour. Reclaimed water should be used whenever possible. b. Cover all trucks hauling soil, sand, and other loose materials or require all trucks to maintain at least two feet of freeboard (i.e., the minimum required space between the top of the load and the top of the trailer). c. All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited. d. Pave all roadways, driveways, sidewalks, etc., as soon as feasible. In addition, building pads should be laid as soon as possible after grading unless seeding or soil binders are used. e. Enclose, cover, water twice daily or apply (non-toxic) soil stabilizers to exposed stockpiles (dirt, sand, etc.). f. Limit vehicle speeds on unpaved roads to 15 miles per hour. g. Idling times on all diesel-fueled commercial vehicles over 10,000 lbs. shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to five minutes (as required by the California Airborne toxics control measure Title 13, Section 2485, of the California Code of Regulations). Clear signage to this effect shall be provided for construction workers at all access points. h. Idling times on all diesel-fueled off-road vehicles over 25 horsepower shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to five minutes and fleet operators must develop a written policy as required by Title 23, Section 2449, of the California Code of Regulations ("California Air Resources Board Off-Road Diesel Regulations"). i. All construction equipment shall be maintained and properly tuned in accordance with the manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation. j. Portable equipment shall be powered by electricity if available. If electricity is not available, propane or natural gas shall be used if feasible. Diesel engines shall only be used if electricity is not available and it is not feasible to use propane or natural gas. 		During construction.
		City of Oakland Bureau of Planning and Building

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
		Schedule	Responsibility
Air Quality (cont.)			
<p>k. All exposed surfaces shall be watered at a frequency adequate to maintain minimum soil moisture of 12 percent. Moisture content can be verified by lab samples or moisture probe.</p> <p>l. All excavation, grading, and demolition activities shall be suspended when average wind speeds exceed 20 mph.</p> <p>m. Install sandbags or other erosion control measures to prevent silt runoff to public roadways.</p> <p>n. Hydroseed or apply (non-toxic) soil stabilizers to inactive construction areas (previously graded areas inactive for one month or more).</p> <p>o. Designate a person or persons to monitor the dust control program and to order increased watering, as necessary, to prevent transport of dust offsite. Their duties shall include holidays and weekend periods when work may not be in progress.</p> <p>p. Install appropriate wind breaks (e.g., trees, fences) on the windward side(s) of actively disturbed areas of the construction site to minimize wind blown dust. Wind breaks must have a maximum 50 percent air porosity.</p> <p>q. Vegetative ground cover (e.g., fast-germinating native grass seed) shall be planted in disturbed areas as soon as possible and watered appropriately until vegetation is established.</p> <p>r. Activities such as excavation, grading, and other ground-disturbing construction activities shall be phased to minimize the amount of disturbed surface area at any one time.</p> <p>s. All trucks and equipment, including tires, shall be washed off prior to leaving the site.</p> <p>t. Site accesses to a distance of 100 feet from the paved road shall be treated with a 6 to 12 inch compacted layer of wood chips, mulch, or gravel.</p> <p>u. All equipment to be used on the construction site and subject to the requirements of Title 13, Section 2449, of the California Code of Regulations ("California Air Resources Board Off-Road Diesel Regulations") must meet emissions and performance requirements one year in advance of any fleet deadlines. Upon request by the City, the project applicant shall provide written documentation that fleet requirements have been met.</p> <p>v. Use low VOC (i.e., ROG) coatings beyond the local requirements (i.e., BAAQMD Regulation 8, Rule 3: Architectural Coatings).</p> <p>w. All construction equipment, diesel trucks, and generators shall be equipped with Best Available Control Technology for emission reductions of NOx and PM.</p> <p>x. Off-road heavy diesel engines shall meet the California Air Resources Board's most recent certification standard.</p> <p>y. Post a publicly-visible large on-site sign that includes the contact name and phone number for the project complaint manager responsible for responding to dust complaints and the telephone numbers of the City's Code Enforcement unit and the Bay Area Air Quality Management District. When contacted, the project complaint manager shall respond and take corrective action within 48 hours.</p>			

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/Monitoring	
Air Quality (cont.)		Schedule	Responsibility
<p>SCA AIR-2 (Standard Condition of Approval 20): Exposure to Air Pollution (Toxic Air Contaminants)</p> <p>a. Health Risk Reduction Measures</p> <p>Requirement: The project applicant shall incorporate appropriate measures into the project design in order to reduce the potential health risk due to exposure to toxic air contaminants. The project applicant shall choose <u>one</u> of the following methods:</p> <p>i. The project applicant shall retain a qualified air quality consultant to prepare a Health Risk Assessment (HRA) in accordance with California Air Resources Board (CARB) and Office of Environmental Health and Hazard Assessment requirements to determine the health risk of exposure of project residents/occupants/users to air pollutants. The HRA shall be submitted to the City for review and approval. If the HRA concludes that the health risk is at or below acceptable levels, then health risk reduction measures are not required. If the HRA concludes that the health risk exceeds acceptable levels, health risk reduction measures shall be identified to reduce the health risk to acceptable levels. Identified risk reduction measures shall be submitted to the City for review and approval and be included on the project drawings submitted for the construction-related permit or on other documentation submitted to the City.</p> <p>- or -</p> <p>ii. The project applicant shall incorporate the following health risk reduction measures into the project. These features shall be submitted to the City for review and approval and be included on the project drawings submitted for the construction-related permit or on other documentation submitted to the City:</p> <ul style="list-style-type: none"> • Installation of air filtration to reduce cancer risks and Particulate Matter (PM) exposure for residents and other sensitive populations in the project that are in close proximity to sources of air pollution. Air filter devices shall be rated MERV-13 or higher. As part of implementing this measure, an ongoing maintenance plan for the building's HVAC air filtration system shall be required. • Where appropriate, install passive electrostatic filtering systems, especially those with low air velocities (i.e., 1 mph). • Phasing of residential developments when proposed within 500 feet of freeways such that homes nearest the freeway are built last, if feasible. • The project shall be designed to locate sensitive receptors as far away as feasible from the source(s) of air pollution. Operable windows, balconies, and building air intakes shall be located as far away from these sources as feasible. If near a distribution center, residents shall be located as far away as feasible from a loading dock or where trucks concentrate to deliver goods. • Sensitive receptors shall be located on the upper floors of buildings, if feasible. • Planting trees and/or vegetation between sensitive receptors and pollution source, if feasible. Trees that are best suited to trapping PM shall be planted, including one or more of the following: Pine (<i>Pinus nigra</i> var. <i>martiana</i>), Cypress (<i>X Cupressocyparis leylandii</i>), Hybrid poplar (<i>Populus deltoides</i> X <i>trichocarpa</i>), and Redwood (<i>Sequoia sempervirens</i>). 		<p>a. Prior to approval of construction-related permit.</p> <p>b. ongoing</p>	<p>a. City of Oakland Bureau of Planning and Building; City of Oakland Bureau of Building Services Division, Zoning Inspections</p> <p>b. City of Oakland Bureau of Building Services Division, Zoning Inspections</p>

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring
Air Quality (cont.)	Schedule	Responsibility
<p>b. Maintenance of Health Risk Reduction Measures</p> <p>Requirement: The project applicant shall maintain, repair, and/or replace installed health risk reduction measures, including but not limited to the HVAC system (if applicable), on an ongoing and as-needed basis. Prior to occupancy, the project applicant shall prepare and then distribute to the building manager/operator an operation and maintenance manual for the HVAC system and filter including the maintenance and replacement schedule for the filter.</p> <p>NOTE: This measure has been implemented by the project applicant and no further action is required.</p> <p>SCA AIR-3 (Standard Condition of Approval 23): Asbestos in Structures</p> <p>Requirement: The project applicant shall comply with all applicable laws and regulations regarding demolition and renovation of Asbestos Containing Materials (ACM), including but not limited to California Code of Regulations, Title 8; California Business and Professions Code, Division 3; California Health and Safety Code sections 25915-25919.7; and Bay Area Air Quality Management District, Regulation 11, Rule 2, as may be amended. Evidence of compliance shall be submitted to the City upon request.</p> <p>See SCA TRA-4, Transportation and Parking Demand Management Plan. See Transportation and Circulation, below.</p> <p>Biological Resources</p> <p>SCA BIO-1 (Standard Condition of Approval 26): Tree Removal During Bird Nesting Season</p> <p>To the extent feasible, removal of any tree and/or other vegetation suitable for nesting of birds shall not occur during the bird breeding season of February 1 to August 15 (or during December 15 to August 15 for trees located in or near marsh, wetland, or aquatic habitats). If tree removal must occur during the bird breeding season, all trees to be removed shall be surveyed by a qualified biologist to verify the presence or absence of nesting raptors or other birds. Pre-removal surveys shall be conducted within 15 days prior to the start of work and shall be submitted to the City for review and approval. If the survey indicates the potential presence of nesting raptors or other birds, the</p>	<p>Prior to approval of construction-related permit</p>	<p>Applicable regulatory agency with jurisdiction</p>
<p>SCA BIO-1 (Standard Condition of Approval 26): Tree Removal During Bird Nesting Season</p> <p>To the extent feasible, removal of any tree and/or other vegetation suitable for nesting of birds shall not occur during the bird breeding season of February 1 to August 15 (or during December 15 to August 15 for trees located in or near marsh, wetland, or aquatic habitats). If tree removal must occur during the bird breeding season, all trees to be removed shall be surveyed by a qualified biologist to verify the presence or absence of nesting raptors or other birds. Pre-removal surveys shall be conducted within 15 days prior to the start of work and shall be submitted to the City for review and approval. If the survey indicates the potential presence of nesting raptors or other birds, the</p>	<p>Prior to removal of trees.</p>	<p>City of Oakland Public Works Department, Tree Division; Bureau of Buildings</p>

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
		Schedule	Responsibility
Biological Resources (cont.)			
<p>biologist shall determine an appropriately sized buffer around the nest in which no work will be allowed until the young have successfully fledged. The size of the nest buffer will be determined by the biologist in consultation with the California Department of Fish and Wildlife, and will be based to a large extent on the nesting species and its sensitivity to disturbance. In general, buffer sizes of 200 feet for raptors and 50 feet for other birds should suffice to prevent disturbance to birds nesting in the urban environment, but these buffers may be increased or decreased, as appropriate, depending on the bird species and the level of disturbance anticipated near the nest.</p>			
<p>SCA BIO-2 (Standard Condition of Approval 27): Tree Permit</p> <p><i>a. Tree Permit Required</i></p> <p>Pursuant to the City's Tree Protection Ordinance (OMC chapter 12.36), the project applicant shall obtain a tree permit and abide by the conditions of that permit.</p> <p><i>b. Tree Protection During Construction</i></p> <p><u>Requirement:</u> Adequate protection shall be provided during the construction period for any trees which are to remain standing, including the following, plus any recommendations of an arborist:</p> <ol style="list-style-type: none"> i. Before the start of any clearing, excavation, construction, or other work on the site, every protected tree deemed to be potentially endangered by said site work shall be securely fenced off at a distance from the base of the tree to be determined by the project's consulting arborist. Such fences shall remain in place for duration of all such work. All trees to be removed shall be clearly marked. A scheme shall be established for the removal and disposal of logs, brush, earth and other debris which will avoid injury to any protected tree. ii. Where proposed development or other site work is to encroach upon the protected perimeter of any protected tree, special measures shall be incorporated to allow the roots to breathe and obtain water and nutrients. Any excavation, cutting, filing, or compaction of the existing ground surface within the protected perimeter shall be minimized. No change in existing ground level shall occur within a distance to be determined by the project's consulting arborist from the base of any protected tree at any time. No burning or use of equipment with an open flame shall occur near or within the protected perimeter of any protected tree. iii. No storage or dumping of oil, gas, chemicals, or other substances that may be harmful to trees shall occur within the distance to be determined by the project's consulting arborist from the base of any protected trees, or any other location on the site from which such substances might enter the protected perimeter. No heavy construction equipment or construction materials shall be operated or stored within a distance from the base of any protected trees to be determined by the project's consulting arborist. Wires, ropes, or other devices shall not be attached to any protected tree, except as needed for support of the tree. No sign, other than a tag showing the botanical classification, shall be attached to any protected tree. iv. Periodically during construction, the leaves of protected trees shall be thoroughly sprayed with water to prevent buildup of dust and other pollution that would inhibit leaf transpiration. 			
		<ol style="list-style-type: none"> a. Prior to approval of construction-related permit b. During construction. c. Prior to building permit final. 	<ol style="list-style-type: none"> a. City of Oakland Public Works Department, Tree Division; Bureau of Buildings b. City of Oakland Public Works Department, Tree Division; Bureau of Buildings c. <i>Initial Approval:</i> Public Works Department, Tree Division <i>Monitoring/Inspection:</i> Bureau of Building

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
		Schedule	Responsibility
Biological Resources (cont.)			
<p>v. If any damage to a protected tree should occur during or as a result of work on the site, the project applicant shall immediately notify the Public Works Department and the project's consulting arborist shall make a recommendation to the City Tree Reviewer as to whether the damaged tree can be preserved.</p> <p>If, in the professional opinion of the Tree Reviewer, such tree cannot be preserved in a healthy state, the Tree Reviewer shall require replacement of any tree removed with another tree or trees on the same site deemed adequate by the Tree Reviewer to compensate for the loss of the tree that is removed.</p> <p>vi. All debris created as a result of any tree removal work shall be removed by the project applicant from the property within two weeks of debris creation, and such debris shall be properly disposed of by the project applicant in accordance with all applicable laws, ordinances, and regulations.</p> <p>c. <i>Tree Replacement Plantings</i> Requirement: Replacement plantings shall be required for tree removals for the purposes of erosion control, groundwater replenishment, visual screening, wildlife habitat, and preventing excessive loss of shade, in accordance with the following criteria:</p> <p>i. No tree replacement shall be required for the removal of nonnative species, for the removal of trees which is required for the benefit of remaining trees, or where insufficient planting area exists for a mature tree of the species being considered.</p> <p>ii. Replacement tree species shall consist of Sequoia sempervirens (Coast Redwood), Quercus agrifolia (Coast Live Oak), Arbutus menziesii (Madrone), Aesculus californica (California Buckeye), Umbellularia californica (California Bay Laurel), or other tree species acceptable to the Tree Division.</p> <p>iii. Replacement trees shall be at least twenty-four (24) inch box size, unless a smaller size is recommended by the arborist, except that three fifteen (15) gallon size trees may be substituted for each twenty-four (24) inch box size tree where appropriate.</p> <p>iv. Minimum planting areas must be available on site as follows:</p> <ul style="list-style-type: none"> • For Sequoia sempervirens, three hundred fifteen (315) square feet per tree; • For other species listed, seven hundred (700) square feet per tree. <p>v. In the event that replacement trees are required but cannot be planted due to site constraints, an in lieu fee in accordance with the City's Master Fee Schedule may be substituted for required replacement plantings, with all such revenues applied toward tree planting in city parks, streets and medians.</p> <p>vi. The project applicant shall install the plantings and maintain the plantings until established. The Tree Reviewer of the Tree Division of the Public Works Department may require a landscape plan showing the replacement plantings and the method of irrigation. Any replacement plantings which fail to become established within one year of planting shall be replanted at the project applicant's expense.</p>			

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	Responsibility
		Schedule	
Cultural Resources			
<p>SCA CUL-1 (Standard Condition of Approval 29): <i>Archaeological and Paleontological Resources – Discovery During Construction</i></p> <p><u>Requirement:</u> Pursuant to CEQA Guidelines section 15064.5(f), in the event that any historic or prehistoric subsurface cultural resources are discovered during ground disturbing activities, all work within 50 feet of the resources shall be halted and the project applicant shall notify the City and consult with a qualified archaeologist or paleontologist, as applicable, to assess the significance of the find. In the case of discovery of paleontological resources, the assessment shall be done in accordance with the Society of Vertebrate Paleontology standards. If any find is determined to be significant, appropriate avoidance measures recommended by the consultant and approved by the City must be followed unless avoidance is determined unnecessary or infeasible by the City. Feasibility of avoidance shall be determined with consideration of factors such as the nature of the find, project design, costs, and other considerations. If avoidance is unnecessary or infeasible, other appropriate measures (e.g., data recovery, excavation) shall be instituted. Work may proceed on other parts of the project site while measures for the cultural resources are implemented.</p> <p>In the event of data recovery of archaeological resources, the project applicant shall submit an Archaeological Research Design and Treatment Plan (ARDTP) prepared by a qualified archaeologist for review and approval by the City. The ARDTP is required to identify how the proposed data recovery program would preserve the significant information the archaeological resource is expected to contain. The ARDTP shall identify the scientific/historic research questions applicable to the expected resource, the data classes the resource is expected to possess, and how the expected data classes would address the applicable research questions. The ARDTP shall include the analysis and specify the curation and storage methods. Data recovery, in general, shall be limited to the portions of the archaeological resource that could be impacted by the proposed project. Destructive data recovery methods shall not be applied to portions of the archaeological resources if nondestructive methods are practicable. Because the intent of the ARDTP is to save as much of the archaeological resource as possible, including moving the resource, if feasible, preparation and implementation of the ARDTP would reduce the potential adverse impact to less than significant. The project applicant shall implement the ARDTP at his/her expense.</p> <p>In the event of excavation of paleontological resources, the project applicant shall submit an excavation plan prepared by a qualified paleontologist to the City for review and approval. All significant cultural materials recovered shall be subject to scientific analysis, professional museum curation, and/or a report prepared by a qualified paleontologist, as appropriate, according to current professional standards and at the expense of the project applicant.</p>		<p>During construction.</p>	<p>City of Oakland Bureau of Building Services Division</p>
<p>SCA CUL-2 (Standard Condition of Approval SCA 31): <i>Human Remains – Discovery During Construction</i></p> <p><u>Requirement:</u> Pursuant to CEQA Guidelines section 15064.5(e)(1), in the event that human skeletal remains are uncovered at the project site during construction activities, all work shall immediately halt and the project applicant shall notify the City and the Alameda County Coroner. If the County Coroner determines that an investigation of the cause of death is required or that the remains are Native American, all work shall cease within 50 feet of the remains until appropriate arrangements are made. In the event that the remains are Native American, the City shall contact the California Native American Heritage Commission (NAHC), pursuant to subdivision (c) of section 7050.5 of the California Health and Safety Code. If the agencies determine that avoidance is not feasible, then an alternative plan shall be prepared with specific steps and timeframe required to resume construction activities. Monitoring, data recovery, determination of significance, and avoidance measures (if applicable) shall be completed expeditiously and at the expense of the project applicant.</p>		<p>During construction.</p>	<p>City of Oakland Bureau of Building Services Division, Zoning Inspections</p>

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring
Cultural Resources (cont.)		Schedule
Cultural Resources (cont.)		Responsibility
<p>See SCA NOI-7, Vibration Impacts on Adjacent Historic Structures or Vibration-Sensitive Activities. See <i>Noise</i>, below.</p> <p>BVDSP Mitigation Measure CUL-1:</p> <p><i>a) Avoidance, Adaptive Reuse, or Appropriate Relocation of Historically Significant Structures.</i></p> <ul style="list-style-type: none"> • <i>Avoidance.</i> The City shall ensure, where feasible, that all future development activities allowable under the Specific Plan, including demolition, alteration, and new construction, would avoid historical resources (i.e., those listed on federal, state, and local registers). • <i>Adaptive Reuse.</i> If avoidance is not feasible, adaptive reuse and rehabilitation of historical resources shall occur in accordance with the <i>Secretary of Interior's Standards for the Treatment of Historic Properties.</i> • <i>Appropriate Relocation.</i> If avoidance or adaptive reuse <i>in situ</i> is not feasible, SCA 56, <i>Compliance with Policy 3.7 of the Historic Preservation Element (Property Relocation Rather than Demolition)</i>, shall be implemented, as required. Projects that relocate the affected historical property to a location consistent with its historic or architectural character could reduce the impact less than significant (Historic Preservation Element Action 3.8.1), unless the property's location is an integral part of its significance, e.g., a contributor to a historic district. <p><i>b) Future Site-specific Surveys and Evaluations.</i></p> <p>Although the Plan Area has been surveyed by the City of Oakland's OCHS and as part of the Broadway Valdez Specific Plan effort by ESA in 2009, evaluations and ratings may change with time and other conditions. There may be previously unidentified historical resources which would be affected by future development activities. For any future projects on or immediately adjacent to buildings 50 years old or older between 2013 and 2038, which is the build-out horizon for the Specific Plan (i.e., by the end of the Plan period, buildings constructed prior to 1988), the City shall require specific surveys and evaluations of such properties to determine their potential historical significance at the federal, state, and local levels. Intensive-level surveys and evaluations shall be completed by a qualified architectural historian who meets the <i>Secretary of the Interior's Standards</i>. For all historical resources identified as a result of site-specific surveys and evaluations, the City shall ensure that future development activities avoid, adaptively reuse and/or appropriately relocate such historical resources in accordance with measure "a" (Avoidance, Adaptive Reuse, or Appropriate Relocation of Historically Significant Structures), above. Site-specific surveys and evaluations that are more than 5 years old shall be updated to account for changes which may have occurred over time.</p> <p><i>c) Recordation and Public Interpretation.</i></p> <p>If measure "a" (Avoidance, Adaptive Reuse, or Appropriate Relocation of Historically Significant Structures) is determined infeasible as part of a future project, the City shall evaluate the feasibility and appropriateness of recordation and public interpretation of such resources prior to any construction activities which would directly affect them. Should City staff decide recordation and or public interpretation is required, the following activities would be performed:</p>	<p>Prior to issuance of a demolition permit</p>	<p>City of Oakland - Building Services Division, Zoning Inspection</p>

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring			
		Schedule	Responsibility		
<p>Cultural Resources (cont.)</p> <ul style="list-style-type: none"> <p><i>Recordation.</i> Recordation shall follow the standards provided in the National Park Service's Historic American Building Survey (HABS) program, which requires photo-documentation of historic structures, a written report, and/or measured drawings (or photo reproduction of original plans if available). The photographs and report would be archived at the Oakland Planning Department and local repositories, such as public libraries, historical societies, and/or the Northwest Information Center at Sonoma State University. The recordation efforts shall occur prior to demolition, alteration, or relocation of any historic resources identified in the Plan Area, including those that are relocated pursuant to measure "a" (Avoidance, Adaptive Reuse, or Appropriate Relocation of Historically Significant Structures). Additional recordation could include (as appropriate) oral history interviews or other documentation (e.g., video) of the resource.</p> <p><i>Public Interpretation.</i> A public interpretation or art program would be developed by a qualified historic consultant or local artist in consultation with the Landmarks Preservation Advisory Board and City staff, based on a City-approved scope of work and submitted to the City for review and approval. The program could take the form of plaques, commemorative markers, or artistic or interpretive displays which explain the historical significance of the properties to the general public. Such displays would be incorporated into project plans as they are being developed, and would typically be located in a publicly accessible location on or near the site of the former historical resource(s). Public interpretation displays shall be installed prior to completion of any construction projects in the Plan Area.</p> <p>Photographic recordation and public interpretation of historically significant properties does not typically mitigate the loss of resources to a less-than-significant level [CEQA Section 15126.4(b)(2)].</p> <p>d) Financial Contributions.</p> <p>If measure "a" (Avoidance, Adaptive Reuse, or Appropriate Relocation of Historically Significant Structures) and measure "b" (Future Site-specific Surveys and Evaluations) are not satisfied, the project applicant shall make a financial contribution to the City of Oakland, which can be used to fund other historic preservation projects within the Plan Area or in the immediate vicinity. Such programs include, without limitation, a Façade Improvement Program or a Property Relocation Assistance Program.</p> <p>This mitigation would conform to Action 3.8.1(9) of the Historic Preservation Element of the City of Oakland General Plan. Contributions to the fund(s) shall be determined by staff at the time of approval of site-specific project plans based on a formula to be determined by the Landmarks Preservation Advisory Board. However, such financial contribution, even in conjunction with measure "c" (Recordation and Public Interpretation), would not reduce the impacts to less-than-significant levels.</p> <p>Geology, Soils, and Geohazards</p> <p>SCA GEO-1 (Standard Condition of Approval 33): Construction-Related Permit(s)</p> <p>Requirement: The project applicant shall obtain all required construction-related permits/approvals from the City. The project shall comply with all standards, requirements and conditions contained in construction-related codes, including but not limited to the Oakland Building Code and the Oakland Grading Regulations, to ensure structural integrity and safe construction.</p>				<p>Prior to approval of construction-related permit.</p>	<p>City of Oakland Bureau of Building Services Division, Zoning Inspections</p>

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
		Schedule	Responsibility
Geology, Soils, and Geohazards (cont.)			
<p>SCA GEO-2 (Standard Condition of Approval 34): Soils Report Requirement: The project applicant shall submit a soils report prepared by a registered geotechnical engineer for City review and approval. The soils report shall contain, at a minimum, field test results and observations regarding the nature, distribution and strength of existing soils, and recommendations for appropriate grading practices and project design. The project applicant shall implement the recommendations contained in the approved report during project design and construction.</p>		Prior to approval of construction-related permit.	City of Oakland Bureau of Building Services Division, Zoning Inspections
Greenhouse Gases and Climate Change			
<p>SCA GHG-1 (Standard Condition of Approval 38): Greenhouse Gas (GHG) Reduction Plan</p> <p>a. Greenhouse Gas (GHG) Reduction Plan Required Requirement: The project applicant shall retain a qualified air quality consultant to develop a Greenhouse Gas (GHG) Reduction Plan for City review and approval and shall implement the approved GHG Reduction Plan. The goal of the GHG Reduction Plan shall be to increase energy efficiency and reduce GHG emissions to below at least one of the Bay Area Quality Management District's (BAAQMD's) CEQA Thresholds of Significance (1,100 metric tons of CO₂e per year or 4.6 metric tons of CO₂e per service population). The GHG Reduction Plan shall include, at a minimum, (a) a detailed GHG emissions inventory for the project under a "business-as-usual" scenario with no consideration of project design features, or other energy efficiencies, (b) an "adjusted" baseline GHG emissions inventory for the project, taking into consideration energy efficiencies included as part of the project (including the City's Standard Conditions of Approval, proposed mitigation measures, project design features, and other City requirements), (c) a comprehensive set of quantified additional GHG reduction measures available to further reduce GHG emissions beyond the adjusted GHG emissions, and (d) requirements for ongoing monitoring and reporting to demonstrate that the additional GHG reduction measures are being implemented. If the project is to be constructed in phases, the GHG Reduction Plan shall provide GHG emission scenarios by phase.</p> <p>Potential GHG reduction measures to be considered include, but are not be limited to, measures recommended in BAAQMD's latest CEQA Air Quality Guidelines, the California Air Resources Board Scoping Plan (December 2008, as may be revised), the California Air Pollution Control Officers Association (CAPCOA) Quantifying Greenhouse Gas Mitigation Measures (August 2010, as may be revised), the California Attorney General's website, and Reference Guides on Leadership in Energy and Environmental Design (LEED) published by the U.S. Green Building Council.</p> <p>The types of allowable GHG reduction measures include the following (listed in order of City preference): (1) physical design features; (2) operational features; and (3) the payment of fees to fund GHG-reducing programs (i.e., the purchase of "carbon credits") as explained below.</p> <p>The allowable locations of the GHG reduction measures include the following (listed in order of City preference): (1) the project site; (2) off-site within the City of Oakland; (3) off-site within the San Francisco Bay Area Air Basin; (4) off-site within the State of California; then (5) elsewhere in the United States.</p> <p>As with preferred locations for the implementation of all GHG reductions measures, the preference for carbon credit purchases include those that can be achieved as follows (listed in order of City preference): (1) within the</p>		<p>a. Prior to approval of construction-related permit.</p> <p>b. During construction.</p> <p>c. Ongoing.</p>	<p>a. City of Oakland Bureau of Planning and Building</p> <p>b. City of Oakland Bureau of Planning and Building</p> <p>c. City of Oakland Bureau of Planning and Building</p>

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
		Schedule	Responsibility
Greenhouse Gases and Climate Change (cont.)			
<p>City of Oakland; (2) within the San Francisco Bay Area Air Basin; (3) within the State of California; then (4) elsewhere in the United States. The cost of carbon credit purchases shall be based on current market value at the time purchased and shall be based on the project's operational emissions estimated in the GHG Reduction Plan or subsequent approved emissions inventory, which may result in emissions that are higher or lower than those estimated in the GHG Reduction Plan.</p> <p>For physical GHG reduction measures to be incorporated into the design of the project, the measures shall be included on the drawings submitted for construction-related permits.</p>			
<p>b. <i>GHG Reduction Plan Implementation During Construction</i></p> <p><u>Requirement:</u> The project applicant shall implement the GHG Reduction Plan during construction of the project. For physical GHG reduction measures to be incorporated into the design of the project, the measures shall be implemented during construction. For physical GHG reduction measures to be incorporated into off-site projects, the project applicant shall obtain all necessary permits/approvals and the measures shall be included on drawings and submitted to the City Planning Director or his/her designee for review and approval. These off-site improvements shall be installed prior to completion of the subject project (or prior to completion of the project phase for phased projects). For GHG reduction measures involving the purchase of carbon credits, evidence of the payment/purchase shall be submitted to the City for review and approval prior to completion of the project (or prior to completion of the project phase, for phased projects).</p>			
<p>c. <i>GHG Reduction Plan Implementation After Construction</i></p> <p><u>Requirement:</u> The project applicant shall implement the GHG Reduction Plan after construction of the project (or at the completion of the project phase for phased projects). For operational GHG reduction measures to be incorporated into the project or off-site projects, the measures shall be implemented on an indefinite and ongoing basis.</p> <p>The project applicant shall satisfy the following requirements for ongoing monitoring and reporting to demonstrate that the additional GHG reduction measures are being implemented. The GHG Reduction Plan requires regular periodic evaluation over the life of the project (generally estimated to be at least 40 years) to determine how the Plan is achieving required GHG emissions reductions over time, as well as the efficacy of the specific additional GHG reduction measures identified in the Plan.</p> <p>Annual Report. Implementation of the GHG reduction measures and related requirements shall be ensured through compliance with Conditions of Approval adopted for the project. Generally, starting two years after the City issues the first Certificate of Occupancy for the project, the project applicant shall prepare each year of the useful life of the project an Annual GHG Emissions Reduction Report ("Annual Report"), for review and approval by the City Planning Director or his/her designee. The Annual Report shall be submitted to an independent reviewer of the City's choosing, to be paid for by the project applicant.</p> <p>The Annual Report shall summarize the project's implementation of GHG reduction measures over the preceding year, intended upcoming changes, compliance with the conditions of the Plan, and include a brief summary of the previous year's Annual Report results (starting the second year). The Annual Report shall include a comparison of annual project emissions to the baseline emissions reported in the GHG Plan.</p>			

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
		Schedule	Responsibility
Greenhouse Gases and Climate Change (cont.)			
<p>The GHG Reduction Plan shall be considered fully attained when project emissions are less than either applicable numeric BAAQMD CEQA Thresholds, as confirmed by the City through an established monitoring program. Monitoring and reporting activities will continue at the City's discretion, as discussed below.</p> <p>Corrective Procedure. If the third Annual Report, or any report thereafter, indicates that, in spite of the implementation of the GHG Reduction Plan, the project is not achieving the GHG reduction goal, the project applicant shall prepare a report for City review and approval, which proposes additional or revised GHG measures to better achieve the GHG emissions reduction goals, including without limitation, a discussion on the feasibility and effectiveness of the menu of other additional measures ("Corrective GHG Action Plan"). The project applicant shall then implement the approved Corrective GHG Action Plan.</p> <p>If, one year after the Corrective GHG Action Plan is implemented, the required GHG emissions reduction target is still not being achieved, or if the project applicant fails to submit a report at the times described above, or if the reports do not meet City requirements outlined above, the City may, in addition to its other remedies, (a) assess the project applicant a financial penalty based upon actual percentage reduction in GHG emissions as compared to the percent reduction in GHG emissions established in the GHG Reduction Plan; or (b) refer the matter to the City Planning Commission for scheduling of a compliance hearing to determine whether the project's approvals should be revoked, altered or additional conditions of approval imposed.</p> <p>The penalty as described in (a) above shall be determined by the City Planning Director or his/her designee and be commensurate with the percentage GHG emissions reduction not achieved (compared to the applicable numeric significance thresholds) or required percentage reduction from the "adjusted" baseline.</p> <p>In determining whether a financial penalty or other remedy is appropriate, the City shall not impose a penalty if the project applicant has made a good faith effort to comply with the GHG Reduction Plan.</p> <p>The City would only have the ability to impose a monetary penalty after a reasonable cure period and in accordance with the enforcement process outlined in Planning Code Chapter 17.152. If a financial penalty is imposed, such penalty sums shall be used by the City solely toward the implementation of the GHG Reduction Plan.</p> <p>Timeline Discretion and Summary. The City shall have the discretion to reasonably modify the timing of reporting, with reasonable notice and opportunity to comment by the applicant, to coincide with other related monitoring and reporting required for the project.</p>			
<p>See SCA AES-2, Landscape Plan. See <i>Aesthetics, Wind, and Shadow</i>, above.</p>			
<p>See SCA AIR-1, Construction-Related Air Pollution Controls (Dust and Equipment Emissions). See <i>Air Quality</i>, above.</p>			
<p>See SCA UTIL-1, Construction and Demolition Waste Reduction and Recycling. See <i>Utilities and Service Systems</i>, below.</p>			
<p>See SCA UTIL-4, Green Building Requirements. See <i>Utilities and Service Systems</i>, below.</p>			
<p>See SCA TRA-4, Transportation and Parking Demand Management Plan. See <i>Transportation and Circulation</i>, below.</p>			

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
		Schedule	Responsibility
<p>Hazards and Hazardous Materials</p> <p>SCA HAZ-1 (Standard Condition of Approval 39): Hazardous Materials Related to Construction Requirement: The project applicant shall ensure that Best Management Practices (BMPs) are implemented by the contractor during construction to minimize potential negative effects on groundwater, soils, and human health. These shall include, at a minimum, the following:</p> <ul style="list-style-type: none"> a. Follow manufacturer's recommendations for use, storage, and disposal of chemical products used in construction; b. Avoid overtopping construction equipment fuel gas tanks; c. During routine maintenance of construction equipment, properly contain and remove grease and oils; d. Properly dispose of discarded containers of fuels and other chemicals; e. Implement lead-safe work practices and comply with all local, regional, state, and federal requirements concerning lead (for more information refer to the Alameda County Lead Poisoning Prevention Program); and f. If soil, groundwater, or other environmental medium with suspected contamination is encountered unexpectedly during construction activities (e.g., identified by odor or visual staining, or if any underground storage tanks, abandoned drums or other hazardous materials or wastes are encountered), the project applicant shall cease work in the vicinity of the suspect material, the area shall be secured as necessary, and the applicant shall take all appropriate measures to protect human health and the environment. Appropriate measures shall include notifying the City and applicable regulatory agency(ies) and implementation of the actions described in the City's Standard Conditions of Approval, as necessary, to identify the nature and extent of contamination. Work shall not resume in the area(s) affected until the measures have been implemented under the oversight of the City or regulatory agency, as appropriate. 		<p>During construction.</p>	<p>City of Oakland Bureau of Building Services Division, Zoning Inspections</p>
<p>SCA HAZ-2 (Standard Condition of Approval 40): Hazardous Building Materials and Site Contamination</p> <p>a. Hazardous Building Materials Assessment Requirement: The project applicant shall submit a comprehensive assessment report to the Bureau of Building signed by a qualified environmental professional, documenting the presence or lack thereof of asbestos-containing materials (ACMs), lead-based paint, polychlorinated biphenyls (PCBs), and any other building materials or stored materials classified as hazardous materials by State or federal law. If lead-based paint, ACMs, PCBs, or any other building materials or stored materials classified as hazardous materials are present, the project applicant shall submit specifications prepared and signed by a qualified environmental professional, for the stabilization and/or removal of the identified hazardous materials in accordance with all applicable laws and regulations. The project applicant shall implement the approved recommendations and submit to the City evidence of approval for any proposed remedial action and required clearances by the applicable local, state, or federal regulatory agency.</p>		<ul style="list-style-type: none"> a. Prior to approval of construction-related permit b. Prior to approval of construction-related permit c. Prior to approval of construction-related permit d. During Construction 	<ul style="list-style-type: none"> a. City of Oakland Bureau of Building Services Division, Zoning Inspections b. Applicable regulatory agency with jurisdiction c. City of Oakland Bureau of Building Services Division, Zoning Inspections d. City of Oakland Bureau of Building Services Division, Zoning Inspections

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
Hazards and Hazardous Materials (cont.)	Schedule	Responsibility	Responsibility
<p>b. Environmental Site Assessment Required Requirement: The project applicant shall submit a Phase I Environmental Site Assessment report, and Phase II Environmental Site Assessment report if warranted by the Phase I report, for the project site for review and approval by the City. The report(s) shall be prepared by a qualified environmental assessment professional and include recommendations for remedial action, as appropriate, for hazardous materials. The project applicant shall implement the approved recommendations and submit to the City evidence of approval for any proposed remedial action and required clearances by the applicable local, state, or federal regulatory agency.</p> <p>c. Health and Safety Plan Required Requirement: The project applicant shall submit a Health and Safety Plan for the review and approval by the City in order to protect project construction workers from risks associated with hazardous materials. The project applicant shall implement the approved Plan.</p> <p>d. Best Management Practices (BMPs) Required for Contaminated Sites Requirement: The project applicant shall ensure that Best Management Practices (BMPs) are implemented by the contractor during construction to minimize potential soil and groundwater hazards. These shall include the following:</p> <ul style="list-style-type: none"> i. Soil generated by construction activities shall be stockpiled on-site in a secure and safe manner. All contaminated soils determined to be hazardous or non-hazardous waste must be adequately profiled (sampled) prior to acceptable reuse or disposal at an appropriate off-site facility. Specific sampling and handling and transport procedures for reuse or disposal shall be in accordance with applicable local, state, and federal requirements. ii. Groundwater pumped from the subsurface shall be contained on-site in a secure and safe manner, prior to treatment and disposal, to ensure environmental and health issues are resolved pursuant to applicable laws and policies. Engineering controls shall be utilized, which include impermeable barriers to prohibit groundwater and vapor intrusion into the building. 			
See SCA AIR-5, Asbestos in Structures. See Air Quality, above.			
Hydrology and Water Quality			
<p>SCA HYD-1 (Standard Condition of Approval 45): Erosion and Sedimentation Control Plan for Construction</p> <p>a. Erosion and Sedimentation Control Plan Required Requirement: The project applicant shall submit an Erosion and Sedimentation Control Plan to the City for review and approval. The Erosion and Sedimentation Control Plan shall include all necessary measures to be taken to prevent excessive stormwater runoff or carrying by stormwater runoff of solid materials on to lands of adjacent property owners, public streets, or to creeks as a result of conditions created by grading and/or construction operations. The Plan shall include, but not be limited to, such measures as short-term erosion control planting, waterproof slope covering, check dams, interceptor ditches, benches, storm drains,</p>		<ul style="list-style-type: none"> a. Prior to approval of construction-related permit. b. During construction. 	City of Oakland Bureau of Building Services Division, Zoning Inspections

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
		Schedule	Responsibility
Hydrology and Water Quality (cont.)			
<p>dissipation structures, diversion dikes, retarding berms and barriers, devices to trap, store and filter out sediment, and stormwater retention basins. Off-site work by the project applicant may be necessary. The project applicant shall obtain permission or easements necessary for off-site work. There shall be a clear notation that the plan is subject to changes as changing conditions occur. Calculations of anticipated stormwater runoff and sediment volumes shall be included, if required by the City. The Plan shall specify that after construction is complete, the project applicant shall ensure that the storm drain system shall be inspected and that the project applicant shall clear the system of any debris or sediment.</p> <p>b. Erosion and Sedimentation Control During Construction</p> <p>Requirement: The project applicant shall implement the approved Erosion and Sedimentation Control Plan. No grading shall occur during the wet weather season (October 15 through April 15) unless specifically authorized in writing by the Bureau of Building.</p> <p>SCA HYD-2 (Standard Condition of Approval 46): State Construction General Permit.</p> <p>Requirement: The project applicant shall comply with the requirements of the Construction General Permit issued by the State Water Resources Control Board (SWRCB). The project applicant shall submit a Notice of Intent (NOI), Stormwater Pollution Prevention Plan (SWPPP), and other required Permit Registration Documents to SWRCB. The project applicant shall submit evidence of compliance with Permit requirements to the City.</p> <p>SCA HYD-3 (Standard Condition of Approval 50): NPDES C.3 Stormwater Requirements for Regulated Projects</p> <p>a. Post-Construction Stormwater Management Plan Required</p> <p>Requirement: The project applicant shall comply with the requirements of Provision C.3 of the Municipal Regional Stormwater Permit issued under the National Pollutant Discharge Elimination System (NPDES). The project applicant shall submit a Post-Construction Stormwater Management Plan to the City for review and approval with the project drawings submitted for site improvements, and shall implement the approved Plan during construction. The Post-Construction Stormwater Management Plan shall include and identify the following:</p> <ul style="list-style-type: none"> i. Location and size of new and replaced impervious surface; ii. Directional surface flow of stormwater runoff; iii. Location of proposed on-site storm drain lines; iv. Site design measures to reduce the amount of impervious surface area; v. Source control measures to limit stormwater pollution; vi. Stormwater treatment measures to remove pollutants from stormwater runoff, including the method used to hydraulically size the treatment measures; and vii. Hydromodification management measures, if required by Provision C.3, so that post-project stormwater runoff flow and duration match pre-project runoff. 		<p>a. Prior to approval of construction-related permit.</p> <p>b. Prior to building permit final.</p>	<p>a. City of Oakland Bureau of Building Services Division, Zoning Inspections; City of Oakland Bureau of Planning and Building</p> <p>b. City of Oakland Bureau of Building Services Division, Zoning Inspections</p>

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring
Hydrology and Water Quality (cont.)	Schedule	Responsibility
<p>b. Maintenance Agreement Required</p> <p>Requirement: The project applicant shall enter into a maintenance agreement with the City, based on the Standard City of Oakland Stormwater Treatment Measures Maintenance Agreement, in accordance with Provision C.3, which provides, in part, for the following:</p> <ul style="list-style-type: none"> i. The project applicant accepting responsibility for the adequate installation/construction, operation, maintenance, inspection, and reporting of any on-site stormwater treatment measures being incorporated into the project until the responsibility is legally transferred to another entity; and ii. Legal access to the on-site stormwater treatment measures for representatives of the City, the local vector control district, and staff of the Regional Water Quality Control Board, San Francisco Region, for the purpose of verifying the implementation, operation, and maintenance of the on-site stormwater treatment measures and to take corrective action if necessary. <p>The maintenance agreement shall be recorded at the County Recorder's Office at the applicant's expense.</p>	<p style="text-align: center;">Noise</p> <p>SCA NOI-1 (Standard Condition of Approval 58): Construction Days/Hours</p> <p>Requirement: The project applicant shall comply with the following restrictions concerning construction days and hours:</p> <ul style="list-style-type: none"> a. Construction activities are limited to between 7:00 a.m. and 7:00 p.m. Monday through Friday, except that pier drilling and/or other extreme noise generating activities greater than 90 dBA shall be limited to between 8:00 a.m. and 4:00 p.m. b. Construction activities are limited to between 9:00 a.m. and 5:00 p.m. on Saturday. In residential zones and within 300 feet of a residential zone, construction activities are allowed from 9:00 a.m. to 5:00 p.m. only within the interior of the building with the doors and windows closed. No pier drilling or other extreme noise generating activities greater than 90 dBA are allowed on Saturday. c. No construction is allowed on Sunday or federal holidays. <p>Construction activities include, but are not limited to, truck idling, moving equipment (including trucks, elevators, etc.) or materials, deliveries, and construction meetings held on-site in a non-enclosed area.</p> <p>Any construction activity proposed outside of the above days and hours for special activities (such as concrete pouring which may require more continuous amounts of time) shall be evaluated on a case-by-case basis by the City, with criteria including the urgency/emergency nature of the work, the proximity of residential or other sensitive uses, and a consideration of nearby residents'/occupants' preferences. The project applicant shall notify property owners and occupants located within 300 feet at least 14 calendar days prior to construction activity proposed outside of the above days/hours. When submitting a request to the City to allow construction activity outside of the above days/hours, the project applicant shall submit information concerning the type and duration of proposed construction activity and the draft public notice for City review and approval prior to distribution of the public notice.</p>	<p style="text-align: center;">City of Oakland Bureau of Building Services Division, Zoning Inspections</p>

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring
Noise (cont.)	Schedule	Responsibility
<p>SCA NOI-2 (Standard Condition of Approval 59): Construction Noise Requirement: The project applicant shall implement noise reduction measures to reduce noise impacts due to construction. Noise reduction measures include, but are not limited to, the following:</p> <ul style="list-style-type: none"> a. Equipment and trucks used for project construction shall utilize the best available noise control techniques (e.g., improved mufflers, equipment redesign, use of intake silencers, ducts, engine enclosures and acoustically-attenuating shields or shrouds) wherever feasible. Except as provided herein, impact tools (e.g., jack hammers, pavement breakers, and rock drills) used for project construction shall be hydraulically or electrically powered to avoid noise associated with compressed air exhaust from pneumatically powered tools. However, where use of pneumatic tools is unavoidable, an exhaust muffler on the compressed air exhaust shall be used; this muffler can lower noise levels from the exhaust by up to about 10 dBA. External jackets on the tools themselves shall be used, if such jackets are commercially available, and this could achieve a reduction of 5 dBA. Quieter procedures shall be used, such as drills rather than impact equipment, whenever such procedures are available and consistent with construction procedures. b. Applicant shall use temporary power poles instead of generators where feasible. c. Stationary noise sources shall be located as far from adjacent properties as possible, and they shall be muffled and enclosed within temporary sheds, incorporate insulation barriers, or use other measures as determined by the City to provide equivalent noise reduction. d. The noisiest phases of construction shall be limited to less than 10 days at a time. Exceptions may be allowed if the City determines an extension is necessary and all available noise reduction controls are implemented. <p>SCA NOI-3 (Standard Condition of Approval 60): Extreme Construction Noise Construction Noise Management Plan Required</p> <p>Requirement: Prior to any extreme noise generating construction activities (e.g., pier drilling, pile driving and other activities generating greater than 90dBA), the project applicant shall submit a Construction Noise Management Plan prepared by a qualified acoustical consultant for City review and approval that contains a set of site-specific noise attenuation measures to further reduce construction impacts associated with extreme noise generating activities. The project applicant shall implement the approved Plan during construction. Potential attenuation measures include, but are not limited to, the following:</p> <ul style="list-style-type: none"> i. Erect temporary plywood noise barriers around the construction site, particularly along on sites adjacent to residential buildings; ii. Implement "quiet" pile driving technology (such as pre-drilling of piles, the use of more than one pile driver to shorten the total pile driving duration), where feasible, in consideration of geotechnical and structural requirements and conditions; iii. Utilize noise control blankets on the building structure as the building is erected to reduce noise emission from the site; 	<p>During construction.</p> <ul style="list-style-type: none"> a. Prior to approval of construction-related permit. b. During construction. 	<p>City of Oakland Bureau of Building Services Division, Zoning Inspections</p>

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
Noise (cont.)		Schedule	Responsibility
<p>iv. Evaluate the feasibility of noise control at the receivers by temporarily improving the noise reduction capability of adjacent buildings by the use of sound blankets for example and implement such measure if such measures are feasible and would noticeably reduce noise impacts; and</p> <p>v. Monitor the effectiveness of noise attenuation measures by taking noise measurements.</p> <p>b. Public Notification Required</p> <p>Requirement: The project applicant shall notify property owners and occupants located within 300 feet of the construction activities at least 14 calendar days prior to commencing extreme noise generating activities. Prior to providing the notice, the project applicant shall submit to the City for review and approval the proposed type and duration of extreme noise generating activities and the proposed public notice. The public notice shall provide the estimated start and end dates of the extreme noise generating activities and describe noise attenuation measures to be implemented.</p> <p>SCA NOI-4 (Standard Condition of Approval 62): Construction Noise Complaints</p> <p>Requirement: The project applicant shall submit to the City for review and approval a set of procedures for responding to and tracking complaints received pertaining to construction noise, and shall implement the procedures during construction. At a minimum, the procedures shall include:</p> <ol style="list-style-type: none"> a. Designation of an on-site construction complaint and enforcement manager for the project; b. A large on-site sign near the public right-of-way containing permitted construction days/hours, complaint procedures, and phone numbers for the project complaint manager and City Code Enforcement unit; c. Protocols for receiving, responding to, and tracking received complaints; and d. Maintenance of a complaint log that records received complaints and how complaints were addressed, which shall be submitted to the City for review upon the City's request. <p>SCA NOI-5 (Standard Condition of Approval 63) Exposure to Community Noise</p> <p>Requirement: The project applicant shall submit a Noise Reduction Plan prepared by a qualified acoustical engineer for City review and approval that contains noise reduction measures (e.g., sound-rated window, wall, and door assemblies) to achieve an acceptable interior noise level in accordance with the land use compatibility guidelines of the Noise Element of the Oakland General Plan. The applicant shall implement the approved Plan during construction. To the maximum extent practicable, interior noise levels shall not exceed the following:</p> <ol style="list-style-type: none"> a. 45 dBA: Residential activities, civic activities, hotels b. 50 dBA: Administrative offices; group assembly activities c. 55 dBA: Commercial activities d. 65 dBA: Industrial activities 	<p>Prior to approval of construction-related permit.</p>	<p>Prior to approval of construction-related permit.</p>	<p>City of Oakland Bureau of Building Services Division, Zoning Inspections</p>
			<p>City of Oakland Bureau of Building Services Division, Zoning Inspections</p>

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
Noise (cont.)		Schedule	Responsibility
<p>SCA NOI-6 (Standard Condition of Approval 64): Operational Noise Requirement: Noise levels from the project site after completion of the project (i.e., during project operation) shall comply with the performance standards of chapter 17.120 of the Oakland Planning Code and chapter 8.18 of the Oakland Municipal Code. If noise levels exceed these standards, the activity causing the noise shall be abated until appropriate noise reduction measures have been installed and compliance verified by the City.</p>		Ongoing.	City of Oakland Bureau of Building Services Division, Zoning Inspections
<p>SCA NOI-7 (Standard Condition of Approval 66): Vibration Impacts on Adjacent Historic Structures or Vibration-Sensitive Activities Requirement: The project applicant shall submit a Vibration Analysis prepared by an acoustical and/or structural engineer or other appropriate qualified professional for City review and approval that establishes pre-construction baseline conditions and threshold levels of vibration that could damage the structure and/or substantially interfere with activities located at 444 24th Street and 443 25th Street. The Vibration Analysis shall identify design means and methods of construction that shall be utilized in order to not exceed the thresholds. The applicant shall implement the recommendations during construction.</p>		Prior to construction.	City of Oakland Bureau of Building Services Division, Zoning Inspections
Transportation and Circulation			
BVDSP TRA Mitigation Measures			
All the mitigation measures identified in the BVDSP EIR are included in the citywide Transportation Impact Fee (TIF). Therefore, the project applicant shall mitigate the project impacts by paying the required TIF.			
SCA TRA-1 (Standard Condition of Approval 68): Construction Activity in the Public Right-of-Way			
<p>a. Obstruction Permit Required Requirement: The project applicant shall obtain an obstruction permit from the City prior to placing any temporary construction-related obstruction in the public right-of-way, including City streets and sidewalks.</p>		a. Prior to approval of construction-related permit.	a. City of Oakland Bureau of Building Services Division, Zoning Inspections
<p>b. Traffic Control Plan Required Requirement: In the event of obstructions to vehicle or bicycle travel lanes, the project applicant shall submit a Traffic Control Plan to the City for review and approval prior to obtaining an obstruction permit. The project applicant shall submit evidence of City approval of the Traffic Control Plan with the application for an obstruction permit. The Traffic Control Plan shall contain a set of comprehensive traffic control measures for auto, transit, bicycle, and pedestrian detours, including detour signs if required, lane closure procedures, signs, cones for drivers, and designated construction access routes. The project applicant shall implement the approved Plan during construction.</p>		b. Prior to approval of construction-related permit. c. Prior to building permit final.	b. Public Works Department, Transportation Services Division c. City of Oakland Bureau of Building Services Division, Zoning Inspections
<p>c. Repair of City Streets Requirement: The project applicant shall repair any damage to the public right-of-way, including streets and sidewalks caused by project construction at his/her expense within one week of the occurrence of the damage (or excessive wear), unless further damage/excessive wear may continue; in such case, repair shall occur prior to approval of the final inspection of the construction-related permit. All damage that is a threat to public health or safety shall be repaired immediately.</p>			

Standard Conditions of Approval/Mitigation Measures	Mitigation Implementation/ Monitoring	Responsibility
Transportation and Circulation (cont.)	Schedule	Responsibility
<p>SCA TRA-2 (Standard Condition of Approval 69): Bicycle Parking Requirement: The project applicant shall comply with the City of Oakland Bicycle Parking Requirements (Chapter 17.118 of the Oakland Planning Code). The project drawings submitted for construction-related permits shall demonstrate compliance with the requirements.</p> <p>SCA TRA-3 (Standard Condition of Approval 70): Transportation Improvements. The project applicant shall implement the recommended on- and off-site transportation-related improvements contained within the Transportation Impact Study for the project (e.g., signal timing adjustments, restriping, signalization, traffic control devices, roadway reconfigurations, and pedestrian and bicyclist amenities). The project applicant is responsible for funding and installing the improvements, and shall obtain all necessary permits and approvals from the City and/or other applicable regulatory agencies such as, but not limited to, Caltrans (for improvements related to Caltrans facilities) and the California Public Utilities Commission (for improvements related to railroad crossings), prior to installing the improvements. To implement this measure for intersection modifications, the project applicant shall submit Plans, Specifications, and Estimates (PS&E) to the City for review and approval. All elements shall be designed to applicable City standards in effect at the time of construction and all new or upgraded signals shall include these enhancements as required by the City. All other facilities supporting vehicle travel and alternative modes through the intersection shall be brought up to both City standards and ADA standards (according to Federal and State Access Board guidelines) at the time of construction. Current City Standards call for, among other items, the elements listed below:</p> <ul style="list-style-type: none"> a. 2070L Type Controller with cabinet accessory b. GPS communication (clock) c. Accessible pedestrian crosswalks according to Federal and State Access Board guidelines with signals (audible and tactile) d. Countdown pedestrian head module switch out e. City Standard ADA wheelchair ramps f. Video detection on existing (or new, if required) g. Mast arm poles, full activation (where applicable) h. Polara Push buttons (full activation) i. Bicycle detection (full activation) j. Pull boxes k. Signal interconnect and communication with trenching (where applicable), or through existing conduit (where applicable), 600 feet maximum l. Conduit replacement contingency m. Fiber switch n. PTZ camera (where applicable) 	<p>Prior to approval of construction-related permit.</p> <p>Prior to building permit final or as otherwise specified</p>	<p>City of Oakland Bureau of Building Services Division, Zoning Inspections</p> <p>Bureau of Building; Public Works Department, Transportation Services Division</p>

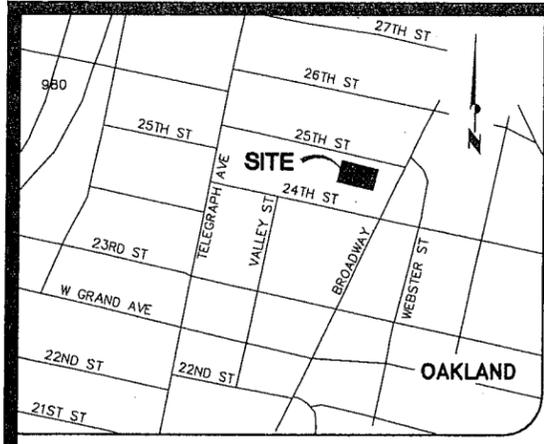
Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
Transportation and Circulation (cont.)		Schedule	Responsibility
<p>o. Transit Signal Priority (TSP) equipment consistent with other signals along corridor</p> <p>p. Signal timing plans for the signals in the coordination group</p>			
<p>SCA TRA-4 (Standard Condition of Approval 71): <i>Transportation and Parking Demand Management</i></p> <p>a. <i>Transportation and Parking Demand Management (TDM) Plan Required</i> Requirement: The project applicant shall submit a Transportation and Parking Demand Management (TDM) Plan for review and approval by the City.</p> <p>i. The goals of the TDM Plan shall be the following:</p> <ul style="list-style-type: none"> • Reduce vehicle traffic and parking demand generated by the project to the maximum extent practicable, consistent with the potential traffic and parking impacts of the project. • Achieve the following project vehicle trip reductions (VTR): <ul style="list-style-type: none"> - Projects generating 50-99 net new a.m. or p.m. peak hour vehicle trips: 10 percent VTR - Projects generating 100 or more net new a.m. or p.m. peak hour vehicle trips: 20 percent VTR • Increase pedestrian, bicycle, transit, and carpool/vanpool modes of travel. All four modes of travel shall be considered, as appropriate. • Enhance the City's transportation system, consistent with City policies and programs. <p>ii. TDM strategies to consider include, but are not limited to, the following:</p> <ul style="list-style-type: none"> • Inclusion of additional long-term and short-term bicycle parking that meets the design standards set forth in chapter five of the Bicycle Master Plan and the Bicycle Parking Ordinance (chapter 17.117 of the Oakland Planning Code), and shower and locker facilities in commercial developments that exceed the requirement. • Construction of and/or access to bikeways per the Bicycle Master Plan; construction of priority bikeways, on-site signage and bike lane striping. • Installation of safety elements per the Pedestrian Master Plan (such as crosswalk striping, curb ramps, count down signals, bulb outs, etc.) to encourage convenient and safe crossing at arterials, in addition to safety elements required to address safety impacts of the project. • Installation of amenities such as lighting, street trees, and trash receptacles per the Pedestrian Master Plan and any applicable streetscape plan. • Construction and development of transit stops/shelters, pedestrian access, way finding signage, and lighting around transit stops per transit agency plans or negotiated improvements. • Direct on-site sales of transit passes purchased and sold at a bulk group rate (through programs such as AC Transit Easy Pass or a similar program through another transit agency). • Provision of a transit subsidy to employees or residents, determined by the project applicant and subject to review by the City, if employees or residents use transit or commute by other alternative modes. 		<p>a. Prior to building permit final.</p> <p>b. Prior to building permit final</p> <p>c. Ongoing</p>	<p>a. City of Oakland Bureau of Planning and Building</p> <p>b. City of Oakland Bureau of Building Services Division, Zoning Inspections</p> <p>c. City of Oakland Bureau of Planning and Building</p>

Standard Conditions of Approval/Mitigation Measures	Mitigation Implementation/ Monitoring
Transportation and Circulation (cont.)	Schedule Responsibility
<p>• Provision of an ongoing contribution to transit service to the area between the project and nearest mass transit station prioritized as follows: 1) Contribution to AC Transit bus service; 2) Contribution to an existing area shuttle service; and 3) Establishment of new shuttle service. The amount of contribution (for any of the above scenarios) would be based upon the cost of establishing new shuttle service (Scenario 3).</p> <p>• Guaranteed ride home program for employees, either through 511.org or through separate program.</p> <p>• Pre-tax commuter benefits (commuter checks) for employees.</p> <p>• Free designated parking spaces for on-site car-sharing program (such as City Car Share, Zip Car, etc.) and/or car-share membership for employees or tenants.</p> <p>• On-site carpooling and/or vanpool program that includes preferential (discounted or free) parking for carpools and vanpools.</p> <p>• Distribution of information concerning alternative transportation options.</p> <p>• Parking spaces sold/leased separately for residential units. Charge employees for parking, or provide a cash incentive or transit pass alternative to a free parking space in commercial properties.</p> <p>• Parking management strategies including attendant/valet parking and shared parking spaces.</p> <p>• Requiring tenants to provide opportunities and the ability to work off-site.</p> <p>• Allow employees or residents to adjust their work schedule in order to complete the basic work requirement of five eight-hour workdays by adjusting their schedule to reduce vehicle trips to the worksite (e.g., working four, ten-hour days; allowing employees to work from home two days per week).</p> <p>• Provide or require tenants to provide employees with staggered work hours involving a shift in the set work hours of all employees at the workplace or flexible work hours involving individually determined work hours.</p> <p>The TDM Plan shall indicate the estimated VTR for each strategy, based on published research or guidelines where feasible. For TDM Plans containing ongoing operational VTR strategies, the Plan shall include an ongoing monitoring and enforcement program to ensure the Plan is implemented on an ongoing basis during project operation. If an annual compliance report is required, as explained below, the TDM Plan shall also specify the topics to be addressed in the annual report.</p> <p>b. <i>TDM Implementation – Physical Improvements</i> Requirement: For VTR strategies involving physical improvements, the project applicant shall obtain the necessary permits/approvals from the City and install the improvements prior to the completion of the project.</p> <p>c. <i>TDM Implementation – Operational Strategies</i> Requirement: For projects that generate 100 or more net new a.m. or p.m. peak hour vehicle trips and contain ongoing operational VTR strategies, the project applicant shall submit an annual compliance report for the first</p>	

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring
Transportation and Circulation (cont.)		Schedule
Responsibility		
<p>five years following completion of the project (or completion of each phase for phased projects) for review and approval by the City. The annual report shall document the status and effectiveness of the TDM program, including the actual VTR achieved by the project during operation. If deemed necessary, the City may elect to have a peer review consultant, paid for by the project applicant, review the annual report. If timely reports are not submitted and/or the annual reports indicate that the project applicant has failed to implement the TDM Plan, the project will be considered in violation of the Conditions of Approval and the City may initiate enforcement action as provided for in these Conditions of Approval. The project shall not be considered in violation of this Condition if the TDM Plan is implemented but the VTR goal is not achieved.</p>		
<p>Utilities and Service Systems</p> <p>SCA UTIL-1 (Standard Condition of Approval 74) Construction and Demolition Waste Reduction and Recycling Requirement: The project applicant shall comply with the City of Oakland Construction and Demolition Waste Reduction and Recycling Ordinance (chapter 15.34 of the Oakland Municipal Code) by submitting a Construction and Demolition Waste Reduction and Recycling Plan (WRRP) for City review and approval, and shall implement the approved WRRP. Projects subject to these requirements include all new construction, renovations/alterations/modifications with construction values of \$50,000 or more (except R-3 type construction), and all demolition (including soft demolition) except demolition of type R-3 construction. The WRRP must specify the methods by which the project will divert construction and demolition debris waste from landfill disposal in accordance with current City requirements. The WRRP may be submitted electronically at www.greenhalosystems.com or manually at the City's Green Building Resource Center. Current standards, FAQs, and forms are available on the City's website and in the Green Building Resource Center.</p>		<p>Prior to approval of construction-related permit</p> <p>City of Oakland Public Works Department, Environmental Services Division</p>
<p>SCA UTIL-2 (Standard Condition of Approval 75) Underground Utilities Requirement: The project applicant shall place underground all new utilities serving the project and under the control of the project applicant and the City, including all new gas, electric, cable, and telephone facilities, fire alarm conduits, street light wiring, and other wiring, conduits, and similar facilities. The new facilities shall be placed underground along the project's street frontage and from the project structures to the point of service. Utilities under the control of other agencies, such as PC&E, shall be placed underground if feasible. All utilities shall be installed in accordance with standard specifications of the serving utilities.</p>		<p>During construction.</p> <p>City of Oakland Bureau of Building Services Division, Zoning Inspections</p>
<p>SCA UTIL-3 (Standard Condition of Approval 76) Recycling Collection and Storage Space Requirement: The project applicant shall comply with the City of Oakland Recycling Space Allocation Ordinance (chapter 17.118 of the Oakland Planning Code). The project drawings submitted for construction-related permits shall contain recycling collection and storage areas in compliance with the Ordinance. For residential projects, at least two cubic feet of storage and collection space per residential unit is required, with a minimum of ten cubic feet. For nonresidential projects, at least two cubic feet of storage and collection space per 1,000 square feet of building floor area is required, with a minimum of ten cubic feet.</p>		<p>Prior to approval of construction-related permit.</p> <p>City of Oakland Bureau of Building Services Division, Zoning Inspections</p>

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
Utilities and Service Systems (cont.)		Schedule	Responsibility
<p>SCA UTIL-4 (Standard Condition of Approval 77) Green Building Requirements</p> <p><i>a. Compliance with Green Building Requirements During Plan-Check</i></p> <p>Requirement: The project applicant shall comply with the requirements of the California Green Building Standards (CALGreen) mandatory measures and the applicable requirements of the City of Oakland Green Building Ordinance (chapter 18.02 of the Oakland Municipal Code).</p> <p>i. The following information shall be submitted to the City for review and approval with the application for a building permit:</p> <ul style="list-style-type: none"> • Documentation showing compliance with Title 24 of the current version of the California Building Energy Efficiency Standards. • Completed copy of the final green building checklist approved during the review of the Planning and Zoning permit. • Copy of the Unreasonable Hardship Exemption, if granted, during the review of the Planning and Zoning permit. • Permit plans that show, in general notes, detailed design drawings, and specifications as necessary, compliance with the items listed in subsection (ii) below. • Copy of the signed statement by the Green Building Certifier approved during the review of the Planning and Zoning permit that the project complied with the requirements of the Green Building Ordinance. • Signed statement by the Green Building Certifier that the project still complies with the requirements of the Green Building Ordinance, unless an Unreasonable Hardship Exemption was granted during the review of the Planning and Zoning permit. • Other documentation as deemed necessary by the City to demonstrate compliance with the Green Building Ordinance. <p>ii. The set of plans in subsection (i) shall demonstrate compliance with the following:</p> <ul style="list-style-type: none"> • CALGreen mandatory measures. • All pre-requisites per the green building checklist approved during the review of the Planning and Zoning permit, or, if applicable, all the green building measures approved as part of the Unreasonable Hardship Exemption granted during the review of the Planning and Zoning permit. • LEED Silver (minimum 50 points) (except the cool roof requirement) per the appropriate checklist approved during the Planning entitlement process. • CALGreen mandatory measures for non-residential construction • Green Building Certification (Green Building Certification Institution and City staff for CALGreen) 		<p>a. Prior to approval of construction-related permit.</p> <p>b. During construction.</p> <p>c. After project completion as specified.</p>	<p>a. City of Oakland Bureau of Building Services Division, Zoning Inspections</p> <p>b. City of Oakland Bureau of Building Services Division, Zoning Inspections</p> <p>c. City of Oakland Bureau of Planning and Building</p>

Standard Conditions of Approval/Mitigation Measures		Mitigation Implementation/ Monitoring	
Utilities and Service Systems (cont.)		Schedule	Responsibility
<ul style="list-style-type: none"> • All green building points identified on the checklist approved during review of the Planning and Zoning permit, unless a Request for Revision Plan-check application is submitted and approved by the Bureau of Planning that shows the previously approved points that will be eliminated or substituted. • The required green building point minimums in the appropriate credit categories. <p>b. Compliance with Green Building Requirements During Construction</p> <p><u>Requirement:</u> The project applicant shall comply with the applicable requirements of CALGreen and the Oakland Green Building Ordinance during construction of the project.</p> <p>The following information shall be submitted to the City for review and approval:</p> <ol style="list-style-type: none"> i. Completed copies of the green building checklists approved during the review of the Planning and Zoning permit and during the review of the building permit. ii. Signed statement(s) by the Green Building Certifier during all relevant phases of construction that the project complies with the requirements of the Green Building Ordinance. iii. Other documentation as deemed necessary by the City to demonstrate compliance with the Green Building Ordinance. <p>c. Compliance with Green Building Requirements After Construction</p> <p><u>Requirement:</u> Within sixty (60) days of the final inspection of the building permit for the project, the Green Building Certifier shall submit the appropriate documentation to Build It Green or Green Building Certification Institute and attain the minimum required certification/point level. Within one year of the final inspection of the building permit for the project, the applicant shall submit to the Bureau of Planning the Certificate from the organization listed above demonstrating certification and compliance with the minimum point/certification level noted above.</p>	<p>SCA UTIL-5 (Standard Condition of Approval 79) Sanitary Sewer System</p> <p><u>Requirement:</u> The project applicant shall prepare and submit a Sanitary Sewer Impact Analysis to the City for review and approval in accordance with the City of Oakland Sanitary Sewer Design Guidelines. The Impact Analysis shall include an estimate of pre-project and post-project wastewater flow from the project site. In the event that the Impact Analysis indicates that the net increase in project wastewater flow exceeds City-projected increases in wastewater flow in the sanitary sewer system, the project applicant shall pay the Sanitary Sewer Impact Fee in accordance with the City's Master Fee Schedule for funding improvements to the sanitary sewer system.</p> <p>SCA UTIL-6 (Standard Condition of Approval 80) Storm Drain System</p> <p><u>Requirement:</u> The project storm drainage system shall be designed in accordance with the City of Oakland's Storm Drainage Design Guidelines. To the maximum extent practicable, peak stormwater runoff from the project site shall be reduced by at least 25 percent compared to the pre-project condition.</p>	<p>Prior to approval of construction-related permit.</p>	<p>City of Oakland Public Works Department, Department of Engineering and Construction</p>
<p>SCA UTIL-5 (Standard Condition of Approval 79) Sanitary Sewer System</p> <p><u>Requirement:</u> The project applicant shall prepare and submit a Sanitary Sewer Impact Analysis to the City for review and approval in accordance with the City of Oakland Sanitary Sewer Design Guidelines. The Impact Analysis shall include an estimate of pre-project and post-project wastewater flow from the project site. In the event that the Impact Analysis indicates that the net increase in project wastewater flow exceeds City-projected increases in wastewater flow in the sanitary sewer system, the project applicant shall pay the Sanitary Sewer Impact Fee in accordance with the City's Master Fee Schedule for funding improvements to the sanitary sewer system.</p> <p>SCA UTIL-6 (Standard Condition of Approval 80) Storm Drain System</p> <p><u>Requirement:</u> The project storm drainage system shall be designed in accordance with the City of Oakland's Storm Drainage Design Guidelines. To the maximum extent practicable, peak stormwater runoff from the project site shall be reduced by at least 25 percent compared to the pre-project condition.</p>	<p>Prior to approval of construction-related permit.</p>	<p>City of Oakland Bureau of Building Services Division, Zoning Inspections</p>	



VICINITY MAP
NOT TO SCALE

TENTATIVE MAP STATEMENT

I HEREBY STATE THAT THIS TENTATIVE MAP ACCURATELY DEPICTS THE LOCATION, WIDTH, TYPE AND RECORDING INFORMATION OF ALL RECORD EASEMENTS LISTED ON THE PRELIMINARY TITLE REPORT ISSUED BY CHICAGO TITLE COMPANY OWNERS POLICY TITLE NO.14-5805421-A-KD DATED APRIL 20, 2015 AND TITLE POLICY NO. 14-58205059-KD DATED APRIL 17, 2015 AND WAS PREPARED IN ACCORDANCE TO THE PROFESSIONAL LAND SURVEYOR'S ACT.

SIGNED: DANIEL DRUMMOND DATED: _____
LS 6333



**VESTING TENTATIVE
PARCEL MAP 10568
FOR CONDOMINIUM PURPOSES**

THE EXISTING PARCELS DESCRIBED IN DOCUMENT NO. 2017 147143 AND IN DOCUMENT NO. 2016 33751 BEING MERGED TO CREATE 4 PARCELS:
PARCEL 1 & 4 ARE FOR RETAIL, PARCEL 3 IS FOR A HOTEL, AND PARCEL 2 IS FOR RESIDENTIAL CONDOMINIUMS (72 RESIDENTIAL CONDOMINIUMS CREATED BY SEPARATE DOCUMENT)

OAKLAND ALAMEDA COUNTY CALIFORNIA

dk CONSULTING

PLANNING-ENGINEERING-CONSTRUCTION
1931 SAN MIGUEL DRIVE, SUITE 100
WALNUT CREEK, CALIFORNIA 94596
(925) 932-6868

SEPTEMBER 8, 2017

ASSESSORS PARCEL NUMBER:

2401 BROADWAY INVESTORS LLC: 009-0674-005, 004, 003-1, 036

AREA:

1.212 ACRES

TITLE REPORT: (TO BE UPDATED)

CHICAGO TITLE COMPANY
OWNERS POLICY TITLE NO.: 14-58205421-A-KD
DATED APRIL 30, 2015
OWNERS POLICY TITLE NO.: 15-58206125-C-KD
DATED JANUARY 12, 2016 (LOVI, NOW 2401 BROADWAY INVESTORS LLC)

TOPOGRAPHIC SURVEY:

THE TOPOGRAPHIC SURVEY IS BASED UPON AERIAL SURVEY PERFORMED ON SEPTEMBER 17, 2015 AND FIELD VERIFIED ON SEPTEMBER 29, 2015.

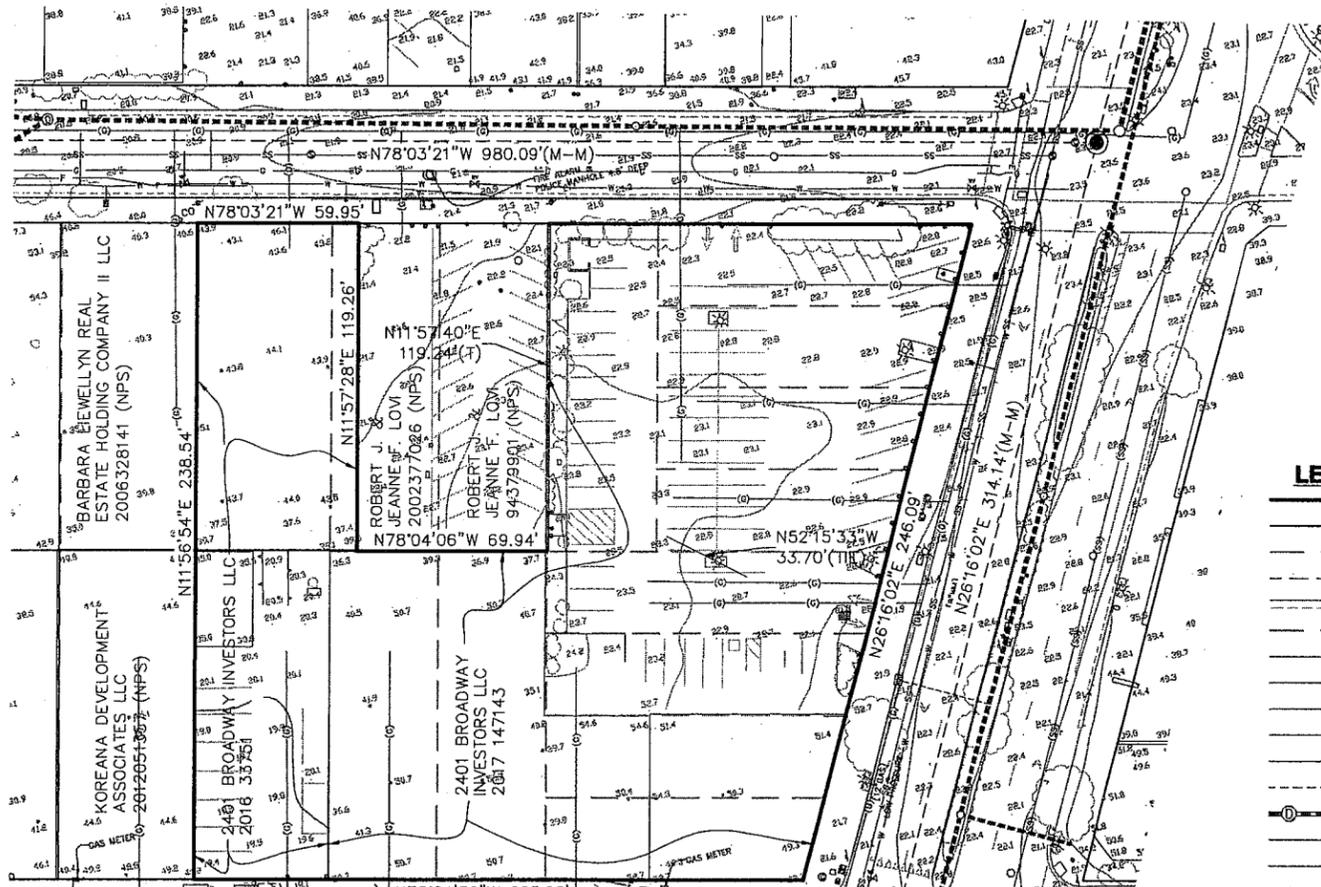
BENCH MARK:

A MONUMENT AT THE TOP OF CURB WEST RETURN OF THE NORTHWEST CORNER OF 23RD STREET AND TELEGRAPH AVENUE, WITH AN ELEVATION OF 17.684 FEET PER THE CITY OF OAKLAND MONUMENT BENCHMARK No 37Jc ON BOOK 18 AT PAGE 71.

BASIS OF BEARINGS:

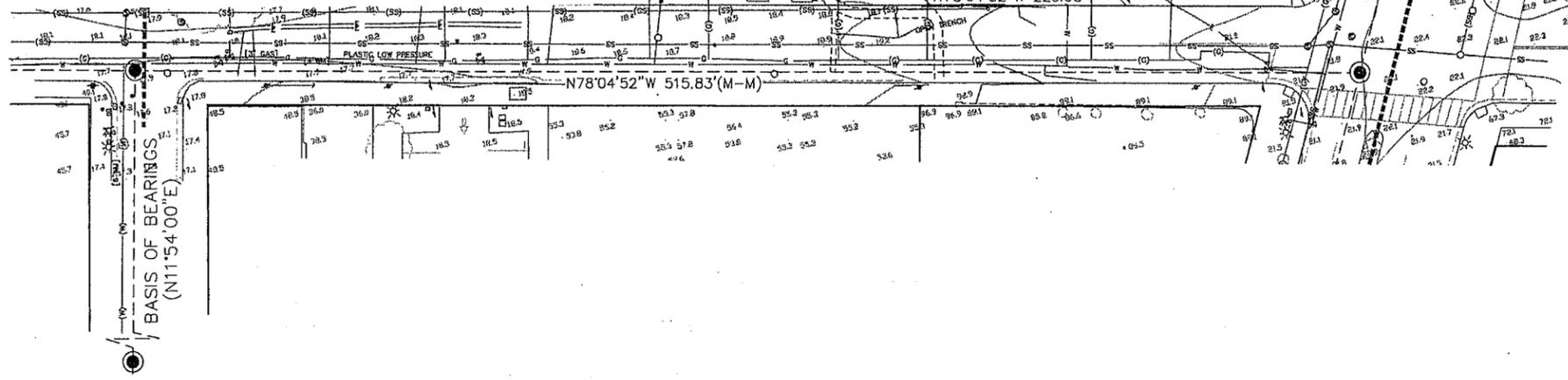
THE MONUMENT LINE ON VALLEY STREET BEING TAKEN AS NORTH 11°54'00" EAST AS SHOWN ON TRACT MAP NO. 8138 (327 M 88).

EXISTING CONDITIONS:



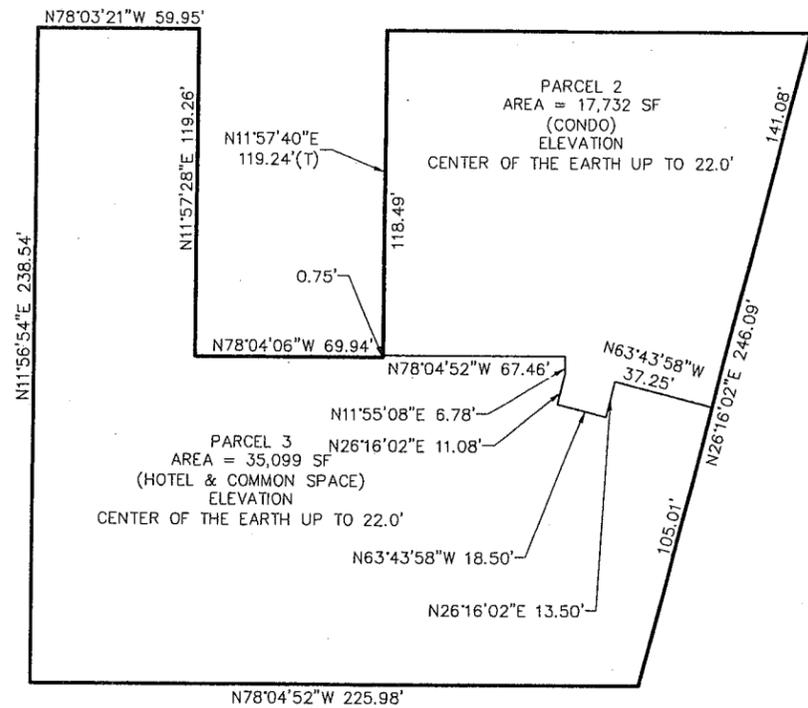
LEGEND:

- BOUNDARY
- - - RIGHT-OF-WAY
- - - ADJACENT PROPERTY
- - - MONUMENT LINE
- - - CURB AND GUTTER LINE
- - - RECORD LOT LINE (3M16)
- - - GAS LINE, FIELD LOCATED, VALVE
- - - RECORD GAS LINE
- - - ELECTRIC LINE, BOX
- - - COMMUNICATION LINE, PAC BELL
- - - RECORD SANITARY SEWER LINE, MANHOLE
- - - RECORD STORM DRAIN LINE, INLET
- - - STORM DRAIN LINE, MANHOLE, CURB INLET
- - - WATER LINE, VALVE, METER
- - - RECORD WATER LINE
- - - UTILITY POLE WITH GUY ANCHOR
- - - VEGETATION
- - - FENCE
- ⊗ FIRE HYDRANT
- ⊙ ELECTROLIER
- POST
- ⊙ SIGN
- ⊙ OTHER MANHOLE, AS NOTED
- ⊙ UTILITY VALVE
- ⊙ FOUND STANDARD STREET MONUMENT
- ⊙ MISSING STREET MONUMENT
- SF SQUARE FEET
- SFNF SEARCHED FOR, NOT FOUND
- M-M MONUMENT TO MONUMENT
- NPS NOT A PART OF THIS SURVEY

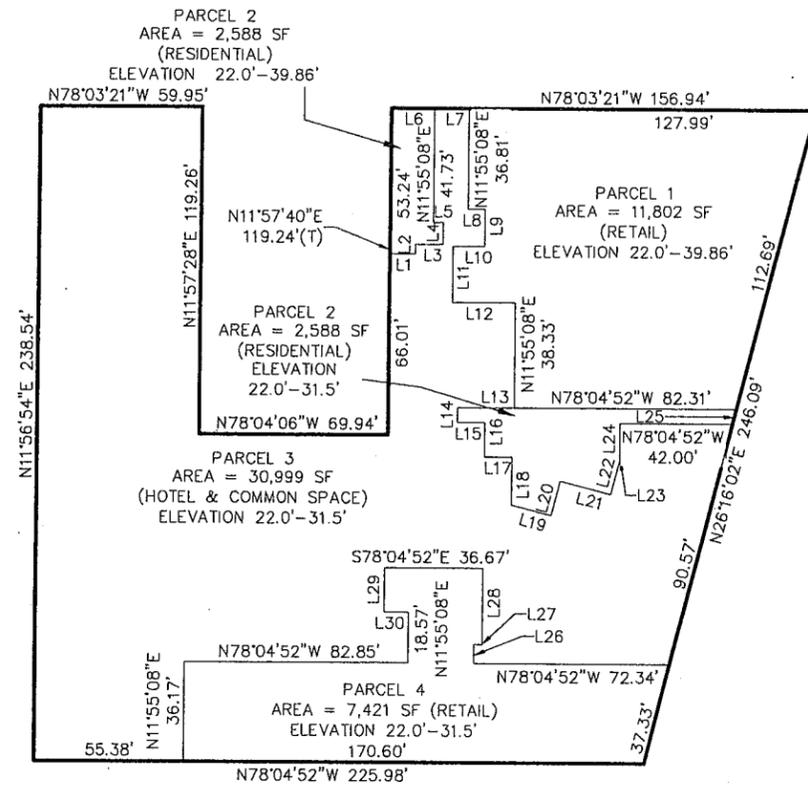


F:\PROJECTS\2015\15-1020-2401 BROADWAY\DWG\SURVEY\PARCEL MAP\1520VTPM.DWG

PROPOSED PARCELS BELOW FLOOR 1



PROPOSED PARCELS FLOOR 1



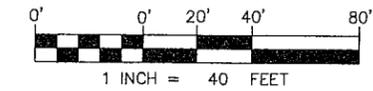
**VESTING TENTATIVE
PARCEL MAP 10568
FOR CONDOMINIUM PURPOSES**

THE EXISTING PARCELS DESCRIBED IN DOCUMENT NO. 2017 147143 AND IN DOCUMENT NO. 2016 33751 BEING MERGED TO CREATE 4 PARCELS:
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OAKLAND ALAMEDA COUNTY CALIFORNIA
dk CONSULTING

PLANNING-ENGINEERING-CONSTRUCTION
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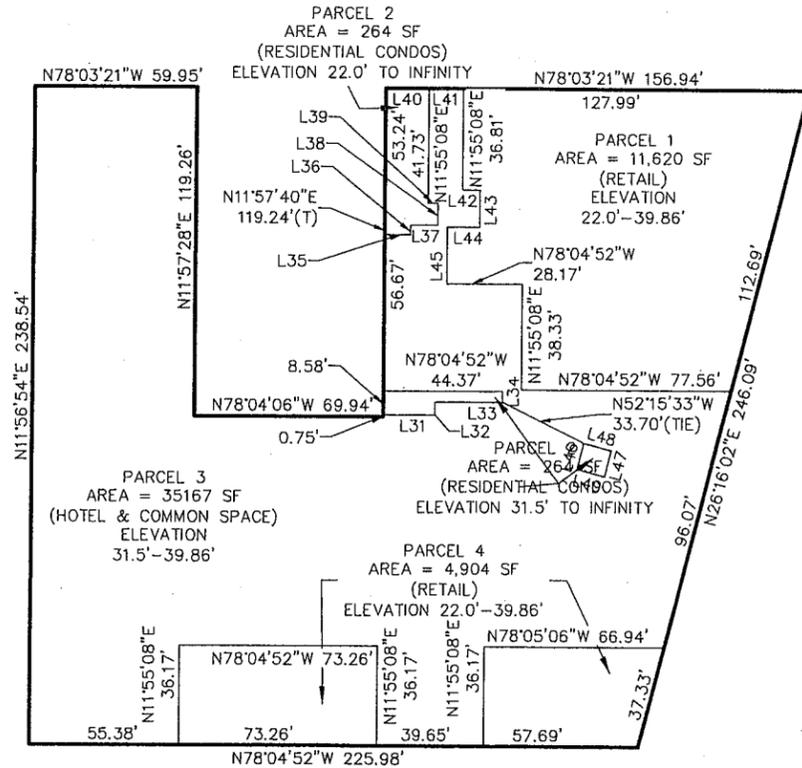
SEPTEMBER 8, 2017



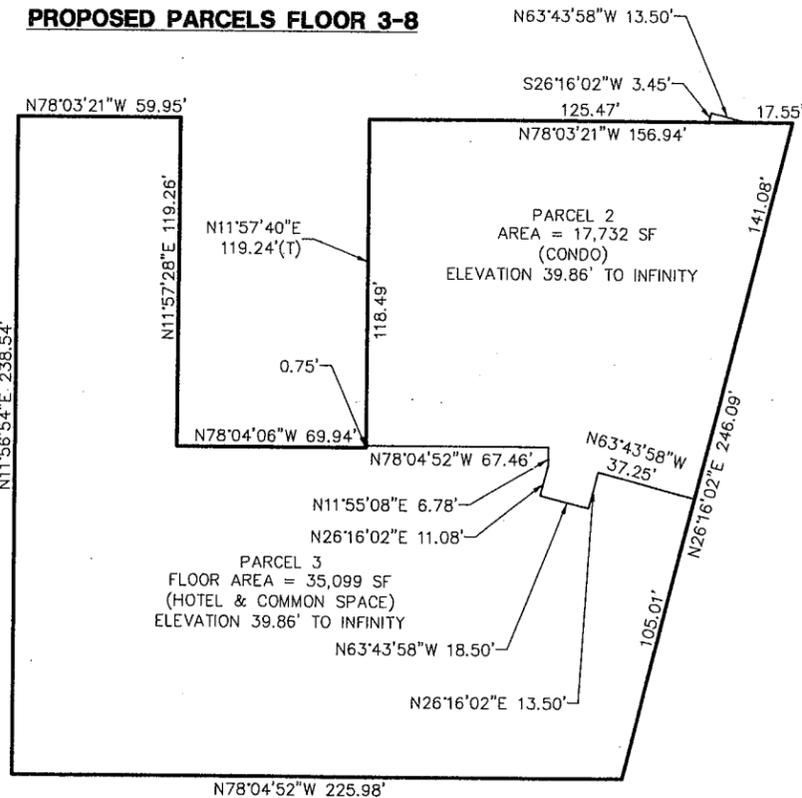
NOTE:

1. PARCEL TWO CONNECTS ON LEVELS 2 AND 3. THE VERTICAL ELEMENTS WILL BE SHOWN ON FINAL ARCHITECTURE PLANS AND ON THE PARCEL MAP 10568.
2. THE PARCEL MAP 10568 IS MERGING LOTS 15-25, 28, 31 AND A PORTION OF 30 OF THE MAP OF THE LINCOLN & WEBSTER HOMESTEADS (BK 3 MAPS PG 16) AS DESCRIBED IN 80-198891 AND IN DOCUMENT NO 2016-33751 TO CREATE 4 NEW PARCELS AS FOLLOWS:
PARCEL 1 RETAIL
PARCEL 2 RESIDENTIAL CONDOMINIUMS, CREATED BY SEPARATE DOC.
PARCEL 3 HOTEL
PARCEL 4 RETAIL

PROPOSED PARCELS FLOOR 2



PROPOSED PARCELS FLOOR 3-8



LINE TABLE			LINE TABLE			LINE TABLE		
LINE #	DIRECTION	LENGTH	LINE #	DIRECTION	LENGTH	LINE #	DIRECTION	LENGTH
L1	S78°04'52"E	9.33'	L21	S63°43'58"E	19.33'	L41	S78°03'25"E	13.00'
L2	N11°55'08"E	3.50'	L22	N26°16'02"E	13.50'	L42	N78°04'52"W	6.33'
L3	S78°04'52"E	10.33'	L23	N63°43'58"W	0.25'	L43	N11°55'08"E	13.58'
L4	N11°55'08"E	8.00'	L24	N11°55'08"E	12.88'	L44	S78°04'52"E	12.00'
L5	N78°04'52"W	3.67'	L25	N26°16'02"E	5.51'	L45	N11°55'08"E	20.51'
L6	N78°03'17"W	15.96'	L26	S11°55'08"W	6.92'	L46	N26°16'02"E	9.75'
L7	S78°03'25"E	13.00'	L27	N78°04'52"W	3.08'	L47	S26°16'02"W	9.75'
L8	N78°04'52"W	6.33'	L28	S11°55'08"W	27.74'	L48	S63°43'58"E	10.67'
L9	N11°55'08"E	13.58'	L29	N11°55'08"E	16.08'	L49	N63°43'58"W	10.67'
L10	S78°04'52"E	12.00'	L30	N78°04'52"W	8.92'			
L11	N11°55'08"E	20.51'	L31	N78°04'52"W	19.27'			
L12	N78°04'52"W	23.42'	L32	S11°55'08"W	4.67'			
L13	N78°04'52"W	21.03'	L33	N78°04'52"W	25.11'			
L14	S11°55'08"W	5.33'	L34	S11°55'08"W	3.92'			
L15	S78°04'52"E	10.36'	L35	S78°04'52"E	9.33'			
L16	S11°55'08"W	12.49'	L36	N11°55'08"E	3.50'			
L17	S78°04'52"E	9.92'	L37	S78°04'52"E	10.33'			
L18	S11°44'45"W	17.60'	L38	N11°55'08"E	8.00'			
L19	S63°43'58"E	15.06'	L39	N78°04'52"W	3.67'			
L20	N26°16'02"E	12.99'	L40	S78°03'17"E	15.96'			



BROADWAY HOTEL & RESIDENCE

ENTITLEMENTS PACKAGE UPDATE
09.18.17

BAR ARCHITECTS

SIGNATURE DEVELOPMENT

PROJECT TEAM

OWNER:
SIGNATURE DEVELOPMENT GROUP
2335 BROADWAY, SUITE 200
OAKLAND, CA 94612
T: (510) 251-9270

CONTACT: JAMIE CHOY
EMAIL: JCHOY@SIGNATUREDEVELOPMENT.COM

ARCHITECT:
BAR ARCHITECTS
908 BATTERY STREET #300
SAN FRANCISCO, CA 94111
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EMAIL: JGOODWIN@BARARCH.COM

CIVIL ENGINEER:
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WALNUT CREEK, CA 94596
T: (925) 932-6868

CONTACT: SCOTT HARTSTEIN
EMAIL: SHARTSTEIN@DKCONSULTING.COM

LANDSCAPE ARCHITECT:
JETT LANDSCAPE ARCHITECTURE + DESIGN
2 THEATRE SQUARE, SUITE 218
ORINDA, CA 94563
T: (925) 254-5422

CONTACT: BRUCE JETT
EMAIL: BRUCEJ@JETT.LAND

BUILDING & SITE DATA

BUILDING GROSS FLOOR AREA 216,236 SF
D-BV-1 SITE AREA 30,158 SF
CC-3 SITE AREA 22,620 SF
TOTAL SITE AREA +/- 1.21 ACRES (+/- 52,778 SF)
FLOOR AREA RATIO 216,236 SF / 52,778 SF = 4.10
HOUSING DENSITY 72 D.U. / 1.21 ACRES = 59.5 D.U./ACRE

BUILDING & ZONING CODE

CONSTRUCTION TYPE I-A, III-A
OCCUPANCY GROUP R-1, R-2, M
BUILDING HEIGHT / STOREYS 73'-6" TOP OF ROOF / 84'-4" TOP OF PENTHOUSE ROOF / 7 STOREYS
USE RESI., HOTEL, RETAIL, RESTAURANT
ZONING DESIGNATION CC-3, D-BV-1
CODE EDITION CBC 2016

D.U. COUNT & MIX

1-BED	20	28%
2-BED	21	29%
3-BED	5	7%
JR 1	20	28%
STUDIO	6	8%
TOTAL	72	

KEY COUNT & MIX

ALT KING	2	1%
EXEC KING	31	19%
EXEC KING (ADA)	3	2%
STD	108	68%
STD (ADA)	5	3%
SUITE	8	5%
SUITE (ADA)	1	1%
SUITE (LOFT)	1	1%
TOTAL	159	

AREA - COMMERCIAL GROUND AND MEZZANINE

* REFERENCE AREA PLAN DIAGRAMS SHEET A25

LEVEL 1	BAR	401 SF
LEVEL 1	CAFE	1,285 SF
LEVEL 1	HOTEL	7,801 SF
LEVEL 1	RESTAURANT	4,806 SF
LEVEL 1	RETAIL	14,572 SF
LEVEL 2	HOTEL	11,448 SF
TOTAL		40,312 SF

40,789 / 600 = 68 SPACES FOR GROUND AND MEZZANINE LEVEL USES

AREA - COMMERCIAL UPPER LEVELS

* REFERENCE AREA PLAN DIAGRAMS SHEET A25

LEVEL 3	BAR	4,030 SF
LEVEL 3	HOTEL	18,698 SF
LEVEL 4	BAR	2,031 SF
LEVEL 4	HOTEL	18,689 SF
LEVEL 5	HOTEL	11,982 SF
LEVEL 6	HOTEL	11,980 SF
LEVEL 7	HOTEL	11,982 SF
ROOF	HOTEL	557 SF
TOTAL		79,984 SF

79,984 / 1,000 = 80 SPACES FOR UPPER LEVEL USES

TOTAL COMMERCIAL PARKING REQUIRED = 80 + 68 = 148 SPACES

PARKING REQUIREMENT

Required Parking

Residential Activities	
Total DU	72
Space per DU in D-BV-1	0.5
Total Res. Parking Required	36

50% Max. Shared Parking Reduction

Residential Parking	36
Commercial Parking	148
Total Parking Before Reduction	184
TOTAL PARKING REQUIRED	92.0

Total Parking Provided	129
-------------------------------	------------

LONG TERM BICYCLE PARKING:

RESIDENTIAL IN D-BV-1 ZONE (17,117.090): 1 PER 2 D.U.
72 UNITS / 2 = 36 BIKE SPACES
REST./RETAIL (COMMERCIAL 17,117.110)
IN D-BV-1 ZONE: 1 PER 8,000 SF
40,308 SF / 8,000 SF = 5 BIKE SPACES
IN CC-3 ZONE: 1 PER 12,000 SF
6,462 SF / 12,000 SF = 1 BIKE SPACE
HOTEL/TRANSIENT: 1 PER 20 ROOMS
159/20 = 8 BIKE SPACES

TOTAL LONG TERM BIKE PARKING REQ'D = 50 BIKE SPACES

Oakland Planning Code Section

17.116.060

17.116.110.D

GROSS AREA BY LEVEL

LEVEL 1	
BAR	401 SF
CAFE	1,285 SF
HOTEL	7,801 SF
PARKING	18,533 SF
RESIDENTIAL	3,048 SF
RESTAURANT	4,806 SF
RETAIL	14,572 SF
	50,446 SF
LEVEL 2	
HOTEL	11,448 SF
RESIDENTIAL	194 SF
	11,642 SF
LEVEL 3	
BAR	4,030 SF
HOTEL	18,698 SF
RESIDENTIAL	14,540 SF
	37,268 SF
LEVEL 4	
BAR	2,031 SF
HOTEL	18,689 SF
RESIDENTIAL	14,748 SF
	35,468 SF
LEVEL 5	
HOTEL	11,982 SF
RESIDENTIAL	14,701 SF
	26,683 SF
LEVEL 6	
HOTEL	11,980 SF
RESIDENTIAL	14,701 SF
	26,681 SF
LEVEL 7	
HOTEL	11,982 SF
RESIDENTIAL	14,701 SF
	26,683 SF
ROOF	
HOTEL	557 SF
RESIDENTIAL	808 SF
	1,365 SF
TOTAL GROSS AREA	216,236 SF

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A7	PERSPECTIVE VIEW - 25th STREET
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A9	FLOOR PLAN - LEVEL 2
A10	FLOOR PLAN - LEVEL 3
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A25	BUILDING SECTIONS
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A28	GREEN BUILDING COMPLIANCE
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C6	DEMOLITION PLAN
C7	SITE PLAN
C8	MASS GRADING PLAN
C9	FINE GRADING PLAN
C10	UTILITY PLAN
C11	EROSION CONTROL PLAN
C12	EROSION CONTROL NOTES & DETAILS
C13	C.3 EXHIBIT
L1	LANDSCAPE PROGRAM
L2	PUBLIC STREETScape PLAN
L3	LEVEL 2 - HOTEL BREAKOUT COURTYARDS
L4	LEVEL 3 - HOTEL VIEWING GARDEN
L5	LEVEL 3 - HOTEL VIEWING GARDENS
L6	LEVEL 3 - BAR & LOUNGE TERRACE
L7	LEVEL 3 - RESIDENTIAL AMENITY COURTYARD
L8	LEVEL 8 - RESIDENTIAL ROOF DECK

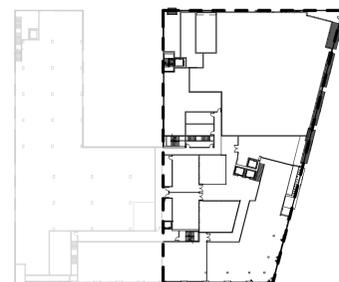
RETAIL PRIORITY SITE 2 CONFORMANCE

PER TABLES 17.101C.05 & 17.101C.06, IF D-BV-1 RETAIL AREA > 15,079 SF (50% OF D-BV-1):
- RESIDENTIAL FACILITY ALLOWED IN D-BV-1
- MAXIMUM BUILDING BASE HEIGHT = 85 FT

RETAIL AREA PROVIDED:	LEVEL 1	17,395 SF
	TOTAL	17,395 SF



L1



L2

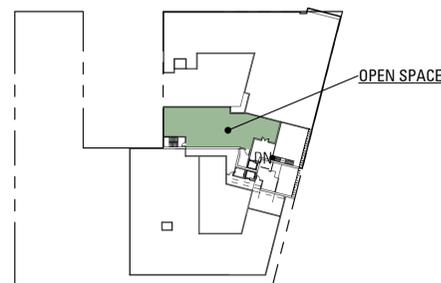
OPEN SPACE REQUIREMENT

PER TABLE 17.101C.06, 75 SF OPEN SPACE REQUIRED PER D.U.
75 SF X 72 UNITS = 5,400 SF OPEN SPACE REQUIRED

OPEN SPACE PROVIDED:	LEVEL 3	PODIUM AMENITY	3,071 SF
	ROOF	ROOF DECK AMENITY	2,948 SF
	TOTAL		6,019 SF



L3

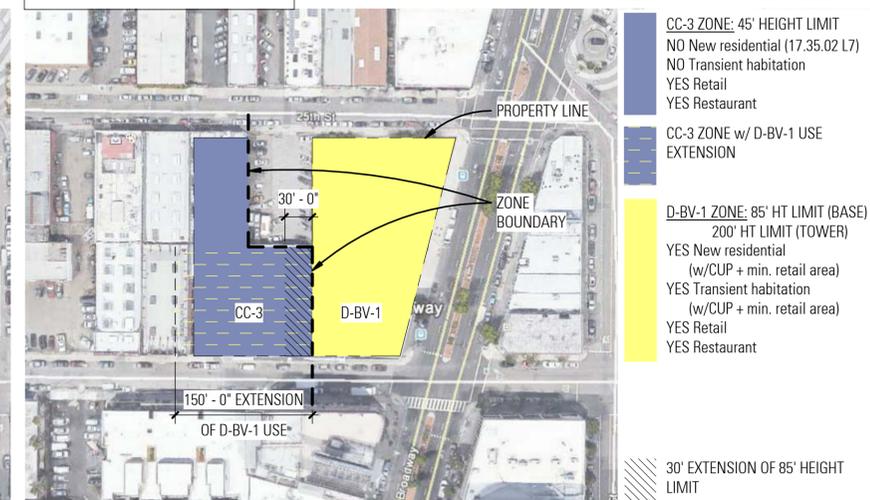


ROOF

VICINITY MAP



ZONING DIAGRAM





9



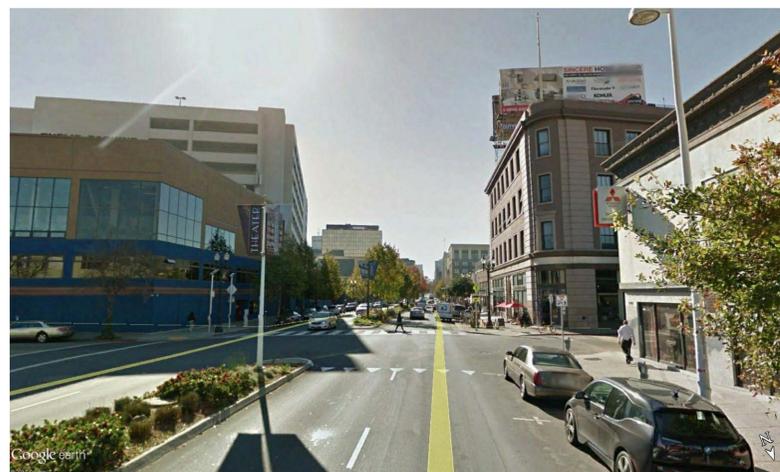
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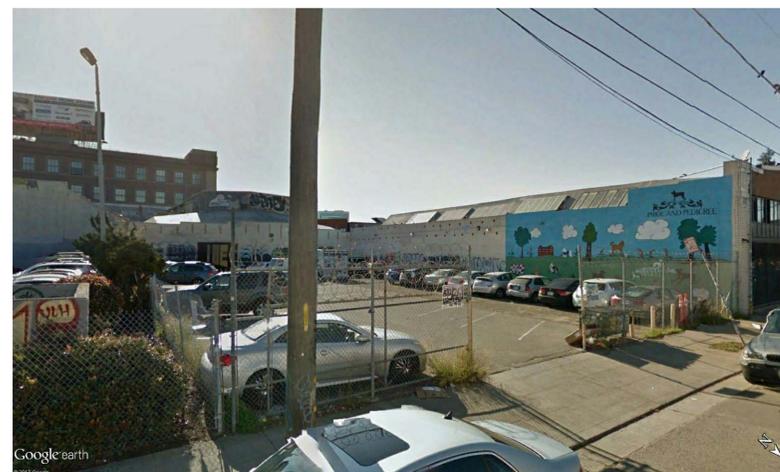
3



8



5



2



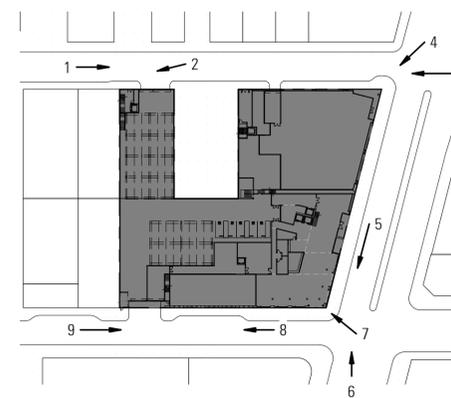
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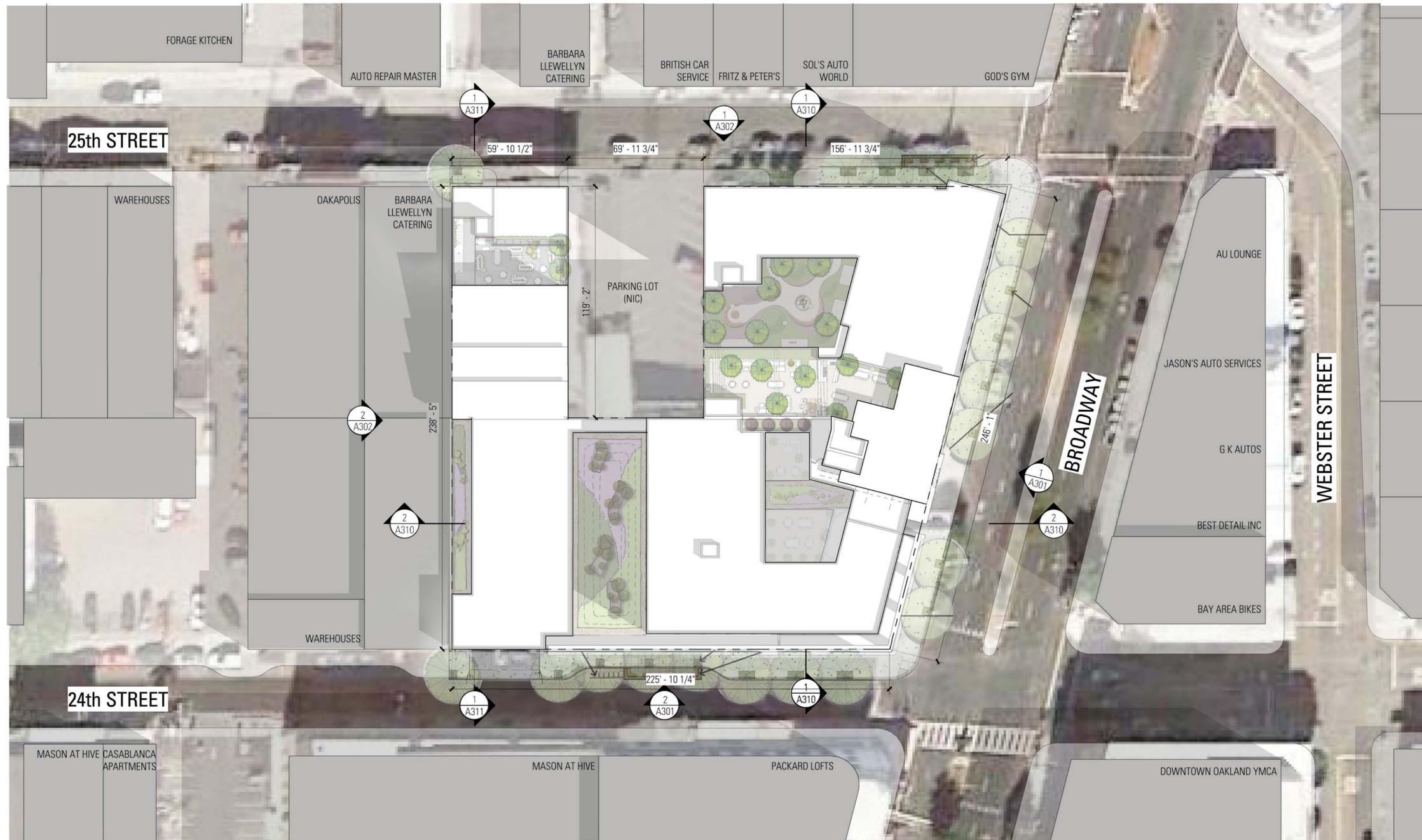
4



1



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SITE PLAN

1" = 30'-0"



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BROADWAY HOTEL
2401 BROADWAY, OAKLAND, CA

PERSPECTIVE VIEW - VIEW FROM SE

09.18.17

17001

BARarchitects

A4



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BROADWAY HOTEL
 2401 BROADWAY, OAKLAND, CA

PERSPECTIVE VIEW - VIEW FROM NE

09.18.17

17001

BARarchitects

A5





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BROADWAY HOTEL
2401 BROADWAY, OAKLAND, CA

PERSPECTIVE VIEW - 25th STREET

09.18.17

17001

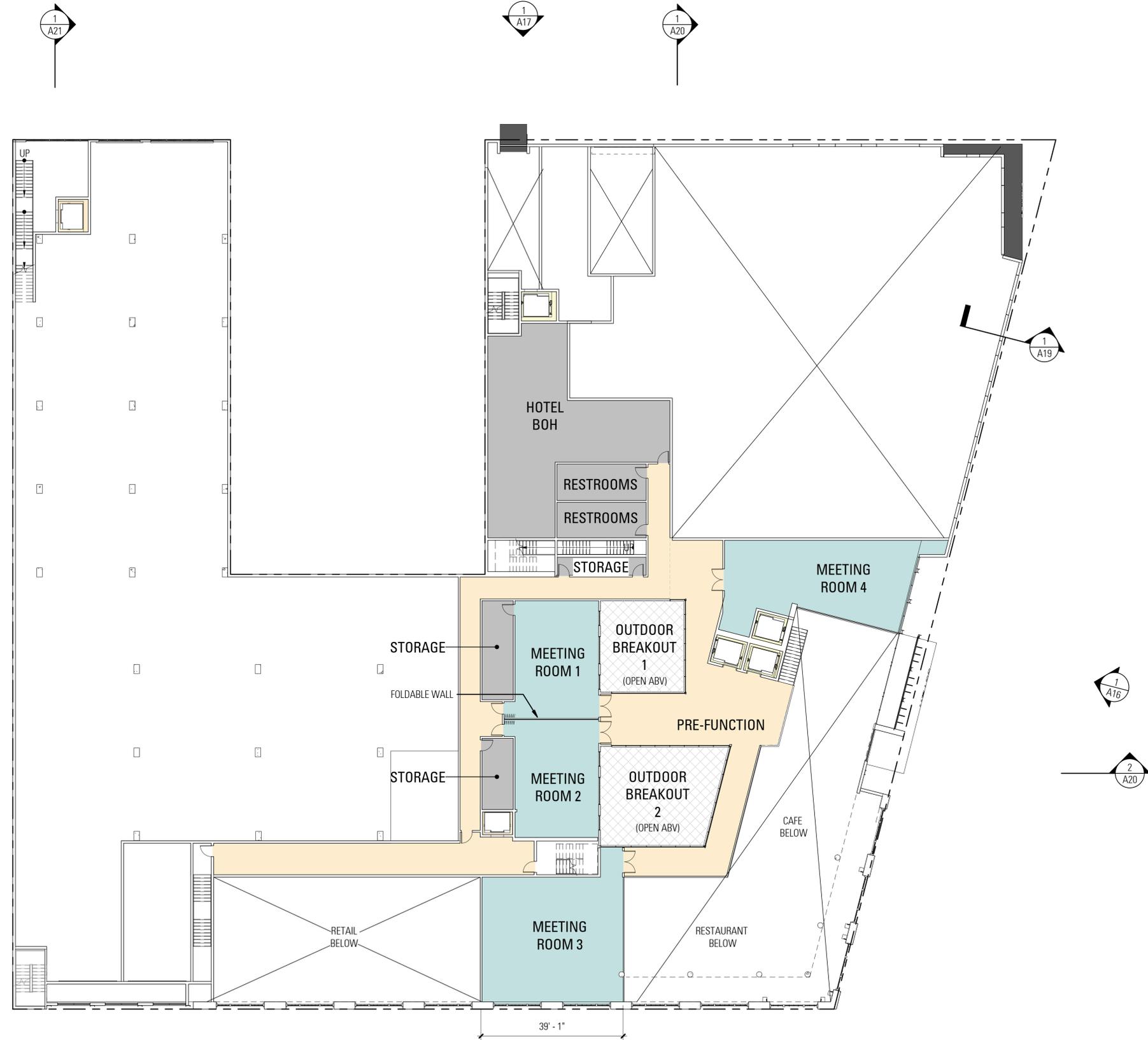
BARarchitects

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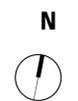
LEGEND

- HOTEL
- HOTEL CIRCULATION
- HOTEL AMENITY
- RESIDENTIAL
- RESIDENTIAL CIRCULATION
- RESIDENTIAL AMENITY
- RETAIL
- RESTAURANT
- SERVICE / BOH
- OUTDOOR

GROSS AREA - LEVEL 2	
HOTEL	11,448 SF
RESIDENTIAL	194 SF
LEVEL 2 TOTAL	11,642 SF



FLOOR PLAN - LEVEL 2
1/16" = 1'-0"



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LEGEND

GROSS AREA - LEVEL 3

BAR	4,030 SF
HOTEL	18,698 SF
RESIDENTIAL	14,540 SF
LEVEL 3 TOTAL	37,268 SF

RESIDENTIAL AREA EFFICIENCY

RENTABLE	57,244 SF	78%
CORE	15,264 SF	
AMENITY	836 SF	
GROSS	73,344 SF	

RESIDENTIAL UNIT TYPE BREAKDOWN

UNIT TYPE	COUNT	ACTUAL
STUDIO	6	8%
JR 1	20	28%
1-BED	20	28%
2-BED	21	29%
3-BED	5	7%
TOTAL	72	

HOTEL AREA EFFICIENCY

RENTABLE	55,417 SF	70%
CORE	18,107 SF	
AMENITY	6,084 SF	
GROSS	79,608 SF	

HOTEL UNIT TYPE BREAKDOWN

UNIT TYPE	COUNT	ACTUAL
ALT KING	2	1%
EXEC KING	31	19%
EXEC KING (ADA)	3	2%
STD	108	68%
STD (ADA)	5	3%
SUITE	8	5%
SUITE (ADA)	1	1%
SUITE (LOFT)	1	1%
TOTAL	159	



FLOOR PLAN - LEVEL 3
1/16" = 1'-0"

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LEGEND

HOTEL	RESIDENTIAL	RETAIL	OUTDOOR
HOTEL CIRCULATION	RESIDENTIAL CIRCULATION	RESTAURANT	
HOTEL AMENITY	RESIDENTIAL AMENITY	SERVICE / BOH	

GROSS AREA - LEVEL 4

BAR	2,031 SF
HOTEL	18,689 SF
RESIDENTIAL	14,748 SF
LEVEL 4 TOTAL	35,468 SF

RESIDENTIAL AREA EFFICIENCY

RENTABLE	57,244 SF	78%
CORE	15,264 SF	
AMENITY	836 SF	
GROSS	73,344 SF	

RESIDENTIAL UNIT TYPE BREAKDOWN

UNIT TYPE	COUNT	ACTUAL
STUDIO	6	8%
JR 1	20	28%
1-BED	20	28%
2-BED	21	29%
3-BED	5	7%
TOTAL	72	

HOTEL AREA EFFICIENCY

RENTABLE	55,417 SF	70%
CORE	18,107 SF	
AMENITY	6,084 SF	
GROSS	79,608 SF	

HOTEL UNIT TYPE BREAKDOWN

UNIT TYPE	COUNT	ACTUAL
ALT KING	2	1%
EXEC KING	31	19%
EXEC KING (ADA)	3	2%
STD	108	68%
STD (ADA)	5	3%
SUITE	8	5%
SUITE (ADA)	1	1%
SUITE (LOFT)	1	1%
TOTAL	159	



FLOOR PLAN - LEVEL 4
1/16" = 1'-0"

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LEGEND

HOTEL	RESIDENTIAL	RETAIL	OUTDOOR
HOTEL CIRCULATION	RESIDENTIAL CIRCULATION	RESTAURANT	
HOTEL AMENITY	RESIDENTIAL AMENITY	SERVICE / BOH	

GROSS AREA - LEVEL 5

HOTEL	11,982 SF
RESIDENTIAL	14,701 SF
LEVEL 5 TOTAL	26,683 SF

RESIDENTIAL AREA EFFICIENCY

RENTABLE	57,244 SF	78%
CORE	15,264 SF	
AMENITY	836 SF	
GROSS	73,344 SF	

RESIDENTIAL UNIT TYPE BREAKDOWN

UNIT TYPE	COUNT	ACTUAL
STUDIO	6	8%
JR 1	20	28%
1-BED	20	28%
2-BED	21	29%
3-BED	5	7%
TOTAL	72	

HOTEL AREA EFFICIENCY

RENTABLE	55,417 SF	70%
CORE	18,107 SF	
AMENITY	6,084 SF	
GROSS	79,608 SF	

HOTEL UNIT TYPE BREAKDOWN

UNIT TYPE	COUNT	ACTUAL
ALT KING	2	1%
EXEC KING	31	19%
EXEC KING (ADA)	3	2%
STD	108	68%
STD (ADA)	5	3%
SUITE	8	5%
SUITE (ADA)	1	1%
SUITE (LOFT)	1	1%
TOTAL	159	



FLOOR PLAN - LEVEL 5
1/16" = 1'-0"

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LEGEND

HOTEL	RESIDENTIAL	RETAIL	OUTDOOR
HOTEL CIRCULATION	RESIDENTIAL CIRCULATION	RESTAURANT	
HOTEL AMENITY	RESIDENTIAL AMENITY	SERVICE / BOH	

GROSS AREA - LEVEL 6

HOTEL	11,980 SF
RESIDENTIAL	14,701 SF
LEVEL 6 TOTAL	26,681 SF

RESIDENTIAL AREA EFFICIENCY

RENTABLE	57,244 SF	78%
CORE	15,264 SF	
AMENITY	836 SF	
GROSS	73,344 SF	

RESIDENTIAL UNIT TYPE BREAKDOWN

UNIT TYPE	COUNT	ACTUAL
STUDIO	6	8%
JR 1	20	28%
1-BED	20	28%
2-BED	21	29%
3-BED	5	7%
TOTAL	72	

HOTEL AREA EFFICIENCY

RENTABLE	55,417 SF	70%
CORE	18,107 SF	
AMENITY	6,084 SF	
GROSS	79,608 SF	

HOTEL UNIT TYPE BREAKDOWN

UNIT TYPE	COUNT	ACTUAL
ALT KING	2	1%
EXEC KING	31	19%
EXEC KING (ADA)	3	2%
STD	108	68%
STD (ADA)	5	3%
SUITE	8	5%
SUITE (ADA)	1	1%
SUITE (LOFT)	1	1%
TOTAL	159	



FLOOR PLAN - LEVEL 6

1/16" = 1'-0"

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LEGEND

 HOTEL	 RESIDENTIAL	 RETAIL	 OUTDOOR
 HOTEL CIRCULATION	 RESIDENTIAL CIRCULATION	 RESTAURANT	
 HOTEL AMENITY	 RESIDENTIAL AMENITY	 SERVICE / BOH	

GROSS AREA - LEVEL 7

HOTEL	11,982 SF
RESIDENTIAL	14,701 SF
LEVEL 7 TOTAL	26,683 SF

RESIDENTIAL AREA EFFICIENCY

RENTABLE	57,244 SF	78%
CORE	15,264 SF	
AMENITY	836 SF	
GROSS	73,344 SF	

RESIDENTIAL UNIT TYPE BREAKDOWN

UNIT TYPE	COUNT	ACTUAL
STUDIO	6	8%
JR 1	20	28%
1-BED	20	28%
2-BED	21	29%
3-BED	5	7%
TOTAL	72	

HOTEL AREA EFFICIENCY

RENTABLE	55,417 SF	70%
CORE	18,107 SF	
AMENITY	6,084 SF	
GROSS	79,608 SF	

HOTEL UNIT TYPE BREAKDOWN

UNIT TYPE	COUNT	ACTUAL
ALT KING	2	1%
EXEC KING	31	19%
EXEC KING (ADA)	3	2%
STD	108	68%
STD (ADA)	5	3%
SUITE	8	5%
SUITE (ADA)	1	1%
SUITE (LOFT)	1	1%
TOTAL	159	



FLOOR PLAN - LEVEL 7
1/16" = 1'-0"

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LEGEND

- HOTEL
- HOTEL CIRCULATION
- HOTEL AMENITY
- RESIDENTIAL
- RESIDENTIAL CIRCULATION
- RESIDENTIAL AMENITY
- RETAIL
- RESTAURANT
- SERVICE / BOH
- OUTDOOR

GROSS AREA - ROOF

GROSS AREA - ROOF	
HOTEL	557 SF
RESIDENTIAL	808 SF
ROOF TOTAL	1,365 SF

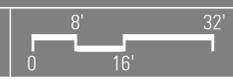


ROOF PLAN

1/16" = 1'-0"



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METAL CHANNEL TRIM



FRITTED GLASS SUNSCREEN



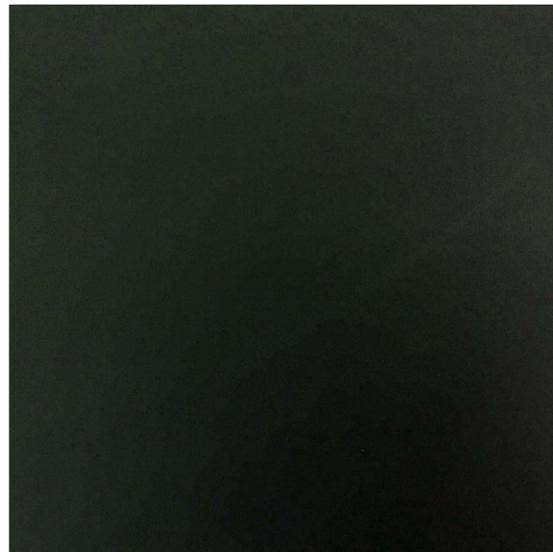
BRICK



DARK WINDOWS



GLASS BALCONY RAIL



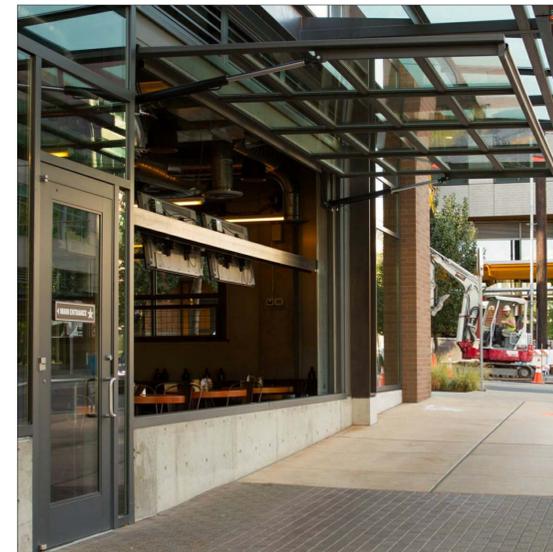
METAL PANEL



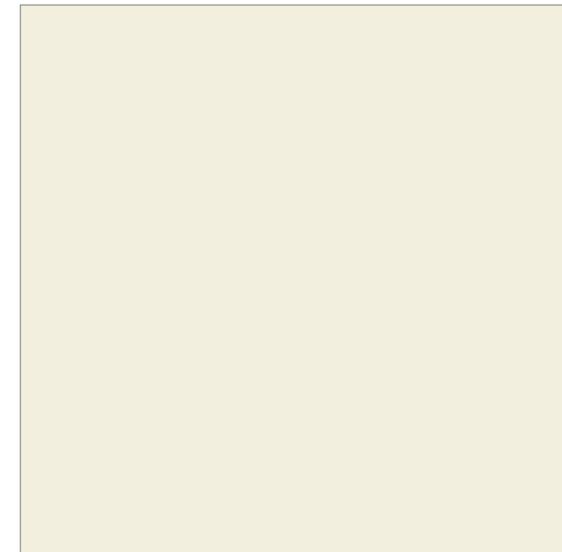
CONCRETE



STOREFRONT



METAL TILT DOOR



STUCCO

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BROADWAY HOTEL
 2401 BROADWAY, OAKLAND, CA

STREET VIEW - SE CORNER

09.18.17

17001

BARarchitects

A17



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BROADWAY HOTEL
2401 BROADWAY, OAKLAND, CA

STREET VIEW - BROADWAY ENTRY

09.18.17

17001

BARarchitects

A18



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BROADWAY HOTEL
 2401 BROADWAY, OAKLAND, CA

STREET VIEW - NE CORNER

09.18.17

17001

BARarchitects

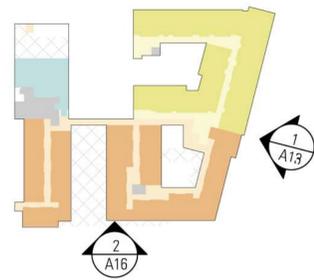
A19



2
A16
EXTERIOR ELEVATION - SOUTH (24TH STREET)
1/16" = 1'-0"



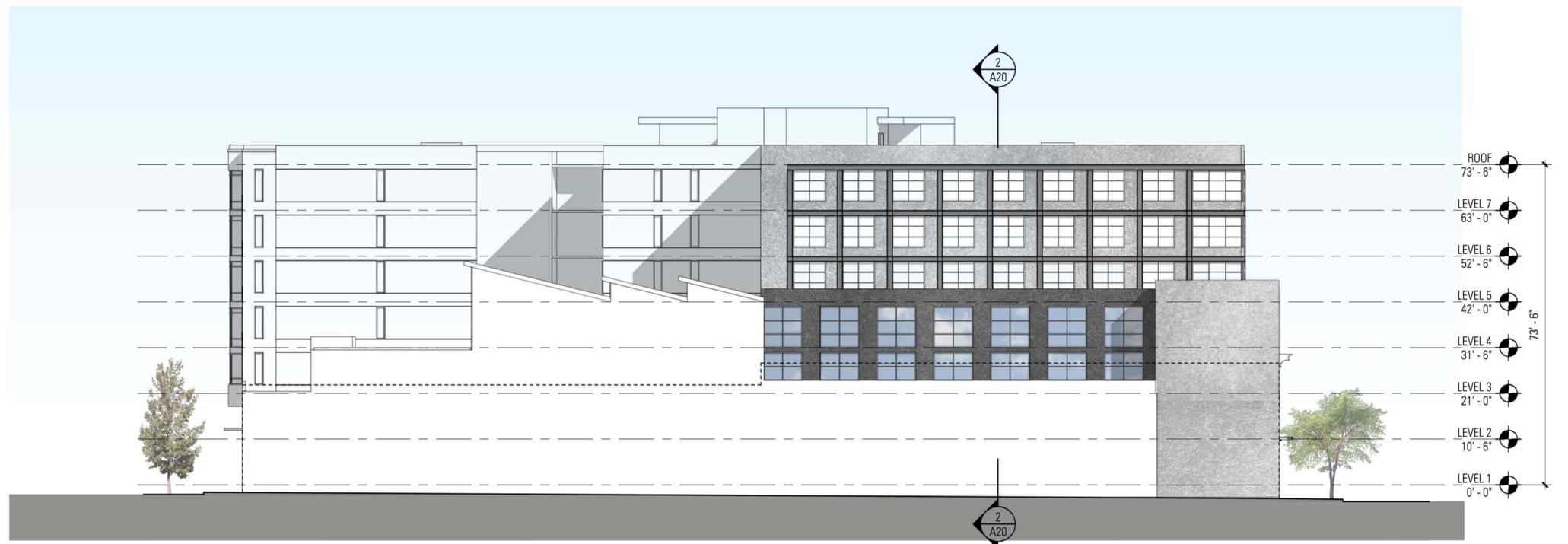
1
A16
EXTERIOR ELEVATION - EAST (BROADWAY)
1/16" = 1'-0"



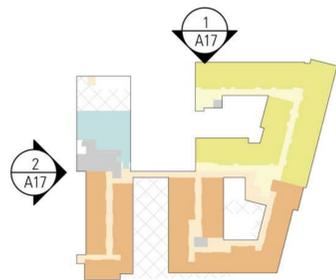
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1
A17
EXTERIOR ELEVATION - NORTH (25TH STREET)
1/16" = 1'-0"

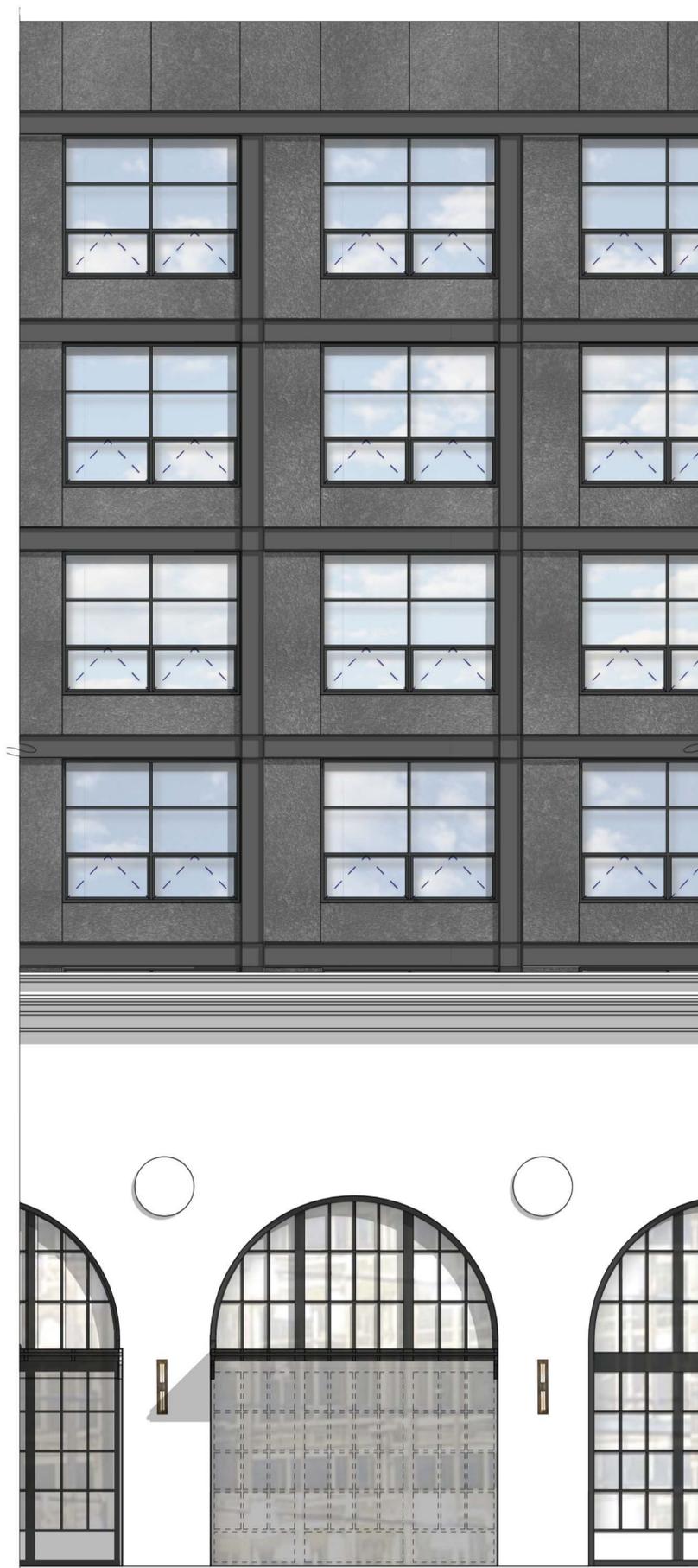
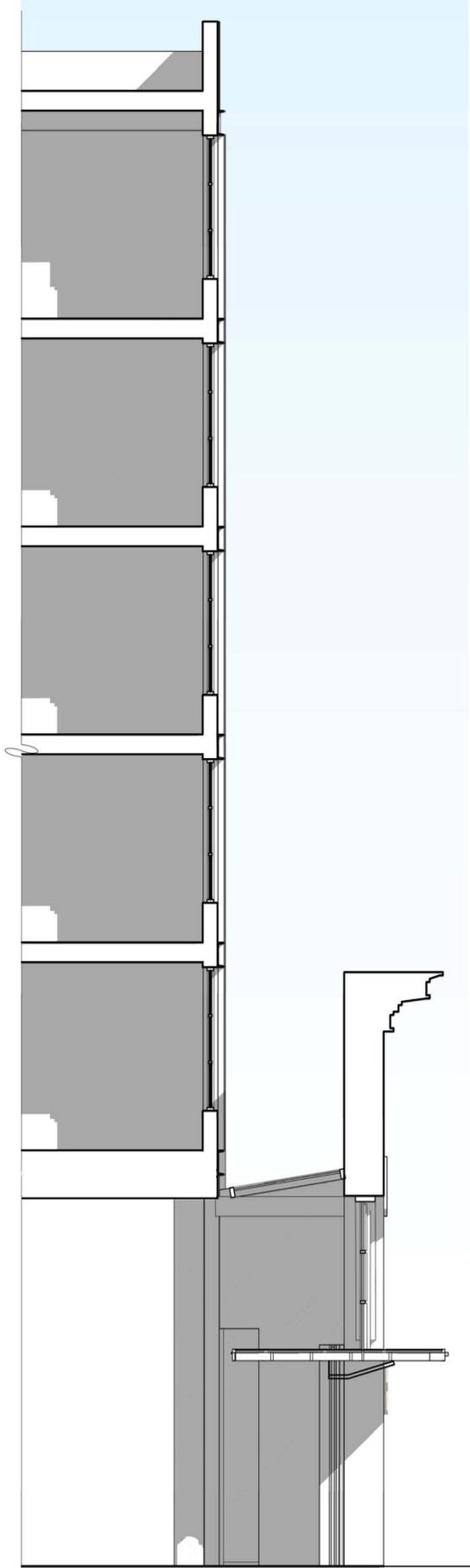


2
A17
EXTERIOR ELEVATION - WEST
1/16" = 1'-0"



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9/15/2017 4:54:19 PM C:\0.REV\17001 2401 Broadway Hotel-CENTRAL_COCommon.rvt



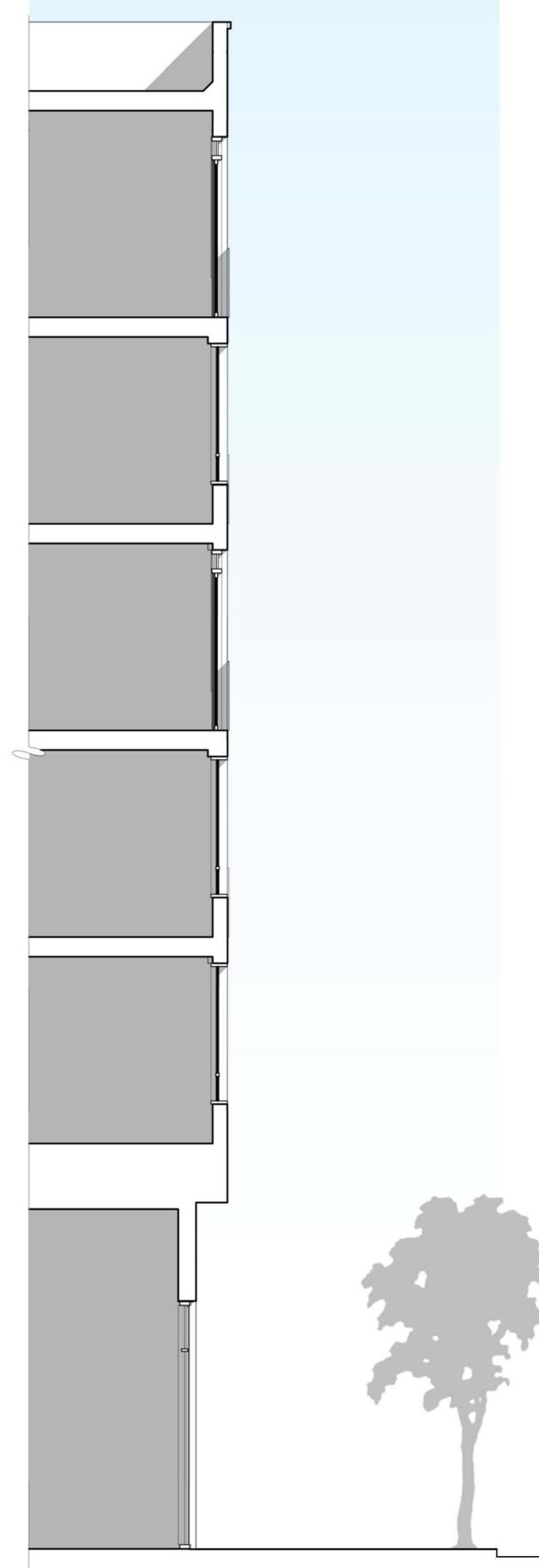
- ROOF
73'-6"
- LEVEL 7
63'-0"
- LEVEL 6
52'-6"
- LEVEL 5
42'-0"
- LEVEL 4
31'-6"
- LEVEL 3
21'-0"
- LEVEL 2
10'-6"
- LEVEL 1
0'-0"

HOTEL WALL SECTION (L)

1/4" = 1'-0"

ENLARGED ELEVATION (R)

1/4" = 1'-0"

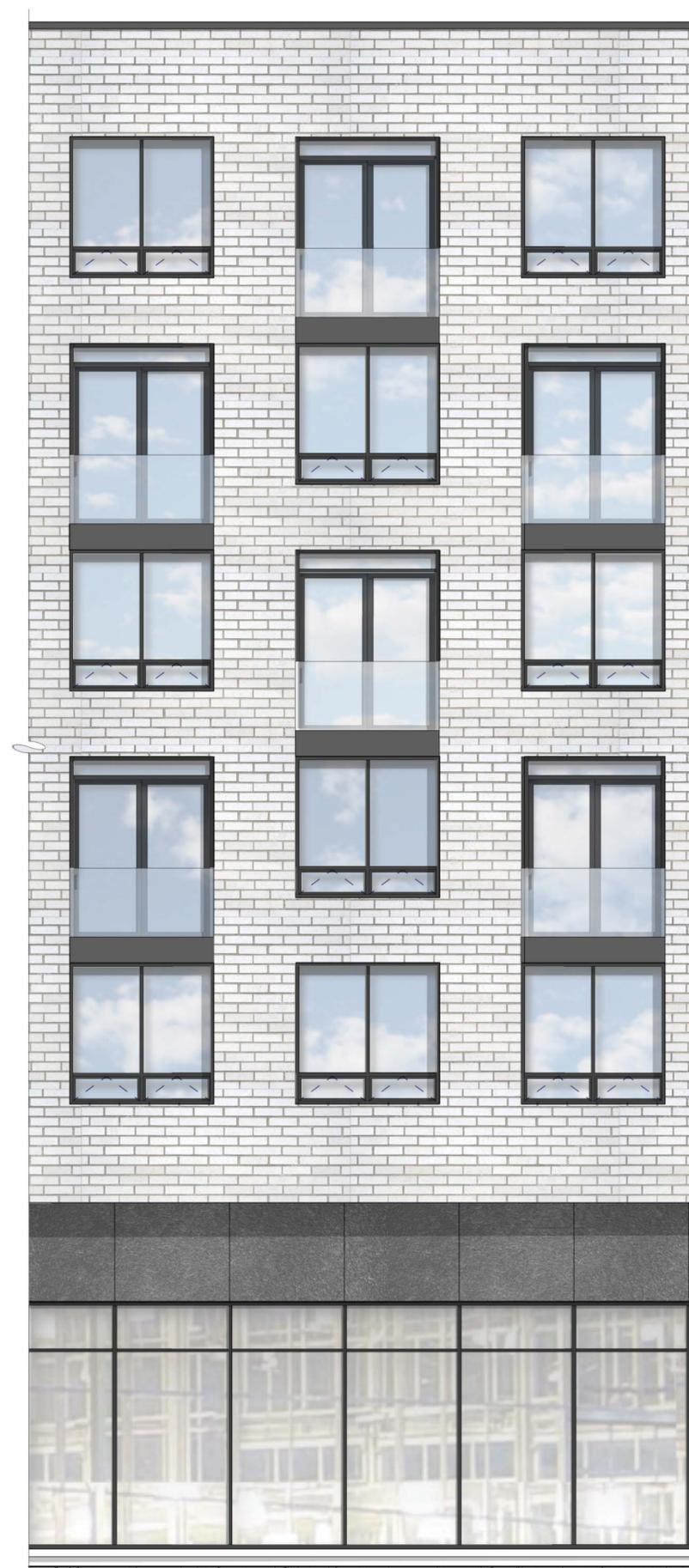


RESIDENTIAL WALL SECTION (L)

1/4" = 1'-0"

ENLARGED ELEVATION (R)

1/4" = 1'-0"



ROOF
73' - 6"

LEVEL 7
63' - 0"

LEVEL 6
52' - 6"

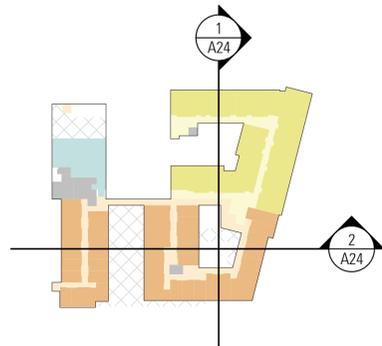
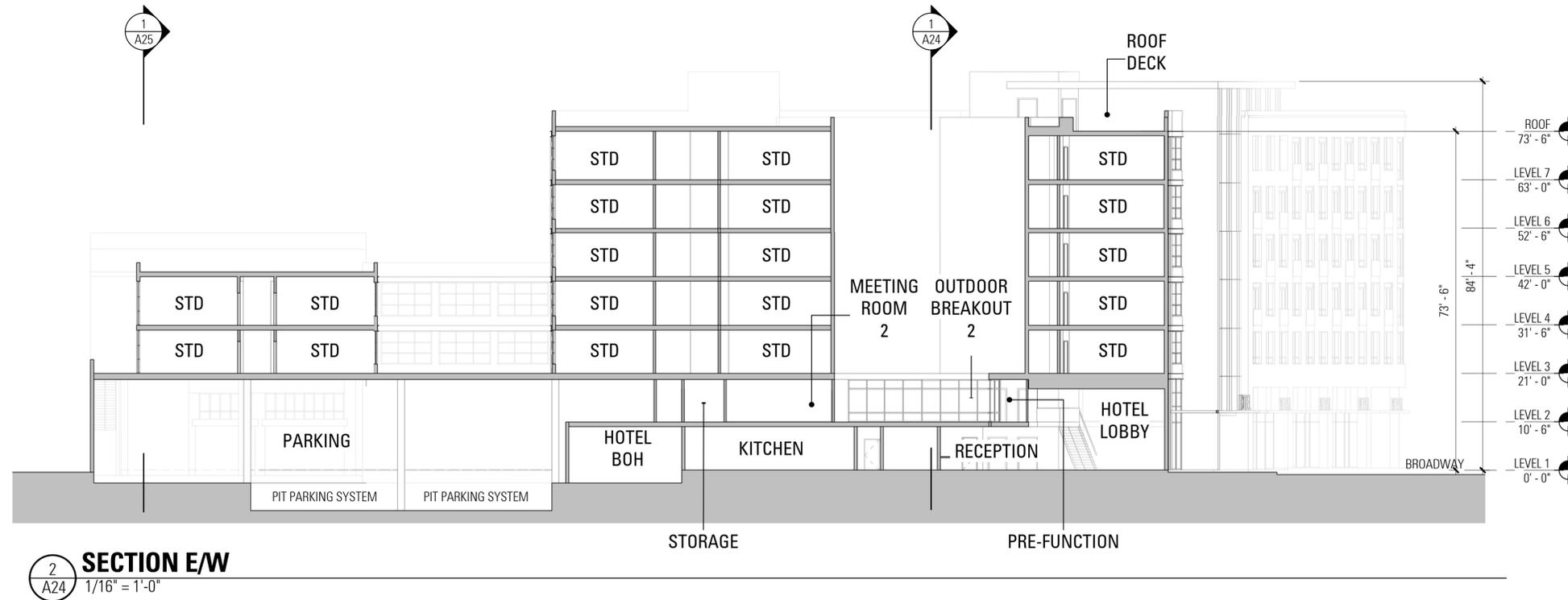
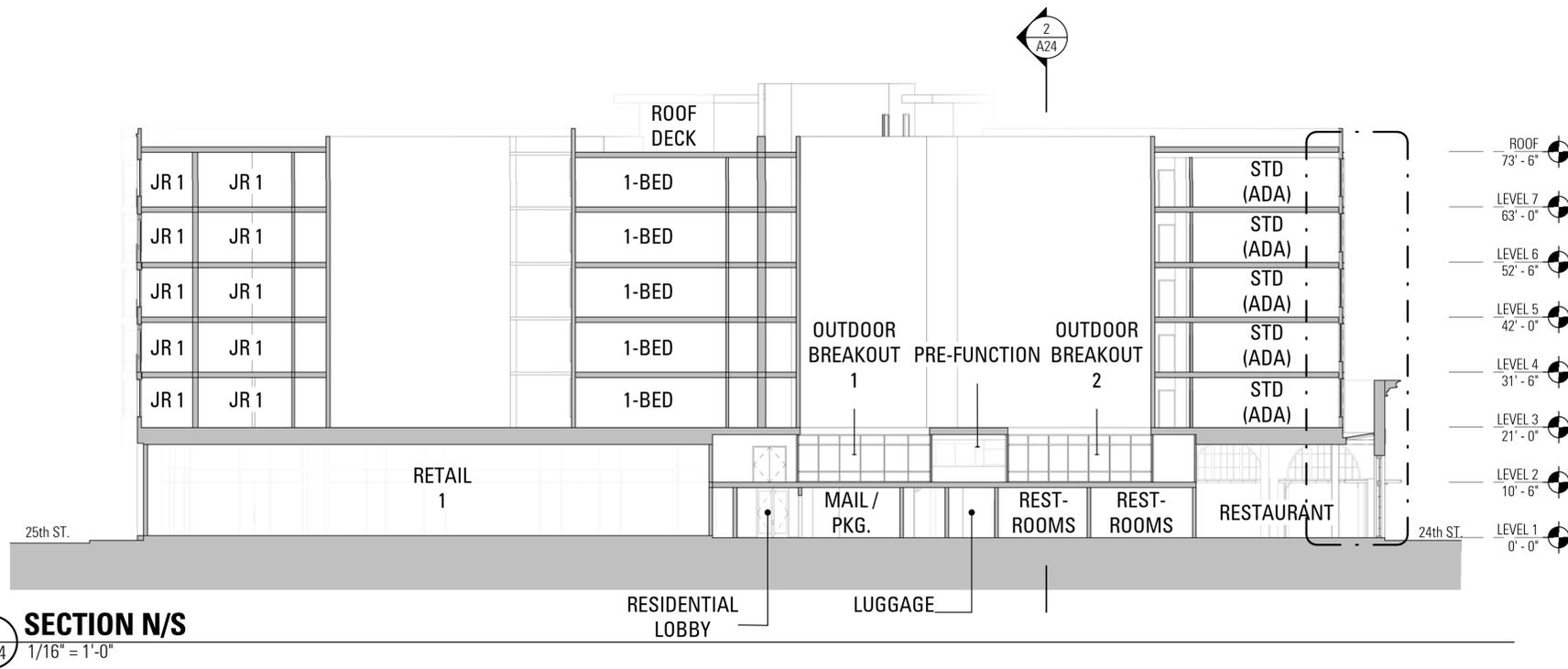
LEVEL 5
42' - 0"

LEVEL 4
31' - 6"

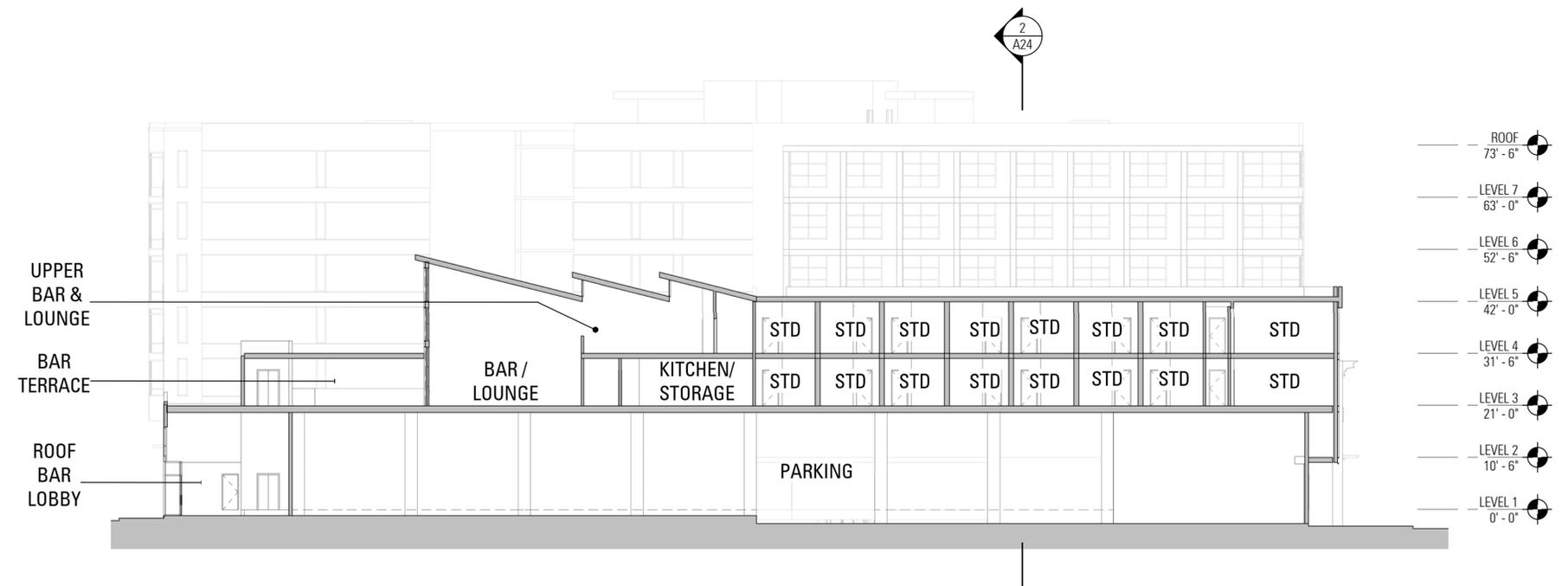
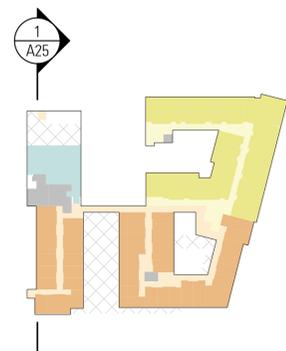
LEVEL 3
21' - 0"

LEVEL 2
10' - 6"

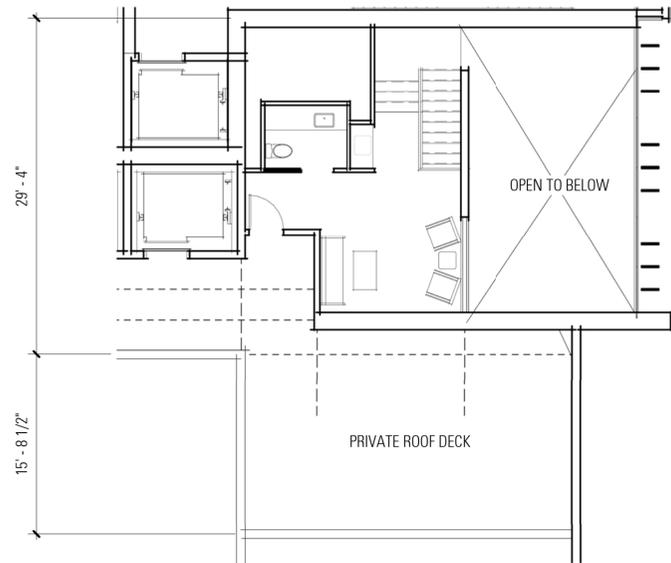
LEVEL 1
0' - 0"



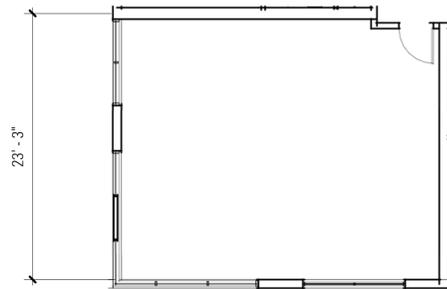
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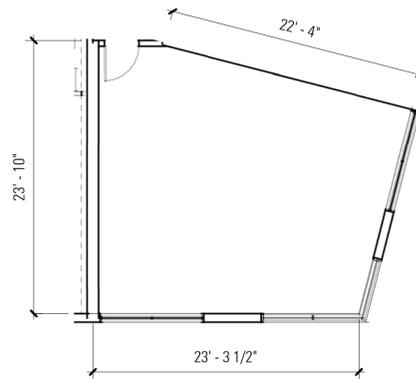
1 SECTION THROUGH ROOF BAR
A25 1/16" = 1'-0"



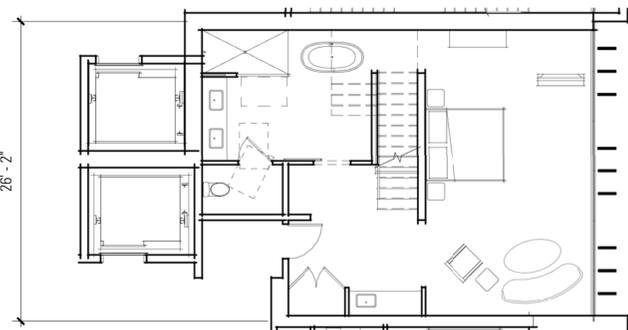
LOFT SUITE - ROOF LEVEL
1/8" = 1'-0"



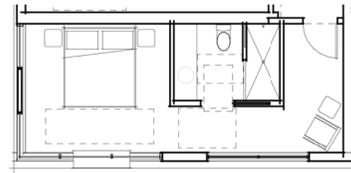
SUITE - SW
1/8" = 1'-0"



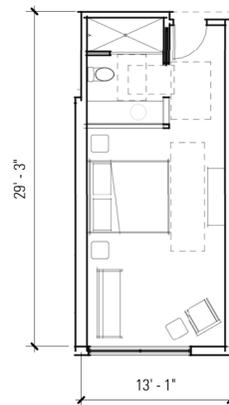
SUITE - SE
1/8" = 1'-0"



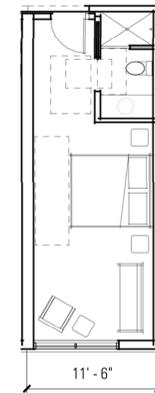
LOFT SUITE - LEVEL 7
1/8" = 1'-0"



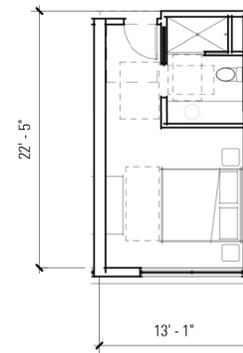
EXECUTIVE KING - SW CORNER
1/8" = 1'-0"



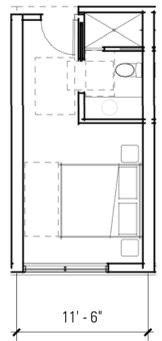
EXECUTIVE KING - 13'-1"
1/8" = 1'-0"



EXECUTIVE KING - 11'-6"
1/8" = 1'-0"



STANDARD KING - 13'-1"
1/8" = 1'-0"

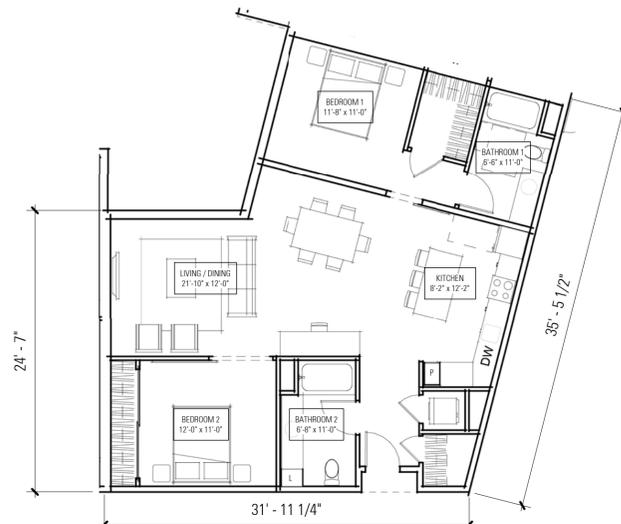


STANDARD KING - 11'-6"
1/8" = 1'-0"

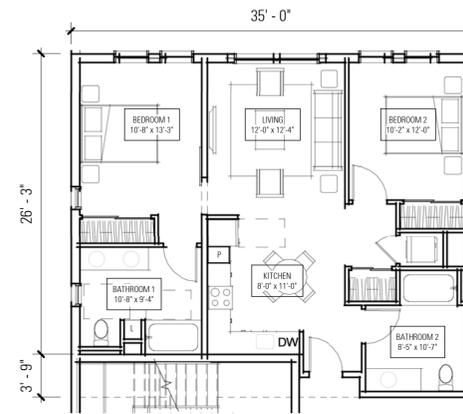
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UNIT D1
1/8" = 1'-0"



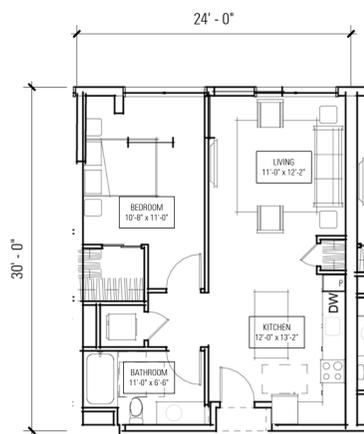
UNIT C3
1/8" = 1'-0"



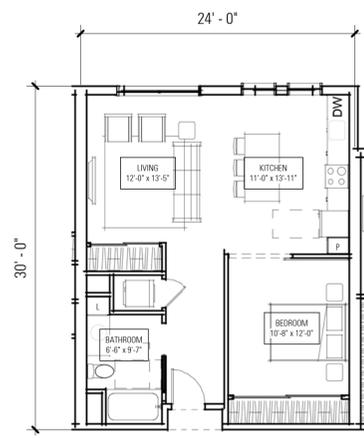
UNIT C2
1/8" = 1'-0"



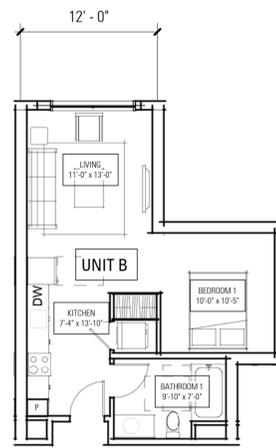
UNIT C1
1/8" = 1'-0"



UNIT B2
1/8" = 1'-0"



UNIT B1
1/8" = 1'-0"



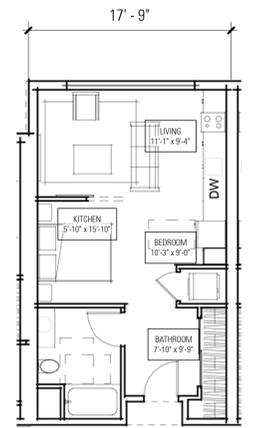
UNIT A3.2
1/8" = 1'-0"



UNIT A3.1
1/8" = 1'-0"



UNIT A2
1/8" = 1'-0"



UNIT A1
1/8" = 1'-0"

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LEED v4 for BD+C: New Construction and Major Renovation
Project Checklist

Project Name:
Date:

Y ? N

Credit	Integrative Process	1
--------	---------------------	---

13	0	0	Location and Transportation	16
Credit			LEED for Neighborhood Development Location	16
1			Sensitive Land Protection	1
1			High Priority Site	2
5			Surrounding Density and Diverse Uses	5
5			Access to Quality Transit	5
1			Bicycle Facilities	1
			Reduced Parking Footprint	1
			Green Vehicles	1

6	0	0	Sustainable Sites	10
Y			Prereq Construction Activity Pollution Prevention	Required
1			Credit Site Assessment	1
			Credit Site Development - Protect or Restore Habitat	2
			Credit Open Space	1
3			Credit Rainwater Management	3
2			Credit Heat Island Reduction	2
			Credit Light Pollution Reduction	1

7	0	0	Water Efficiency	11
Y			Prereq Outdoor Water Use Reduction	Required
Y			Prereq Indoor Water Use Reduction	Required
Y			Prereq Building-Level Water Metering	Required
2			Credit Outdoor Water Use Reduction	2
4			Credit Indoor Water Use Reduction	6
			Credit Cooling Tower Water Use	2
1			Credit Water Metering	1

6	0	0	Energy and Atmosphere	33
Y			Prereq Fundamental Commissioning and Verification	Required
Y			Prereq Minimum Energy Performance	Required
Y			Prereq Building-Level Energy Metering	Required
Y			Prereq Fundamental Refrigerant Management	Required
			Credit Enhanced Commissioning	6
			Credit Optimize Energy Performance	18
5			Credit Advanced Energy Metering	1
			Credit Demand Response	2
			Credit Renewable Energy Production	3
1			Credit Enhanced Refrigerant Management	1
			Credit Green Power and Carbon Offsets	2

2	0	0	Materials and Resources	13
Y			Prereq Storage and Collection of Recyclables	Required
Y			Prereq Construction and Demolition Waste Management Planning	Required
			Credit Building Life-Cycle Impact Reduction	5
			Credit Building Product Disclosure and Optimization - Environmental Product Declarations	2
			Credit Building Product Disclosure and Optimization - Sourcing of Raw Materials	2
			Credit Building Product Disclosure and Optimization - Material Ingredients	2
2			Credit Construction and Demolition Waste Management	2

5	0	0	Indoor Environmental Quality	16
Y			Prereq Minimum Indoor Air Quality Performance	Required
Y			Prereq Environmental Tobacco Smoke Control	Required
			Credit Enhanced Indoor Air Quality Strategies	2
3			Credit Low-Emitting Materials	3
1			Credit Construction Indoor Air Quality Management Plan	1
			Credit Indoor Air Quality Assessment	2
1			Credit Thermal Comfort	1
			Credit Interior Lighting	2
			Credit Daylight	3
			Credit Quality Views	1
			Credit Acoustic Performance	1

1	0	0	Innovation	6
			Credit Innovation	5
1			Credit LEED Accredited Professional	1

0	0	0	Regional Priority	4
			Credit Regional Priority: Specific Credit	1
			Credit Regional Priority: Specific Credit	1
			Credit Regional Priority: Specific Credit	1
			Credit Regional Priority: Specific Credit	1

40	0	0	TOTALS	Possible Points: 110
Certified: 40 to 49 points, Silver: 50 to 59 points, Gold: 60 to 79 points, Platinum: 80 to 110				

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ROOF - PARKING ANALYSIS

1" = 30'-0"

AREA - ROOF	
HOTEL	557 SF
ROOF TOTAL	557 SF



LEVELS 5-7 - PARKING ANALYSIS

1" = 30'-0"

AREA - LEVEL 5 (6, 7 SIM.)	
HOTEL	11,982 SF
LEVEL 5 - 7 TYPICAL	11,982 SF
LEVELS 5-7 TOTAL =	35,946 SF



LEVEL 4 - PARKING ANALYSIS

1" = 30'-0"

AREA - LEVEL 4	
BAR	2,031 SF
HOTEL	18,689 SF
LEVEL 4 TOTAL	20,720 SF



LEVEL 3 - PARKING ANALYSIS

1" = 30'-0"

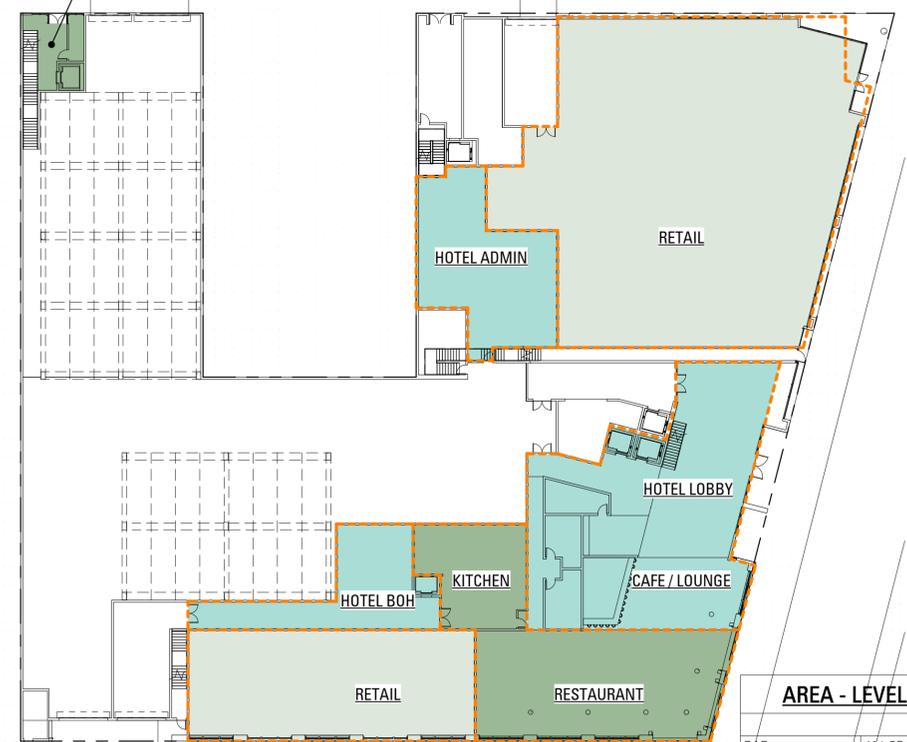
AREA - LEVEL 3	
BAR	4,030 SF
HOTEL	18,698 SF
LEVEL 3 TOTAL	22,728 SF



LEVEL 2 - PARKING ANALYSIS

1" = 30'-0"

AREA - LEVEL 2	
HOTEL	11,448 SF
LEVEL 2 TOTAL	11,448 SF
TO BE INCLUDED AS MEZZANINE FOR PARKING COUNTS	

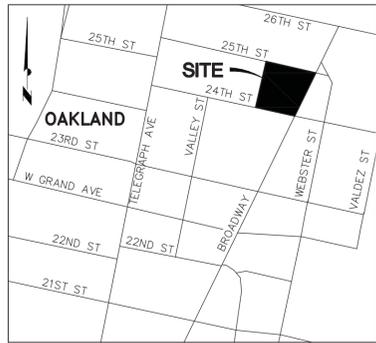


LEVEL 1 - PARKING ANALYSIS

1" = 30'-0"

AREA - LEVEL 1	
BAR	401 SF
CAFE	1,285 SF
HOTEL	7,801 SF
RESTAURANT	4,806 SF
RETAIL	14,572 SF
LEVEL 1 TOTAL	28,865 SF

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VICINITY MAP
NOT TO SCALE

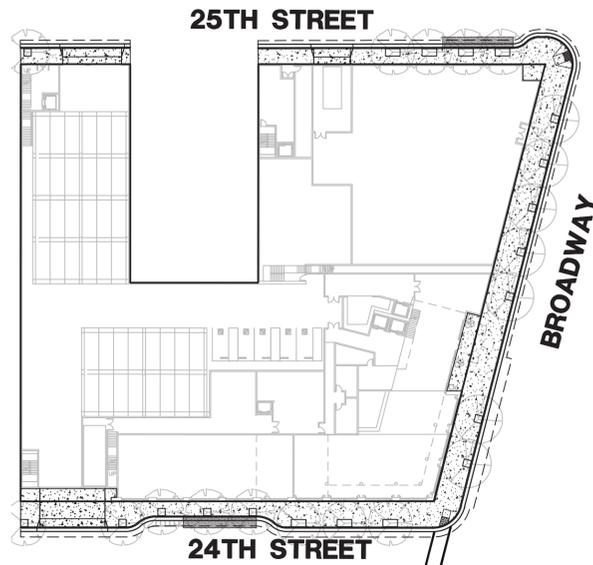
ABBREVIATIONS

AB	AGGREGATE BASE	L	LEFT
AC	ASPHALT CONCRETE	LE	LANDSCAPE EASEMENT
AD	AREA DRAIN	LF	LINEAL FEET
AP	ANGLE POINT	LIP	LIP OF GUTTER
ARV	AIR RELEASE VALVE	LP	LOW POINT
BC	BEGINNING OF CURVE	MAX	MAXIMUM
BFP	BACKFLOW PREVENTER	MEP	MECHANICAL, ELECTRICAL, PLUMBING
BOV	BLOW-OFF VALVE	MIN	MINIMUM
BOW	BACK OF WALK	MON	MONUMENT
BW	BOTTOM OF WALL	(N)	NORTH
BVC	BEGINNING OF VERTICAL CURVE	NEC.	NECESSARY
C&G	CURB & GUTTER	OC	ON CENTER
CB	CATCH BASIN	PCC	POINT OF COMPOUND CURVE
CTV	CABLE TELEVISION	PG	PAVEMENT GRADE
CL	CENTERLINE	PL	PROPERTY LINE
CMP	CORRUGATED METAL PIPE	POC	POINT OF CONNECTION
CO	CLEANOUT	PR	PEDESTRIAN RAMP
CONC	CONCRETE	PSDE	PRIVATE STORM DRAIN EASEMENT
CONF.	CONFORM TO	PUE	PUBLIC UTILITY EASEMENT
CONST	CONSTRUCT	PVC	POLYVINYL CHLORIDE
COTG	CLEANOUT TO GRADE	R	RIGHT
CR	CURB RETURN	(R)	RADIAL
D/W	DRIVEWAY	RCP	REINFORCED CONCRETE PIPE
DIA	DIAMETER	REC	RECORD INFORMATION
DR	DRIVE	RD	ROAD
DTL	DETAIL	RL	RESTRAIN LENGTH
DWG	DRAWING	R/W	RIGHT OF WAY
(E)	EAST	(S)	SOUTH
EC	END OF CURVE	SBL	SETBACK LINE
EG	EXISTING GRADE	SD	STORM DRAIN
ELEC	ELECTRICAL	SDHW	STORM DRAIN HEADWALL
EP	EDGE OF PAVEMENT	SDMH	STORM DRAIN MANHOLE
EVAE	EMERGENCY VEHICLE ACCESS EASEMENT	SF	SQUARE FEET
EVC	END OF VERTICAL CURVE	SHT	SHEET
EX	EXISTING	S-LAT	SANITARY SEWER LATERAL
(F)	FUTURE	SS	SANITARY SEWER
FC	FACE OF CURB	SSMH	SANITARY SEWER MANHOLE
FDC	FIRE DEPARTMENT CONNECTION	SSRI	SANITARY SEWER RODDING INLET
FF	FINISH FLOOR	STD	STANDARD
FG	FINISH GRADE	S/W	SIDEWALK
FH	FIRE HYDRANT	T	TELEPHONE
FI	FIELD INLET	TC	TOP OF CURB
FL	FLOW LINE	TD	TRENCH DRAIN
F-MAIN	FORCE MAIN	TG	TOP OF GRADE
G	GAS	T-MAIN	TRANSMISSION MAIN
GB	GRADE BREAK	TS	TOP OF SLAB
GR	GRATE	TW	TOP OF WALL
HCR	HANDICAP RAMP	TYP	TYPICAL
HDPE	HIGH DENSITY POLYETHYLENE	VC	VERTICAL CURB
HP	HIGH POINT	W	WATER MAIN
INTX	INTERSECTION	(W)	WEST
INV	INVERT	W-LAT	WATER LATERAL
IRR	IRRIGATION	[]	RECORD INFORMATION
JP	JOINT POLE		

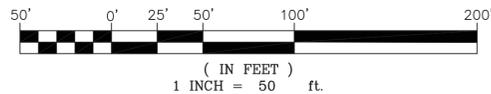
2401 BROADWAY

DESIGN DEVELOPMENT

OAKLAND, CALIFORNIA



SITE MAP



DESIGN TEAM

- DEVELOPER/CONTRACTOR: SIGNATURE DEVELOPMENT GROUP
2335 BROADWAY, SUITE 200
OAKLAND, CA 94612
(510) 251-9276
CONTACT - JAMIE CHOY
- CIVIL ENGINEER: dk CONSULTING
1931 SAN MIGUEL DRIVE, SUITE 100
WALNUT CREEK, CA 94596
(925) 932-6868
CONTACT - SCOTT E HARTSTEIN
- ARCHITECT: BAR ARCHITECTS
901 BATTERY STREET, SUITE 300
SAN FRANCISCO, CA 94111
(415) 293-5700
CONTACT - BRADLEY SUGARMAN
- LANDSCAPE ARCHITECT: BRUCE JETT ASSOCIATES, INC.
2 THEATRE SQUARE, STE 218
ORINDA, CA 94563
(925) 254-5422
CONTACT - JESSE MARKMAN
- GEOTECHNICAL ENGINEER: ENGEQ, INC.
2010 GROW CANYON PLACE, SUITE 250
SAN RAMON, CA 94583
(925) 866-9000
CONTACT - JEFF FIPPIN

PROJECT INFORMATION

- PROJECT INFORMATION: APN: 008-0674-036, -005
008-0674-004, -003-1
SIZE: ±1.21 ACRES
ZONING: D-BV1
CC-3
- TITLE REPORT: CHICAGO TITLE COMPANY
OWNERS POLICY TITLE NO.
14-58205703-B-KD
DATED NOVEMBER 21, 2014
- BASIS OF BEARINGS: THE MONUMENT LINE ON 24TH STREET TAKEN AS
NORTH 78°04'52" WEST, PARCEL MAP 9872
(314M53) SHOWN AS NORTH 78°05'47" WEST.
- BENCHMARK: TOP OF CURB WEST RETURN OF THE NORTHWEST
CORNER OF 23RD STREET AND TELEGRAPH AVENUE,
WITH AN ELEVATION OF 17.684 FEET PER THE CITY
OF OAKLAND MONUMENT BENCHMARK No 37Jc ON
BOOK 18 AT PAGE 71.
- TOPOGRAPHIC SURVEY: BUILDING LOCATIONS ARE BASED UPON AERIAL
SURVEY PERFORMED ON SEPTMEBER 17, 2015 AND
FIELD VERIFIED ON SEPTEMBER 29, 2015 AND ARE
PRELIMINARY. AN UPDATE TO THE BUILDING FACE TO
FOLLOW.

LEGEND

EXISTING	PROPOSED	BOUNDARY
		RIGHT-OF-WAY
		ADJACENT PROPERTY
		MONUMENT LINE
		CURB AND GUTTER LINE
		GAS LINE, FIELD LOCATED, VALVE
		RECORD GAS LINE
		ELECTRIC LINE, BOX
		COMMUNICATION MANHOLE AND LINE, PACIFIC BELL
		RECORD SANITARY SEWER LINE, MANHOLE, CLEANOUT
		RECORD STORM DRAIN LINE, FIELD INLET
		STORM DRAIN LINE, MANHOLE, CURB INLET
		C.3 TREATMENT
		WATER LINE, FIELD LOCATED, VALVE, METER, FH
		RECORD WATER LINE
		SAWCUT LINE
		CONTOUR LINE
		SANITARY SEWER, SS LATERAL
		UTILITY POLE WITH GUY ANCHOR
		VEGETATION, TREE
		FENCE
		FIRE HYDRANT
		ELECTROLIER
		POST
		SIGN
		OTHER MANHOLE, AS NOTED
		UTILITY VALVE
		FOUND STANDARD STREET MONUMENT
		MISSING STREET MONUMENT
		CALTRANS CURB RAMP
		DRIVEWAY
		CONCRETE SIDEWALK
		PARKLET
		BULB OUT
		LOT NUMBER
		RECORD UTILITY INFORMATION
		PROPORTION FACTOR
		SEARCHED FOR, NOT FOUND
		MONUMENT TO MONUMENT
		NOT A PART OF THIS SURVEY
		SPOT ELECTION
		SLOPE

SHEET INDEX

SHEET	DESCRIPTION
C1.0	COVER SHEET
C1.1	GENERAL NOTES
C2.0	DETAILS
C2.1	DETAILS
C3.0	TOPOGRAPHIC SURVEY
C4.0	DEMOLITION PLAN
C5.0	SITE PLAN
C6.0	MASS GRADING PLAN
C6.1	FINE GRADING PLAN
C7.0	UTILITY PLAN
C8.0	EROSION CONTROL PLAN
C8.1	EROSION CONTROL NOTES & DETAILS
C9.0	C.3 EXHIBIT

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CAUTION:
CONTRACTOR TO EXERCISE EXTREME CAUTION TO AVOID DAMAGING EXISTING UNDERGROUND UTILITIES. OBSTRUCTIONS INDICATED ARE FOR INFORMATION ONLY. IT IS THE CONTRACTORS RESPONSIBILITY TO VERIFY THE LOCATION AND DEPTH WITH THE APPROPRIATE AGENCIES. NEITHER THE OWNER NOR THE ENGINEER ASSUMES RESPONSIBILITY THAT THE OBSTRUCTIONS INDICATED WILL ACTUALLY BE THE OBSTRUCTIONS ENCOUNTERED. POTHOLES ARE REQUIRED.

GENERAL NOTES

1. THE ENGINEER ASSUMES NO RESPONSIBILITY BEYOND THE ADEQUACY OF HIS DESIGN CONTAINED HEREIN.
2. ALL STAKING REQUESTS SHALL BE DIRECTED TO THE ENGINEER AT LEAST 2 WORKING DAYS PRIOR TO ACTUAL NEED. THE PROTECTION OF THOSE STAKES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ANY ADDITIONAL STAKING OR RESTAKING WILL BE DONE ONLY AS DIRECTED AND AUTHORIZED BY THE OWNER OR HIS AUTHORIZED AGENT.
3. OBSTRUCTIONS INDICATED ARE FOR INFORMATION ONLY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION AND DEPTH WITH THE APPROPRIATE AGENCIES. NEITHER THE OWNER NOR THE ENGINEER ASSUMES RESPONSIBILITY THAT THE OBSTRUCTIONS INDICATED WILL ACTUALLY BE THE OBSTRUCTIONS ENCOUNTERED. THE CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICE ALERT (800) 642-2222, TWO WORKING DAYS PRIOR TO ANY EXCAVATION.
4. CONTRACTOR SHALL COMPLY WITH THE RULES AND REGULATIONS OF THE STATE CONSTRUCTION SAFETY ORDERS.
5. THE CONTRACTOR WILL LIMIT CONSTRUCTION ACTIVITIES TO MONDAYS THROUGH FRIDAYS FROM THE HOURS SPECIFIED IN THE ENCROACHMENT PERMIT. CONSTRUCTION WILL NOT BE ALLOWED LATER THAN THESE HOURS AND/OR ON SATURDAYS, SUNDAYS OR FEDERAL HOLIDAYS UNLESS PRIOR APPROVAL IS GRANTED BY THE CITY AFTER CONSULTATION WITH ADJACENT USES.
6. CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY, AND HOLD THE CITY, PROJECT ENGINEER, HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF OWNER OR ENGINEER.
7. IF IT APPEARS THAT THE WORK TO BE DONE, OR ANY MATTER RELATIVE THERETO, IS NOT SUFFICIENTLY DETAILED OR EXPLAINED ON THESE PLANS, THE CONTRACTOR SHALL CONTACT dk CONSULTING (925) 932-6868 FOR SUCH FURTHER EXPLANATIONS AS MAY BE NECESSARY.
8. ALL EXISTING ELEVATIONS SHOWN ARE AS MEASURED IN THE FIELD, UNLESS OTHERWISE NOTED.
9. THE WORKSITE SHALL BE MAINTAINED IN AN ORDERLY FASHION. FOLLOWING THE CESSATION OF CONSTRUCTION ACTIVITY, ALL CONSTRUCTION DEBRIS SHALL BE REMOVED FROM THE SITE.
10. THE CONTRACTORS SHALL FIT ALL INTERNAL COMBUSTION ENGINES WITH MUFFLERS AND SHALL LOCATE NOISE-GENERATING EQUIPMENT SUCH AS AIR COMPRESSORS, CONCRETE PUMPERS AND POWER GENERATORS AS FAR AWAY FROM EXISTING RESIDENCES AS POSSIBLE. UNNECESSARY IDLING OF ENGINES SHALL BE PROHIBITED. NEIGHBORS ADJACENT TO CONSTRUCTION AREA SHALL BE NOTIFIED OF THE CONSTRUCTION SCHEDULE IN WRITING. THE TELEPHONE NUMBER OF THE DESIGNATED DISTURBANCE COORDINATOR SHALL BE POSTED AT THE SITE.
11. CONTRACTOR SHALL OBTAIN ALL APPROPRIATE PERMITS FROM REQUIRED AGENCIES OR PUBLIC UTILITIES PRIOR TO COMMENCEMENT OF WORK.
12. CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR AND RESTORATION OF ALL EXISTING IMPROVEMENTS DAMAGED OR DESTROYED BY HIS PERFORMANCE OF THE WORK SHOWN HEREIN AT HIS EXPENSE. SAID REPAIRS SHALL BE PERFORMED AS DIRECTED BY THE CITY ENGINEER.
13. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROMPT CLEAN UP OF ANY MATERIALS SPILLED OR DROPPED ON ANY ABUTTING STREETS DURING CONSTRUCTION. THE CONTRACTOR SHALL REPAIR ANY DAMAGE CAUSED TO THESE STREETS BY CONSTRUCTION VEHICLES ASSOCIATED WITH SUCH CONSTRUCTION. THE CITY ENGINEER MAY REQUIRE THE OWNER TO POST A CASH DEPOSIT PRIOR TO THE START OF CONSTRUCTION TO CAUSE TIMELY CLEAN UP AND REPAIR OF STREETS.
14. CONTRACTOR SHALL PROVIDE FOR INGRESS AND EGRESS TO PRIVATE PROPERTY ADJACENT TO WORK AREA THROUGHOUT CONSTRUCTION.
15. CONTRACTOR TO PROVIDE ALL LIGHTS, SIGNS, BARRICADES, FLAGMEN, AND OTHER DEVICES NECESSARY TO PROVIDE FOR PUBLIC SAFETY AND TO MAINTAIN TRAFFIC CONTROL AND AS DIRECTED BY THE CITY TRAFFIC ENGINEER.
16. CONTRACTOR SHALL NOT DISTURB OR DESTROY ANY PERMANENT SURVEY POINTS WITHOUT THE CONSENT OF THE CITY ENGINEER. ANY PERMANENT MONUMENTS OR POINTS DESTROYED SHALL BE REPLACED BY A SURVEYOR AT THE CONTRACTOR'S EXPENSE.
17. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION 2009 EDITION, THE CITY OF OAKLAND MODIFICATIONS TO THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION AND STANDARD DETAILS FOR PUBLIC WORK CONSTRUCTION 2002 EDITION AS AMENDED AND SUPPLEMENTED BY THE CITY OF OAKLAND.

GRADING NOTES

1. IN THE EVENT AN ARCHEOLOGICAL RESOURCE IS DISCOVERED DURING DEMOLITION AND/OR CONSTRUCTION, CONTRACTOR SHALL NOTIFY THE CITY AND EARTHWORK WITHIN 100 FEET OF THESE MATERIALS SHALL BE STOPPED UNTIL A PROFESSIONAL ARCHAEOLOGIST WHO IS CERTIFIED BY THE SOA AND/OR SOPA HAS HAD AN OPPORTUNITY TO EVALUATE THE SIGNIFICANCE OF THE FIND AND SUGGEST APPROPRIATE MITIGATION MEASURES, IF DEEMED NECESSARY PRIOR TO COMMENCEMENT OF ACTIVITIES. THE ARCHAEOLOGIST SHALL BE HIRED AT THE APPLICANT'S EXPENSE.
2. IN THE EVENT A UNIQUE PALEONTOLOGICAL RESOURCE OR SITE IS DISCOVERED DURING DEMOLITION AND/OR CONSTRUCTION, THE APPLICANT SHALL STOP WORK IMMEDIATELY AND NOTIFY THE CITY. A CERTIFIED ARCHAEOLOGIST SHALL BE HIRED AT THE APPLICANT'S EXPENSE TO INVESTIGATE THE DISCOVERY AND RECOMMEND FURTHER ACTIONS PRIOR TO COMMENCEMENT OF ACTIVITIES.
3. IN THE EVENT HUMAN REMAINS ARE DISCOVERED DURING DEMOLITION AND/OR CONSTRUCTION, THE APPLICANT SHALL STOP WORK IMMEDIATELY AND NOTIFY THE CITY. A CERTIFIED ARCHAEOLOGIST SHALL BE HIRED AT THE APPLICANT'S EXPENSE TO INVESTIGATE THE DISCOVERY AND RECOMMEND FURTHER ACTIONS PRIOR TO COMMENCEMENT OF ACTIVITIES.
4. IF FOSSILS OF POTENTIAL SCIENTIFIC SIGNIFICANCE ARE FOUND DURING CONSTRUCTION, GRADING WITHIN 50 FEET OF THE FOSSIL LOCATION SHALL BE SUSPENDED UNTIL THE SIGNIFICANCE OF THE FIND HAS BEEN EVALUATED BY A PALEONTOLOGIST. THE CITY SHALL BE NOTIFIED WITHIN 24 HOURS AND ADVISED OF THE EVALUATION OF THE PALEONTOLOGIST. RESUMPTION OF WORK IN THE FOSSIL AREA SHALL REQUIRE WRITTEN APPROVAL FROM THE CITY OF OAKLAND PLANNING DEPARTMENT.
5. ALL CHEMICALS AND PETROLEUM PRODUCTS STORED ON-SITE DURING CONSTRUCTION SHALL BE WITHIN A BERMED CONTAINMENT AREA OR OTHER APPROPRIATE FACILITY. THE HANDLING, STORAGE AND DISPOSAL OF ANY HAZARDOUS MATERIALS USED ON THE SITE WILL BE IN ACCORDANCE WITH A BUSINESS PLAN (OR EQUIVALENT) ON FILE WITH THE COUNTY HEALTH SERVICES DEPARTMENT, HAZARDOUS MATERIALS DIVISION. ALL REFUELING AND VEHICLE MAINTENANCE ACTIVITY SHALL BE LOCATED AWAY FROM ANY DRAINAGE PATHWAYS. REFER TO EROSION CONTROL PLAN.
6. ALL GRADING, SITE PREPARATION, PLACING AND COMPACTION OF FILL TO BE DONE IN ACCORDANCE WITH CITY OF OAKLAND ORDINANCE; ALSO UNDER THE DIRECT SUPERVISION OF THE GEOTECHNICAL ENGINEER. SUBSEQUENT TO COMPLETION OF WORK, THE GEOTECHNICAL ENGINEER SHALL SUBMIT TO THE CITY OF OAKLAND ENGINEERING DEPARTMENT A REPORT STATING THAT ALL WORK HAS BEEN DONE TO ITS SATISFACTION.
7. ALL REVISIONS TO THIS PLAN MUST BE REVIEWED BY THE CITY OF OAKLAND ENGINEERING DEPARTMENT PRIOR TO CONSTRUCTION AND SHALL BE ACCURATELY SHOWN ON REVISED PLANS STAMPED AND DISTRIBUTED BY THE ENGINEERING DEPARTMENT PRIOR TO THE WORK BEING ADVANCED AND ACCEPTANCE OF THE WORK AS COMPLETE.
8. ALL TRENCHES SHALL BE SHORED IN ACCORDANCE WITH CAL-OSHA "CONSTRUCTION SAFETY ORDERS" CURRENT EDITION. ALSO AS SPECIFIED BY THE GEOTECHNICAL ENGINEER. CONTRACTOR MUST HAVE VALID TRENCH SHORING PERMIT ISSUED BY CAL-OSHA.
9. THE EXCAVATION TRENCH WIDTH FOR ALL PIPES, WITH THE EXCEPTION OF C.I.P.P., SHALL BE A MINIMUM OF 24" GREATER THAN THE OUTSIDE DIAMETER OF THE PIPE. ONE-HALF OF THE MINIMUM DIMENSION SHALL BE AVAILABLE ON EITHER SIDE OF THE PIPE. THE SAFETY REQUIREMENTS OF THE OCCUPATION SAFETY AND HEALTH ACT FOR TRENCH SHORING AND BRACING SHALL BE COMPLIED WITH WHERE APPLICABLE.
10. THE CONTRACTOR SHALL PROVIDE THE NECESSARY SAFETY TESTING OF EQUIPMENT AND PERSONNEL.
11. CONTRACTOR'S ATTENTION IS DIRECTED TO THE REQUIREMENTS OF THE DIVISION OF INDUSTRIAL SAFETY PERTAINING TO "CONFINED SPACES". ANY MANHOLES, CULVERT, DROP INLET OR TRENCH WHICH COULD CONTAIN AIR WHICH IS NOT READILY VENTILATED MAY BE CONSIDERED A "CONFINED SPACE"
12. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO IMMEDIATELY NOTIFY THE CITY OF OAKLAND ENGINEERING INSPECTOR AND THE DESIGN ENGINEER UPON DISCOVERY OF ANY FIELD CONFLICTS.
13. ANY DEVIATIONS OR CHANGES IN THESE PLANS WITHOUT OFFICIAL APPROVAL OF THE DESIGN ENGINEER SHALL ABSOLVE THE DESIGN ENGINEER OF ANY AND ALL RESPONSIBILITY OF SAID DEVIATION OR CHANGE.
14. DURING THE GRADING OPERATION THE CONTRACTOR SHALL CONTROL THE GENERATION OF DUST BY FULLY SPRINKLING THE SITE AS DETERMINED TO BE NEEDED BY THE COUNTY GRADING INSPECTOR IN ACCORDANCE WITH THE COUNTY GRADING ORDINANCE.
15. COMPACTION TESTS WILL BE PERFORMED ON ALL STREET WORK TO VERIFY THE COMPACTION CONFORMS TO CITY OF OAKLAND STANDARDS AND THE GEOTECHNICAL REPORT (ENGEO INC PROJECT NO. 12215.000.000 DATED JULY 28, 2015). THE TEST OF SUBGRADE TO BE PERFORMED BY THE GEOTECHNICAL ENGINEER. INSPECTION OF ROCK AND PAVING TO BE PERFORMED BY THE CITY OF OAKLAND. (TWO WORKING DAYS NOTICE REQUIRED) WHERE UNSTABLE OR UNSUITABLE MATERIALS ARE ENCOUNTERED DURING SUBGRADE PREPARATION, THE AREA IN QUESTION SHALL BE OVER EXCAVATED AND REPLACED BY SELECT BACKFILL AS DIRECTED IN THE FIELD BY THE GEOTECHNICAL ENGINEER. ANY SLIDE REPAIR WORK, SUBDRAIN INSTALLATION, AND LINED DITCH WORK SHALL BE INSPECTED BY THE CITY OF OAKLAND ENGINEERING DEPARTMENT. A REPORT FROM THE GEOTECHNICAL ENGINEER SHALL BE SUBMITTED TO THE CITY OF OAKLAND ENGINEERING DEPARTMENT REGARDING THE SLIDE REPAIR AND/OR SUBDRAIN INSTALLATION. CUT AND GRADED SLOPES SHALL BE PERIODICALLY INSPECTED DURING GRADING OPERATION BY AN ENGINEERING GEOLOGIST OR GEOTECHNICAL ENGINEER WITH PERIODIC PROGRESS REPORTS AND A GRADING COMPLETION REPORT.
16. APPLICANT SHALL COMPLY WITH ALL RULES, REGULATIONS AND PROCEDURES OF THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM.

IMPROVEMENT NOTES

1. PRODUCTS AND MATERIALS SPECIFIED ARE SUBJECT TO CHANGE WITH APPROVED EQUIVALENCE. JURISDICTION SHALL REVIEW AND APPROVE PROPOSED SUBSTITUTIONS. SHOULD THE CONTRACTOR REQUEST A CHANGE OF MATERIALS, THE CONTRACTOR SHALL PAY FOR THE COST INVOLVED IN PLAN CHANGES AND PROCESSING THE CHANGE THROUGH THE APPROPRIATE AGENCY.
2. THE CONTRACTOR IS REQUIRED TO OBTAIN AN ENCROACHMENT PERMIT FOR ALL WORK WITHIN EXISTING CITY RIGHTS OF WAY. APPLICATIONS FOR ENCROACHMENT PERMIT, SUBMITTED MORE THAN 90 DAYS PAST ENGINEERING "REVIEWED" DATE STAMP, MAY REQUIRE ADDITIONAL TIME TO PROCESS. FOR FURTHER PERMIT INFORMATION, CONTACT THE ENGINEERING DEPARTMENT AT (510) 238-3659.
3. TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), PART 6 - TEMPORARY TRAFFIC CONTROL AND THE 2009 EDITION OF MUTCD.
4. IN CONFORM PAVING AREAS, IF A FULL STREET STRUCTURAL SECTION IS NOT FOUND AT THE APPARENT EDGE OF PAVEMENT, FURTHER STREET EXCAVATION WILL BE REQUIRED UNTIL THE FULL SECTION IS ENCOUNTERED. PAVING CONFORMS SHALL BE MADE AT A SMOOTHLY TRIMMED BUTT JOINT. DO NOT OVERLAP EXISTING PAVEMENT.
5. TRENCH BACKFILL, WHICH LIES WITHIN EITHER EXISTING OR NEW ROADWAYS, SHALL CONFORM TO THE CITY OF OAKLAND STANDARD DETAIL D-22 FOR TRENCH BACKFILL.
6. THE CONTRACTOR IS RESPONSIBLE FOR MATCHING EXISTING PAVEMENT, SIDEWALK, SURROUNDING LANDSCAPING AND OTHER IMPROVEMENTS WITH A SMOOTH TRANSITION IN PAVING, CURB AND GUTTER, GRADING, ETC. AND TO AVOID ANY ABRUPT OR APPARENT CHANGES IN GRADES OR CROSS SLOPES, LOW SPOTS OR HAZARDOUS CONDITIONS.
7. ALL ELECTRICAL AND GAS UTILITIES TO BE PROVIDED BY P.C.& E. AND INSTALLED UNDERGROUND PRIOR TO THE CONSTRUCTION OF CURB, GUTTER AND SIDEWALKS.
8. ANY COST OF RELOCATING UTILITIES DUE TO IMPROVEMENTS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
9. ALL UNDERGROUND UTILITIES WITHIN THE RIGHT-OF-WAY, INCLUDING MAINS, LATERALS AND CROSSINGS, SHALL BE INSTALLED, BACKFILLED AND COMPLETED PRIOR TO THE COMMENCEMENT OF CURB, GUTTER AND SIDEWALK CONSTRUCTION.
10. ALL UNDERGROUND FACILITIES SHALL BE INSTALLED PRIOR TO THE CONSTRUCTION OF CURB AND FINAL PREPARATION OF SUBGRADE AND PLACEMENT OF BASE MATERIAL. CURB AND GUTTER SHALL BE COMPLETE PRIOR TO THE PLACEMENT OF STREET BASE MATERIAL, UNLESS OTHERWISE APPROVED.
11. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE ALL NECESSARY UTILITY RELOCATION WITH THE APPROPRIATE UTILITY AGENCY.
12. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR FINAL GRADE OF CONCRETE UNLESS FORMS ARE CHECKED PRIOR TO POURING.
13. ALL PEDESTRIAN IMPROVEMENTS SHALL CONFORM WITH THE REQUIREMENTS OF TITLE 24 OF THE CALIFORNIA CODE OF REGULATIONS AND THE AMERICANS WITH DISABILITIES ACT.
14. ALL WORK SHALL CONFORM TO THE CITY OF OAKLAND STANDARDS.

CONCRETE CURB AND GUTTER

TYPE "A"
6" P.C.C. PER SECTION 201-1.1.2

TYPE "B"
3" A.C. PER SECTION 306-1.5.2
6" P.C.C. PER SECTION 201-1.1.2

TYPE "C"
18" A.B. PER SECTION 306-1.3.1

TYPE "D"
12" A.B. PER SECTION 306-1.3

PROFILE
SLOPE 2% MIN.
CONCRETE CURB & GUTTER

NOTES

- OMIT THIS PORTION OF CURB AT DRIVEWAYS.
- STANDARD SLOPE EXCEPT AS SHOWN ON PLANS.
- PLACE 4" OF PROCESSED MISCELLANEOUS BASE UNDER CURB, GUTTER AND SIDEWALK UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

CITY OF OAKLAND DESIGN AND CONSTRUCTION SERVICES DEPARTMENT

CONCRETE CURB AND GUTTER

ENGINEERING DESIGN MANAGER: [Signature]
DATE: JANUARY 2022
REV. DATE: [Blank]
DWS
S-1

CONCRETE DRIVEWAY

PERSPECTIVE

GENERAL NOTES

- IN ALL COMMERCIAL, INDUSTRIAL OR SPECIAL DISTRICTS, THE DRIVEWAY OPENINGS SHALL BE NOT LESS THAN TWELVE (12) FEET IN WIDTH NOR MORE THAN THIRTY-FIVE (35) FEET IN WIDTH.
- IN RESIDENTIAL DISTRICTS, THE DRIVEWAY OPENINGS SHALL BE NOT LESS THAN TEN (10) FEET IN WIDTH NOR MORE THAN NINETEEN (19) FEET IN WIDTH. PROVIDED, HOWEVER, A DRIVEWAY OPENING SERVING TWO OR MORE PARCELS MAY BE TWENTY-NINE (29) FEET IN WIDTH. FOR THE SPECIAL CASE OF FOUR UNIT RESIDENTIAL BUILDINGS, THE DRIVEWAY OPENING MAY BE TWENTY-SEVEN (27) FEET IN WIDTH.
- DRIVEWAYS SERVING A SINGLE PARCEL OF PROPERTY OR SERVING ANY OF SEVERAL ADJACENT PARCELS UNDER SINGLE OWNERSHIP SHALL BE SEPARATED BY AT LEAST TWENTY-FIVE (25) FEET OF FULL VERTICAL CURB.
- DRIVEWAYS SERVING SEPARATE BUT ADJOINING PARCELS OF PROPERTY UNDER DIFFERENT OWNERSHIPS SHALL BE SEPARATED BY AT LEAST TEN (10) FEET OF FULL VERTICAL CURB.
- DRIVEWAYS SERVING CORNER LOTS SHALL BE SO LOCATED THAT: (A) A DRIVEWAY ON EITHER STREET SHALL BE AT LEAST TWENTY FIVE (25) FEET FROM THE PROJECTED CURVE LINE OF THE INTERSECTING STREET. (B) NO PART OF THE DRIVEWAY SHALL EXTEND INTO THE CURB RETURN A DISTANCE GREATER THAN FIVE (5) FEET.
- DRIVEWAYS SHALL FORM AN ANGLE OF 90° OR LESS OFF OF A LINE PERPENDICULAR TO OR RADIAL TO THE STREET ALIGNMENT, FOR A DISTANCE OF EIGHTEEN (18) FEET BEHIND THE PROPERTY LINE MEASURED ALONG THE SHORTEST SIDE OF SAID DRIVEWAY.
- ALL DRIVEWAY CURB TRANSITIONS SHALL BE EIGHTEEN (18) INCHES WITH STRAIGHT SIX-INCH CURBS.
- NO DRIVEWAY SHALL BE CONSTRUCTED OR RECONSTRUCTED WITHIN THIRTY (30) INCHES OF ANY EXISTING OBSTRUCTION IN THE STREET AREA.
- REFER TO CHAPTER 12.04 OF THE OAKLAND MUNICIPAL CODE REGULATING THE CONSTRUCTION AND REPAIR OF SIDEWALKS, DRIVEWAYS AND CURBS.
- CONCRETE SHALL BE MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 2000 PSI IN ACCORDANCE WITH SUB-SECTION 201-1 UNLESS OTHERWISE NOTED.
- BASE MATERIAL SHALL CONFORM TO SUB-SECTION 200-2.5 UNLESS OTHERWISE NOTED.
- EXPANSION JOINTS 1/4" THICK SHALL EXTEND THROUGH THE ENTIRE THICKNESS OF THE SIDEWALK AT EACH SIDE OF DWYS. CURB RETURNS, AS DIRECTED BY THE ENGINEER TO CONFORM TO EX. CONDITIONS.

RESIDENTIAL

OTHER THAN RESIDENTIAL

CITY OF OAKLAND ENGINEERING DESIGN SERVICES DIVISION

CONCRETE DRIVEWAY

ENGINEERING DESIGN MANAGER: [Signature]
DATE: JANUARY 2022
REV. DATE: [Blank]
DWS
S-2

TRENCH DETAIL

PAVEMENT TYPES

TYPE A
6" P.C.C. PER SECTION 201-1.1.2

TYPE B
3" A.C. PER SECTION 306-1.5.2

TYPE C
4" A.C. PER SECTION 306-1.5.2

TYPE D
3" A.C. PER SECTION 306-1.5.2

TYPE F
VARIABLE DEPTH A.C. PER SECTION 306-1.5.2

SAW CUT FULL DEPTH PCC PAVEMENT - CONSTRUCT A MIN. 6" PAVEMENT SECTION

WIDTH VARIES WITH PIPE DIAMETER

EX PAVEMENT **PAVEMENT REPLACEMENT AREAS** **UNIMPROVED AREAS** **EX GROUND**

BACKFILL WITH BASE MATERIAL AS DESCRIBED IN THE "GREENBOOK" (SECTION 200-2.1)

95% COMPACTION FOR PAVED AREAS

95% COMPACTION IN UNIMPROVED AREAS

12" MAX COVERAGE

BEDDING MATERIAL (SECTION 306-1.2.1)

0.4 x O.D. MIN

PIPE O.D.

9" MIN. EACH SIDE

ADDITIONAL BEDDING IF DIRECTED BY THE ENGINEER

NOTE: THIS PLAN IS NOT A LEGAL ENGINEERING DOCUMENT BUT AN ELECTRONIC DUPLICATE. THE BOUNDARY OF OAKLAND STANDARD DETAILS FOR PUBLIC WORKS CONSTRUCTION IS AVAILABLE FOR PURCHASE FROM THE CONTRACT ADMINISTRATION DEPARTMENT.

CITY OF OAKLAND DESIGN AND CONSTRUCTION SERVICES DEPARTMENT

TRENCH DETAIL

ENGINEERING DESIGN MANAGER: [Signature]
DATE: JANUARY 2022
REV. DATE: [Blank]
DWS
D-22.0

TRENCH DETAIL

NOTES:

- Excavated material shall not be used to fill voids caused by overexcavation; such voids shall be filled with compacted bedding material. Unless directed by the Engineer, no separate payment will be made for overexcavation.
- Import backfill material shall conform to Subsection 306-1.31
- Aggregate base shall conform to crushed miscellaneous base (Section 200-2.4) or better. The base material must be approved by the Engineer.
- The streets of Oakland are generally paved with either AC, PCC, or a combination of the two. The existing pavement may differ from the replacement Pavement Type (A to F) indicated on the plans.
- Compaction by jetting is not permitted.
- When flexible pipe (HDPE, etc.) is used, pipe shall be backfilled to the spring line, compacted and backfill tested prior to completing initial backfill.
- The compacted temporary resurfacing shall be a minimum of 2" thick placed on the required base, and shall be removed prior to placing the permanent paving.
- Backfill testing is required and results shall be approved by the Engineer prior to paving.
- No longitudinal joints or seams are allowed in bike lanes. If a longitudinal joint is constructed due to the Contractor's work or this requirement, the Contractor shall remove a minimum of 2" of asphalt from the pavement across the entire bike lane using a method approved by the City and then resurface the bike lane to the Engineer's satisfaction.
- During backfill operations, the trench shall be backfilled, compacted, and tested to the spring line of any utilities crossing the trench before proceeding with further backfill.
- Unless specified otherwise, measurement for payment of additional bedding, imported backfill and temporary paving (when listed as separate pay items) shall be based upon the trench widths defined on this detail. The lower trench width for 8" pipe shall be 30".
- Clean and tuckcoat sides of excavation and between paving courses with spray application of SS-1 emulsion before placing asphalt-concrete pavement section.
- If the distance to the edge of gutter is less than 3' from one trench edge, the pavement replacement shall extend to the edge of existing gutter.
- In areas where existing paving consists of rubberized AC, the trench area should be repaved with the equivalent pavement section of rubberized AC. As an alternate, a dense graded AC section equivalent to two-times the thickness of the rubberized AC may be substituted.

CITY OF OAKLAND DESIGN AND CONSTRUCTION SERVICES DEPARTMENT

TRENCH DETAIL

ENGINEERING DESIGN MANAGER: [Signature]
DATE: JANUARY 2022
REV. DATE: [Blank]
DWS
D-22.1

LAMPHOLE, CLEANOUT AND DROP CONNECTION

DROP CONNECTION

FRAME AND COVER LAMPHOLE AND CLEANOUT

SECTIONAL PLAN LAMPHOLE

SECTIONAL ELEVATION LAMPHOLE

SECTIONAL ELEVATION CLEANOUT

MANHOLE DIMENSIONS	REINFORCING BARS	1'-10"		5'-2"		2'-1"		1'-8"	
		NO.	LENGTH	NO.	LENGTH	NO.	LENGTH	NO.	LENGTH
36"	7	14'-0"	4	7'-8"	17	5'-2"	2	6'-10"	153.8
39"	7	14'-0"	4	7'-4"	19	5'-2"	2	6'-10"	164.0
42"	7	15'-2"	4	7'-0"	19	5'-2"	2	6'-10"	164.4
45"	7	15'-4"	4	6'-8"	19	5'-2"	2	6'-10"	165.5
48"	7	17'-2"	4	7'-0"	19	5'-2"	2	6'-10"	181.1
51"	7	18'-0"	4	7'-2"	19	5'-2"	2	6'-10"	183.9
54"	7	18'-10"	4	7'-5"	23	5'-2"	2	6'-10"	206.9
57"	7	19'-8"	4	7'-7"	23	5'-2"	2	6'-10"	213.2
60"	7	20'-4"	4	7'-10"	23	5'-2"	2	6'-10"	219.5
63"	7	21'-4"	4	8'-1"	25	5'-2"	2	6'-10"	239.2
66"	7	22'-2"	4	8'-3"	25	5'-2"	2	6'-10"	245.3
69"	7	23'-0"	4	8'-5"	28	5'-2"	2	6'-10"	254.6
72"	7	23'-10"	4	8'-8"	28	5'-2"	2	6'-10"	267.7
75"	7	24'-8"	4	9'-11"	28	5'-2"	2	6'-10"	273.7

GENERAL NOTES

- PRECAST CONCRETE SHAFT IS OPTIONAL WHEN "D" IS 4'-0" OR GREATER.
- ALL CONTACT AND BEARING SURFACES OF BOTH FRAME AND COVER SHALL BE MACHINED TO FIT ACCURATELY.
- CONVERSION TO METRIC EQUIVALENTS SHALL BE IN ACCORDANCE WITH ASTM A-360.
- SHAFT OPENING FOR TYPE "H" FRAMES AND COVERS SHALL BE 2'-0" DIAMETER.
- STEPS SHALL BE 3/4" DIAM. HOT DIPPED GALVANIZED STEEL ROD OR FIBERGLASS. SEE SHEET D-11 FOR STEP DETAILS.
- ALL STEEL ASOS REINFORCING STEEL NO. 4 BARS.
- REINFORCING STEEL MAY BE SPREAD TO ADMIT SMALL PIPES ENTERING MANHOLE SIDES.
- WHERE TWO SIZES OF PIPE ENTER AND LEAVE A MANHOLE, USE DIMENSIONS AND QUANTITIES FOR THE LARGER SIZE PIPE.

CITY OF OAKLAND DESIGN AND CONSTRUCTION SERVICES DEPARTMENT

LAMPHOLE, CLEANOUT AND DROP CONNECTION

ENGINEERING DESIGN MANAGER: [Signature]
DATE: JANUARY 2022
REV. DATE: [Blank]
DWS
D-2

MANHOLE TYPE 2 (FOR 36" TO 75" DIA. PIPE CONDUIT)

PLAN

SECTION A-A

FRAME AND COVER DETAILS

MANHOLE DIMENSIONS	REINFORCING BARS	1'-10"		5'-2"		2'-1"		1'-8"	
		NO.	LENGTH	NO.	LENGTH	NO.	LENGTH	NO.	LENGTH
36"	7	14'-0"	4	7'-8"	17	5'-2"	2	6'-10"	153.8
39"	7	14'-0"	4	7'-4"	19	5'-2"	2	6'-10"	164.0
42"	7	15'-2"	4	7'-0"	19	5'-2"	2	6'-10"	164.4
45"	7	15'-4"	4	6'-8"	19	5'-2"	2	6'-10"	165.5
48"	7	17'-2"	4	7'-0"	19	5'-2"	2	6'-10"	181.1
51"	7	18'-0"	4	7'-2"	19	5'-2"	2	6'-10"	183.9
54"	7	18'-10"	4	7'-5"	23	5'-2"	2	6'-10"	206.9
57"	7	19'-8"	4	7'-7"	23	5'-2"	2	6'-10"	213.2
60"	7	20'-4"	4	7'-10"	23	5'-2"	2	6'-10"	219.5
63"	7	21'-4"	4	8'-1"	25	5'-2"	2	6'-10"	239.2
66"	7	22'-2"	4	8'-3"	25	5'-2"	2	6'-10"	245.3
69"	7	23'-0"	4	8'-5"	28	5'-2"	2	6'-10"	254.6
72"	7	23'-10"	4	8'-8"	28	5'-2"	2	6'-10"	267.7
75"	7	24'-8"	4	9'-11"	28	5'-2"	2	6'-10"	273.7

GENERAL NOTES

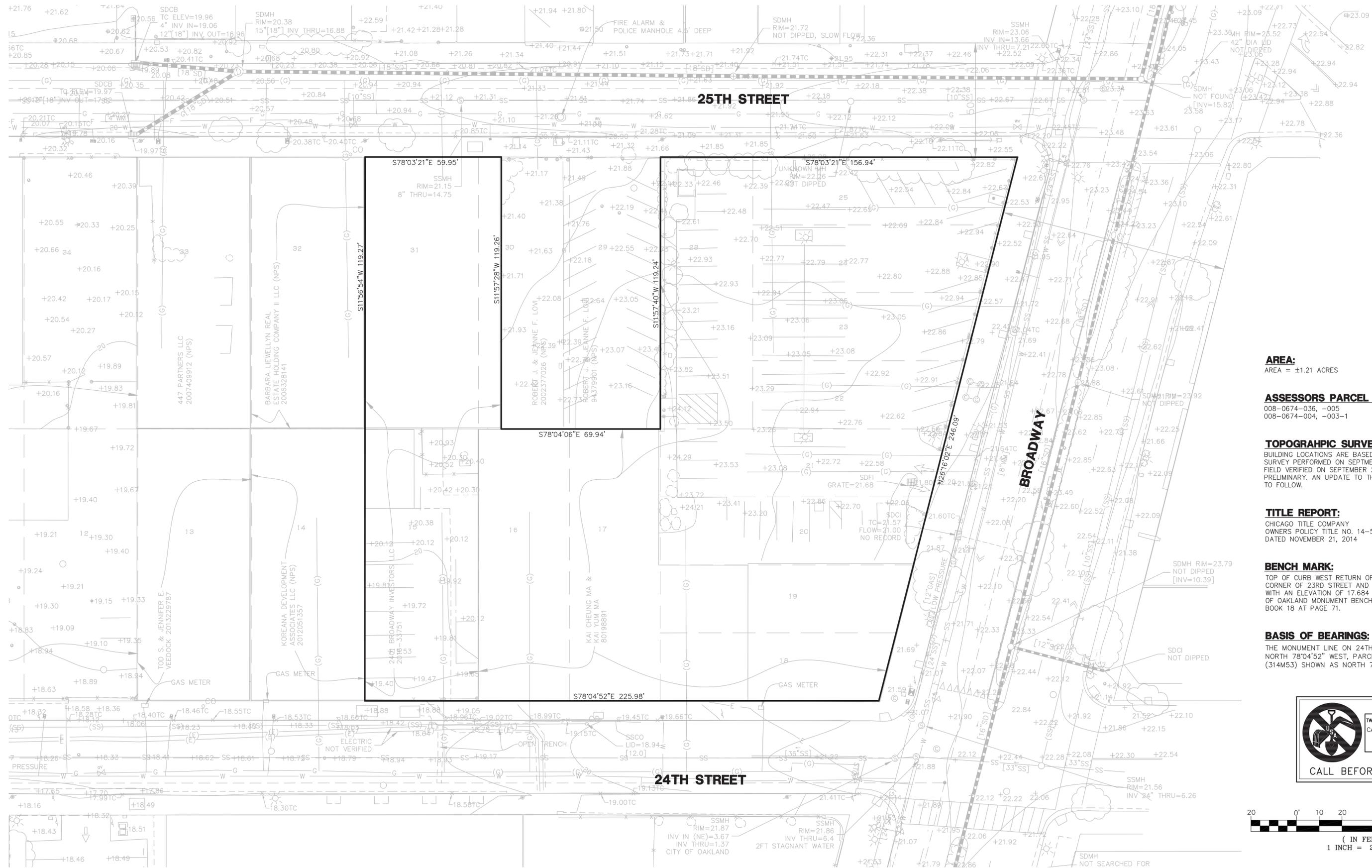
- PRECAST CONCRETE SHAFT IS OPTIONAL WHEN "D" IS 4'-0" OR GREATER.
- ALL CONTACT AND BEARING SURFACES OF BOTH FRAME AND COVER SHALL BE MACHINED TO FIT ACCURATELY.
- CONVERSION TO METRIC EQUIVALENTS SHALL BE IN ACCORDANCE WITH ASTM A-360.
- SHAFT OPENING FOR TYPE "H" FRAMES AND COVERS SHALL BE 2'-0" DIAMETER.
- STEPS SHALL BE 3/4" DIAM. HOT DIPPED GALVANIZED STEEL ROD OR FIBERGLASS. SEE SHEET D-11 FOR STEP DETAILS.
- ALL STEEL ASOS REINFORCING STEEL NO. 4 BARS.
- REINFORCING STEEL MAY BE SPREAD TO ADMIT SMALL PIPES ENTERING MANHOLE SIDES.
- WHERE TWO SIZES OF PIPE ENTER AND LEAVE A MANHOLE, USE DIMENSIONS AND QUANTITIES FOR THE LARGER SIZE PIPE.

CITY OF OAKLAND DESIGN AND CONSTRUCTION SERVICES DEPARTMENT

MANHOLE TYPE 2 (FOR 36" TO 75" DIA. PIPE CONDUIT)

ENGINEERING DESIGN MANAGER: [Signature]
DATE: JANUARY 2022
REV. DATE: [Blank]
DWS
D-12

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AREA:
AREA = ±1.21 ACRES

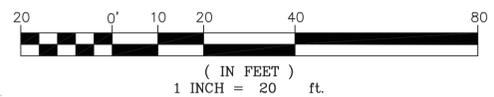
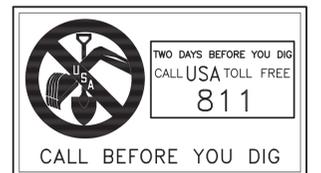
ASSESSORS PARCEL NUMBER:
008-0674-036, -005
008-0674-004, -003-1

TOPOGRAPHIC SURVEY:
BUILDING LOCATIONS ARE BASED UPON AERIAL SURVEY PERFORMED ON SEPTEMBER 17, 2015 AND FIELD VERIFIED ON SEPTEMBER 29, 2015 AND ARE PRELIMINARY. AN UPDATE TO THE BUILDING FACE TO FOLLOW.

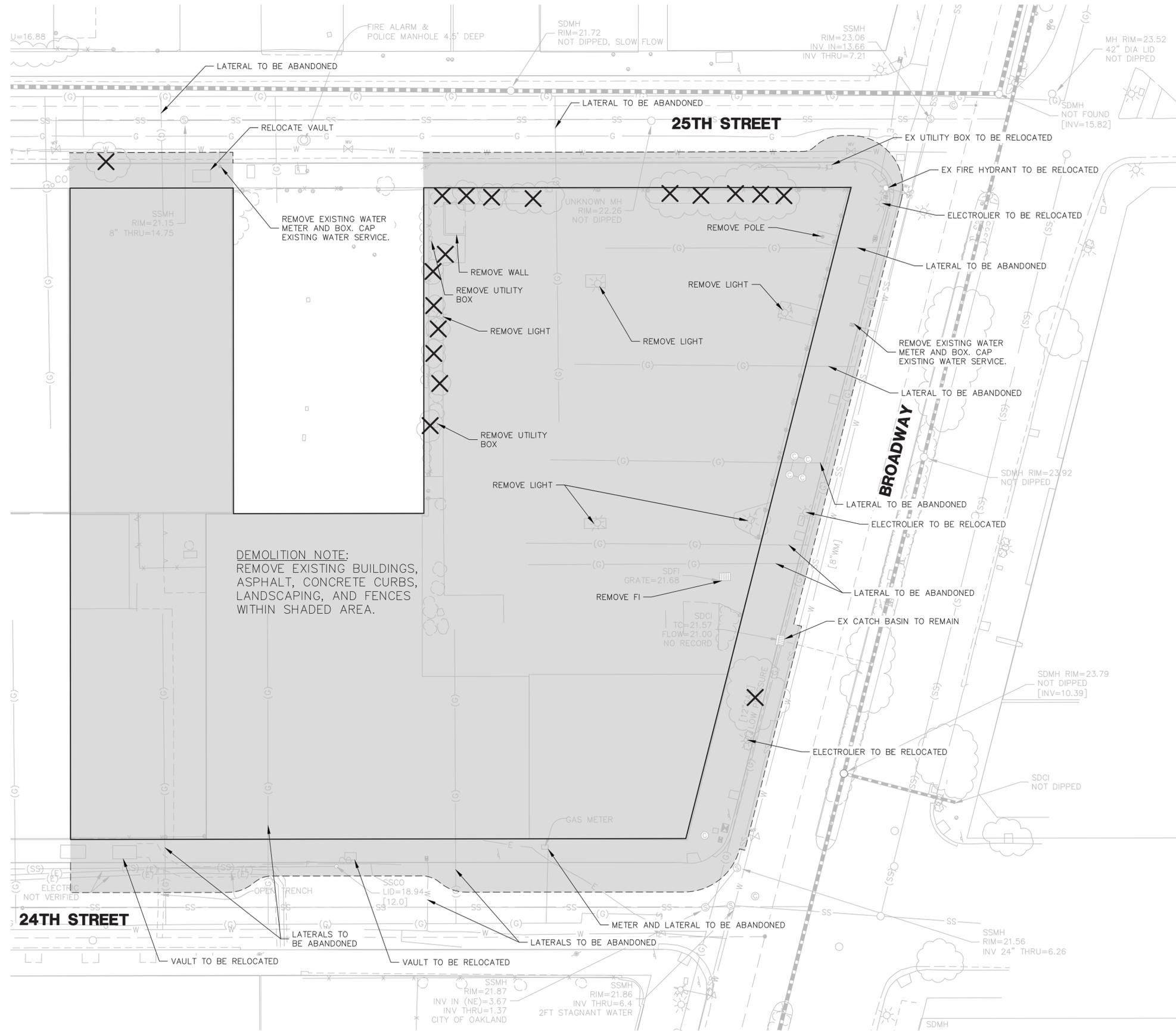
TITLE REPORT:
CHICAGO TITLE COMPANY
OWNERS POLICY TITLE NO. 14-58205703-B-KD
DATED NOVEMBER 21, 2014

BENCH MARK:
TOP OF CURB WEST RETURN OF THE NORTHWEST CORNER OF 23RD STREET AND TELEGRAPH AVENUE, WITH AN ELEVATION OF 17.684 FEET PER THE CITY OF OAKLAND MONUMENT BENCHMARK No 37jc ON BOOK 18 AT PAGE 71.

BASIS OF BEARINGS:
THE MONUMENT LINE ON 24TH STREET TAKEN AS NORTH 78°04'52" WEST, PARCEL MAP 9872 (314M53) SHOWN AS NORTH 78°05'47" WEST.



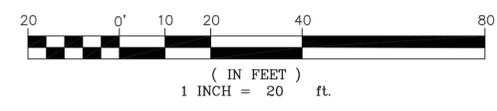
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LEGEND

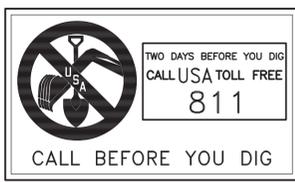
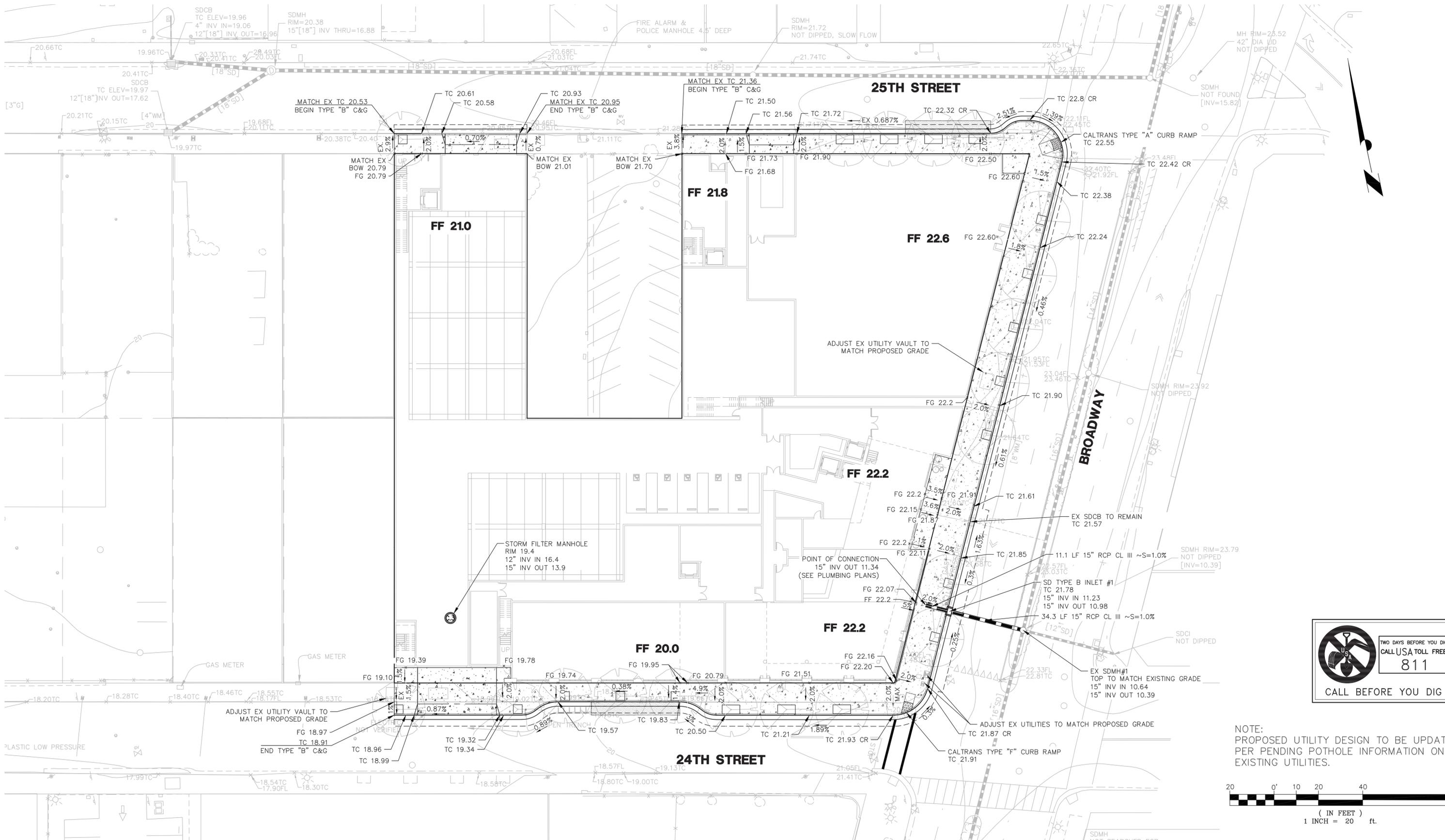
- LIMIT OF DEMOLITION
- SAWCUT LINE
- REMOVE TREE

- DEMOLITION NOTES**
1. ALL UNDERGROUND UTILITIES SHALL REMAIN AND BE PROTECTED IN PLACE UNLESS OTHERWISE NOTED.
 2. TURN OFF ALL UTILITIES SERVING THE EXISTING BUILDING PRIOR TO DEMOLITION OF THE BUILDING.
 3. DUST CONTROL MEASURES SHALL BE IMPLEMENTED DURING DEMOLITION.
 4. SEE JOINT TRENCH PLAN FOR DRY UTILITY RELOCATION AND DRY UTILITY BOXES TO BE REMOVED, MODIFIED, OR ADJUSTED TO GRADE.
 5. CONTRACTOR SHALL VERIFY THE LOCATION OF EXISTING UTILITIES BY CONTACTING USA AT 1-800-227-2600, OR 811.
 6. TRAFFIC AND PEDESTRIAN CONTROL PLANS SHALL BE PREPARED PRIOR TO ANY SIDEWALK OR LANE CLOSURES.
 7. PRIOR TO ANY DEMOLITION, EXISTING TREES TO REMAIN SHALL BE PROTECTED AS REQUIRED BY PROJECT ARBORIST.
 8. ALL OTHER STRUCTURES SHALL REMAIN AND TO BE PROTECTED IN PLACE UNLESS OTHERWISE NOTED.
 9. ALL WORK INVOLVING CROSS WALK STRIPING, PAINTING OF CURB FACES, REMOVAL AND/OR PLACEMENT OF NEW STREET SIGNS, PARKING STALL STRIPING, AND REPLACING OF PARKING METERS NEED TO BE REVIEWED AND APPROVED BY PUBLIC WORKS TRANSPORTATION SERVICES DIVISION.

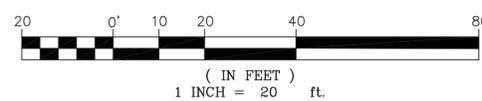


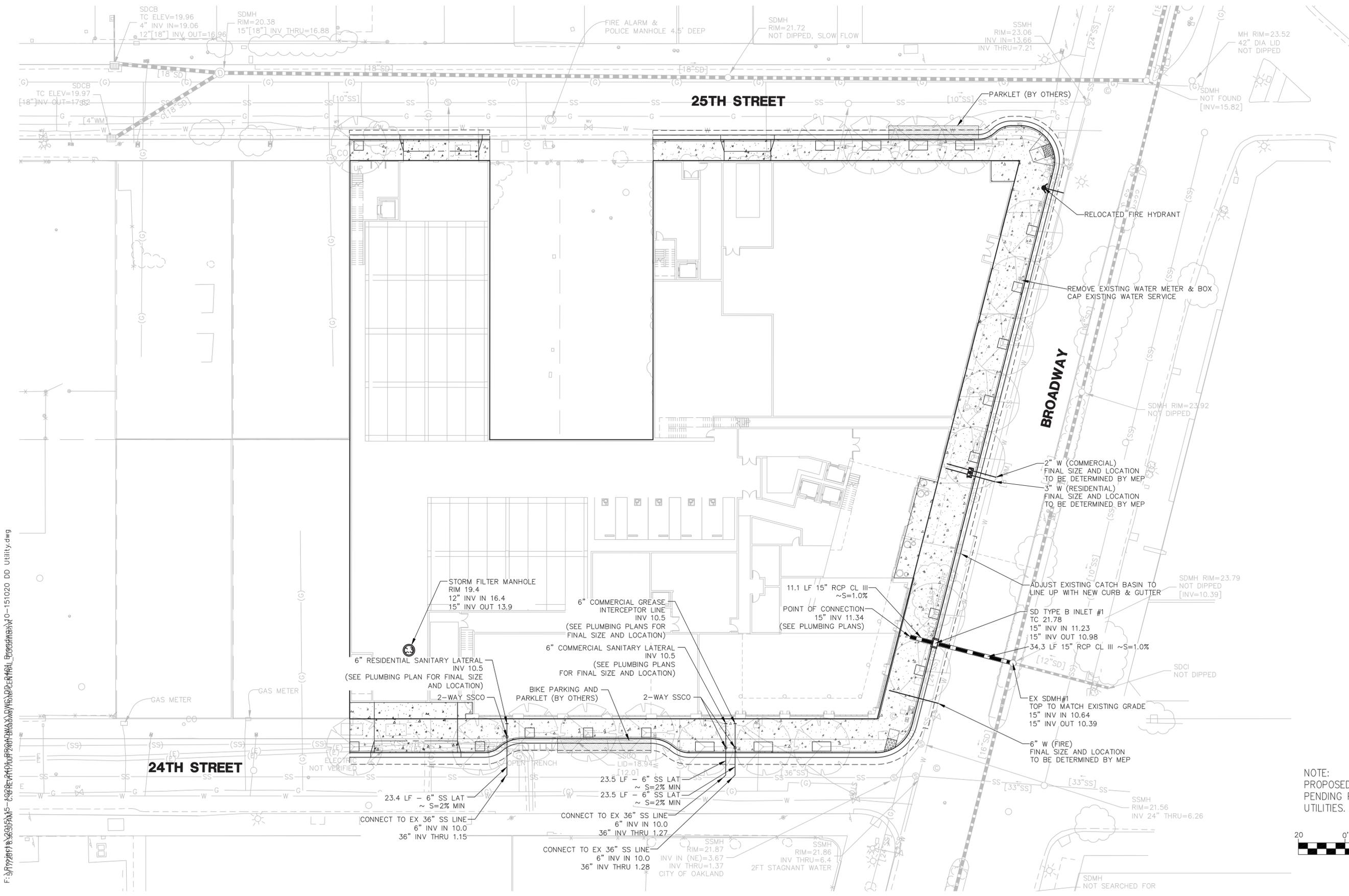
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F:\p772617\836335\AN5 - 0926\FIN\07\2901 BROADWAY\6-DRAWING\09-151020 DD Fine Grading.dwg

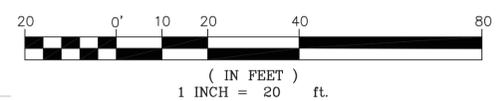


NOTE:
 PROPOSED UTILITY DESIGN TO BE UPDATED
 PER PENDING POT HOLE INFORMATION ON
 EXISTING UTILITIES.

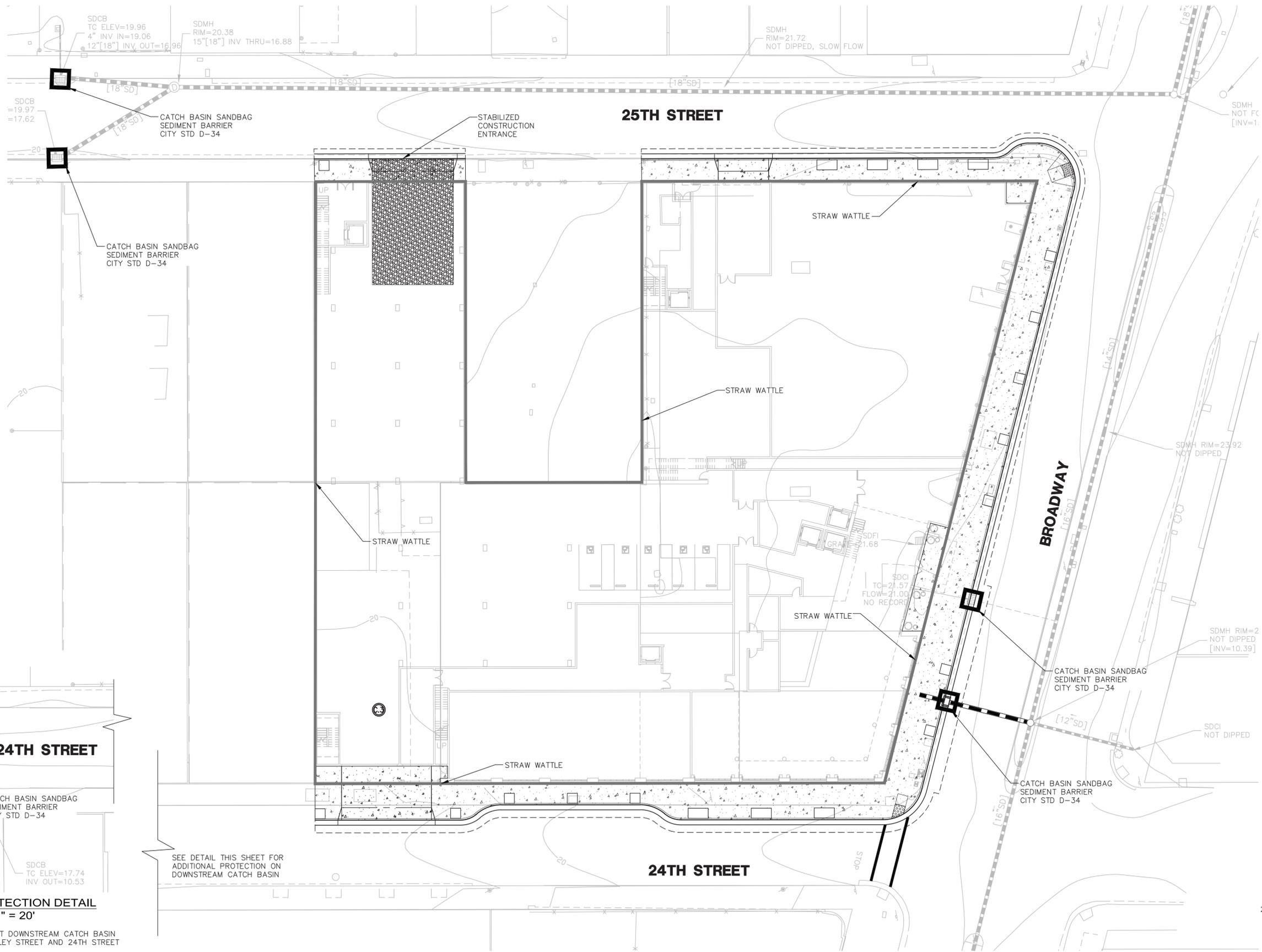




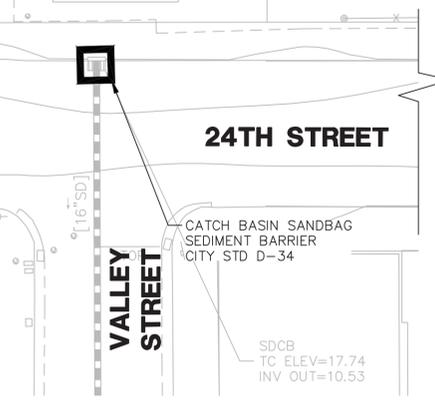
NOTE:
PROPOSED UTILITY DESIGN TO BE UPDATED PER
PENDING POT HOLE INFORMATION ON EXISTING
UTILITIES.



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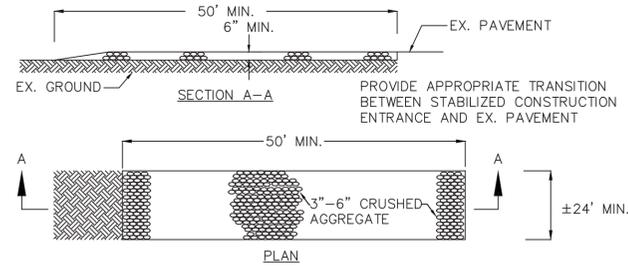
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CATCH BASIN PROTECTION DETAIL
SCALE: 1" = 20'

NOTE: PROVIDE PROTECTION AT DOWNSTREAM CATCH BASIN AT THE INTERSECTION OF VALLEY STREET AND 24TH STREET

SEE DETAIL THIS SHEET FOR ADDITIONAL PROTECTION ON DOWNSTREAM CATCH BASIN

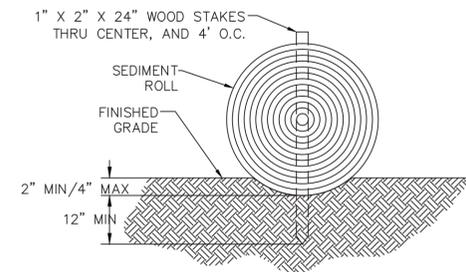
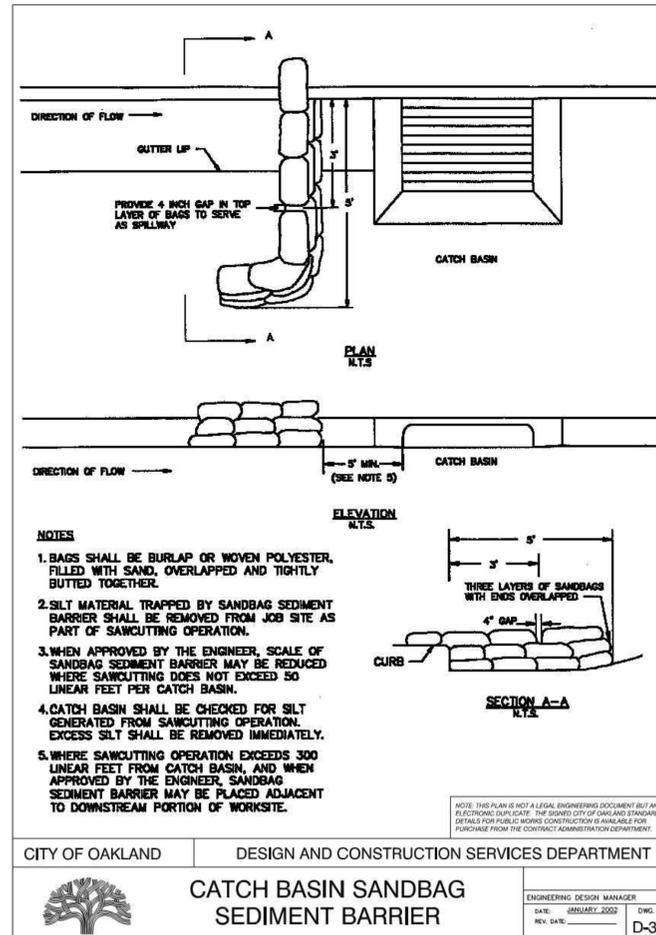


STABILIZED CONSTRUCTION ENTRANCE DETAIL

NOT TO SCALE

EROSION CONTROL NOTES

1. TEMPORARY EROSION CONTROL DEVICES SHOWN ON GRADING PLAN WHICH INTERFERE WITH THE WORK SHALL BE RELOCATED OR MODIFIED WHEN THE INSPECTOR SO DIRECTS AS THE WORK PROGRESSES.
2. EXCEPT AS OTHERWISE DIRECTED BY THE INSPECTOR, ALL DEVICES SHOWN ON THE EROSION CONTROL PLAN SHALL BE IN PLACE AT THE END OF EACH WORKING DAY. ALL EROSION CONTROL FACILITIES MUST BE INSPECTED AND REPAIRED AT THE END OF EACH WORKING DAY DURING THE RAINY SEASON AND MAINTAINED DURING THE RAINY SEASON (OCTOBER 1 TO APRIL 15).
3. ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE PROVISIONS OF THE ASSOCIATION OF BAY AREA GOVERNMENTS (ABAG) "MANUAL OF STANDARDS FOR EROSION AND SEDIMENT CONTROL MEASURES" UNLESS OTHERWISE STATED WITHIN THESE GENERAL NOTES. CONTROL MEASURES ARE SUBJECT TO THE INSPECTION AND APPROVAL OF THE ENGINEERING DIVISION OF THE PUBLIC SERVICES DEPARTMENT. SCHEDULE AN ENGINEERING INSPECTION BY CALLING 925-943-5839 AT LEAST 48 HOURS PRIOR TO THE START OF ANY WORK.
4. ALL LOOSE SOIL AND DEBRIS SHALL BE REMOVED FROM THE STREET AREAS UPON STARTING OPERATIONS AND PERIODICALLY THEREAFTER AS DIRECTED BY THE INSPECTOR. THE SITE SHALL BE MAINTAINED SO AS TO MINIMIZE SEDIMENT LADEN RUNOFF TO ANY STORM DRAIN SYSTEM.
5. A CONCRETE WASHOUT IS REQUIRED FOR ALL CONCRETE WORK. THE WASHOUT SHALL CONSIST OF A CONTAINMENT AREA ENCLOSED BY AN EARTHEN DIKE. PLASTIC TARP, COVERING THE CONTAINMENT AREA AND EARTHEN DIKE, SHALL BE STAKED IN AT OUTSIDE EDGE OF EARTHEN DIKE.
6. ADDITIONAL CONTAINMENT METHODS MUST BE PROVIDED FOR ANY WASTE STORAGE AREA, STOCKPILE/MATERIAL STORAGE AREA AND/OR CONSTRUCTION TOILET AREA.
7. STAND-BY CREWS SHALL BE ALERTED BY THE PERMITTEE OR CONTRACTOR FOR EMERGENCY WORK DURING RAINSTORMS.
8. AFTER OCTOBER 1, ALL EROSION CONTROL MEASURES WILL BE INSPECTED DAILY AND AFTER EACH STORM. AFTER OCTOBER 1, BREACHES IN DIKES AND SWALES WILL BE REPAIRED AT THE CLOSE OF EACH DAY AND WHENEVER RAIN IS FORECAST.
9. AS A PART OF THE EROSION CONTROL MEASURES, UNDERGROUND STORM DRAIN FACILITIES AND CONCRETE SHALL BE INSTALLED COMPLETE AS SHOWN ON THE GRADING PLAN.
10. IF ANY GRADING OPERATIONS, OTHER THAN LOT FINISH GRADING, ARE TO BE PERFORMED DURING THE RAINY SEASON, OCTOBER 1 THROUGH APRIL 15, AN EROSION CONTROL PLAN MUST BE SUBMITTED BY SEPTEMBER 1 AND THE PLAN MUST BE APPROVED BY THE CITY OF OAKLAND PRIOR TO THE COMMENCEMENT OF ANY SUCH GRADING OPERATIONS.
11. SANDBAGS, STRAW WATTLES AND/OR STRAW BALES SHALL BE STOCKPILED ON SITE AND PLACED AT INTERVALS SHOWN ON EROSION CONTROL PLANS, WHEN THE RAIN FORECAST IS 40% OR GREATER, OR WHEN DIRECTED BY THE INSPECTOR.
12. SANDBAGS REFERRED TO IN THE PRECEDING ITEMS MUST BE FULL. APPROVED SANDBAG FILL MATERIALS ARE DECOMPOSED GRANITE AND/OR GRAVEL, OR OTHER MATERIALS APPROVED BY THE INSPECTOR.
13. THIS PLAN MAY NOT COVER ALL THE SITUATIONS THAT ARISE DURING CONSTRUCTION DUE TO UNANTICIPATED FIELD CONDITIONS. VARIATIONS MAY BE MADE TO THESE PLANS IN THE FIELD, SUBJECT TO APPROVAL OF THE CITY ENGINEER.
14. EROSION CONTROL STRUCTURES SHALL BE ADJUSTED BY THE CONTRACTOR TO REFLECT ALL CHANGES IN DRAINAGE AS STREETS AND BUILDING PADS ARE BEING INSTALLED.



STRAW WATTLES DETAIL

NOT TO SCALE

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NOTES

1. ROOF LEADERS WILL DIRECT STORMWATER RUNOFF TO C.3 TREATMENT DEVICE
2. PROPOSED SOURCE CONTROL MEASURES:
 - DIRECT DISCHARGE FROM COVERED TRASH, FOOD WASTE, AND COMPACTOR ENCLOSURES TO THE SANITARY SEWER.
 - DISCHARGE FIRE SPRINKLER TEST WATER TO THE SANITARY SEWER.

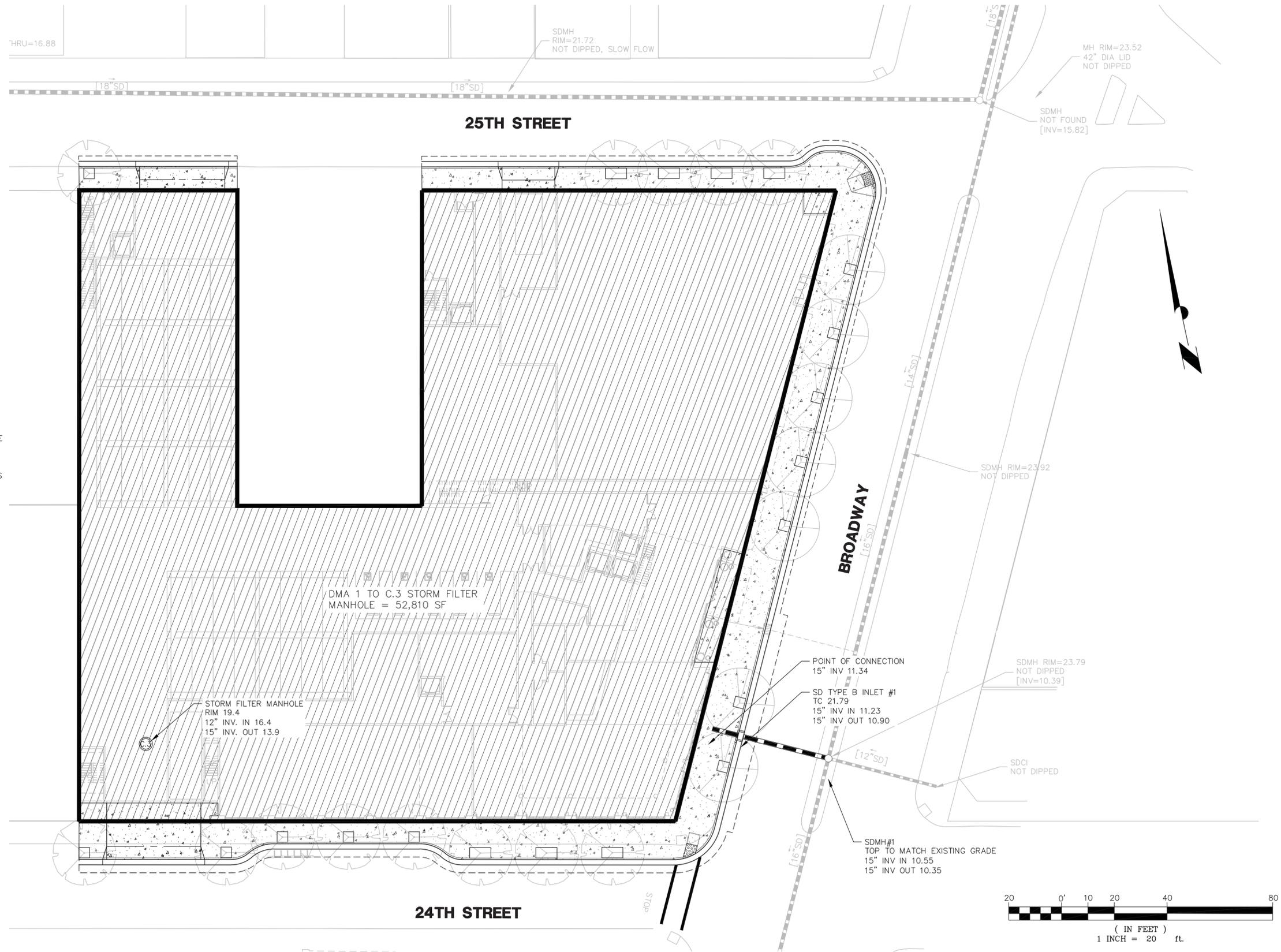
C.3 STORMWATER CONTROL EXHIBIT NOTES

1. CALCULATIONS ARE BASED ON THE ALAMEDA COUNTY CLEAN WATER PROGRAM'S "C.3 STORMWATER TECHNICAL GUIDANCE", VERSION 5.1, MAY 2, 2016 (GUIDEBOOK).
2. THE PROJECT TOTAL SITE AREA IS 1.21 ACRES AND THE TOTAL AREA OF LAND DISTURBED IS 1.50 ACRES.
3. THIS PROJECT PROPOSES TO CREATE 2,377 SQUARE FEET OF NEW IMPERVIOUS SURFACE AND WILL REPLACE 50,433 SQUARE FEET OF EXISTING IMPERVIOUS SURFACE AREA ON-SITE. THE TOTAL PRE-PROJECT IMPERVIOUS SURFACE AREA IS 1.16 ACRES ON-SITE. THE TOTAL POST-PROJECT IMPERVIOUS SURFACE AREA IS 1.21 ACRES ON-SITE.

IN ADDITION, PROJECT PROPOSES TO REPLACE 11,951 SQUARE FEET OF EXISTING OFF-SITE IMPERVIOUS AREA, INCLUDING THE PUBLIC STREETS (SIDEWALK, DRIVEWAYS, AND ROAD). THE PRE-PROJECT OFF-SITE IMPERVIOUS AREA IS 12,383 SQUARE FEET, INCLUDING PUBLIC STREETS (ROAD AND SIDEWALK). THERE IS A REDUCTION OF OFF-SITE IMPERVIOUS AREA DUE TO THE ADDITIONS OF TREE GRATES ALONG 25TH STREET, BROADWAY, AND 24TH STREET.
4. PROVISION C.3 PROJECT REQUIREMENTS FOR CATEGORY B: LARGER INFILL PROJECTS UNDER APPENDIX J.3 INCLUDE:
 - 4.1. BE BUILT AS PART OF THE MUNICIPALITY'S STATED OBJECTIVE TO PRESERVE OR ENHANCE A PEDESTRIAN-ORIENTED TYPE OF URBAN DESIGN.
 - 4.2. BE LOCATED IN A PERMITTEE'S DESIGNATED CENTRAL BUSINESS DISTRICT, DOWNTOWN CORE AREA OR DOWNTOWN CORE ZONING DISTRICT, NEIGHBORHOOD BUSINESS DISTRICT OR COMPARABLE PEDESTRIAN ORIENTED COMMERCIAL DISTRICT, OR HISTORIC PRESERVATION SITE AND/OR DISTRICT.
 - 4.3. CREATE AND/OR REPLACE GREATER THAN ONE-HALF ACRE, BUT NO MORE THAN 2 ACRES OF IMPERVIOUS SURFACE AREA.
 - 4.4. INCLUDE NO SURFACE PARKING, EXCEPT FOR INCIDENTAL SURFACE PARKING. INCIDENTAL SURFACE PARKING IS ALLOWED ONLY FOR EMERGENCY VEHICLE ACCESS, ADA ACCESSIBILITY, AND PASSENGER AND FREIGHT LOADING ZONES.
 - 4.5. HAVE AT LEAST 85% COVERAGE FOR THE ENTIRE PROJECT SITE BY PERMANENT STRUCTURES. THE REMAINING 15% PORTION OF THE SITE IS TO BE USED FOR SAFETY ACCESS, PARKING STRUCTURE ENTRANCES, TRASH AND RECYCLING SERVICE, UTILITY ACCESS, PEDESTRIAN CONNECTIONS, PUBLIC USES, LANDSCAPING, AND STORMWATER TREATMENT.
5. DESIGN CRITERIA
 - 5.1. MEAN ANNUAL PRECIPITATION = 21 INCHES PER ALAMEDA COUNTY FLOOD CONTROL AND WATER CONSERVATION DISTRICT, FIGURE 9
 - 5.2. HYDRAULIC DESIGN CRITERIA: 0.2 INCHES PER HOUR RAINFALL INTENSITY
6. THIS PROJECT QUALIFIES AS A SPECIAL PROJECT UNDER CATEGORY B IN ACCORDANCE WITH THE ALAMEDA COUNTY CLEAN WATER PROGRAM'S "C.3 STORMWATER TECHNICAL GUIDANCE", VERSION 5.1. THE PERCENTAGE OF NON-LID TREATMENT IS 100%, RESULTING IN 1.21 ACRES OF STORMWATER RUNOFF TO BE TREATED BY HIGH FLOW-RATE MEDIA FILTERS.

LEGEND

EXISTING	PROPOSED	
		STORM DRAIN
		STORM DRAIN MANHOLE
		CATCH BASIN
		STORM FILTER MANHOLE
		ROOF AREA DRAINING TO C.3 STORM FILTER MANHOLE



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LANDSCAPE PROGRAM

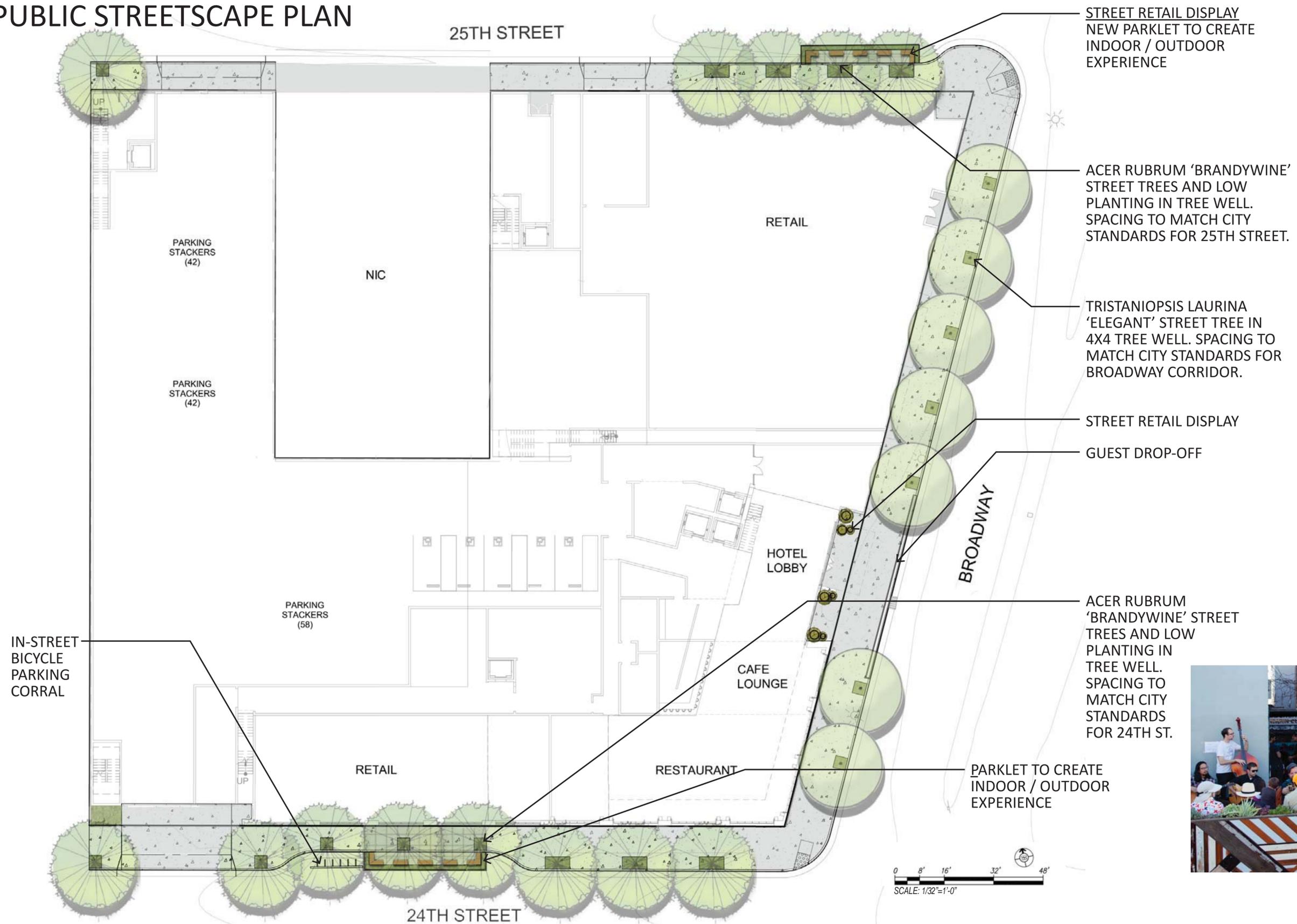


LEGEND

- ① PUBLIC STREETScape
- ② HOTEL BREAKOUT COURTYARD 1
- ③ HOTEL BREAKOUT COURTYARD 2
- ④ HOTEL VIEWING GARDEN 1
- ⑤ HOTEL VIEWING GARDEN 2
- ⑥ HOTEL VIEWING GARDEN 3
- ⑦ BAR & LOUNGE TERRACE
- ⑧ RESIDENTIAL AMENITY COURTYARD
- ⑨ RESIDENTIAL ROOF DECK AT CLUB ROOM

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PUBLIC STREETSCAPE PLAN



STREET RETAIL DISPLAY
NEW PARKLET TO CREATE
INDOOR / OUTDOOR
EXPERIENCE

ACER RUBRUM 'BRANDYWINE'
STREET TREES AND LOW
PLANTING IN TREE WELL.
SPACING TO MATCH CITY
STANDARDS FOR 25TH STREET.

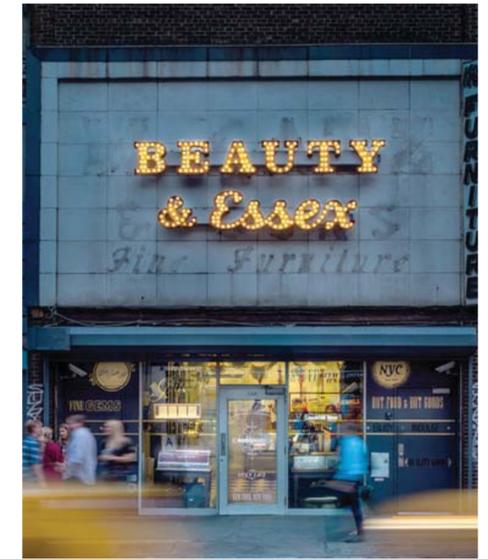
TRISTANIOPSIS LAURINA
'ELEGANT' STREET TREE IN
4X4 TREE WELL. SPACING TO
MATCH CITY STANDARDS FOR
BROADWAY CORRIDOR.

STREET RETAIL DISPLAY

GUEST DROP-OFF

ACER RUBRUM
'BRANDYWINE' STREET
TREES AND LOW
PLANTING IN
TREE WELL.
SPACING TO
MATCH CITY
STANDARDS
FOR 24TH ST.

PARKLET TO CREATE
INDOOR / OUTDOOR
EXPERIENCE



Unique wayfinding



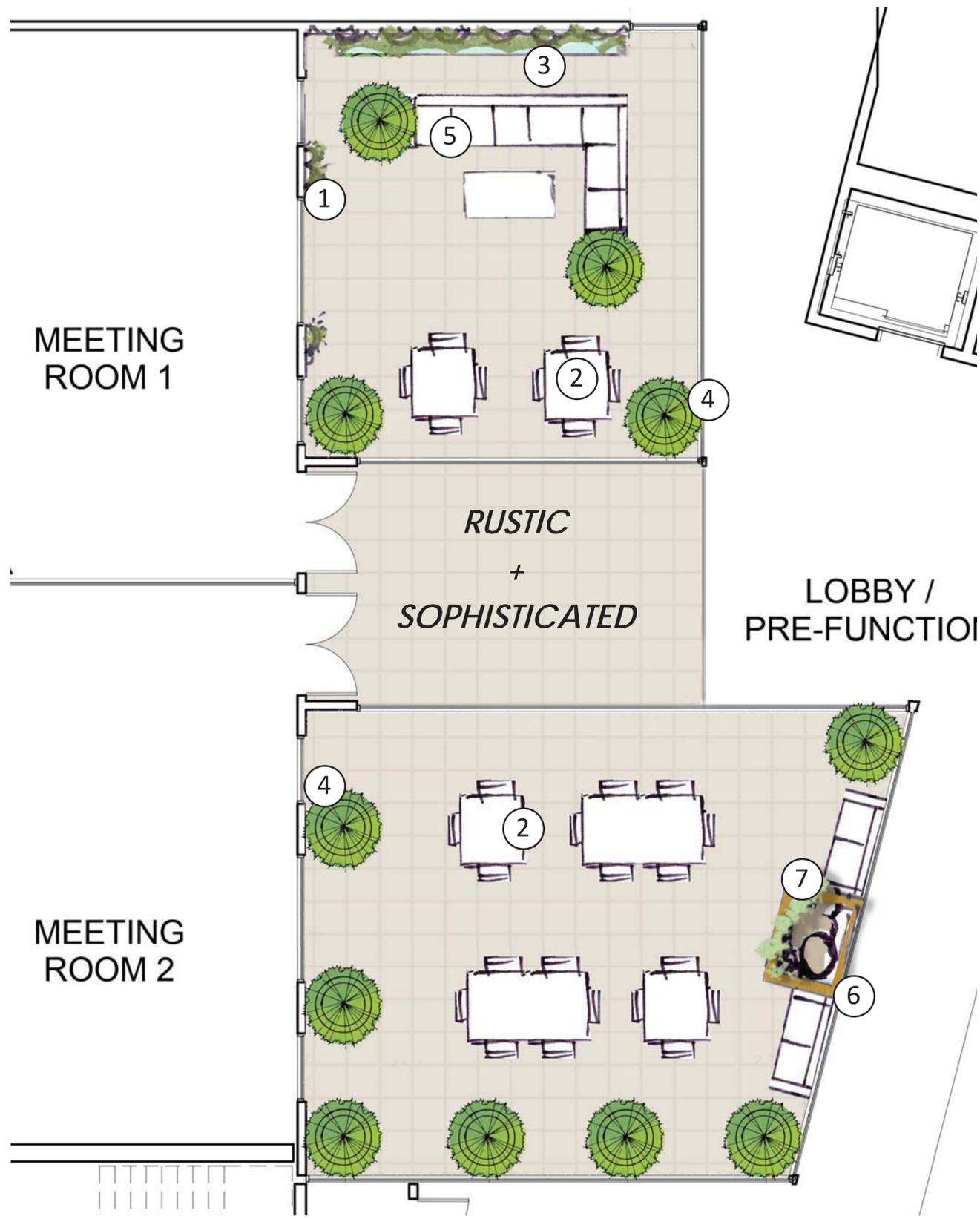
Street retail display



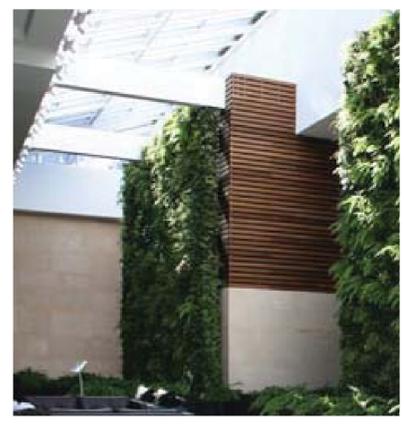
Parklet

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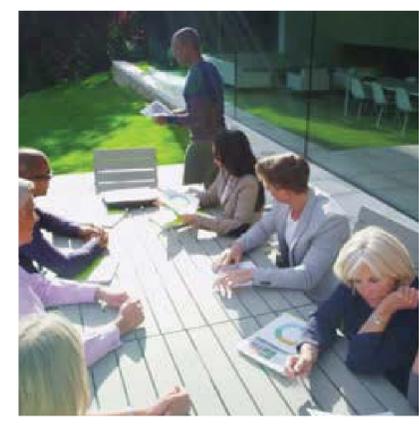
LEVEL 2 - HOTEL BREAKOUT COURTYARDS 1 & 2



① Wall art



Atrium



② Meeting tables



Lighting



③ Wall water feature



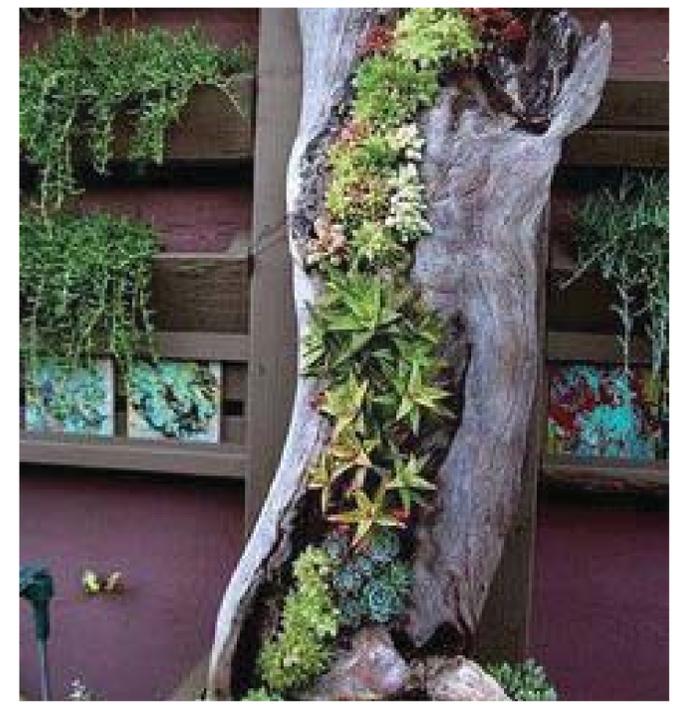
④ Mobile planters



⑤ Lounge seating



⑥ Reclaimed redwood mobile planter



⑦ Sculptural planting / Art

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LEVEL 3 - HOTEL VIEWING GARDEN 1



Sculptural planting element



Landscape massing

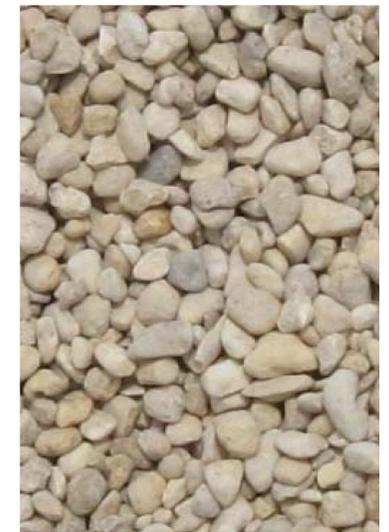
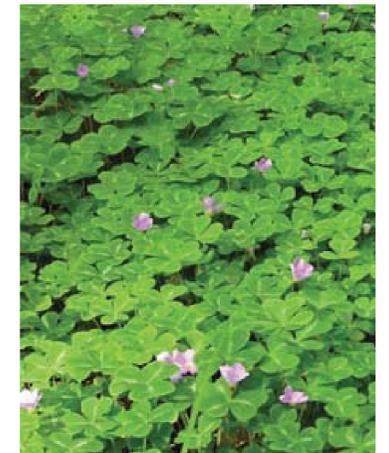


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LEVEL 3 - HOTEL VIEWING GARDENS 2 & 3

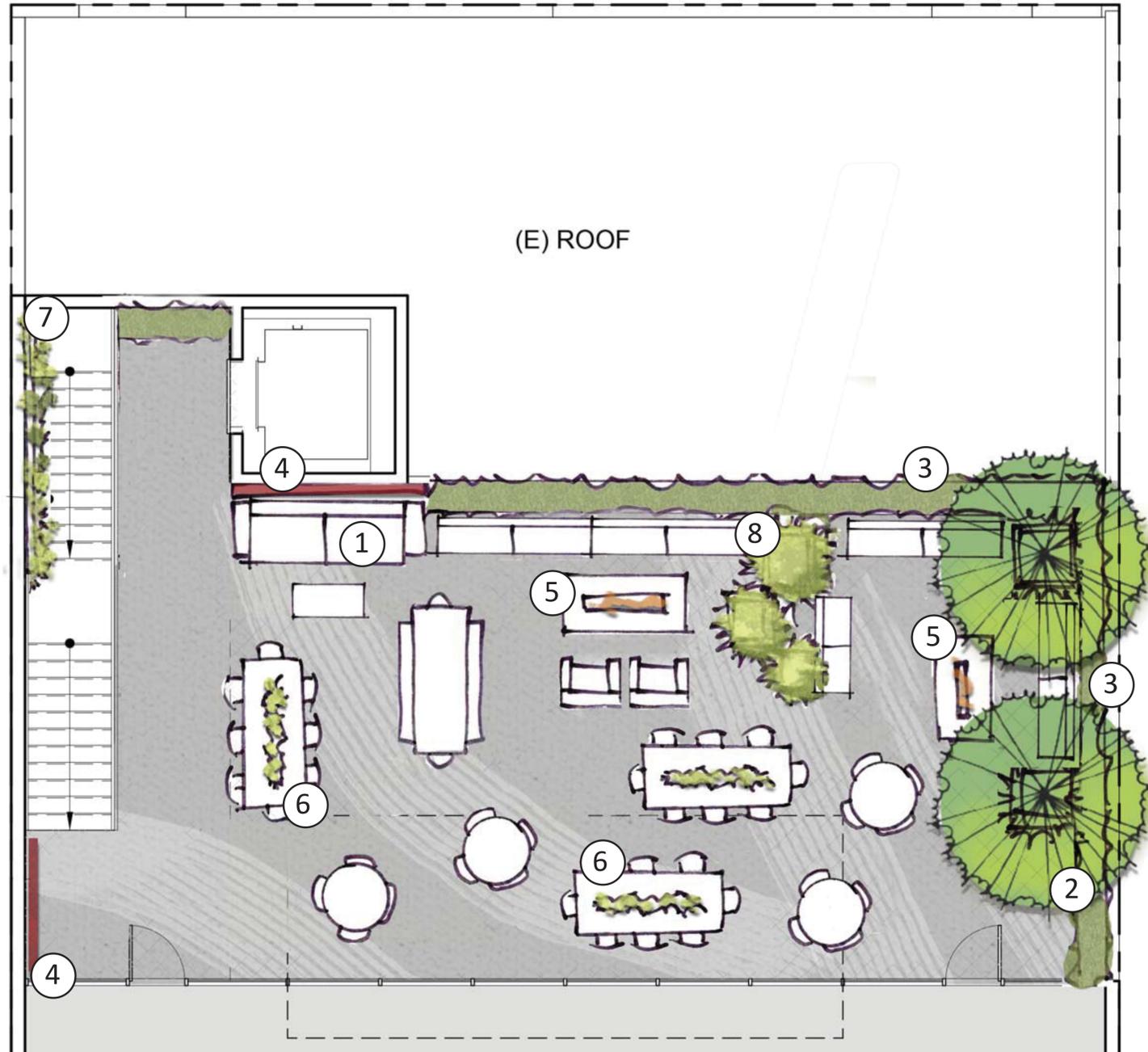


Emergent Landscape

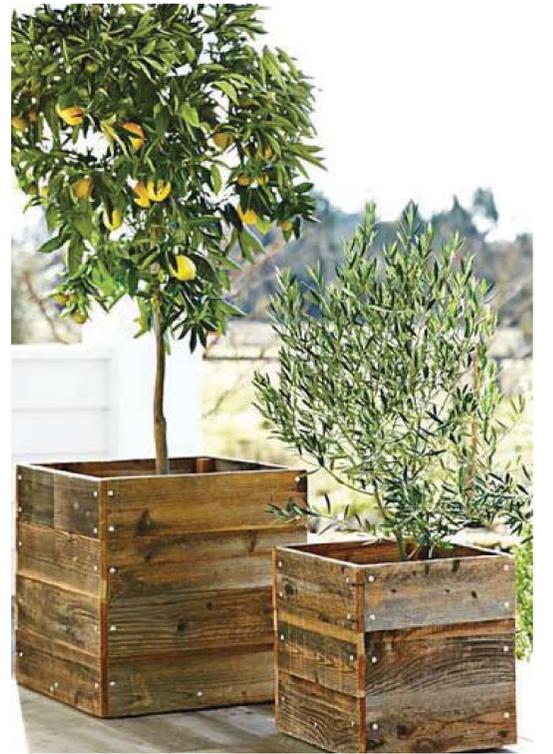


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LEVEL 3 - BAR & LOUNGE TERRACE



① Lounge furniture



② Citrus trees + herbs



Connection + community



③ Reclaimed wood screening + planters



④ Local art



⑤ Firepits



⑥ Community tables



⑦ Greenwall



⑧ Cityscape planters

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LEVEL 3 - RESIDENTIAL AMENITY COURTYARD



① Curved seating



② Landscape mounds



③ Art sculpture / Water feature

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LEVEL 8 - RESIDENTIAL ROOF DECK AT CLUB ROOM



⑥ Lounge Seating



⑤ Bar Rail



① Double sided fireplace



② Outdoor kitchen



③ String lighting



④ Ornamental Vegetable + herb gardens

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