

Case File Number: PLN15-245-PUDF-01**October 21, 2015**

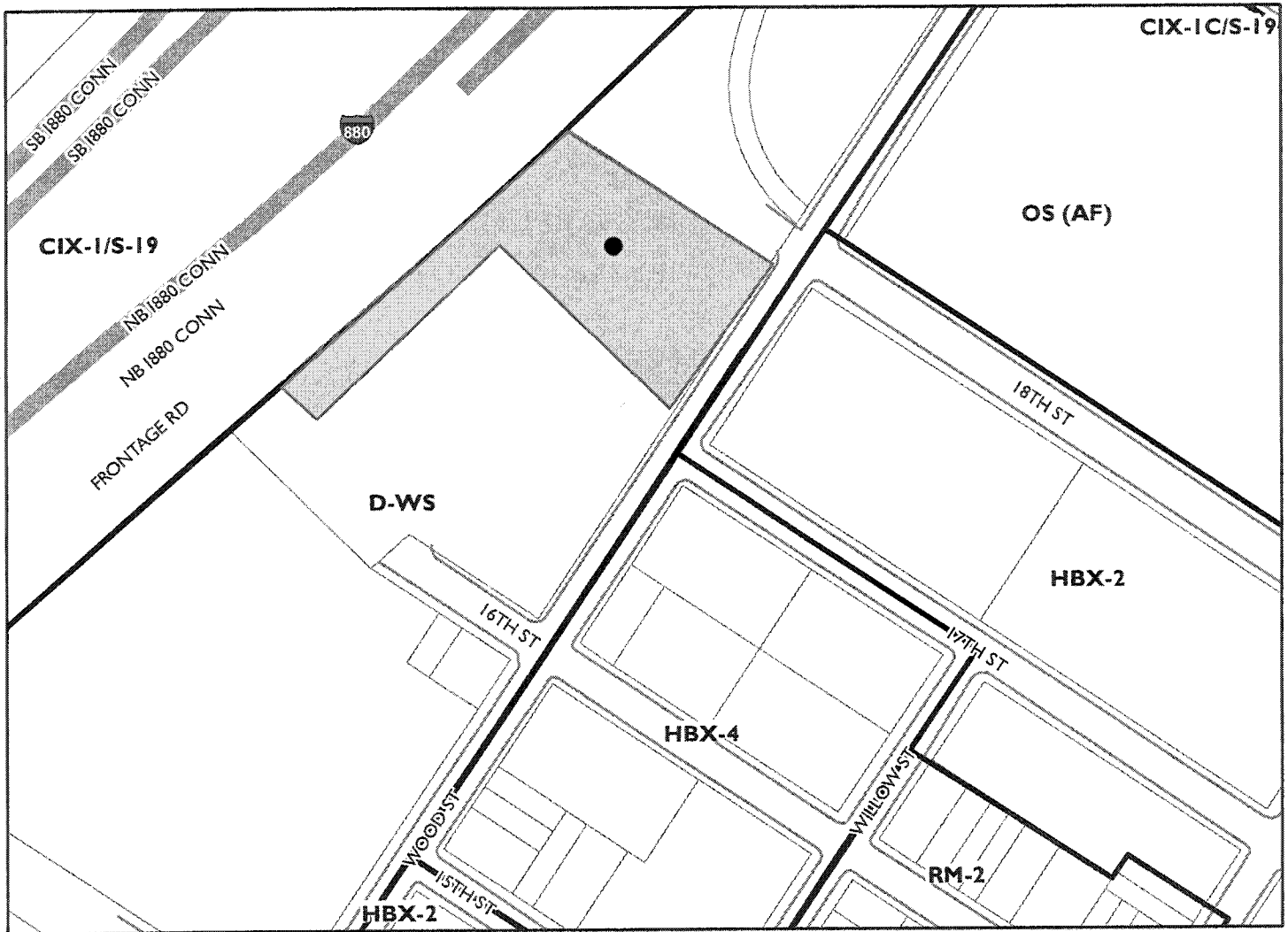
Location:	1405 Wood Street - Development Area 6
Assessors Parcel Number:	018-0310-013-01
Proposal:	To construct a new 44-unit townhouse-style residential condominium development (Signal House) on a vacant 1.98-acre site (Tentative Parcel Map for condos to be filed later).
Owner:	Build West Oakland, LLC
Applicant:	City Ventures – Claire Han (415)722-6534
Planning Permits Required:	Preliminary and Final Development Plan; Design Review for construction of 44 new townhouse-style residential condominiums
General Plan:	Urban Residential
Zoning:	D-WS Wood Street Zoning District – Development Area 6
Environmental Determination:	State CEQA Guidelines: The project relies on previous EIR (ER03-0023) for Wood Street certified on March 16, 2005 and the West Oakland Specific Plan (WOSP) EIR certified on July 29, 2014, and Section 15183, projects consistent with a community plan, general plan or zoning.
Historic Status:	Not a Potential Designated Historic Property (PDHP); Survey Rating: N/A
Service Delivery District:	1
City Council District:	3
Status:	Pending
Action to be Taken:	Decision on application based on staff report
Staff Recommendation:	Approval subject to conditions
Finality of Decision:	Appealable to City Council
For Further Information:	Contact case planner Maurice Brenyah-Addow at (510) 238-6342 or by email at mbrenyah@oaklandnet.com

SUMMARY

City Ventures is requesting approval of a combined Preliminary and Final Development Plan, and Design Review for a new 44-unit townhouse-style residential condominium development. The Tentative Parcel Map (TPM) to create the condominiums will be filed at a later date. The project is proposed to be constructed on a vacant 1.98-acre site adjacent to and north of the historic 16th Street Train Station and bounded by Wood Street to the east, 18th Street to the north, and Frontage Road to the west. The site is designated as Development Area 6 in the Wood Street Zoning District (WSZD).

The proposed project is substantially consistent with the applicable land use regulations and property development standards as specified in the WSZD for approving a Preliminary Development Plan (PDP) and Final Development Plan (FDP), and Regular Design Review for new construction subject to the attached conditions of approval (See “Findings” and “Conditions of Approval” sections of this report for details).

CITY OF OAKLAND PLANNING COMMISSION



0 125 250 500 750 1,000 Feet



Case File: PLN15245 / PUDF01

Applicant: City Ventures – Clare Han

Address: 1405 Wood Street - Development Area 6

Zone: D-WS

Staff believes that the project will complement the Wood Street neighborhood's goal of creating an active, pedestrian-oriented urban community in West Oakland and therefore recommend that the Commission confirm the environmental determination and approve the Preliminary and Final Development Plan, and Design Review application for the proposed project, subject to the attached Conditions of Approval.

BACKGROUND

The Wood Street Zoning District (WSZD) Project is a mixed-use redevelopment of under-utilized land around the 16th Street Train Station in West Oakland with the goal of creating an active, pedestrian-oriented urban community in West Oakland. The 29-acre Wood Street Development Project, involving five vesting tentative parcel maps, was approved by the City Council in June 2005. The plan area lies between 10th Street to the south, West Grand Avenue to the north, Wood Street to the east and 1-880 Frontage Road to the west.

The WSZD regulations include Land Use Regulations, Development Standards and Design Guidelines aimed at ensuring that individual developments will be visually and functionally integrated, and that collectively the project will be compatible with the existing neighborhood. For the purposes of appropriate regulations, the WSZD is divided into nine (9) Development Areas, each subject to specific regulations and to be developed within specific timelines by the respective owners. The WSZD project also approved a series of Vesting Tentative Maps (VTPM8551–8555) for all the parcels in the district of which VTPM8554 was approved to create Parcels 1, 2, 3, and 4 for Development Areas 5 and 6.

Since the WSZD regulations were adopted, three main development projects have been completed for Development Areas 2 and 3. These include 1.) the 163-unit Pacific Cannery Lofts (PCL) by Holliday Developments; 2.) the 130-unit Zephyr Gate townhomes by Pulte Homes; and 3.) the 99-unit Ironhorse rental apartments by Bridge Housing. In 2007 the Planning Commission approved the HFH Apartments project (a 301-unit apartment development) for Development Area 4 but it was not built and recently a new City Ventures mixed-use project involving 176-unit residential and 5 commercial condominiums was approved by the Planning Commission on August 6, 2014. Some initial site preparation work is underway for this project and construction is scheduled to commence soon. On December 3, 2014, the Planning Commission approved another Holiday Development project for a mixed-use (residential/commercial) development involving 235 residential units and 13,615 flex commercial spaces to be completed in two phases for Development Area 8. On May 20, 2015, the Planning Commission approved another City Ventures project for a 47 unit residential development at Parcel 2 of Development Area 1. The proposed 44 units is the latest official proposal (in addition to the above-listed projects) that the City has received towards implementation of aspects of the Wood Street Project.

PROPERTY DESCRIPTION

The WSZD Development Area 6 (Parcel 3) is one of 4 parcels approved as part of Vesting Tentative Parcel Map 8554. Parcel 3 of Development Area 6 is a 1.98-acre vacant site bounded by the 16th Street Train Station to the south, Wood Street to the east, 18th Street to the north, and Frontage Road to the west. Prominent uses and developments in the vicinity include the historic 16th Street Train Station, the Zephyr Gate townhouses, the Ironhorse housing development, the Pacific Cannery Lofts and a mixture of uses including the Bea's Hotel, the California Waste Solutions recycling facility, historic Victorian homes, warehouses, and construction/light industrial yards.

PROJECT DESCRIPTION

The proposed project involves the construction of a new 44-unit townhouse-style residential condominium development. The approximately 1,662 square-foot average-sized 2 bedroom and studio units will be three stories of residential occupancy with ground level parking garages. The project provides private decks and patios for some of the units and group useable open space in the form of a landscaped pocket park. The site plan features a set of six detached row-house structures that contain groups of the townhouse-style condominium units. The new structures are organized along internal circulation alleyways and driveways with a main entrance and exit on 18th Street.

The exterior materials and treatments include cement-board, stucco, and cast concrete. The designs incorporate various elements and detailing such as stoops for the units facing Wood Street, private decks, projecting/recessed planes. The proposed landscaping include trees, shrubs, ground cover, as well as hardscape such as decorative pavers, planters and other features that enhance the visual quality, functionality, and experience of the open areas.

ENVIRONMENTAL DETERMINATION

An Environmental Impact Report (EIR) ER03-0023 was certified for the Wood Street Development Project on March 16, 2005, and an EIR for the West Oakland Specific Plan was certified on July 29, 2014 (collectively, "Previous CEQA Documents"). No further/additional CEQA review is required. None of the circumstances necessitating preparation of additional CEQA review as specified in CEQA and the CEQA Guidelines, including without limitation Public Resources Code section 21166 and CEQA Guidelines sections 15162 and 15163 are present, in that:

1. There are no substantial changes to the project that would result in new significant environmental impacts or a substantial increase in the severity of significant impacts already identified in the Previous CEQA Documents;
2. There are no substantial changes in circumstances that would result in new significant environmental impacts or a substantial increase in the severity of significant impacts already identified in the Previous CEQA Documents; and

3. There is no new information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the Previous CEQA Documents were certified, which is expected to result in: (a) new significant environmental effects or a substantial increase in the severity of significant environmental effects already identified in the Previous CEQA Documents; or (b) mitigation measures which were previously determined not to be feasible would in fact be feasible, or which are considerably different from those recommended in the Previous CEQA Documents and which would substantially reduce significant effects of the project, but the City declines to adopt them.

As a separate and independent basis, the project is also exempt from CEQA review pursuant to CEQA Guidelines Section 15183, projects consistent with a community plan, general plan or zoning.

GENERAL PLAN ANALYSIS

The subject site is located in the Urban Residential land use classification according to the City of Oakland General Plan Land Use and Transportation Element (LUTE). According to the LUTE, the intent of the Urban Residential classification is to “create, maintain, and enhance areas of the City that are appropriate for multi-unit, mid-rise or high-rise residential structures in locations with good access to transportation and other services.” Furthermore, the primary future use in this classification is residential. The proposed 44 residential development is consistent with the general plan in that it is located within the Wood Street Development Project Area with easy access to major arterial streets such as West Grand Avenue and has good access to public transit such as Bart and AC Transit.

The proposed project is consistent in all significant respects with the following General Plan objectives and policies:

- Objective N3: Encourage the construction, conservation, and enhancement of housing resources in order to meet the current and future housing needs of the Oakland community. The project will provide the Oakland community with 44 new dwelling units.
- Policy N3.1, Facilitating Housing Construction: *Facilitating the construction of housing units should be considered a high priority for the City of Oakland.* The City of Oakland’s Planning and Zoning Department has streamlined its systems in order to facilitate the construction of new homes by assisting developers to navigate the permitting process smoothly and in a timely manner.
- Policy N3.2, Encouraging Infill Development: *In order to facilitate the construction of needed housing units, infill development that is consistent with the General Plan should take place throughout the City Oakland.* The project is proposed for a site located in an urban area of West Oakland. The new development will be an infill development for the currently vacant site.
- Policy N6.2, Increased Home Ownership: *Housing developments that increase home ownership opportunities for households of all incomes are desirable.* The project will provide home ownership opportunities for 44 new households. The developer intends to build these units as condominiums which will create homeownership opportunities and make the units more affordable for purchasing.

ZONING ANALYSIS

The WSZD regulations include Land Use Regulations, Development Standards and Design Guidelines aimed at ensuring that individual developments will be visually and functionally integrated with each other as well as collectively compatible with the existing neighborhood.

The WSZD requires both a Preliminary and a Final Development Plan which shows such details as streets, location of details of structures, uses, estimated population, landscaping features, phasing plan, public and private infrastructure, etc. for each development area.

Preliminary and Final Development Plan (Wood Street Zoning District)

The Preliminary Development Plan (PDP) and Final Development Plan (FDP) in the context of the Wood Street Zoning District are unique and although similar in concept to a Planned Unit Development (PUD), it is different in format and review criteria from the PUD process in the Planning Code. The procedure for Design Review for PDP/FDP in the WSZD follow the schedule outlined in Section 17.136.060 of the Planning Code, but not the criteria. The WSZD limits Design Review to a determination of whether or not the proposed design is in substantial compliance with the design guidelines specified in WSZD regulations. (See findings and attachment "B" for checklist).

Land Use Regulations

Development Area 6 of the WSZD allows a maximum density of 1 unit per 549.06 sq. ft. of lot area which translates into a maximum of 157 units for the 1.98-acre project site. The proposed 44-unit project is therefore consistent with the Development Area 6 prescribed density. The proposed 44 residential units (Permanent Residential Activities) is a permitted use in Development Area 6 of the WSZD.

In order to approve the project, the WSZD regulations require the Planning Commission to make written findings that the Preliminary and Final Development Plan for the project is in substantial conformance with the Wood Street Zoning District Regulations - Design Review criteria (See Findings) and the WSZD development standards (See attachment B).

Project Consistency with Required Development Standards

Table 5.10.1	Required	Proposed
Max. Residential Density		
1. Min land area/dwelling unit	549.06 sq. ft.	1,959.2 sq. ft.
2. Max. dwelling unit/acre	79.33 DU/A	22.22 DU/A
3. Max number of units	157	44
Min. Residential Density	N/A	N/A
Max. FAR	N/A	N/A
Max. Height	65 ft. (some exceptions apply)	39.5 ft.
Min. Street setbacks		

-Wood Street	10ft.	N/A.
-12 th Street	0	N/A
-Frontage Road	0	N/A
-14 th Street	0	N/A
-Public Access Areas	0	N/A
Min. Interior setbacks	5 ft.	10 ft.
Min. useable open space	75 sq. ft./unit)	157.93 sq. ft./unit
Reqd. off-street parking- - Residential	1.1 space/DU	1.4 spaces/DU

KEY ISSUES AND IMPACTS

Overall, the proposed site plan and design for the proposed project is successful and consistent with the Wood Street development plan's goal of creating an active, pedestrian-oriented urban community in West Oakland. The proposed townhouse-style/row-house structures will be built as condominium units (TPM for condos to be filed later) and appropriate for this parcel of land in the Wood Street development area. The key issues and impacts are discussed below:

Density/Setbacks

The 44-unit project is significantly below the maximum of 157 units for the 1.98-acre project site. The proposed structures will comply with all the required setbacks along Wood Street and the remaining property lines.

Automobile Parking

The project provides at least one off-street parking space for all the residential units within an enclosed garage. A few units have two-car garages for a total of 62 spaces for the entire development.

Site Plan and Building Design

The site plan presents an urban medium-density type development that responds to the context of urbanized development in the vicinity. The units are proposed in a series of 3-story row-house structures with contemporary design styles to blend-in with the recent developments in the Wood Street District. The exterior materials and treatments include cement board, stucco, wood, and cast concrete. The designs incorporate various elements and detailing such as projecting/recessed bays and frames to achieve a well-composed building volume that is proportionally scaled to fit the site. The site plan responds to the 16th Street Train station and plaza with landscaping, low fencing, access points, and building elevations that read similar to front facades.

Useable Open Space

The WSZD requires 75 square feet of useable open space per dwelling unit. The project provides an average of 90 square feet of private open space per unit. The project incorporates landscaping (softscape and hardscape) to enhance the open areas to achieve a net reduction of peak flow stormwater run-off.

Landscaping

The project incorporates various landscaping elements to achieve an attractive street and front yard ambience. The project involves a landscaped pocket park that extends approximately midblock of 18th Street up to Frontage Road. The proposed landscaping include trees, shrubs, ground cover, as well as hardscape such as decorative pavers, planters and other sculptural features that enhance the visual quality, functionality, and experience of the open areas.

Project Phasing

The project is proposed to be completed in 3 phases. Phase I involves public and private infrastructure and Buildings 1 and 4 and other site improvements. Phase II will complete Buildings 2 and 3, and phase III will complete Buildings 5 and 6. Construction is projected to commence in the second quarter of 2016.

Historic 16th Street Train Station

The project site is located adjacent to, and north of the historic 16th Street train station. The development is not expected to change the significance of, or have any negative impacts on the train station. Instead, the project involves improvements at a neighboring vacant site and expected to bring more residents to the area, generate more pedestrian activity, spur economic development in the area and serve as a catalyst for similar developments in the area and contribute to the subsequent rehabilitation of the train station building.

CEQA Analysis

As described in the "Environmental Determination" section, above, none of the circumstances necessitating further CEQA review are present. Thus, the City can rely on the Previous CEQA Documents.

Traffic Mitigation and fair Share Contributions

As part of mitigation measures for the Wood Street Project, certain traffic mitigation measures were identified to study and improve specific intersections. The project shall be required to comply with the Traffic Mitigations and Fair Share contributions as per the mitigation measures associated with VTPM8554 (Parcel 3) and ER03-0023 for the Wood Street project with details as follows:

VTPM Condition 25 (Fair Share Contribution to Improvements at Frontage Road and W. Grand);

VTPM Condition 26 (Fair Share of Modifications at the West Grand Avenue/Mandela Parkway Intersection);

VTPM Condition 27 (Fair Share of Modifications at the 7th Street/Mandela Parkway Intersection);

VTPM Condition 28 (Fair Share of Improvements at West Grand Avenue/Maritime Street and 3rd Street/Market Street Intersections): See Attachment "D" (letter of May 29, 2009 from Eric Angstadt, establishing a Fair Share Payment of \$11,737 for COA's 25, 26, 27 and 28 for the "Central Station LLC" site);

VTPM Condition 76 (Public or Private Shuttle Service between the Project Area and the West Oakland BART Station): From 2007 to 2009, the Project Sponsors engaged in a study of this topic, soliciting a number of bids for private shuttle service, surveying residents and future residents about their potential utilization of such service, and engaging in discussions with AC

Transit and BART. As a result of this study, the Project Sponsors elected not to utilize a private shuttle, for the following reasons:

- AC Transit opposed the private shuttle service, which they believed would negatively impact their ridership
- In surveys conducted at the time, incoming homeowners expressed very low levels of support for a private shuttle, a strong indicator that the shuttle was unlikely to be "used sufficiently to result in a substantial reduction in private vehicle use by Project residents and occupants"
- In consultation with AC Transit, the Project Sponsors provided a new stop on the 26 line at 12th and Wood
- In response to anticipated new peak hour utilization, AC Transit agreed to reduce peak hour headways at the new stop to 15 minutes, with a 5 minute travel time to the West Oakland BART station
- Condition 76 explicitly allows latitude on the parts of both the Project Sponsors and the Planning Director to implement the best solution to facilitate use of transit and reduce private vehicle use. Due to more recent budget cutbacks and ridership analysis, headways at the local stop have increased during peak hours to 18-20 minutes, although travel time to the West Oakland BART station remains 5 minutes.

For VTPM8554 COA's 25, 26, 27, and 28, the applicant would be required to pay the Fair Share contribution identified in Eric Angstadt's letter of May 29, 2009 prior to issuance of building permit (See Attachment D).

VTPM8554 COA's 29 and 30, which addresses BART ridership and fare gate capacity are no longer CEQA thresholds however they are required as mitigation for traffic impacts. Staff recommends that the applicant collaborate with the other Wood Street Project sponsors to re-engage discussions with BART to see if they have completed and implemented methods to address capacity impacts on BART for new developments.

For VTPM8554 COA 76, the applicant should collaborate with the other Wood Street Project sponsors to re-engage discussions with AC Transit to see if service on the 26 line can be returned to 15 minute headways, and as development of the Wood Street Zoning District progresses to the north, in coordination with AC Transit, locate an additional AC Transit stop proximate to the new uses.

Specific COAs related to VTPM8554

The proposed project is also subject to the specific and relevant conditions of approval pertaining to VTPM8554 enclosed to this staff report as "Attachment C".

City Engineer's Report

Section 3.20 and 3.40 of the WSZD regulations require that the City Engineer review the plans and determine whether the submittal is complete. The PDP/FDP plans were circulated for review and

comment to the Public Works agency, Building Services, and Fire Prevention Services and all departments have reviewed and cleared the project for approval subject to the conditions of approval. (See Conditions of Approval).

CONCLUSION:

The proposed new residential development involving 44 residential units is an appropriate development for the WSZD of West Oakland. The project is consistent with the development standards of the WSZD land use regulations of the Oakland Planning Code. The project would not result in new or more severe environmental impacts beyond those identified in the Wood Street EIR and therefore no subsequent or supplemental environmental review is warranted. The site plan, building designs, layout, heights, materials, colors, open areas, and landscaping all combine as a cohesive whole that is appropriate for the subject site. Staff believes that this project will serve as a catalyst for further development within the Wood Street Project area and eventually lead to the rehabilitation and reuse of the 16th Street Train Station and Plaza.

RECOMMENDATIONS:

Based on the analysis contained in this report, the findings, and the conditions of approval attached to this report and elsewhere within the administrative record, staff believes that the proposed project is an appropriate development that will further the overall objectives of the WSZD and the Oakland General Plan, particularly related to new housing development. Thus, staff recommends that the Commission:

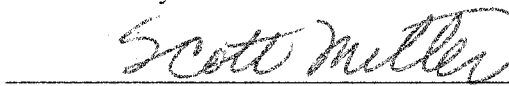
1. Affirm staff's CEQA determination; and
2. Approve the Preliminary/Final Development Plan, Design Review subject to the attached Findings and Conditions of Approval.

Prepared by:



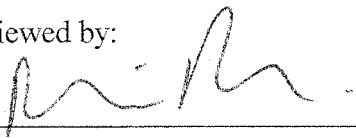
MAURICE BRENYAH-ADDOW - Planner III

Reviewed by:



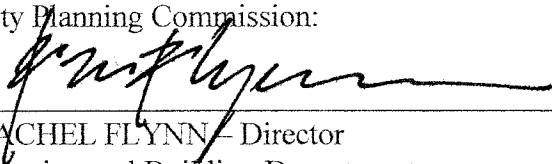
SCOTT MILLER - Zoning Manager

Reviewed by:



DARIN RANELLETTI – Deputy Director
Bureau of Planning

Approved for forwarding to the
City Planning Commission:



RACHEL FLYNN – Director
Planning and Building Department

- ATTACHMENTS:**
- A. Project Plans
 - B. Project Conformance Checklist
 - C. VTPM8554 Relevant Conditions of Approval
 - D. Fair Share Contribution

FINDINGS FOR APPROVAL:

This proposal meets the required findings and standards under Section 17.101A DW-S Review and Development Standards of the Oakland Planning Code as set forth below. Required findings are shown in **bold type**; explanations as to why these findings can be made are in normal type.

Findings for Approval of Preliminary development Plan/Final Development Plan

Approval of the PDP/FDP for the Development Area 8 Wood Street project is supported by the following findings: The project conformance with the Wood Street Zoning District is not limited to the findings identified below, but is also included in the previous analysis of this staff report, the Wood Street Development EIR, and the Wood Street Zoning District Development Standards Checklist prepared by Baran Studio (Attachment B, incorporated by reference into these findings) as well as elsewhere in the record of proceedings leading up to these approvals.

I. California Environmental Quality Act (CEQA) Findings

An Environmental Impact Report (EIR) ER03-0023 was certified for the Wood Street Development Project on March 16, 2005, and an EIR for the West Oakland Specific Plan was certified on July 29, 2014 (collectively, "Previous CEQA Documents"). No further/additional CEQA review is required. None of the circumstances necessitating preparation of additional CEQA review as specified in CEQA and the CEQA Guidelines, including without limitation Public Resources Code section 21166 and CEQA Guidelines sections 15162 and 15163 are present, in that:

1. There are no substantial changes to the project that would result in new significant environmental impacts or a substantial increase in the severity of significant impacts already identified in the Previous CEQA Documents;
2. There are no substantial changes in circumstances that would result in new significant environmental impacts or a substantial increase in the severity of significant impacts already identified in the Previous CEQA Documents; and
3. There is no new information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the Previous CEQA Documents were certified, which is expected to result in: (a) new significant environmental effects or a substantial increase in the severity of significant environmental effects already identified in the Previous CEQA Documents; or (b) mitigation measures which were previously determined not to be feasible would in fact be feasible, or which are considerably different from those recommended in the Previous CEQA Documents and which would substantially reduce significant effects of the project, but the City declines to adopt them.

As a separate and independent basis, the project is also exempt from CEQA review pursuant to CEQA Guidelines Section 15183, projects consistent with a community plan, general plan or zoning.

II. Findings for Approval of Preliminary Development Plan (Section 3.20) and Final Development Plan (Section 3.40)

The Planning Commission find that the Preliminary Development Plan (PDP) and final Development Plan (FDP) for the project is in substantial conformance with the Wood Street Zoning District.

- A. The PDP and FDP have been prepared by a professional design team consisting of a licensed architect (Hunt Hale Jones), registered civil engineer (Sandis), and professional landscape architect (Zeki Abed – Dan Dorn Abed Landscape Architects, Inc.).
- B. The layout, design and size of the proposed project are appropriate for the location and compatible with the surrounding area, which has a variety of land uses and building types.
- C. The proposed project is consistent with the applicable Wood Street Zoning District regulations and development standards relating to density, height, building frontage, useable open space, number of off-street parking spaces, location of garages, etc.
- D. The proposed project's substantial conformance to the Wood Street Zoning District regulations and development standards, including the manner in which various project features implement the stated intent behind certain regulations, is described fully in the Wood Street Zoning District Development Standards Checklist (Attachment B, incorporated by reference into these findings). The analysis contained in the Compliance Checklist is carried forward by the Planning Commission and made part of these findings.
- E. The utility and infrastructure plans meet the requirement of the Wood Street Zoning District.
- F. The public facilities financing plan for the proposed project has improvements to 18th Street being funded in cash by the applicant (Public Facilities Financing Letter provided). See condition 81 of Wood Street Project approval.

III. Findings for Final Design Review – Sections 3.50 and 6.00 of the Wood Street Zoning District Regulations

The Planning Commission finds that the project is substantially in compliance with the Section 6.00 Design Guidelines, of the Wood Street Zoning District.

General Guidelines

1. **Each development project shall by use of massing, articulation, materials and detail establish a coherent integrated architectural character that is consistent with each development project. All built aspects of the development project shall be considered as contributing to the architectural character of the development project, including but not limited to; site structures and furnishings, exterior lighting, paving and signage.**

The 3 story structures proposed to be constructed apply similar exterior materials and treatments such as cement board siding, stucco, projecting frames, etc. The design incorporates various architectural elements and detailing such as projections and recesses, varied rooflines, some decks, etc. to achieve a series of distinct and well-composed building volumes that are proportionally scaled for the long narrow site. Together, the five main buildings, create a

sequence of structures with landscaped front and rear yards that create visual interest while infilling a vacant site.

2. Buildings are not restricted to any specific architectural style. Within the overall scope of the Wood Street Zoning District, a variety of architectural styles are encouraged as a means of enhancing the mixed-use character of the development project.

The proposed 3 story buildings are designed to have six detached batches of single family townhomes. The contemporary style of the design blends-in with the recent developments in the Wood Street District and other buildings in the neighborhood. The designs incorporate various elements and detailing to achieve a well-composed building volume that is proportionally scaled to fit the site. The proposed development will preserve the urban residential character of the neighborhood. The new residents will serve as potential new patrons to the local businesses.

Compliance with Guidelines:

- A. The Planning Commission finds that the project satisfies the design intent set forth in the Wood Street Zoning District Design guidelines. The proposed new project is well related to the completed developments (PCL, Zephyr Gate, Ironhorse), planned open spaces (16th Street Train Station Plaza), the planned development (remaining sites) within the Wood Street project and the adjacent uses.
- B. The proposed materials and textures will complement both the residential developments as well as the non-residential structures in the existing neighborhood. The use of multiple building materials and textures will provide some variety and differentiation while enhancing desirable neighborhood characteristics in the area.
- C. The architectural character, pedestrian connections, building massing, building articulation, parking garages, fenestration, exterior materials, colors, lighting, open spaces and landscaping are consistent with the Design Guidelines of the Wood Street Zoning District.

IV. Findings relating to Phasing of Development and processing of Final Maps

- A. The Planning Commission finds that it is in the best interest of the City to provide the flexibility for the developer to either commence construction in 2016 and complete the entire project in three phases.
- B. The Planning Commission must determine that the phasing plan is appropriate, in the best interest of the City, and will result in the optimal development scenario of the proposed 44 residential project for Development Area 6.
- C. The Planning Commission must determine that the phasing plan is consistent with VTPM8554.
- D. The Planning Commission must determine and direct that in the future processing relating to VTPM8554, City staff take all actions necessary to implement this phasing plan, including without limitation, an administrative amendment/adjustment to the phasing plan as needed to extend it as needed.

V. Conditions of Approval for the proposed project including the Preliminary Development Plan, Final Development Plan, and Vesting Tentative Parcel Map 8554 – Section 1.30 Wood Street Zoning District

FINDINGS

- A. The project shall be constructed and operated in accordance with the authorized use as described in the application materials, staff report, and the plans dated September 8, 2015.
- B. Each of the Conditions of Approval and Mitigation measures referenced in the Mitigation Monitoring Reporting Program relating to Vesting Tentative Parcel Map 8554 and incorporated herein by reference shall apply to the subject project site as specified in the conditions themselves.
- C. Public improvements shall be completed to the satisfaction of the City Engineer as set forth in the Subdivision Improvement Agreement.
- D. The project applicant has provided a letter to assure the city that all required public improvements including Wood Street, 18th Street, and Frontage Road adjacent to the subject site shall be completed as part of this development.
- E. Site improvement and building permit plans shall include information related to site design and security features that may include parking signs and lighting; retail business identification signs, lighting for project identification signs (and/or monument sign); functional security light; street lights and nighttime light.

Note:

The entire text of the Wood Street Zoning District regulations and associated documents including EIR, is available at:

<http://www2.oaklandnet.com/Government/o/PBN/OurOrganization/PlanningZoning/DOWD008410>

The entire text of the West Oakland Specific Plan and associated documents including EIR, is available at:

<http://www2.oaklandnet.com/oakeal/groups/ceda/documents/agenda/oak047695.pdf>

CONDITIONS OF APPROVAL

1. Approved Use

The project shall be constructed and operated in accordance with the authorized use as described in the approved application materials, Staff report and the approved plans **dated August 10, 2015 and submitted August 10, 2015**, as amended by the following conditions of approval and mitigation measures, if applicable (“Conditions of Approval” or “Conditions”).

2. Effective Date, Expiration, Extensions and Extinguishment

This Approval shall become effective immediately, unless the Approval is appealable, in which case the Approval shall become effective in ten calendar days unless an appeal is filed. Unless a different termination date is prescribed, this Approval shall expire **Two Calendar Years** from the Approval date, or from the date of the final decision in the event of an appeal, unless within such period all necessary permits for construction or alteration have been issued, or the authorized activities have commenced in the case of a permit not involving construction or alteration. Upon written request and payment of appropriate fees submitted no later than the expiration date of this Approval, the Director of City Planning or designee may grant a one-year extension of this date, with additional extensions subject to approval by the approving body. Expiration of any necessary building permit or other construction-related permit for this project may invalidate this Approval if said Approval has also expired. If litigation is filed challenging this Approval, or its implementation, then the time period stated above for obtaining necessary permits for construction or alteration and/or commencement of authorized activities is automatically extended for the duration of the litigation.

3. Compliance with Other Requirements

The project applicant shall comply with all other applicable federal, state, regional, and local laws/codes, requirements, regulations, and guidelines, including but not limited to those imposed by the City’s Bureau of Building, Fire Marshal, and Public Works Department. Compliance with other applicable requirements may require changes to the approved use and/or plans. These changes shall be processed in accordance with the procedures contained in Condition #4.

4. Minor and Major Changes

- a. Minor changes to the approved project, plans, Conditions, facilities, or use may be approved administratively by the Director of City Planning.
- b. Major changes to the approved project, plans, Conditions, facilities, or use shall be reviewed by the Director of City Planning to determine whether such changes require submittal and approval of a revision to the Approval by the original approving body or a new independent permit/approval. Major revisions shall be reviewed in accordance with

the procedures required for the original permit/approval. A new independent permit/approval shall be reviewed in accordance with the procedures required for the new permit/approval.

5. Compliance with Conditions of Approval

- a. The project applicant and property owner, including successors, (collectively referred to hereafter as the “project applicant” or “applicant”) shall be responsible for compliance with all the Conditions of Approval and any recommendations contained in any submitted and approved technical report at his/her sole cost and expense, subject to review and approval by the City of Oakland.
- b. The City of Oakland reserves the right at any time during construction to require certification by a licensed professional at the project applicant’s expense that the as-built project conforms to all applicable requirements, including but not limited to, approved maximum heights and minimum setbacks. Failure to construct the project in accordance with the Approval may result in remedial reconstruction, permit revocation, permit modification, stop work, permit suspension, or other corrective action.
- c. Violation of any term, Condition, or project description relating to the Approval is unlawful, prohibited, and a violation of the Oakland Municipal Code. The City of Oakland reserves the right to initiate civil and/or criminal enforcement and/or abatement proceedings, or after notice and public hearing, to revoke the Approval or alter these Conditions if it is found that there is violation of any of the Conditions or the provisions of the Planning Code or Municipal Code, or the project operates as or causes a public nuisance. This provision is not intended to, nor does it, limit in any manner whatsoever the ability of the City to take appropriate enforcement actions. The project applicant shall be responsible for paying fees in accordance with the City’s Master Fee Schedule for inspections conducted by the City or a City-designated third-party to investigate alleged violations of the Approval or Conditions.

6. Signed Copy of the Approval/Conditions

A copy of the Approval letter and Conditions shall be signed by the project applicant, attached to each set of permit plans submitted to the appropriate City agency for the project, and made available for review at the project job site at all times.

7. Blight/Nuisances

The project site shall be kept in a blight/nuisance-free condition. Any existing blight or nuisance shall be abated within 60 days of approval, unless an earlier date is specified elsewhere.

8. Indemnification

- a. To the maximum extent permitted by law, the project applicant shall defend (with counsel acceptable to the City), indemnify, and hold harmless the City of Oakland, the

Oakland City Council, the Oakland Redevelopment Successor Agency, the Oakland City Planning Commission, and their respective agents, officers, employees, and volunteers (hereafter collectively called "City") from any liability, damages, claim, judgment, loss (direct or indirect), action, causes of action, or proceeding (including legal costs, attorneys' fees, expert witness or consultant fees, City Attorney or staff time, expenses or costs) (collectively called "Action") against the City to attack, set aside, void or annul this Approval or implementation of this Approval. The City may elect, in its sole discretion, to participate in the defense of said Action and the project applicant shall reimburse the City for its reasonable legal costs and attorneys' fees.

- b. Within ten (10) calendar days of the filing of any Action as specified in subsection (a) above, the project applicant shall execute a Joint Defense Letter of Agreement with the City, acceptable to the Office of the City Attorney, which memorializes the above obligations. These obligations and the Joint Defense Letter of Agreement shall survive termination, extinguishment, or invalidation of the Approval. Failure to timely execute the Letter of Agreement does not relieve the project applicant of any of the obligations contained in this Condition or other requirements or Conditions of Approval that may be imposed by the City.

9. Severability

The Approval would not have been granted but for the applicability and validity of each and every one of the specified Conditions, and if one or more of such Conditions is found to be invalid by a court of competent jurisdiction this Approval would not have been granted without requiring other valid Conditions consistent with achieving the same purpose and intent of such Approval.

10. Special Inspector/Inspections, Independent Technical Review, Project Coordination and Monitoring

The project applicant may be required to cover the full costs of independent third-party technical review and City monitoring and inspection, including without limitation, special inspector(s)/inspection(s) during times of extensive or specialized plan-check review or construction, and inspections of potential violations of the Conditions of Approval. The project applicant shall establish a deposit with the Bureau of Building, if directed by the Building Official, Director of City Planning, or designee, prior to the issuance of a construction-related permit and on an ongoing as-needed basis.

11. Public Improvements

The project applicant shall obtain all necessary permits/approvals, such as encroachment permits, obstruction permits, curb/gutter/sidewalk permits, and public improvement ("p-job") permits from the City for work in the public right-of-way, including but not limited to, streets, curbs, gutters, sidewalks, utilities, and fire hydrants. Prior to any work in the public right-of-way, the applicant shall submit plans for review and approval by the Bureau of

Planning, the Bureau of Building, and other City departments as required. Public improvements shall be designed and installed to the satisfaction of the City.

12. Compliance Matrix

The project applicant shall submit a Compliance Matrix, in both written and electronic form, for review and approval by the Bureau of Planning and the Bureau of Building that lists each Condition of Approval (including each mitigation measure if applicable) in a sortable spreadsheet. The Compliance Matrix shall contain, at a minimum, each required Condition of Approval, when compliance with the Condition is required, and the status of compliance with each Condition. For multi-phased projects, the Compliance Matrix shall indicate which Condition applies to each phase. The project applicant shall submit the initial Compliance Matrix prior to the issuance of the first construction-related permit and shall submit an updated matrix upon request by the City.

13. Construction Management Plan

Prior to the issuance of the first construction-related permit, the project applicant and his/her general contractor shall submit a Construction Management Plan (CMP) for review and approval by the Bureau of Planning, Bureau of Building, and other relevant City departments such as the Fire Department and the Public Works Department as directed. The CMP shall contain measures to minimize potential construction impacts including measures to comply with all construction-related Conditions of Approval (and mitigation measures if applicable) such as dust control, construction emissions, hazardous materials, construction days/hours, construction traffic control, waste reduction and recycling, stormwater pollution prevention, noise control, complaint management, and cultural resource management (see applicable Conditions below). The CMP shall provide project-specific information including descriptive procedures, approval documentation, and drawings (such as a site logistics plan, fire safety plan, construction phasing plan, proposed truck routes, traffic control plan, complaint management plan, construction worker parking plan, and litter/debris clean-up plan) that specify how potential construction impacts will be minimized and how each construction-related requirement will be satisfied throughout construction of the project.

14. Regulatory Permits and Authorizations from Other Agencies

Requirement: The project applicant shall obtain all necessary regulatory permits and authorizations from applicable resource/regulatory agencies including, but not limited to, the Regional Water Quality Control Board, Bay Area Air Quality Management District, Bay Conservation and Development Commission, California Department of Fish and Wildlife, U. S. Fish and Wildlife Service, and Army Corps of Engineers and shall comply with all requirements and conditions of the permits/authorizations. The project applicant shall submit evidence of the approved permits/authorizations to the City, along with evidence demonstrating compliance with any regulatory permit/authorization conditions of approval.

When Required: Prior to activity requiring permit/authorization from regulatory agency

Initial Approval: Approval by applicable regulatory agency with jurisdiction; evidence of approval submitted to Bureau of Planning

Monitoring/Inspection: Applicable regulatory agency with jurisdiction

15. **Graffiti Control**

Requirement:

- a. During construction and operation of the project, the project applicant shall incorporate best management practices reasonably related to the control of graffiti and/or the mitigation of the impacts of graffiti. Such best management practices may include, without limitation:
 - i. Installation and maintenance of landscaping to discourage defacement of and/or protect likely graffiti-attracting surfaces.
 - ii. Installation and maintenance of lighting to protect likely graffiti-attracting surfaces.
 - iii. Use of paint with anti-graffiti coating.
 - iv. Incorporation of architectural or design elements or features to discourage graffiti defacement in accordance with the principles of Crime Prevention Through Environmental Design (CPTED).
 - v. Other practices approved by the City to deter, protect, or reduce the potential for graffiti defacement.
- b. The project applicant shall remove graffiti by appropriate means within seventy-two (72) hours. Appropriate means include the following:
 - i. Removal through scrubbing, washing, sanding, and/or scraping (or similar method) without damaging the surface and without discharging wash water or cleaning detergents into the City storm drain system.
 - ii. Covering with new paint to match the color of the surrounding surface.
 - iii. Replacing with new surfacing (with City permits if required).

When Required: Ongoing

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

16. **Landscape Plan**

a. Landscape Plan Required

Requirement: The project applicant shall submit a final Landscape Plan for City review and approval that is consistent with the approved Landscape Plan. The Landscape Plan shall be included with the set of drawings submitted for the construction-related permit and shall comply with the landscape requirements of chapter 17.124 of the Planning Code.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Planning

Monitoring/Inspection: N/A

b. *Landscape Installation*

Requirement: The project applicant shall implement the approved Landscape Plan unless a bond, cash deposit, letter of credit, or other equivalent instrument acceptable to the Director of City Planning, is provided. The financial instrument shall equal the greater of \$2,500 or the estimated cost of implementing the Landscape Plan based on a licensed contractor's bid.

When Required: Prior to building permit final

Initial Approval: Bureau of Planning

Monitoring/Inspection: Bureau of Building

c. *Landscape Maintenance*

Requirement: All required planting shall be permanently maintained in good growing condition and, whenever necessary, replaced with new plant materials to ensure continued compliance with applicable landscaping requirements. The property owner shall be responsible for maintaining planting in adjacent public rights-of-way. All required fences, walls, and irrigation systems shall be permanently maintained in good condition and, whenever necessary, repaired or replaced.

When Required: Ongoing

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

17. Lighting

Requirement: Proposed new exterior lighting fixtures shall be adequately shielded to a point below the light bulb and reflector to prevent unnecessary glare onto adjacent properties.

When Required: Prior to building permit final

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

18. Construction-Related Air Pollution Controls (Dust and Equipment Emissions)

Requirement: The project applicant shall implement all of the following applicable air pollution control measures during construction of the project:

- a. Water all exposed surfaces of active construction areas at least twice daily. Watering should be sufficient to prevent airborne dust from leaving the site. Increased watering frequency may be necessary whenever wind speeds exceed 15 miles per hour. Reclaimed water should be used whenever feasible.

- b. Cover all trucks hauling soil, sand, and other loose materials or require all trucks to maintain at least two feet of freeboard (i.e., the minimum required space between the top of the load and the top of the trailer).
- c. All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited.
- d. Pave all roadways, driveways, sidewalks, etc. within one month of site grading or as soon as feasible. In addition, building pads should be laid within one month of grading or as soon as feasible unless seeding or soil binders are used.
- e. Enclose, cover, water twice daily, or apply (non-toxic) soil stabilizers to exposed stockpiles (dirt, sand, etc.).
- f. Limit vehicle speeds on unpaved roads to 15 miles per hour.
- g. Idling times on all diesel-fueled commercial vehicles over 10,000 lbs. shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to five minutes (as required by the California airborne toxics control measure Title 13, Section 2485, of the California Code of Regulations). Clear signage to this effect shall be provided for construction workers at all access points.
- h. Idling times on all diesel-fueled off-road vehicles over 25 horsepower shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to five minutes and fleet operators must develop a written policy as required by Title 23, Section 2449, of the California Code of Regulations ("California Air Resources Board Off-Road Diesel Regulations").
- i. All construction equipment shall be maintained and properly tuned in accordance with the manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation.
- j. Portable equipment shall be powered by electricity if available. If electricity is not available, propane or natural gas shall be used if feasible. Diesel engines shall only be used if electricity is not available and it is not feasible to use propane or natural gas.
- k. All exposed surfaces shall be watered at a frequency adequate to maintain minimum soil moisture of 12 percent. Moisture content can be verified by lab samples or moisture probe.
- l. All excavation, grading, and demolition activities shall be suspended when average wind speeds exceed 20 mph.
- m. Install sandbags or other erosion control measures to prevent silt runoff to public roadways.
- n. Hydroseed or apply (non-toxic) soil stabilizers to inactive construction areas (previously graded areas inactive for one month or more).
- o. Designate a person or persons to monitor the dust control program and to order increased watering, as necessary, to prevent transport of dust offsite. Their duties shall include holidays and weekend periods when work may not be in progress.

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- p. Install appropriate wind breaks (e.g., trees, fences) on the windward side(s) of actively disturbed areas of the construction site to minimize wind blown dust. Wind breaks must have a maximum 50 percent air porosity.
- q. Vegetative ground cover (e.g., fast-germinating native grass seed) shall be planted in disturbed areas as soon as possible and watered appropriately until vegetation is established.
- r. Activities such as excavation, grading, and other ground-disturbing construction activities shall be phased to minimize the amount of disturbed surface area at any one time.
- s. All trucks and equipment, including tires, shall be washed off prior to leaving the site.
- t. Site accesses to a distance of 100 feet from the paved road shall be treated with a 6 to 12 inch compacted layer of wood chips, mulch, or gravel.
- u. All equipment to be used on the construction site and subject to the requirements of Title 13, Section 2449, of the California Code of Regulations ("California Air Resources Board Off-Road Diesel Regulations") must meet emissions and performance requirements one year in advance of any fleet deadlines. Upon request by the City, the project applicant shall provide written documentation that fleet requirements have been met.
- v. Use low VOC (i.e., ROG) coatings beyond the local requirements (i.e., BAAQMD Regulation 8, Rule 3: Architectural Coatings).
- w. All construction equipment, diesel trucks, and generators shall be equipped with Best Available Control Technology for emission reductions of NOx and PM.
- x. Off-road heavy diesel engines shall meet the California Air Resources Board's most recent certification standard.
- y. Post a publicly-visible large on-site sign that includes the contact name and phone number for the project complaint manager responsible for responding to dust complaints and the telephone numbers of the City's Code Enforcement unit and the Bay Area Air Quality Management District. When contacted, the project complaint manager shall respond and take corrective action within 48 hours.

When Required: During construction

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

19. Exposure to Air Pollution (Toxic Air Contaminants)

a. *Health Risk Reduction Measures*

Requirement: The project applicant shall incorporate appropriate measures into the project design in order to reduce the potential health risk due to exposure to toxic air contaminants. The project applicant shall choose one of the following methods:

- i. The project applicant shall retain a qualified air quality consultant to prepare a Health Risk Assessment (HRA) in accordance with California Air Resources

Board (CARB) and Office of Environmental Health and Hazard Assessment requirements to determine the health risk of exposure of project residents/occupants/users to air pollutants. The HRA shall be submitted to the City for review and approval. If the HRA concludes that the health risk is at or below acceptable levels, then health risk reduction measures are not required. If the HRA concludes that the health risk exceeds acceptable levels, health risk reduction measures shall be identified to reduce the health risk to acceptable levels. Identified risk reduction measures shall be submitted to the City for review and approval and be included on the project drawings submitted for the construction-related permit or on other documentation submitted to the City.

- or -

- ii. The project applicant shall incorporate the following health risk reduction measures into the project. These features shall be submitted to the City for review and approval and be included on the project drawings submitted for the construction-related permit or on other documentation submitted to the City:
 - Installation of air filtration to reduce cancer risks and Particulate Matter (PM) exposure for residents and other sensitive populations in the project that are in close proximity to sources of air pollution. Air filter devices shall be rated MERV-16 or higher. As part of implementing this measure, an ongoing maintenance plan for the building's HVAC air filtration system shall be required.
 - Where appropriate, install passive electrostatic filtering systems, especially those with low air velocities (i.e., 1 mph).
 - Phasing of residential developments when proposed within 500 feet of freeways such that homes nearest the freeway are built last, if feasible.
 - The project shall be designed to locate sensitive receptors as far away as feasible from the source(s) of air pollution. Operable windows, balconies, and building air intakes shall be located as far away from these sources as feasible. If near a distribution center, residents shall be located as far away as feasible from a loading dock or where trucks concentrate to deliver goods.
 - Sensitive receptors shall be located on the upper floors of buildings, if feasible.
 - Planting trees and/or vegetation between sensitive receptors and pollution source, if feasible. Trees that are best suited to trapping PM shall be planted, including one or more of the following: Pine (*Pinus nigra* var. *maritima*), Cypress (*X Cupressocyparis leylandii*), Hybrid poplar (*Populus deltoids X trichocarpa*), and Redwood (*Sequoia sempervirens*).
 - Sensitive receptors shall be located as far away from truck activity areas, such as loading docks and delivery areas, as feasible.
 - Existing and new diesel generators shall meet CARB's Tier 4 emission standards, if feasible.

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- Emissions from diesel trucks shall be reduced through implementing the following measures, if feasible:
 - Installing electrical hook-ups for diesel trucks at loading docks.
 - Requiring trucks to use Transportation Refrigeration Units (TRU) that meet Tier 4 emission standards.
 - Requiring truck-intensive projects to use advanced exhaust technology (e.g., hybrid) or alternative fuels.
 - Prohibiting trucks from idling for more than two minutes.
 - Establishing truck routes to avoid sensitive receptors in the project. A truck route program, along with truck calming, parking, and delivery restrictions, shall be implemented.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Planning

Monitoring/Inspection: Bureau of Building

b. Maintenance of Health Risk Reduction Measures

Requirement: The project applicant shall maintain, repair, and/or replace installed health risk reduction measures, including but not limited to the HVAC system (if applicable), on an ongoing and as-needed basis. Prior to occupancy, the project applicant shall prepare and then distribute to the building manager/operator an operation and maintenance manual for the HVAC system and filter including the maintenance and replacement schedule for the filter.

When Required: Ongoing

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

20. Tree Permit

a. Tree Permit Required

Requirement: Pursuant to the City's Tree Protection Ordinance (OMC chapter 12.36), the project applicant shall obtain a tree permit and abide by the conditions of that permit.

When Required: Prior to approval of construction-related permit

Initial Approval: Permit approval by Public Works Department, Tree Division; evidence of approval submitted to Bureau of Building

Monitoring/Inspection: Bureau of Building

b. Tree Protection During Construction

Requirement: Adequate protection shall be provided during the construction period for any trees which are to remain standing, including the following, plus any recommendations of an arborist:

- i. Before the start of any clearing, excavation, construction, or other work on the site, every protected tree deemed to be potentially endangered by said site work shall be securely fenced off at a distance from the base of the tree to be determined by the project's consulting arborist. Such fences shall remain in place for duration of all such work. All trees to be removed shall be clearly marked. A scheme shall be established for the removal and disposal of logs, brush, earth and other debris which will avoid injury to any protected tree.
- ii. Where proposed development or other site work is to encroach upon the protected perimeter of any protected tree, special measures shall be incorporated to allow the roots to breathe and obtain water and nutrients. Any excavation, cutting, filing, or compaction of the existing ground surface within the protected perimeter shall be minimized. No change in existing ground level shall occur within a distance to be determined by the project's consulting arborist from the base of any protected tree at any time. No burning or use of equipment with an open flame shall occur near or within the protected perimeter of any protected tree.
- iii. No storage or dumping of oil, gas, chemicals, or other substances that may be harmful to trees shall occur within the distance to be determined by the project's consulting arborist from the base of any protected trees, or any other location on the site from which such substances might enter the protected perimeter. No heavy construction equipment or construction materials shall be operated or stored within a distance from the base of any protected trees to be determined by the project's consulting arborist. Wires, ropes, or other devices shall not be attached to any protected tree, except as needed for support of the tree. No sign, other than a tag showing the botanical classification, shall be attached to any protected tree.
- iv. Periodically during construction, the leaves of protected trees shall be thoroughly sprayed with water to prevent buildup of dust and other pollution that would inhibit leaf transpiration.
- v. If any damage to a protected tree should occur during or as a result of work on the site, the project applicant shall immediately notify the Public Works Department and the project's consulting arborist shall make a recommendation to the City Tree Reviewer as to whether the damaged tree can be preserved. If, in the professional opinion of the Tree Reviewer, such tree cannot be preserved in a healthy state, the Tree Reviewer shall require replacement of any tree removed with another tree or trees on the same site deemed adequate by the Tree Reviewer to compensate for the loss of the tree that is removed.
- vi. All debris created as a result of any tree removal work shall be removed by the project applicant from the property within two weeks of debris creation, and such debris shall be properly disposed of by the project applicant in accordance with all applicable laws, ordinances, and regulations.

When Required: During construction

Initial Approval: Public Works Department, Tree Division

Monitoring/Inspection: Bureau of Building

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c. *Tree Replacement Plantings*

Requirement: Replacement plantings shall be required for tree removals for the purposes of erosion control, groundwater replenishment, visual screening, wildlife habitat, and preventing excessive loss of shade, in accordance with the following criteria:

- i. No tree replacement shall be required for the removal of nonnative species, for the removal of trees which is required for the benefit of remaining trees, or where insufficient planting area exists for a mature tree of the species being considered.
- ii. Replacement tree species shall consist of *Sequoia sempervirens* (Coast Redwood), *Quercus agrifolia* (Coast Live Oak), *Arbutus menziesii* (Madrone), *Aesculus californica* (California Buckeye), *Umbellularia californica* (California Bay Laurel), or other tree species acceptable to the Tree Division.
- iii. Replacement trees shall be at least twenty-four (24) inch box size, unless a smaller size is recommended by the arborist, except that three fifteen (15) gallon size trees may be substituted for each twenty-four (24) inch box size tree where appropriate.
- iv. Minimum planting areas must be available on site as follows:
 - For *Sequoia sempervirens*, three hundred fifteen (315) square feet per tree;
 - For other species listed, seven hundred (700) square feet per tree.
- v. In the event that replacement trees are required but cannot be planted due to site constraints, an in lieu fee in accordance with the City's Master Fee Schedule may be substituted for required replacement plantings, with all such revenues applied toward tree planting in city parks, streets and medians.
- vi. The project applicant shall install the plantings and maintain the plantings until established. The Tree Reviewer of the Tree Division of the Public Works Department may require a landscape plan showing the replacement plantings and the method of irrigation. Any replacement plantings which fail to become established within one year of planting shall be replanted at the project applicant's expense.

When Required: Prior to building permit final

Initial Approval: Public Works Department, Tree Division

Monitoring/Inspection: Bureau of Building

21. Archaeological and Paleontological Resources – Discovery During Construction

Requirement: Pursuant to CEQA Guidelines section 15064.5(f), in the event that any historic or prehistoric subsurface cultural resources are discovered during ground disturbing activities, all work within 50 feet of the resources shall be halted and the project applicant shall notify the City and consult with a qualified archaeologist or paleontologist, as applicable, to assess the significance of the find. In the case of discovery of paleontological resources, the assessment shall be done in accordance with the Society of Vertebrate Paleontology standards. If any find is determined to be significant, appropriate avoidance

measures recommended by the consultant and approved by the City must be followed unless avoidance is determined unnecessary or infeasible by the City. Feasibility of avoidance shall be determined with consideration of factors such as the nature of the find, project design, costs, and other considerations. If avoidance is unnecessary or infeasible, other appropriate measures (e.g., data recovery, excavation) shall be instituted. Work may proceed on other parts of the project site while measures for the cultural resources are implemented.

In the event of data recovery of archaeological resources, the project applicant shall submit an Archaeological Research Design and Treatment Plan (ARDTP) prepared by a qualified archaeologist for review and approval by the City. The ARDTP is required to identify how the proposed data recovery program would preserve the significant information the archaeological resource is expected to contain. The ARDTP shall identify the scientific/historic research questions applicable to the expected resource, the data classes the resource is expected to possess, and how the expected data classes would address the applicable research questions. The ARDTP shall include the analysis and specify the curation and storage methods. Data recovery, in general, shall be limited to the portions of the archaeological resource that could be impacted by the proposed project. Destructive data recovery methods shall not be applied to portions of the archaeological resources if nondestructive methods are practicable. Because the intent of the ARDTP is to save as much of the archaeological resource as possible, including moving the resource, if feasible, preparation and implementation of the ARDTP would reduce the potential adverse impact to less than significant. The project applicant shall implement the ARDTP at his/her expense.

In the event of excavation of paleontological resources, the project applicant shall submit an excavation plan prepared by a qualified paleontologist to the City for review and approval. All significant cultural materials recovered shall be subject to scientific analysis, professional museum curation, and/or a report prepared by a qualified paleontologist, as appropriate, according to current professional standards and at the expense of the project applicant.

When Required: During construction

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

22. Archaeologically Sensitive Areas – Pre-Construction Measures

Requirement: The project applicant shall implement either Provision A (Intensive Pre-Construction Study) or Provision B (Construction ALERT Sheet) concerning archaeological resources.

Provision A: Intensive Pre-Construction Study.

The project applicant shall retain a qualified archaeologist to conduct a site-specific, intensive archaeological resources study for review and approval by the City prior to soil-disturbing activities occurring on the project site. The purpose of the site-specific, intensive archaeological resources study is to identify early the potential presence of history-period archaeological resources on the project site. At a minimum, the study shall include:

- a. Subsurface presence/absence studies of the project site. Field studies may include, but are not limited to, auguring and other common methods used to identify the presence of archaeological resources.
- b. A report disseminating the results of this research.
- c. Recommendations for any additional measures that could be necessary to mitigate any adverse impacts to recorded and/or inadvertently discovered cultural resources.

If the results of the study indicate a high potential presence of historic-period archaeological resources on the project site, or a potential resource is discovered, the project applicant shall hire a qualified archaeologist to monitor any ground disturbing activities on the project site during construction and prepare an ALERT sheet pursuant to Provision B below that details what could potentially be found at the project site. Archaeological monitoring would include briefing construction personnel about the type of artifacts that may be present (as referenced in the ALERT sheet, required per Provision B below) and the procedures to follow if any artifacts are encountered, field recording and sampling in accordance with the Secretary of Interior's Standards and Guidelines for Archaeological Documentation, notifying the appropriate officials if human remains or cultural resources are discovered, and preparing a report to document negative findings after construction is completed if no archaeological resources are discovered during construction.

Provision B: Construction ALERT Sheet.

The project applicant shall prepare a construction "ALERT" sheet developed by a qualified archaeologist for review and approval by the City prior to soil-disturbing activities occurring on the project site. The ALERT sheet shall contain, at a minimum, visuals that depict each type of artifact that could be encountered on the project site. Training by the qualified archaeologist shall be provided to the project's prime contractor, any project subcontractor firms (including demolition, excavation, grading, foundation, and pile driving), and utility firms involved in soil-disturbing activities within the project site.

The ALERT sheet shall state, in addition to the basic archaeological resource protection measures contained in other standard conditions of approval, all work must stop and the City's Environmental Review Officer contacted in the event of discovery of the following cultural materials: concentrations of shellfish remains; evidence of fire (ashes, charcoal, burnt earth, fire-cracked rocks); concentrations of bones; recognizable Native American artifacts (arrowheads, shell beads, stone mortars [bowls], humanly shaped rock); building foundation remains; trash pits, privies (outhouse holes); floor remains; wells; concentrations of bottles, broken dishes, shoes, buttons, cut animal bones, hardware, household items, barrels, etc.; thick layers of burned building debris (charcoal, nails, fused glass, burned plaster, burned dishes); wood structural remains (building, ship, wharf); clay roof/floor tiles; stone walls or footings; or gravestones. Prior to any soil-disturbing activities, each contractor shall be responsible for ensuring that the ALERT sheet is circulated to all field personnel, including machine operators, field crew, pile drivers, and supervisory personnel. The ALERT sheet shall also be posted in a visible location at the project site.

When Required: Prior to approval of construction-related permit; during construction

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

23. Human Remains – Discovery During Construction

Requirement: Pursuant to CEQA Guidelines section 15064.5(e)(1), in the event that human skeletal remains are uncovered at the project site during construction activities, all work shall immediately halt and the project applicant shall notify the City and the Alameda County Coroner. If the County Coroner determines that an investigation of the cause of death is required or that the remains are Native American, all work shall cease within 50 feet of the remains until appropriate arrangements are made. In the event that the remains are Native American, the City shall contact the California Native American Heritage Commission (NAHC), pursuant to subdivision (c) of section 7050.5 of the California Health and Safety Code. If the agencies determine that avoidance is not feasible, then an alternative plan shall be prepared with specific steps and timeframe required to resume construction activities. Monitoring, data recovery, determination of significance, and avoidance measures (if applicable) shall be completed expeditiously and at the expense of the project applicant.

When Required: During construction

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

24. Construction-Related Permit(s)

Requirement: The project applicant shall obtain all required construction-related permits/approvals from the City. The project shall comply with all standards, requirements and conditions contained in construction-related codes, including but not limited to the Oakland Building Code and the Oakland Grading Regulations, to ensure structural integrity and safe construction.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

25. Soils Report

Requirement: The project applicant shall submit a soils report prepared by a registered geotechnical engineer for City review and approval. The soils report shall contain, at a minimum, field test results and observations regarding the nature, distribution and strength of existing soils, and recommendations for appropriate grading practices and project design. The project applicant shall implement the recommendations contained in the approved report during project design and construction.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

26. Seismic Hazards Zone (Landslide/Liquefaction)

Requirement: The project applicant shall submit a site-specific geotechnical report, consistent with California Geological Survey Special Publication 117 (as amended), prepared by a registered geotechnical engineer for City review and approval containing at a minimum a description of the geological and geotechnical conditions at the site, an evaluation of site-specific seismic hazards based on geological and geotechnical conditions, and recommended measures to reduce potential impacts related to liquefaction and/or slope stability hazards. The project applicant shall implement the recommendations contained in the approved report during project design and construction.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

27. Hazardous Materials Related to Construction

Requirement: The project applicant shall ensure that Best Management Practices (BMPs) are implemented by the contractor during construction to minimize potential negative effects on groundwater, soils, and human health. These shall include, at a minimum, the following:

- a. Follow manufacture's recommendations for use, storage, and disposal of chemical products used in construction;
- b. Avoid overtopping construction equipment fuel gas tanks;
- c. During routine maintenance of construction equipment, properly contain and remove grease and oils;
- d. Properly dispose of discarded containers of fuels and other chemicals;
- e. Implement lead-safe work practices and comply with all local, regional, state, and federal requirements concerning lead (for more information refer to the Alameda County Lead Poisoning Prevention Program); and
- f. If soil, groundwater, or other environmental medium with suspected contamination is encountered unexpectedly during construction activities (e.g., identified by odor or visual staining, or if any underground storage tanks, abandoned drums or other hazardous materials or wastes are encountered), the project applicant shall cease work in the vicinity of the suspect material, the area shall be secured as necessary, and the applicant shall take all appropriate measures to protect human health and the environment. Appropriate measures shall include notifying the City and applicable regulatory agency(ies) and implementation of the actions described in the City's Standard Conditions of Approval, as necessary, to identify the nature and extent of contamination. Work shall not resume in the area(s) affected until the measures have been implemented under the oversight of the City or regulatory agency, as appropriate.

When Required: During construction

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

28. Hazardous Materials Business Plan

Requirement: The project applicant shall submit a Hazardous Materials Business Plan for review and approval by the City, and shall implement the approved Plan. The approved Plan shall be kept on file with the City and the project applicant shall update the Plan as applicable. The purpose of the Hazardous Materials Business Plan is to ensure that employees are adequately trained to handle hazardous materials and provides information to the Fire Department should emergency response be required. Hazardous materials shall be handled in accordance with all applicable local, state, and federal requirements. The Hazardous Materials Business Plan shall include the following:

- a. The types of hazardous materials or chemicals stored and/or used on-site, such as petroleum fuel products, lubricants, solvents, and cleaning fluids.
- b. The location of such hazardous materials.
- c. An emergency response plan including employee training information.
- d. A plan that describes the manner in which these materials are handled, transported, and disposed.

When Required: Prior to building permit final

Initial Approval: Oakland Fire Department

Monitoring/Inspection: Oakland Fire Department

29. Fire Safety Phasing Plan

Requirement: The project applicant shall submit a Fire Safety Phasing Plan for City review and approval, and shall implement the approved Plan. The Fire Safety Phasing Plan shall include all of the fire safety features incorporated into each phase of the project and the schedule for implementation of the features.

When Required: Prior to approval of construction-related permit

Initial Approval: Oakland Fire Department

Monitoring/Inspection: Bureau of Building

30. State Construction General Permit

Requirement: The project applicant shall comply with the requirements of the Construction General Permit issued by the State Water Resources Control Board (SWRCB). The project applicant shall submit a Notice of Intent (NOI), Stormwater Pollution Prevention Plan (SWPPP), and other required Permit Registration Documents to SWRCB. The project applicant shall submit evidence of compliance with Permit requirements to the City.

When Required: Prior to approval of construction-related permit

Initial Approval: State Water Resources Control Board; evidence of compliance submitted to Bureau of Building

Monitoring/Inspection: State Water Resources Control Board

31. Drainage Plan for Post-Construction Stormwater Runoff on Hillside Properties

Requirement: The project applicant shall submit and implement a Drainage Plan to be reviewed and approved by the City. The Drainage Plan shall include measures to reduce the volume and velocity of post-construction stormwater runoff to the maximum extent practicable. Stormwater runoff shall not be augmented to adjacent properties, creeks, or storm drains. The Drainage Plan shall be included with the project drawings submitted to the City for site improvements.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

32. NPDES C.3 Stormwater Requirements for Regulated Projects

a. Post-Construction Stormwater Management Plan Required

Requirement: The project applicant shall comply with the requirements of Provision C.3 of the Municipal Regional Stormwater Permit issued under the National Pollutant Discharge Elimination System (NPDES). The project applicant shall submit a Post-Construction Stormwater Management Plan to the City for review and approval with the project drawings submitted for site improvements, and shall implement the approved Plan during construction. The Post-Construction Stormwater Management Plan shall include and identify the following:

- i. Location and size of new and replaced impervious surface;
- ii. Directional surface flow of stormwater runoff;
- iii. Location of proposed on-site storm drain lines;
- iv. Site design measures to reduce the amount of impervious surface area;
- v. Source control measures to limit stormwater pollution;
- vi. Stormwater treatment measures to remove pollutants from stormwater runoff, including the method used to hydraulically size the treatment measures; and
- vii. Hydromodification management measures, if required by Provision C.3, so that post-project stormwater runoff flow and duration match pre-project runoff.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Planning; Bureau of Building

Monitoring/Inspection: Bureau of Building

b. Maintenance Agreement Required

Requirement: The project applicant shall enter into a maintenance agreement with the City, based on the Standard City of Oakland Stormwater Treatment Measures Maintenance Agreement, in accordance with Provision C.3, which provides, in part, for the following:

- i. The project applicant accepting responsibility for the adequate installation/construction, operation, maintenance, inspection, and reporting of any on-site stormwater treatment measures being incorporated into the project until the responsibility is legally transferred to another entity; and
- ii. Legal access to the on-site stormwater treatment measures for representatives of the City, the local vector control district, and staff of the Regional Water Quality Control Board, San Francisco Region, for the purpose of verifying the implementation, operation, and maintenance of the on-site stormwater treatment measures and to take corrective action if necessary.

The maintenance agreement shall be recorded at the County Recorder's Office at the applicant's expense.

When Required: Prior to building permit final

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

33. NPDES C.3 Stormwater Requirements for Small Projects

Requirement: Pursuant to Provision C.3 of the Municipal Regional Stormwater Permit issued under the National Pollutant Discharge Elimination System (NPDES), the project applicant shall incorporate one or more of the following site design measures into the project:

- a. Direct roof runoff into cisterns or rain barrels for reuse;
- b. Direct roof runoff onto vegetated areas;
- c. Direct runoff from sidewalks, walkways, and/or patios onto vegetated areas;
- d. Direct runoff from driveways and/or uncovered parking lots onto vegetated areas;
- e. Construct sidewalks, walkways, and/or patios with permeable surfaces; or
- f. Construct bike lanes, driveways, and/or uncovered parking lots with permeable surfaces.

The project drawings submitted for construction-related permits shall include the proposed site design measure(s) and the approved measure(s) shall be installed during construction. The design and installation of the measure(s) shall comply with all applicable City requirements.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Planning; Bureau of Building

Monitoring/Inspection: Bureau of Building

34. Structures in a Flood Zone

Requirement: The project shall be designed to ensure that new structures within a 100-year flood zone do not interfere with the flow of water or increase flooding. The project applicant shall submit plans and hydrological calculations for City review and approval with

the construction-related drawings that show finished site grades and floor elevations elevated above the Base Flood Elevation (BFE).

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

35. Construction Days/Hours

Requirement: The project applicant shall comply with the following restrictions concerning construction days and hours:

- a. Construction activities are limited to between 7:00 a.m. and 7:00 p.m. Monday through Friday, except that pier drilling and/or other extreme noise generating activities greater than 90 dBA shall be limited to between 8:00 a.m. and 4:00 p.m.
- b. Construction activities are limited to between 9:00 a.m. and 5:00 p.m. on Saturday. In residential zones and within 300 feet of a residential zone, construction activities are allowed from 9:00 a.m. to 5:00 p.m. only within the interior of the building with the doors and windows closed. No pier drilling or other extreme noise generating activities greater than 90 dBA are allowed on Saturday.
- c. No construction is allowed on Sunday or federal holidays.

Construction activities include, but are not limited to, truck idling, moving equipment (including trucks, elevators, etc.) or materials, deliveries, and construction meetings held on-site in a non-enclosed area.

Any construction activity proposed outside of the above days and hours for special activities (such as concrete pouring which may require more continuous amounts of time) shall be evaluated on a case-by-case basis by the City, with criteria including the urgency/emergency nature of the work, the proximity of residential or other sensitive uses, and a consideration of nearby residents'/occupants' preferences. The project applicant shall notify property owners and occupants located within 300 feet at least 14 calendar days prior to construction activity proposed outside of the above days/hours. When submitting a request to the City to allow construction activity outside of the above days/hours, the project applicant shall submit information concerning the type and duration of proposed construction activity and the draft public notice for City review and approval prior to distribution of the public notice.

When Required: During construction

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

36. Construction Noise

Requirement: The project applicant shall implement noise reduction measures to reduce noise impacts due to construction. Noise reduction measures include, but are not limited to, the following:

- a. Equipment and trucks used for project construction shall utilize the best available noise control techniques (e.g., improved mufflers, equipment redesign, use of intake silencers, ducts, engine enclosures and acoustically-attenuating shields or shrouds) wherever feasible.
- b. Except as provided herein, impact tools (e.g., jack hammers, pavement breakers, and rock drills) used for project construction shall be hydraulically or electrically powered to avoid noise associated with compressed air exhaust from pneumatically powered tools. However, where use of pneumatic tools is unavoidable, an exhaust muffler on the compressed air exhaust shall be used; this muffler can lower noise levels from the exhaust by up to about 10 dBA. External jackets on the tools themselves shall be used, if such jackets are commercially available, and this could achieve a reduction of 5 dBA. Quieter procedures shall be used, such as drills rather than impact equipment, whenever such procedures are available and consistent with construction procedures.
- c. Applicant shall use temporary power poles instead of generators where feasible.
- d. Stationary noise sources shall be located as far from adjacent properties as possible, and they shall be muffled and enclosed within temporary sheds, incorporate insulation barriers, or use other measures as determined by the City to provide equivalent noise reduction.
- e. The noisiest phases of construction shall be limited to less than 10 days at a time. Exceptions may be allowed if the City determines an extension is necessary and all available noise reduction controls are implemented.

When Required: During construction

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

37. Extreme Construction Noise

a. Construction Noise Management Plan Required

Requirement: Prior to any extreme noise generating construction activities (e.g., pier drilling, pile driving and other activities generating greater than 90dBA), the project applicant shall submit a Construction Noise Management Plan prepared by a qualified acoustical consultant for City review and approval that contains a set of site-specific noise attenuation measures to further reduce construction impacts associated with extreme noise generating activities. The project applicant shall implement the approved Plan during construction. Potential attenuation measures include, but are not limited to, the following:

- i. Erect temporary plywood noise barriers around the construction site, particularly along on sites adjacent to residential buildings;
- ii. Implement “quiet” pile driving technology (such as pre-drilling of piles, the use of more than one pile driver to shorten the total pile driving duration), where feasible, in consideration of geotechnical and structural requirements and conditions;

- iii. Utilize noise control blankets on the building structure as the building is erected to reduce noise emission from the site;
- iv. Evaluate the feasibility of noise control at the receivers by temporarily improving the noise reduction capability of adjacent buildings by the use of sound blankets for example and implement such measure if such measures are feasible and would noticeably reduce noise impacts; and
- v. Monitor the effectiveness of noise attenuation measures by taking noise measurements.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

b. Public Notification Required

Requirement: The project applicant shall notify property owners and occupants located within 300 feet of the construction activities at least 14 calendar days prior to commencing extreme noise generating activities. Prior to providing the notice, the project applicant shall submit to the City for review and approval the proposed type and duration of extreme noise generating activities and the proposed public notice. The public notice shall provide the estimated start and end dates of the extreme noise generating activities and describe noise attenuation measures to be implemented.

When Required: During construction

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

38. Project-Specific Construction Noise Reduction Measures

Requirement: The project applicant shall submit a Construction Noise Management Plan prepared by a qualified acoustical consultant for City review and approval that contains a set of site-specific noise attenuation measures to further reduce construction noise impacts. The project applicant shall implement the approved Plan during construction

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

39. Construction Noise Complaints

Requirement: The project applicant shall submit to the City for review and approval a set of procedures for responding to and tracking complaints received pertaining to construction noise, and shall implement the procedures during construction. At a minimum, the procedures shall include:

- a. Designation of an on-site construction complaint and enforcement manager for the project;

- b. A large on-site sign near the public right-of-way containing permitted construction days/hours, complaint procedures, and phone numbers for the project complaint manager and City Code Enforcement unit;
- c. Protocols for receiving, responding to, and tracking received complaints; and
- d. Maintenance of a complaint log that records received complaints and how complaints were addressed, which shall be submitted to the City for review upon the City's request.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

40. Operational Noise

Requirement: Noise levels from the project site after completion of the project (i.e., during project operation) shall comply with the performance standards of chapter 17.120 of the Oakland Planning Code and chapter 8.18 of the Oakland Municipal Code. If noise levels exceed these standards, the activity causing the noise shall be abated until appropriate noise reduction measures have been installed and compliance verified by the City.

When Required: Ongoing

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

41. Vibration Impacts on Adjacent Historic Structures or Vibration-Sensitive Activities

Requirement: The project applicant shall submit a Vibration Analysis prepared by an acoustical and/or structural engineer or other appropriate qualified professional for City review and approval that establishes pre-construction baseline conditions and threshold levels of vibration that could damage the structure and/or substantially interfere with activities located at **16th Street Train Station**. The Vibration Analysis shall identify design means and methods of construction that shall be utilized in order to not exceed the thresholds. The applicant shall implement the recommendations during construction.

When Required: Prior to construction

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

42. Construction Activity in the Public Right-of-Way

a. Obstruction Permit Required

Requirement: The project applicant shall obtain an obstruction permit from the City prior to placing any temporary construction-related obstruction in the public right-of-way, including City streets and sidewalks.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

b. Traffic Control Plan Required

Requirement: In the event of obstructions to vehicle or bicycle travel lanes, the project applicant shall submit a Traffic Control Plan to the City for review and approval prior to obtaining an obstruction permit. The project applicant shall submit evidence of City approval of the Traffic Control Plan with the application for an obstruction permit. The Traffic Control Plan shall contain a set of comprehensive traffic control measures for auto, transit, bicycle, and pedestrian detours, including detour signs if required, lane closure procedures, signs, cones for drivers, and designated construction access routes. The project applicant shall implement the approved Plan during construction.

When Required: Prior to approval of construction-related permit

Initial Approval Public Works Department, Transportation Services Division

Monitoring/Inspection: Bureau of Building

c. Repair of City Streets

Requirement: The project applicant shall repair any damage to the public right-of way, including streets and sidewalks caused by project construction at his/her expense within one week of the occurrence of the damage (or excessive wear), unless further damage/excessive wear may continue; in such case, repair shall occur prior to approval of the final inspection of the construction-related permit. All damage that is a threat to public health or safety shall be repaired immediately.

When Required: Prior to building permit final

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

43. Bicycle Parking

Requirement: The project applicant shall comply with the City of Oakland Bicycle Parking Requirements (chapter 17.118 of the Oakland Planning Code). The project drawings submitted for construction-related permits shall demonstrate compliance with the requirements.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Planning

Monitoring/Inspection: Bureau of Building

44. Transportation Improvements

Requirement: The project applicant shall implement the recommended on- and off-site transportation-related improvements contained within the Transportation Impact Study for the project (e.g., signal timing adjustments, restriping, signalization, traffic control devices, roadway reconfigurations, and pedestrian and bicyclist amenities). The project applicant is responsible for funding and installing the improvements, and shall obtain all necessary permits and approvals from the City and/or other applicable regulatory agencies such as, but

not limited to, Caltrans (for improvements related to Caltrans facilities) and the California Public Utilities Commission (for improvements related to railroad crossings), prior to installing the improvements. To implement this measure for intersection modifications, the project applicant shall submit Plans, Specifications, and Estimates (PS&E) to the City for review and approval. All elements shall be designed to applicable City standards in effect at the time of construction and all new or upgraded signals shall include these enhancements as required by the City. All other facilities supporting vehicle travel and alternative modes through the intersection shall be brought up to both City standards and ADA standards (according to Federal and State Access Board guidelines) at the time of construction. Current City Standards call for, among other items, the elements listed below:

- a. 2070L Type Controller with cabinet accessory
- b. GPS communication (clock)
- c. Accessible pedestrian crosswalks according to Federal and State Access Board guidelines with signals (audible and tactile)
- d. Countdown pedestrian head module switch out
- e. City Standard ADA wheelchair ramps
- f. Video detection on existing (or new, if required)
- g. Mast arm poles, full activation (where applicable)
- h. Polara Push buttons (full activation)
- i. Bicycle detection (full activation)
- j. Pull boxes
- k. Signal interconnect and communication with trenching (where applicable), or through existing conduit (where applicable), 600 feet maximum
- l. Conduit replacement contingency
- m. Fiber switch
- n. PTZ camera (where applicable)
- o. Transit Signal Priority (TSP) equipment consistent with other signals along corridor
- p. Signal timing plans for the signals in the coordination group

When Required: Prior to building permit final or as otherwise specified

Initial Approval: Bureau of Building; Public Works Department, Transportation Services Division

Monitoring/Inspection: Bureau of Building

45. Construction and Demolition Waste Reduction and Recycling

Requirement: The project applicant shall comply with the City of Oakland Construction and Demolition Waste Reduction and Recycling Ordinance (chapter 15.34 of the Oakland Municipal Code) by submitting a Construction and Demolition Waste Reduction and Recycling Plan (WRRP) for City review and approval, and shall implement the approved WRRP. Projects subject to these requirements include all new construction,

renovations/alterations/modifications with construction values of \$50,000 or more (except R-3 type construction), and all demolition (including soft demolition) except demolition of type R-3 construction. The WRRP must specify the methods by which the project will divert construction and demolition debris waste from landfill disposal in accordance with current City requirements. The WRRP may be submitted electronically at www.greenhalosystems.com or manually at the City's Green Building Resource Center. Current standards, FAQs, and forms are available on the City's website and in the Green Building Resource Center.

When Required: Prior to approval of construction-related permit

Initial Approval: Public Works Department, Environmental Services Division

Monitoring/Inspection: Public Works Department, Environmental Services Division

46. Underground Utilities

Requirement: The project applicant shall place underground all new utilities serving the project and under the control of the project applicant and the City, including all new gas, electric, cable, and telephone facilities, fire alarm conduits, street light wiring, and other wiring, conduits, and similar facilities. The new facilities shall be placed underground along the project's street frontage and from the project structures to the point of service. Utilities under the control of other agencies, such as PG&E, shall be placed underground if feasible. All utilities shall be installed in accordance with standard specifications of the serving utilities.

When Required: During construction

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

47. Recycling Collection and Storage Space

Requirement: The project applicant shall comply with the City of Oakland Recycling Space Allocation Ordinance (chapter 17.118 of the Oakland Planning Code). The project drawings submitted for construction-related permits shall contain recycling collection and storage areas in compliance with the Ordinance. For residential projects, at least two cubic feet of storage and collection space per residential unit is required, with a minimum of ten cubic feet. For nonresidential projects, at least two cubic feet of storage and collection space per 1,000 square feet of building floor area is required, with a minimum of ten cubic feet.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Planning

Monitoring/Inspection: Bureau of Building

48. Green Building Requirements**a. Compliance with Green Building Requirements During Plan-Check**

Requirement: The project applicant shall comply with the requirements of the California Green Building Standards (CALGreen) mandatory measures and the applicable requirements of the City of Oakland Green Building Ordinance (chapter 18.02 of the Oakland Municipal Code).

- i. The following information shall be submitted to the City for review and approval with the application for a building permit:
 - Documentation showing compliance with Title 24 of the current version of the California Building Energy Efficiency Standards.
 - Completed copy of the final green building checklist approved during the review of the Planning and Zoning permit.
 - Copy of the Unreasonable Hardship Exemption, if granted, during the review of the Planning and Zoning permit.
 - Permit plans that show, in general notes, detailed design drawings, and specifications as necessary, compliance with the items listed in subsection (ii) below.
 - Copy of the signed statement by the Green Building Certifier approved during the review of the Planning and Zoning permit that the project complied with the requirements of the Green Building Ordinance.
 - Signed statement by the Green Building Certifier that the project still complies with the requirements of the Green Building Ordinance, unless an Unreasonable Hardship Exemption was granted during the review of the Planning and Zoning permit.
 - Other documentation as deemed necessary by the City to demonstrate compliance with the Green Building Ordinance.
- ii. The set of plans in subsection (i) shall demonstrate compliance with the following:
 - CALGreen mandatory measures.
 - All pre-requisites per the green building checklist approved during the review of the Planning and Zoning permit, or, if applicable, all the green building measures approved as part of the Unreasonable Hardship Exemption granted during the review of the Planning and Zoning permit.
 - **23** per the appropriate checklist approved during the Planning entitlement process.
 - All green building points identified on the checklist approved during review of the Planning and Zoning permit, unless a Request for Revision Plan-check application is submitted and approved by the Bureau of Planning that shows the previously approved points that will be eliminated or substituted.
 - The required green building point minimums in the appropriate credit categories.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Building

Monitoring/Inspection: N/A

b. *Compliance with Green Building Requirements During Construction*

Requirement: The project applicant shall comply with the applicable requirements of CALGreen and the Oakland Green Building Ordinance during construction of the project.

The following information shall be submitted to the City for review and approval:

- i. Completed copies of the green building checklists approved during the review of the Planning and Zoning permit and during the review of the building permit.
- ii. Signed statement(s) by the Green Building Certifier during all relevant phases of construction that the project complies with the requirements of the Green Building Ordinance.
- iii. Other documentation as deemed necessary by the City to demonstrate compliance with the Green Building Ordinance.

When Required: During construction

Initial Approval: N/A

Monitoring/Inspection: Bureau of Building

c. *Compliance with Green Building Requirements After Construction*

Requirement: Within sixty (60) days of the final inspection of the building permit for the project, the Green Building Certifier shall submit the appropriate documentation to **Green Building Certification Institute** and attain the minimum required certification/point level. Within one year of the final inspection of the building permit for the project, the applicant shall submit to the Bureau of Planning the Certificate from the organization listed above demonstrating certification and compliance with the minimum point/certification level noted above.

When Required: After project completion as specified

Initial Approval: Bureau of Planning

Monitoring/Inspection: Bureau of Building

49. Sanitary Sewer System

Requirement: The project applicant shall prepare and submit a Sanitary Sewer Impact Analysis to the City for review and approval in accordance with the City of Oakland Sanitary Sewer Design Guidelines. The Impact Analysis shall include an estimate of pre-project and post-project wastewater flow from the project site. In the event that the Impact Analysis indicates that the net increase in project wastewater flow exceeds City-projected increases in wastewater flow in the sanitary sewer system, the project applicant shall pay the Sanitary Sewer Impact Fee in accordance with the City's Master Fee Schedule for funding improvements to the sanitary sewer system.

When Required: Prior to approval of construction-related permit

Initial Approval: Public Works Department, Department of Engineering and Construction

Monitoring/Inspection: N/A

50. Storm Drain System

Requirement: The project storm drainage system shall be designed in accordance with the City of Oakland's Storm Drainage Design Guidelines. To the maximum extent practicable, peak stormwater runoff from the project site shall be reduced by at least 25 percent compared to the pre-project condition.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Building

Monitoring/Inspection: Bureau of Building

51. Recycled Water

Requirement: Pursuant to section 16.08.030 of the Oakland Municipal Code, the project applicant shall provide for the use of recycled water in the project for landscape irrigation purposes unless the City determines that there is a higher and better use for the recycled water, the use of recycled water is not economically justified for the project, or the use of recycled water is not financially or technically feasible for the project. The project applicant shall contact the New Business Office of the East Bay Municipal Utility District (EBMUD) for a recycled water feasibility assessment by the Office of Water Recycling. If recycled water is to be provided in the project, the project drawings submitted for construction-related permits shall include the proposed recycled water system and the project applicant shall install the recycled water system during construction.

When Required: Prior to approval of construction-related permit

Initial Approval: Bureau of Planning; Bureau of Building

Monitoring/Inspection: Bureau of Building

52. Traffic Mitigation

The project shall be required to comply with the Traffic Mitigations and Fair Share contributions to study and improved identified intersections as per the mitigation measures associated with VTPM8554 and ER03-0023 for the Wood Street project.

For VTPM8554 COA's 25, 26, 27, and 28, the applicant would be required to pay the Fair Share contribution identified in Eric Angstadt's letter of May 29, 2009 (See Attachment D) prior to issuance of building permit.

For VTPM8554 COA 76, the applicant in collaboration with the other Wood Street Project sponsors shall re-engage discussions with AC Transit to see if service on the 26 line can be returned to 15 minute headways, and as development of the Wood Street Zoning District progresses to the north, in coordination with AC Transit, locate an additional AC Transit stop proximate to the new uses.

When Required: Prior to issuance of a certificate of occupancy for the first unit

Initial Approval: Bureau of Planning; Bureau of Building

Monitoring/Inspection: Bureau of Building

53. Public Improvements – Vesting Tentative Parcel map 8554 (COA 83)

All public improvements shall be constructed in substantial conformance with the individual vesting tentative maps submitted by the project sponsors for each Development Area for the approval of the Wood Street Project.

When Required: Ongoing

Initial Approval: Bureau of Planning; Bureau of Building

Monitoring/Inspection: Bureau of Building

54. Conditions of approval associated with VTPM8554

All the relevant conditions of approval associated with VTPM8554 (see Attachment C) shall apply to PLN15-245-PUDF01 unless otherwise modified by the COAs outlined in this staff report.

When Required: Ongoing

Initial Approval: Bureau of Planning; Bureau of Building

Monitoring/Inspection: Bureau of Building

55. Public Art for Private Development Condition of Approval

This project is subject to the City's Public Art Requirements for Private Development, adopted by Ordinance No. 13275 C.M.S. ("Ordinance"). As a residential project, the public art contribution requirement is equivalent to one half percent (0.5%) of building development costs for the project. The contribution requirement can be met through the commission or acquisition and installation of publicly accessible art on the development site, payment of an in-lieu contribution to the City's established public art fund, or satisfaction of alternative compliance methods described in the Ordinance. The applicant shall provide proof of full payment of the in lieu contribution, or provide proof of installation of artwork on the development site prior to the City's issuance of a final certificate of occupancy for each Phase unless a separate, legal binding instrument is executed ensuring compliance within a timely manner, subject to City approval. On-site art installation shall be designed by independent artists, or artists working in conjunction with arts or community organizations, that are verified by the City to either hold a valid Oakland business license and/or be an Oakland-based 501(c)(3) tax designated organization in good standing.

When Required: Prior to issuance of Final Certificate of Occupancy for the first unit and Ongoing

Initial Approval: Bureau of Planning; Bureau of Building

Monitoring/Inspection: Bureau of Building

56. Tentative Parcel Map

In order for the proposed project to be completed as condominiums, the project applicant shall apply and receive approval for a Tentative and Final Parcel Map with the City of Oakland Planning and Building Departments.

When Required: Prior to approval of issuance of certificate of occupancy

Initial Approval: Bureau of Planning; Bureau of Building

Monitoring/Inspection: Bureau of Building

57. Fire Accessways

The applicant shall obtain approval for Alternate Materials and Methods of Construction (OFC Sec.104.2.8) for the proposed 20-foot fire access turnaround subject to the following requirements:

- All fire access roads shall be unobstructed at all times;
- The automatic fire sprinkler system shall be designed to simultaneously discharge 8 most hydraulically-remote heads;
- Bollards, if installed, shall be removable, or allow unobstructed emergency vehicle access; and
- There shall be no parked vehicles on the required minimum 26-foot and 20-foot fire access roads. 'No Parking' signs shall be posted per City's Public Works Agency signage requirements.

When Required: Ongoing

Initial Approval: Bureau of Planning; Bureau of Building

Monitoring/Inspection: Bureau of Building

Applicant Statement

I have read and accept responsibility for the Conditions of Approval. I agree to abide by and conform to the Conditions of Approval, as well as to all provisions of the Oakland Planning Code and Oakland Municipal Code pertaining to the project.

Name of Project Applicant

Signature of Project Applicant

Date

APPROVED BY:

City Planning Commission: _____ (date) _____ (vote)

CONDITIONS OF APPROVAL

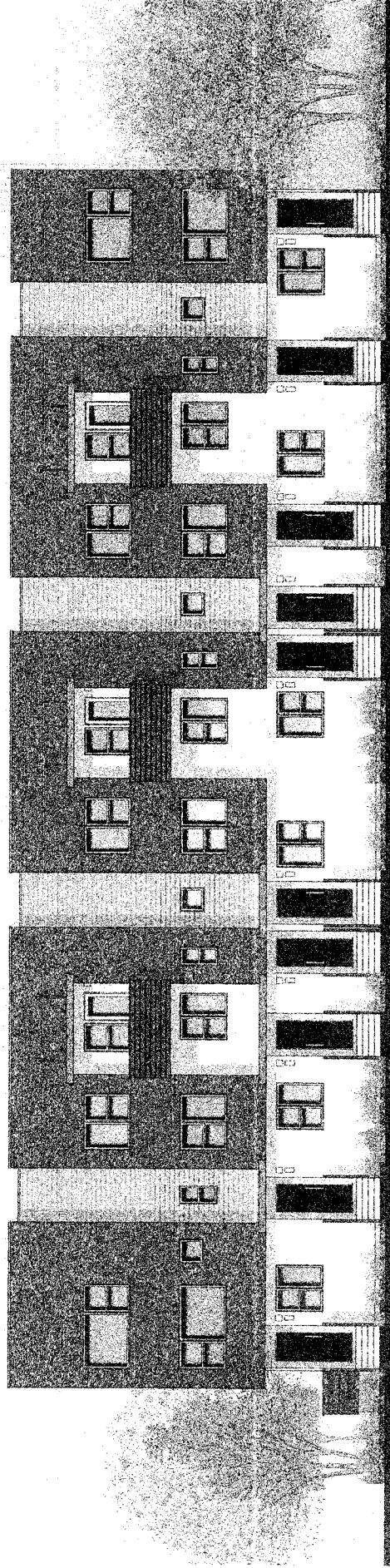
SIGNALHOUSE

OAKLAND, CALIFORNIA



City Ventures

SUBMITTED: OCTOBER 5, 2015



ATTACHMENT A

SIGNALHOUSE

OAKLAND, CALIFORNIA



City Ventures

APPLICANT:
CITY VENTURES
 444 SPEAR STREET, SUITE 200
 SAN FRANCISCO, CA 94105
 CONTACT: CLAIRE HAN
 Development Associate
 PHONE: 415.722.6534
 E-MAIL: claire@cityventures.com
 www.cityventures.com

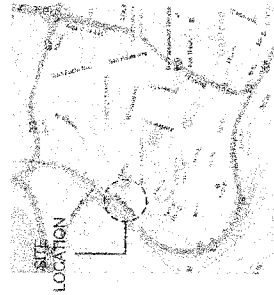
ARCHITECT:
HUNT HALE JONES ARCHITECTS
 444 SPEAR STREET, SUITE 105
 SAN FRANCISCO, CA 94105
 CONTACT: DAN HALE
 Architect
 PHONE: 415.568.3833
 E-MAIL: d hale@hhja.com
 www.hhja.com

CIVIL ENGINEER:
SANDIS.
 636 9TH ST.
 OAKLAND, CALIFORNIA
 CONTACT: ANDREA FORTUN, PE
 Project Manager
 PHONE: 510-573-8866
 E-MAIL: afortun@sandis.net
 www.sandis.net

LANDSCAPE:
VAN DORN ABED
 81 14TH STREET
 SAN FRANCISCO, CA 94103
 CONTACT: ZEKI ABED, MELISSA WILLMANN
 Landscape Architect
 PHONE: 415-864-1921
 E-MAIL: zeki@valainc.com
 www.valainc.com

SUBMITTAL DATE: OCTOBER 5, 2015

LOCATION MAP



VICINITY MAP

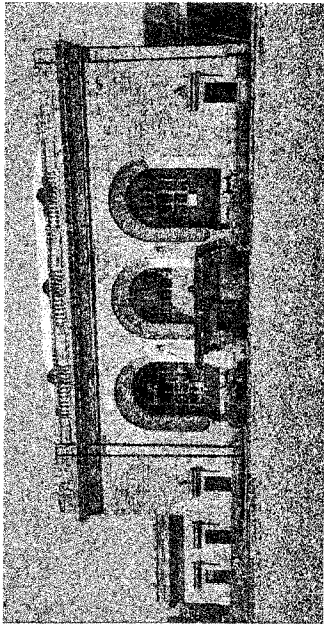


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 CS1 - COVER SHEET
 CS2 - SHEET INDEX
 CS3 - PROJECT DATA
 CS4 - PROJECT SUMMARY
 SP - SITE PLAN
 PP - PHASING PLAN
 0.2 - CONTEXT PHOTOS & KEY MAP 1
 0.3 - CONTEXT PHOTOS 2
 0.4 - CONTEXT PHOTOS 3
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 A0.2 - PLAZA ELEVATION
 A1.1 - TYP. UNIT PLAN 1-2-9
 A1.2 - TYP. UNIT PLAN 3-4 RAISED FLOOR
 A1.3 - TYP. UNIT PLAN 5-6
 A1.4 - TYP. UNIT PLAN 7-8 RAISED FLOOR
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 A2.2 - BUILDING 1 UPPER-ROOF
 A2.3 - BUILDING 1 ELEVATIONS
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 A6.1 - BUILDING 5 GROUND-MAIN-UPPER-ROOF
 A6.2 - BUILDING 5 ELEVATIONS
 A7.1 - BUILDING 6 GROUND-MAIN-UPPER-ROOF
 A7.2 - BUILDING 6 ELEVATIONS
 A8.1 - MATERIAL AND COLOR BOARD
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 C-1.00 - BOUNDARY EXHIBIT
 C-1.01 - ONSITE GRADING PLAN
 C-2.01 - ONSITE UTILITY PLAN
 C-3.01 - STORMWATER MANAGEMENT PLAN
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 L1.1 - AREA IDENTIFICATION PLAN
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 L2.2 - PLANTING ENLARGEMENT - EVA
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 L3.0 - CONCEPTUAL TREE PALETTE & IMAGERY
 L3.1 - CONCEPTUAL PLANT PALETTE & IMAGERY



9. HISTORIC CENTRAL STATION ON WOOD STREET



10. FRONTAGE ROAD



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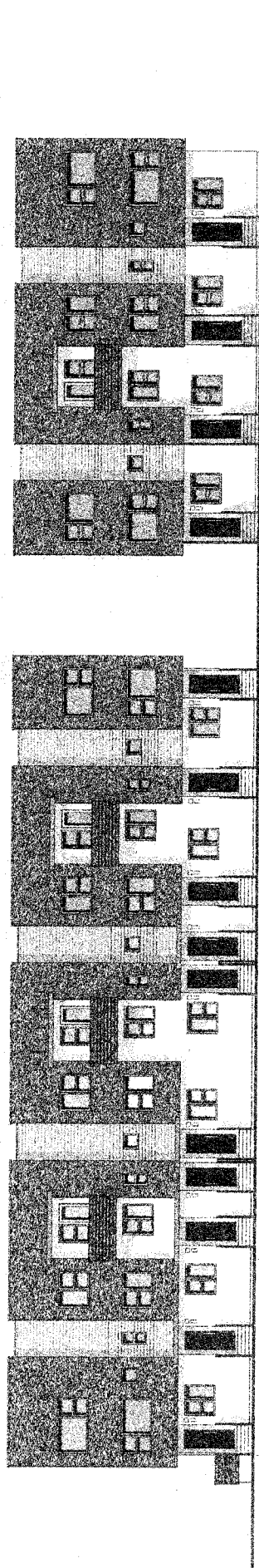
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CONTEXT PHOTOS - 3

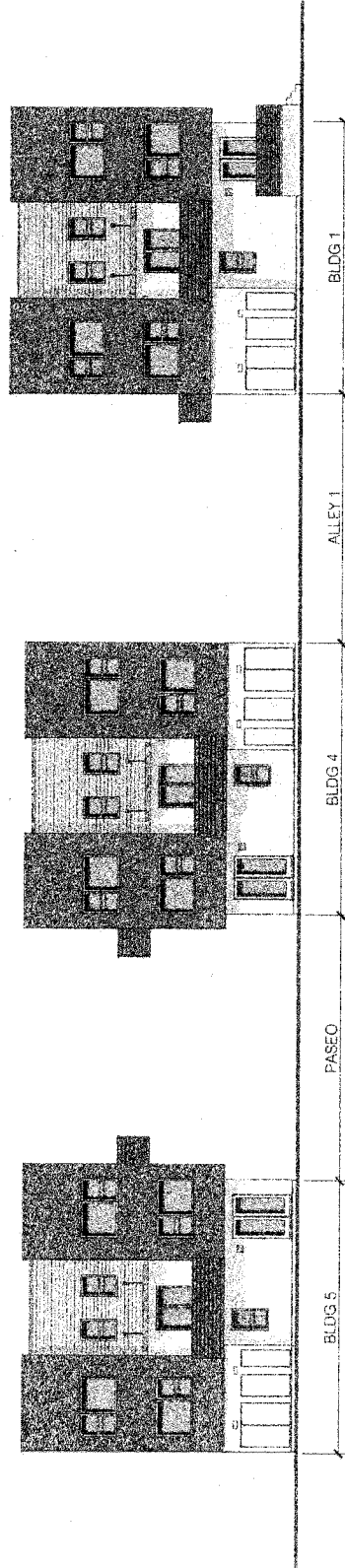
0.4

SCALE: N.T.S.
DATE: 03/24/2015
PROJECT: 986959



WOOD STREET STREETSCAPE ELEVATION

BUILDING 1 AND 2



PLAZA ELEVATION

BUILDING 3-5

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STREETSCAPE

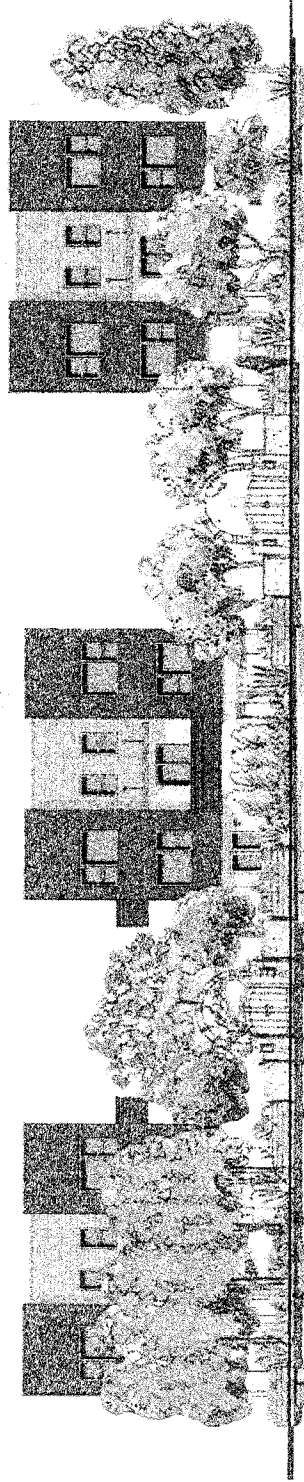
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DATE: 09/28/2015
PROJECT: 517743

BUILDING 5

BUILDING 4

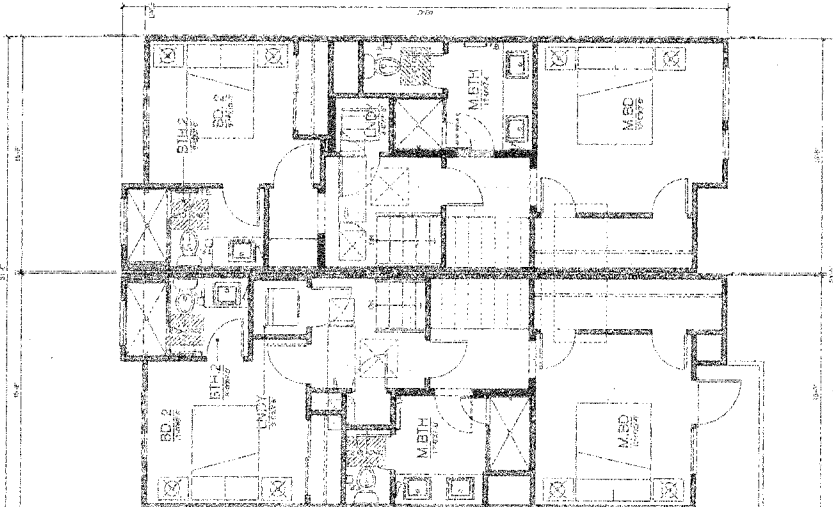
BUILDING 1



SIGNALHOUSE
OAKLAND, CA

PLAZA ELEVATION
A0.2

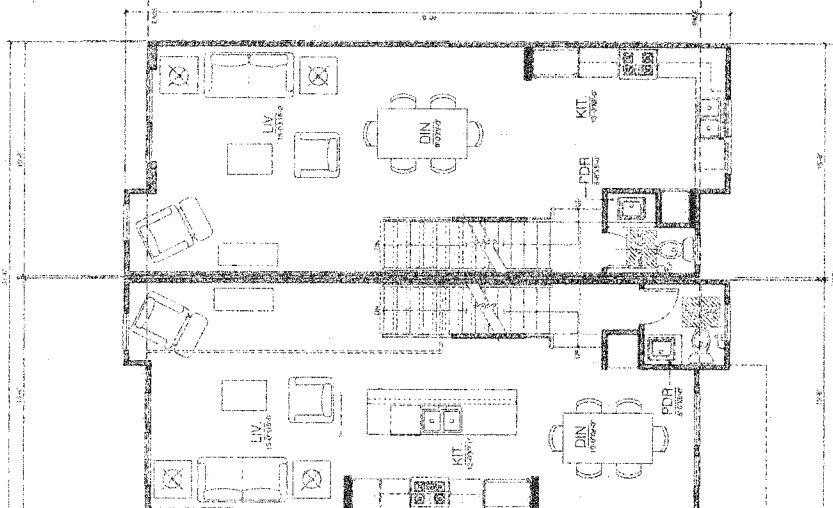

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PLAN 1
UPPER LIVING: 588 SQ. FT.

PLAN 2
UPPER LIVING: 588 SQ. FT.

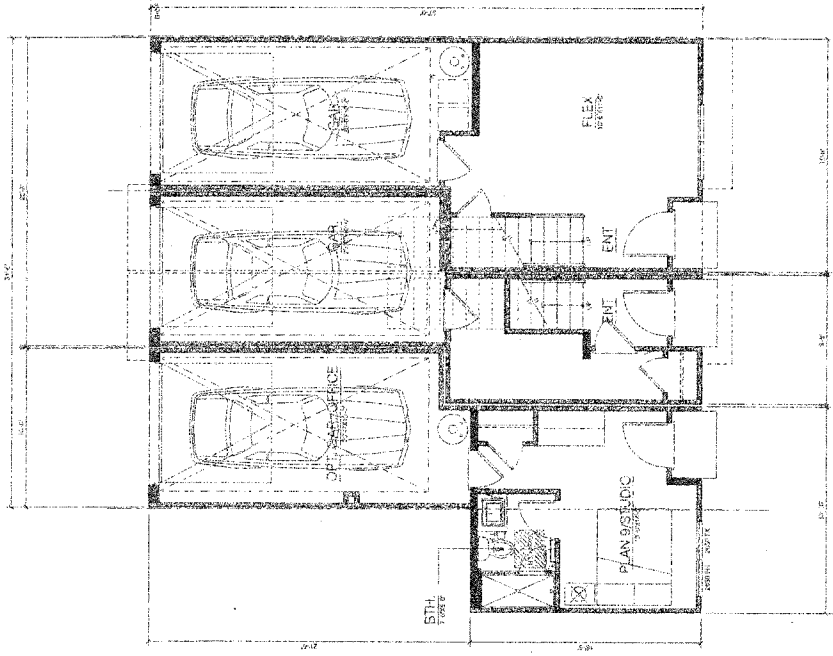
UPPER FLOOR



PLAN 1
MAIN LIVING: 600 SQ. FT.

PLAN 2
MAIN LIVING: 608 SQ. FT.

MAIN FLOOR



PLAN 1
LIVING: 205 SQ. FT.
GARAGE: 222 SQ. FT.

PLAN 2
LOWER LIVING: 140 SQ. FT.
TOTAL LIVING: 1,452 SQ. FT.
GARAGE: 204 SQ. FT.

LOWER FLOOR



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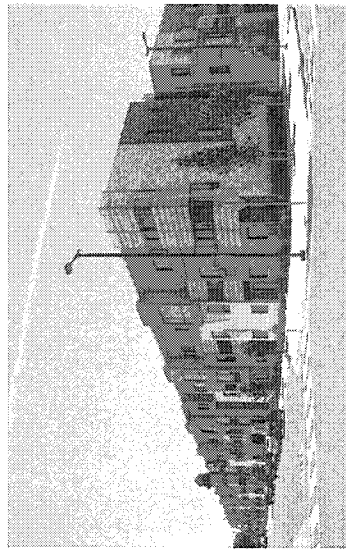
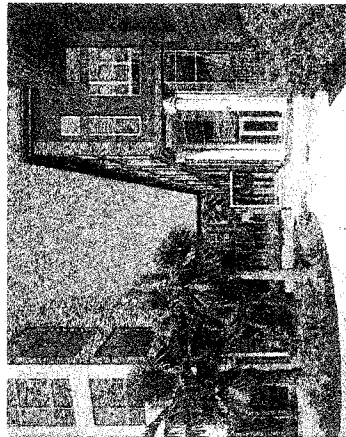
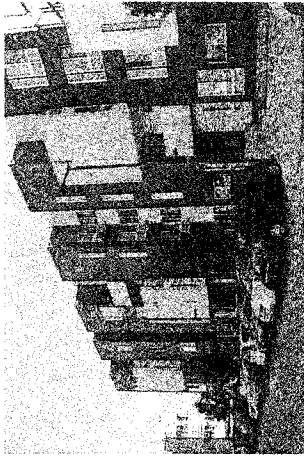
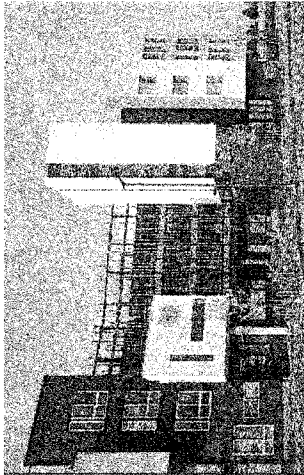
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1.415.266.0285

TYP. UNIT PLAN 1-2-9

A1.1

SCALE: 1/8" = 1'-0"
DATE: 09/20/2015
PROJECT: 317043



DEVELOPMENT AREA 6
OAKLAND

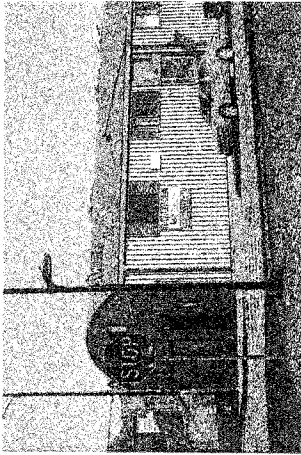
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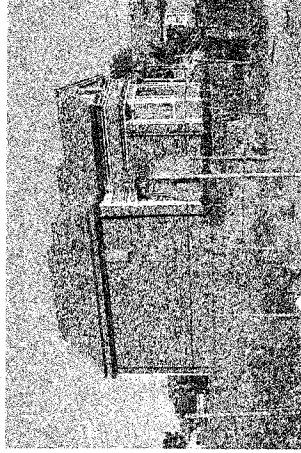
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DATE: 03/24/2015
PROJECT: 9999999

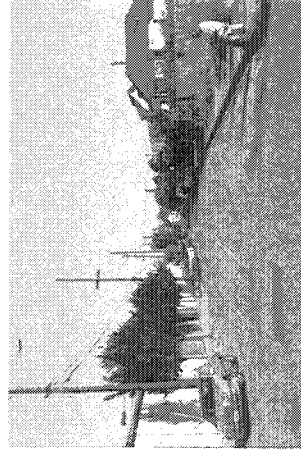
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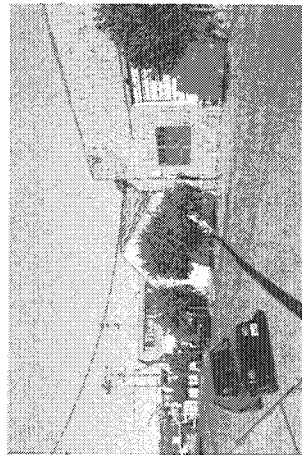
4. COMMERCIAL SPACE - 14TH &
WOOD STREET



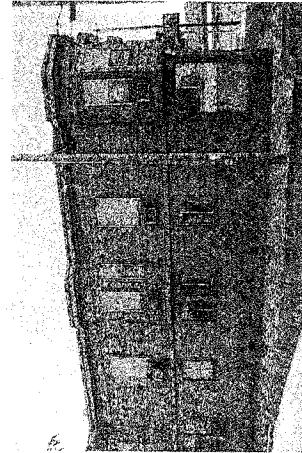
5. VACANT LOT & SINGLE FAMILY
HOME ON 14TH STREET



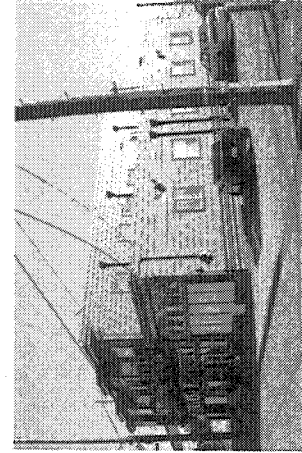
6. INDUSTRIAL/RESIDENTIAL TRANSITION
AT 15TH STREET



7. COMMERCIAL SPACE ON WOOD
STREET BETWEEN 15TH & 16TH STREET



8. BEA'S HOTEL - 16TH AND WOOD STREET



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CONTEXT PHOTOS - 2

0.3

SCALE: N.T.S.
DATE: 03/24/2015
PROJECT: 889599

BUILDING 1			
UNIT	DECK	PATIO	UNIT TOTAL
1	34 SQ. FT.	0	34 SQ. FT.
2	34 SQ. FT.	0	34 SQ. FT.
3	0	0	0
4	34 SQ. FT.	0	34 SQ. FT.
5	34 SQ. FT.	0	34 SQ. FT.
6	34 SQ. FT.	0	34 SQ. FT.
7	34 SQ. FT.	0	34 SQ. FT.
8	34 SQ. FT.	0	34 SQ. FT.
9	34 SQ. FT.	0	34 SQ. FT.
10	34 SQ. FT.	0	34 SQ. FT.
BLDG 1 TOTAL	272 SQ. FT.	0	272 SQ. FT.

BUILDING 3			
UNIT	DECK	PATIO	UNIT TOTAL
15	34 SQ. FT.	0	34 SQ. FT.
16	34 SQ. FT.	0	34 SQ. FT.
17	0	0	0
18	34 SQ. FT.	0	34 SQ. FT.
19	34 SQ. FT.	0	34 SQ. FT.
20	34 SQ. FT.	0	34 SQ. FT.
21	34 SQ. FT.	0	34 SQ. FT.
22	0	0	0
23	34 SQ. FT.	0	34 SQ. FT.
24	34 SQ. FT.	0	34 SQ. FT.
BLDG 3 TOTAL	272 SQ. FT.	0	272 SQ. FT.

BUILDING 5			
UNIT	DECK	PATIO	UNIT TOTAL
35	0	168 SQ. FT.	168 SQ. FT.
36	0	168 SQ. FT.	168 SQ. FT.
37	0	168 SQ. FT.	168 SQ. FT.
38	0	168 SQ. FT.	168 SQ. FT.
39	46 SQ. FT.	168 SQ. FT.	214 SQ. FT.
40	0	168 SQ. FT.	168 SQ. FT.
BLDG 5 TOTAL	46 SQ. FT.	1,008 SQ. FT.	1,054 SQ. FT.

BUILDING 6			
UNIT	DECK	PATIO	UNIT TOTAL
41	0	168 SQ. FT.	168 SQ. FT.
42	0	168 SQ. FT.	168 SQ. FT.
43	46 SQ. FT.	168 SQ. FT.	214 SQ. FT.
44	0	168 SQ. FT.	168 SQ. FT.
BLDG 6 TOTAL	46 SQ. FT.	672 SQ. FT.	718 SQ. FT.

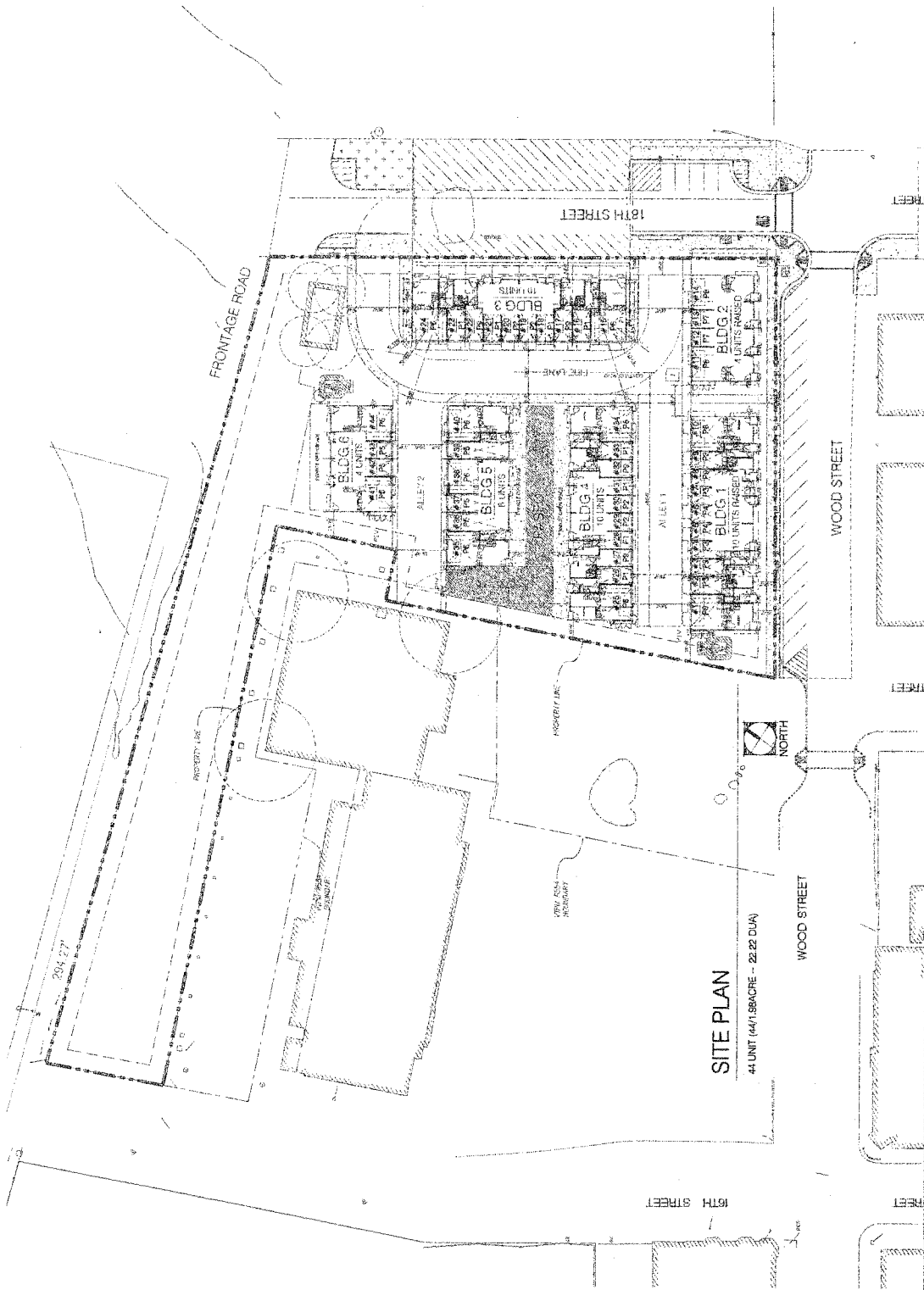
DEVELOPMENT STANDARDS			
	DA 6	PROPOSED	
MAX DENSITY	79.38 DUA	22.22 DUA	
MIN DENSITY	N/A	22.22 DUA	
MAX HEIGHT	65	38'-6"	
MIN SETBACK FROM WOOD ST	10	10	
MIN INT SETBACK	5	10	
MIN PARKING RESIDENTIAL	49 (1.70 UNIT)	64 (1.40 UNIT)	
MIN USEABLE OPEN SPACE	SEE TABULATION BELOW		

PRIVATE OPEN SPACE TABULATION:

MINIMUM OPEN SPACE
 REQUIRED (75 SQ. FT./UNIT X 44 UNITS) = 3,300 SF.
 PROVIDED:
 TOTAL COMMON (80% X 3,300) = 924 SF.
 TOTAL PRIVATE (70% X 3,300) = 2,376 SF.
 TOTAL COMMON = 924 SQ. FT.
 TOTAL PRIVATE = 2,376 SQ. FT.

PROJECT DATA			
PROJECT NAME: SIGNALHOUSE			
APL: 176010112-11			
PROJECT ADDRESS: 17TH ST & WOOD ST			
OAKLAND, CALIFORNIA			
LOT AREA	86,206 SF.	1.86 AC	
TOTAL BUILDING FOOTPRINT	22,386 SF.	0.51 AC	
LOT COVERAGE	26%		
TOTAL LIVING AREA	54,001 SF.		
TOTAL FLOOR AREA (INC. GAR)	84,107 SF.		
TOTAL DWELLING UNITS	44		
ESTIMATE POPULATION (3 PER UNIT)	132 PERSONS		
RESIDENT PARKING	62	1.4 PER UNIT	
UNIT DATA	TOTAL	TOTAL LIVING SF.	
PLAN 1 (C BEDROOM)	8	1,228 SF.	
PLAN 2 (C BEDROOM)	4	1,482 SF.	
PLAN 3 (C BEDROOM)	4	1,214 SF.	
PLAN 4 (C BEDROOM)	2	1,428 SF.	
PLAN 5 (C BEDROOM)	5	1,203 SF.	
PLAN 6 (C BEDROOM)	9	1,452 SF.	
PLAN 7 (C BEDROOM)	2	1,445 SF.	
PLAN 8 (C BEDROOM)	4	1,515 SF.	
PLAN 9 (STUDIO)	6	255 SF.	
TOTAL	44 DU		

PROJECT SUMMARY



SITE PLAN

44 UNIT (441,584 SQ. FT. - 22.22 DUA)

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SITE PLAN

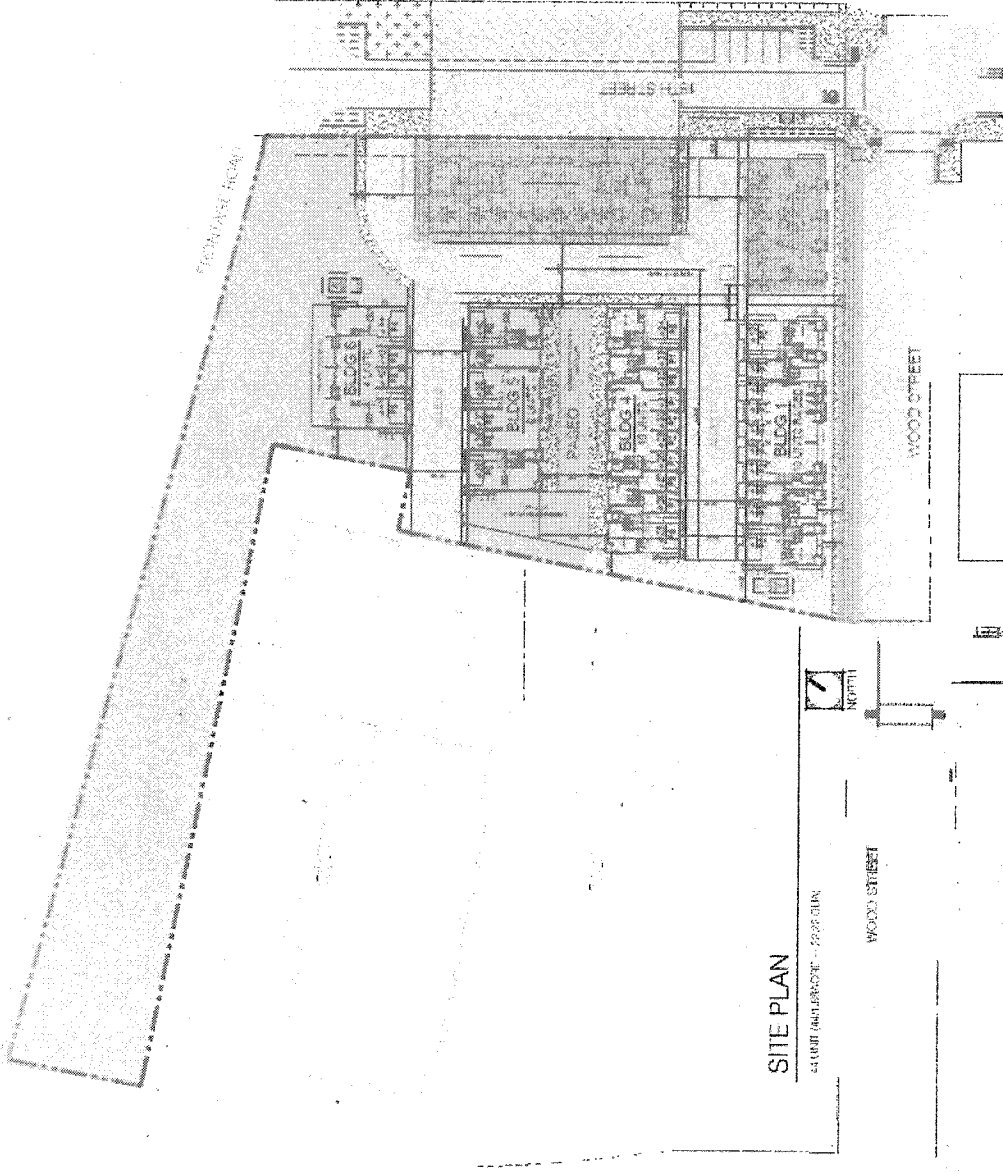
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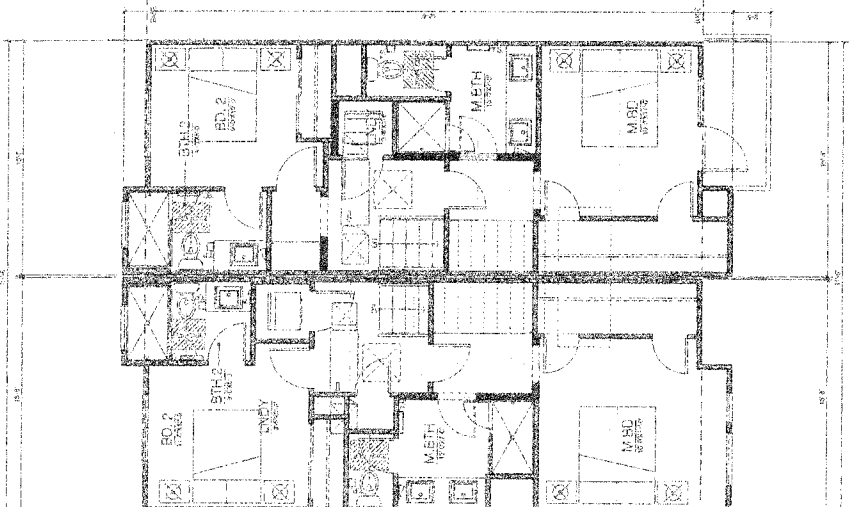
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PROJECT: 317543

PHASE I

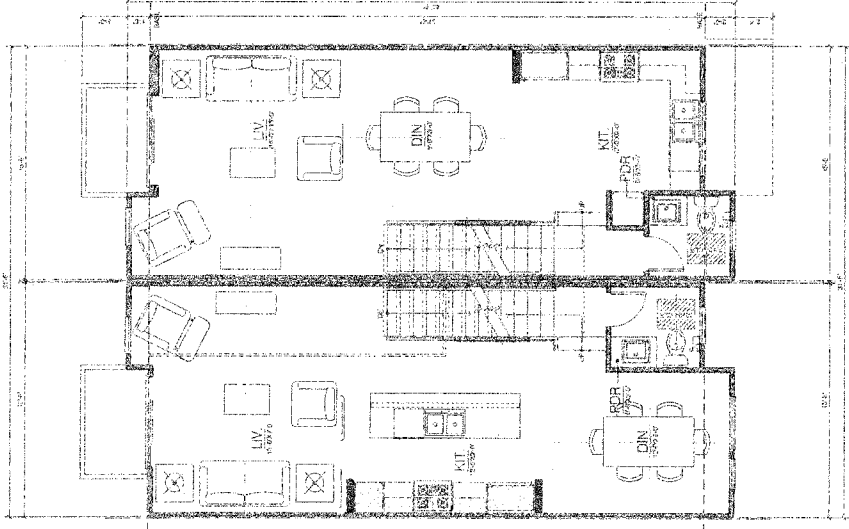
PHASE II

PHASE III

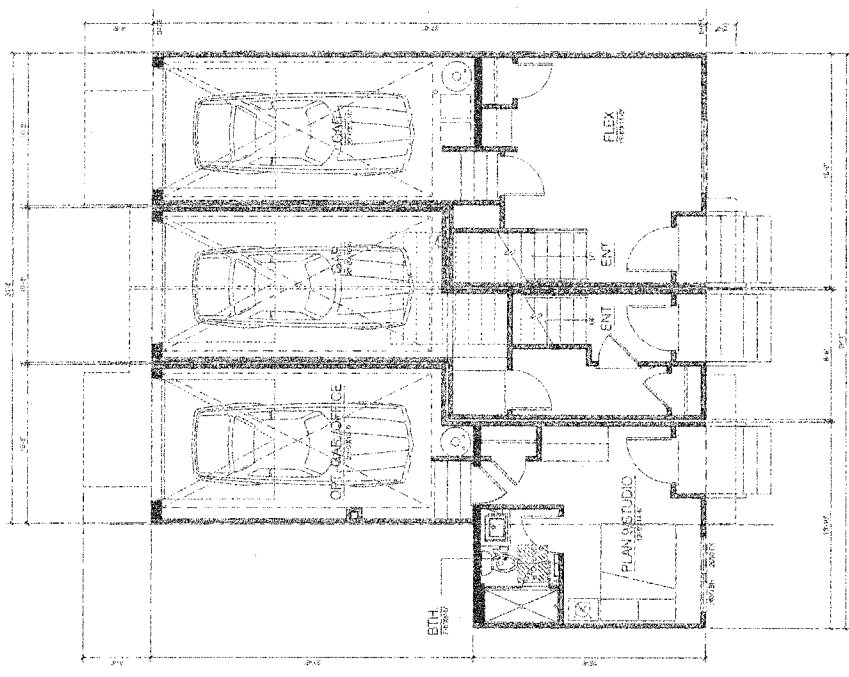




PLAN 3
UPPER LIVING: 596 SQ. FT.
UPPER LIVING: 585 SQ. FT.



PLAN 3
MAIN LIVING: 608 SQ. FT.
MAIN LIVING: 600 SQ. FT.



PLAN 3
LIVING: 205 SQ. FT.
GARAGE: 222 SQ. FT.

PLAN 4
LOWER LIVING: 105 SQ. FT.
TOTAL LIVING: 1370 SQ. FT.
GARAGE: 238 SQ. FT.

UPPER FLOOR

MAIN FLOOR

LOWER FLOOR



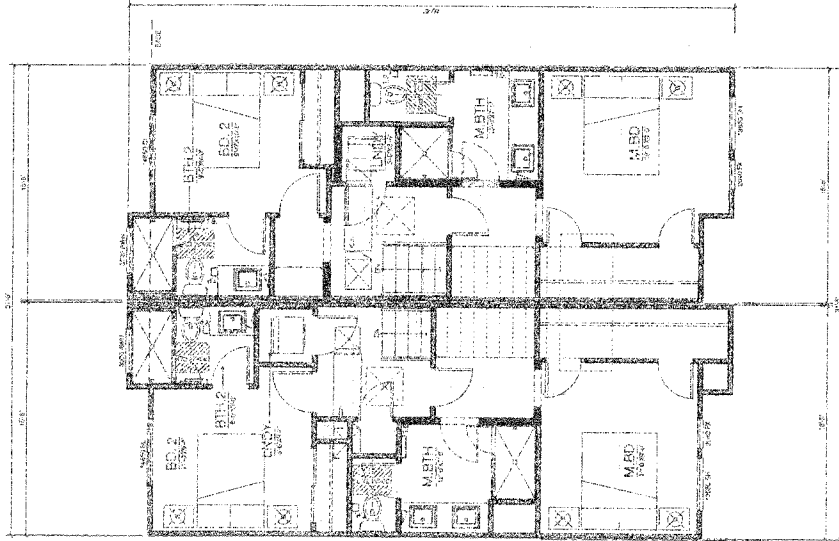
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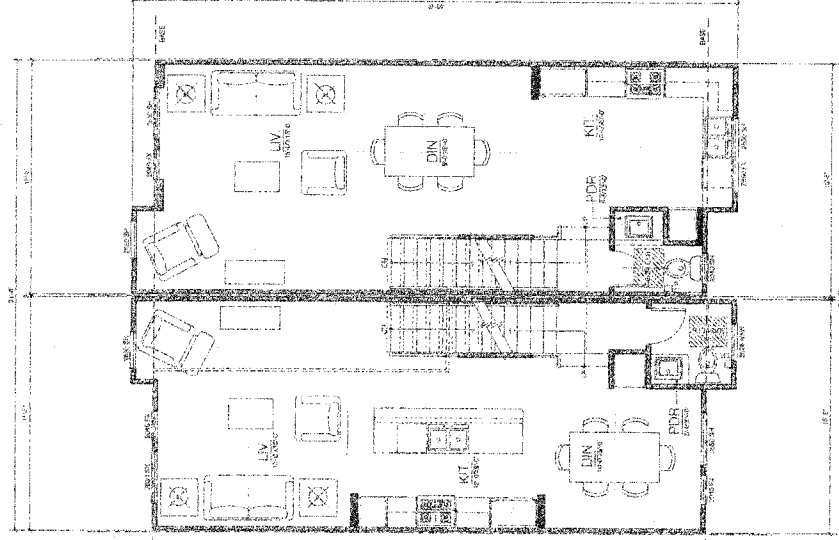
TYP. UNIT PLAN 3-4 RAISED FLOOR
A1.2
SCALE: 1/8" = 1'-0"
DATE: 09/28/2015
PROJECT: 337043



PLAN 5
UPPER LIVING: 588 SQ. FT.

PLAN 6
UPPER LIVING: 588 SQ. FT.

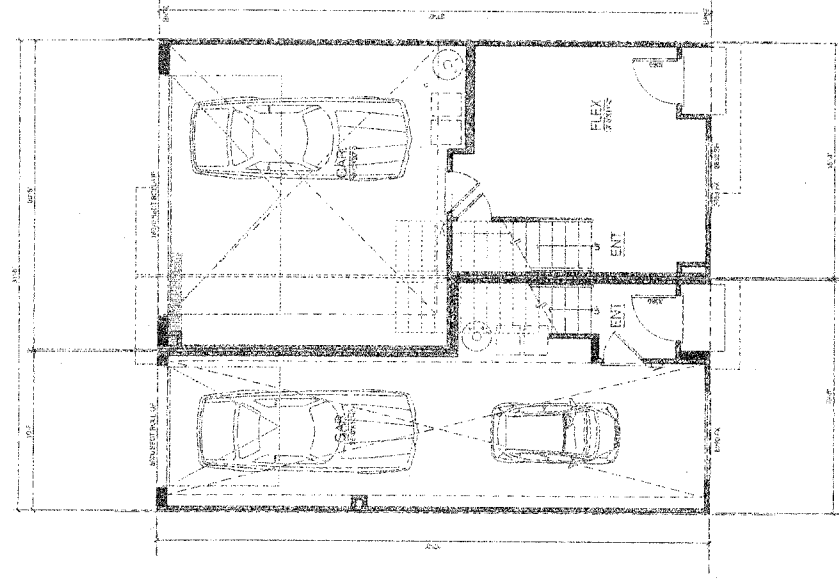
UPPER FLOOR



PLAN 5
MAIN LIVING: 600 SQ. FT.

PLAN 6
MAIN LIVING: 608 SQ. FT.

MAIN FLOOR



PLAN 5
LOWER LIVING: 48 SQ. FT.
TOTAL LIVING: 1,233 SQ. FT.
GARAGE: 425 SQ. FT.

PLAN 6
LOWER LIVING: 257 SQ. FT.
TOTAL LIVING: 1,458 SQ. FT.
GARAGE: 411 SQ. FT.

LOWER FLOOR



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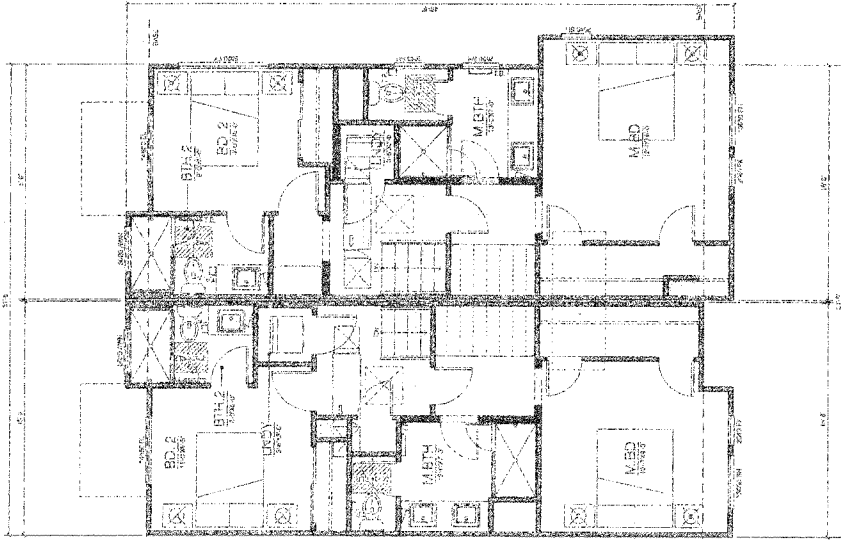
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TYP. UNIT PLAN 5-6

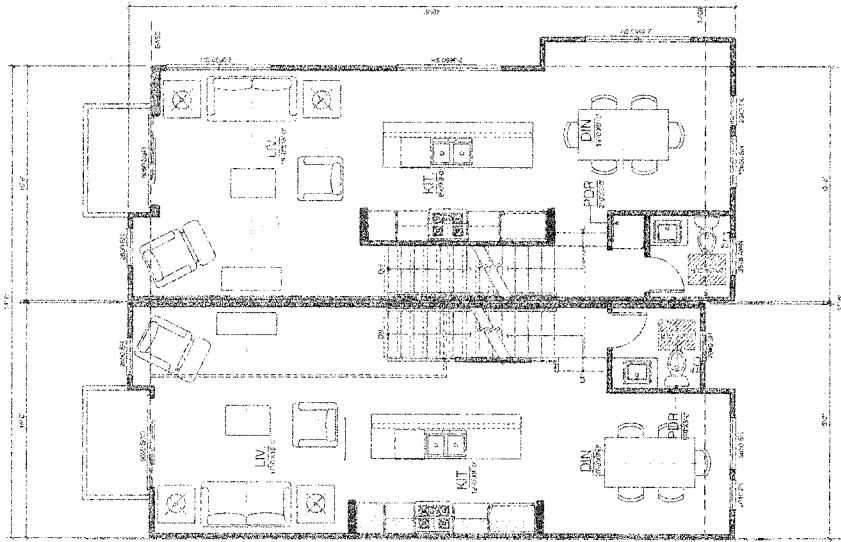
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PROJECT: 817043

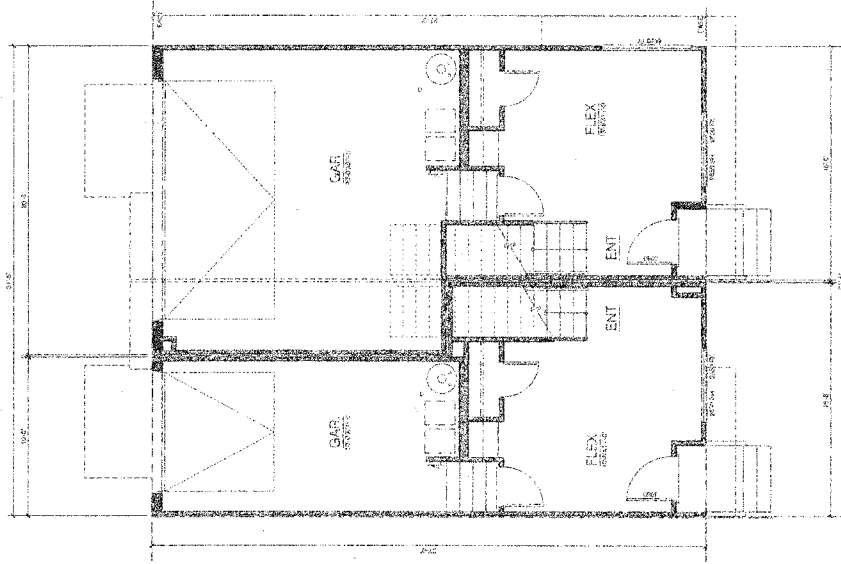
21317043_cadent_41Dwg010A-SD (Lower Unit Plans) 1-2-17-2016 3:09:41 PM, yam



PLAN 7
UPPER LIVING: 596 SQ. FT.
UPPER LIVING: 630 SQ. FT.



PLAN 7
MAIN LIVING: 608 SQ. FT.
MAIN LIVING: 646 SQ. FT.



PLAN 7
LOWER LIVING: 240 SQ. FT.
TOTAL LIVING: 1,443 SQ. FT.
GARAGE: 231 SQ. FT.

UPPER FLOOR

MAIN FLOOR

LOWER FLOOR



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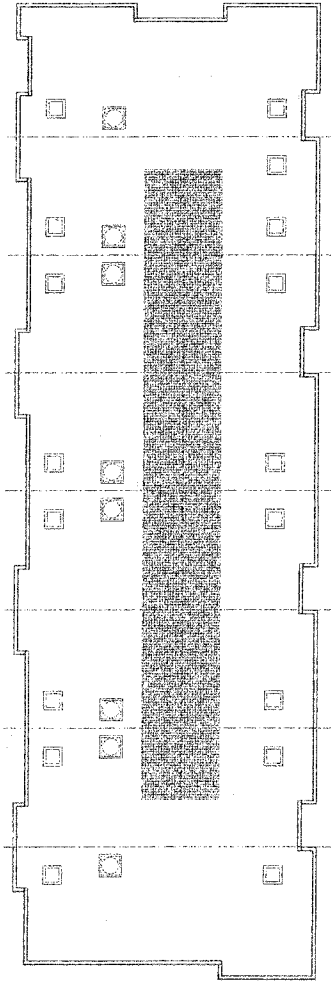
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TYP. UNIT PLAN 7-8 RAISED FLOOR

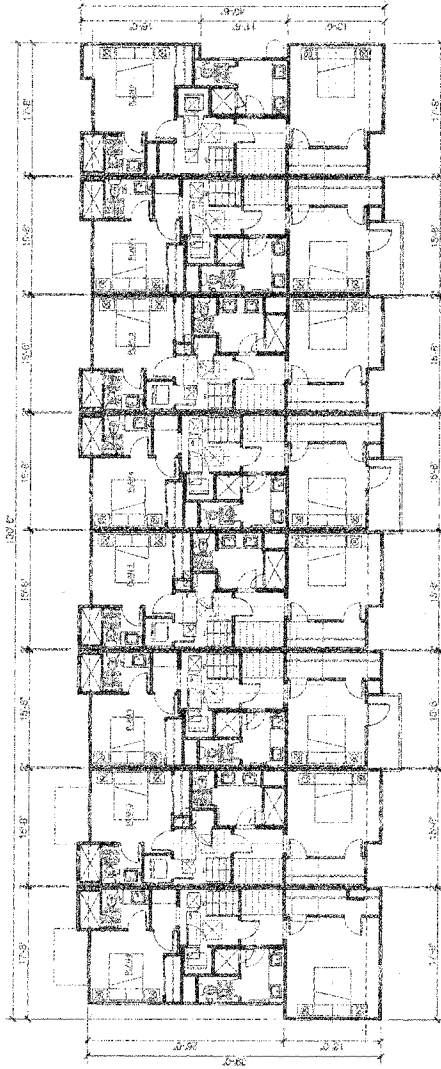
A1.4

SCALE: 1/8" = 1'-0"
DATE: 09/28/2015
PROJECT: 317043



ROOF PLAN

BUILDING 1



UPPER LEVEL

BUILDING 1

UPPER LIVING: 4987 SQ. FT.

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BUILDING 1 UPPER-ROOF

A2.2

SCALE: 1/8" = 1'-0"
DATE: 09/28/2015
PROJECT: 317043

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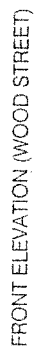


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ARTISTES

1. PAINTING ELEMENTS
2. L'ARTISTE
3. ENVOI ENVOI
4. COMING TO STONE
5. WOOD BURNING
6. WOOD PANEL BURNING
7. FRENCH DOLLA
8. DUAL DUAL WOOD
9. DUAL DUAL WOOD
10. WOOD BURNING
11. SLIDING GLASS DOOR



City Ventures

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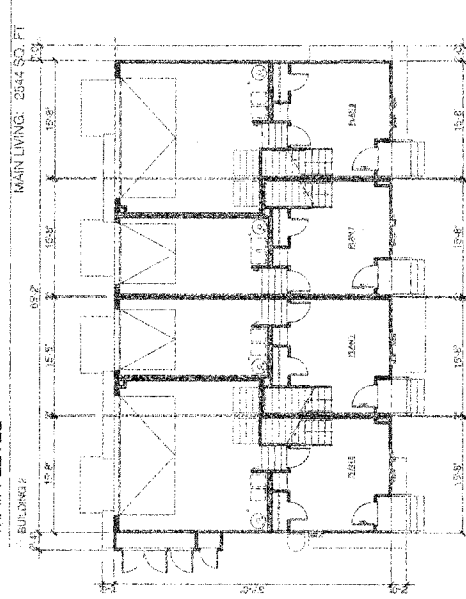
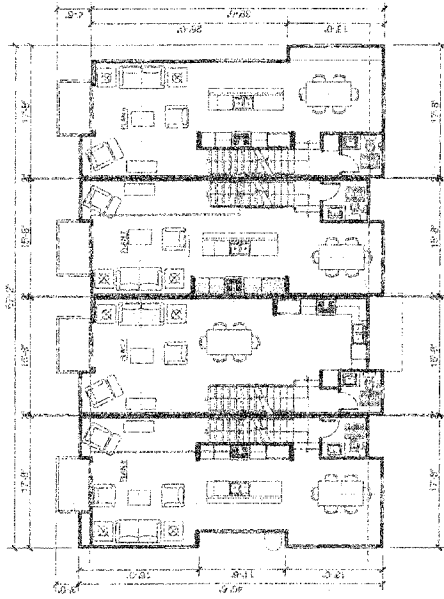
HUNT
HALE
JONES

BUILDING 1 ELEVATIONS

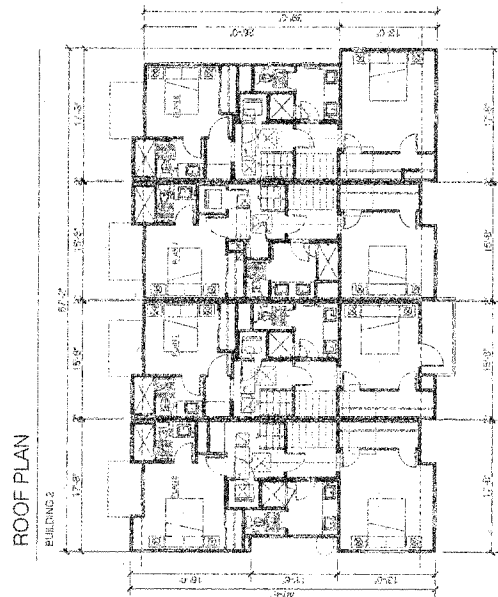
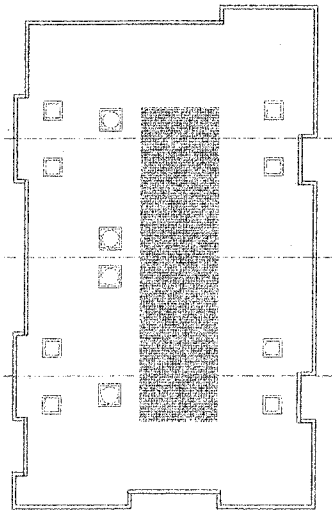
A2.3

SCALE: 1/8" = 1'-0"
DATE: 09/28/2015
PROJECT: 317043

\\fs1317043_oakland_4\Owgs\DR-5D\Owgs\Buildings\Bldg 1\FE1317043_Bldg 1 EE.dwg, 10/5/2015 3:10:07 PM, yanni



BUILDING 2		
NUMBER OF PARKING SPACES: 6	LOWER LIVING: 853 SQ. FT.	
	GARAGE: 1343 SQ. FT.	
	TOTAL LIVING: 6,047 SQ. FT.	



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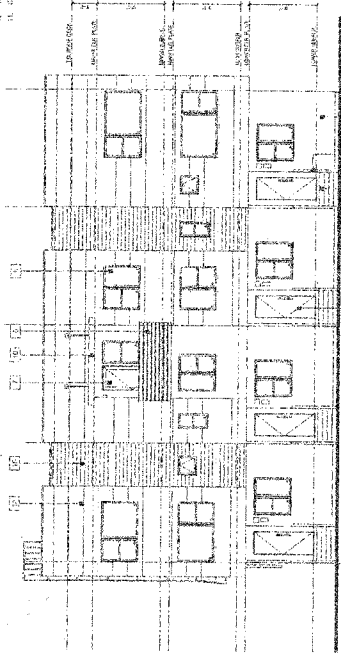
A3.1

SCALE: 1/8" = 1'-0"
DATE: 09/28/2015
PROJECT: 317043

Z:\317043_oakland\ADWG\Bldg-5\DWG\Building\Bldg_2\FP\317043_BLDG 2 FP.dwg, 10/2/2015 3:10:19 PM, yamou

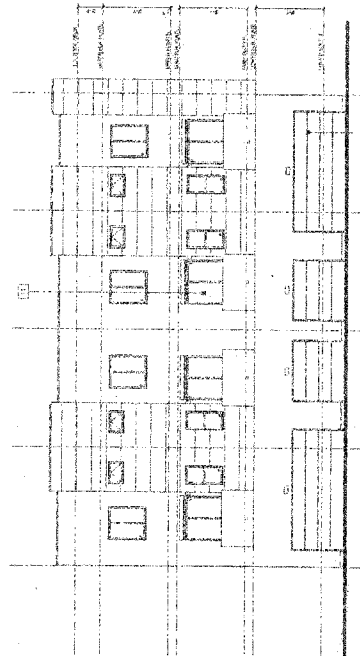
ELEVATION NOTES

1. FINISH: EXTERIOR STONE
2. FINISH: INTERIOR STONE
3. FINISH: INTERIOR WOOD
4. FINISH: INTERIOR PLASTER
5. FINISH: INTERIOR PAINT
6. FINISH: INTERIOR TILE
7. FINISH: INTERIOR GLASS
8. FINISH: INTERIOR METAL
9. FINISH: INTERIOR FABRIC
10. FINISH: INTERIOR LEATHER
11. FINISH: INTERIOR RUG



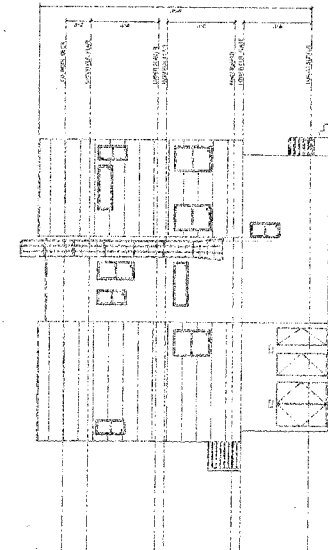
FRONT ELEVATION (WOOD STREET)

BUILDING 2



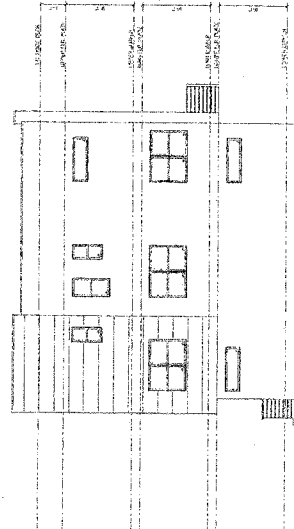
REAR ELEVATION

BUILDING 2



LEFT ELEVATION

BUILDING 2



RIGHT ELEVATION

BUILDING 2

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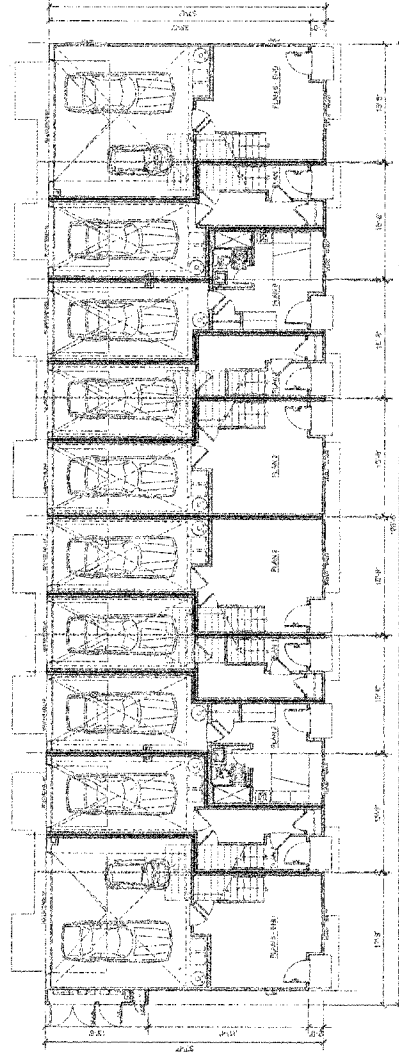
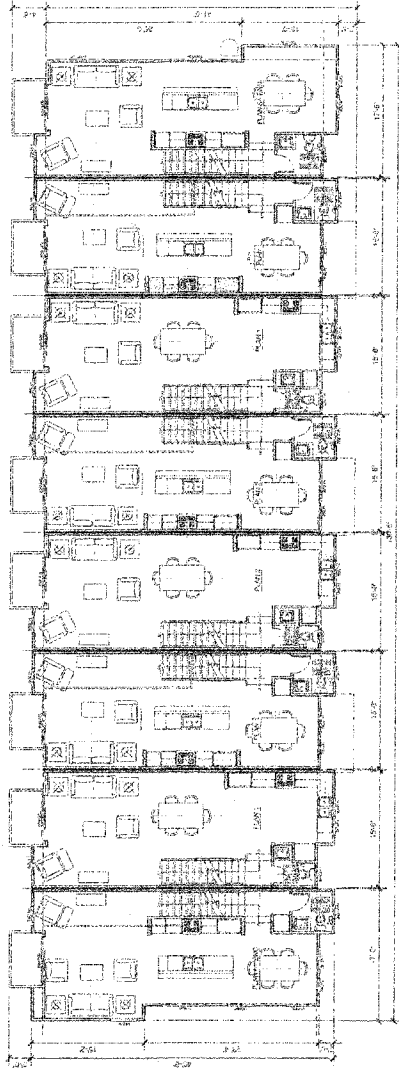
BUILDING 2 ELEVATIONS
A3.2

SCALE: 1/8" = 1'-0"
DATE: 09/28/2015
PROJECT: 317043

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E:\317043_oakland_41Days\DR-SD\Drawings\900g_317043_BLDG 2 ELEV.dwg 10/5/2015 3:10:35 PM, jason



NUMBER OF
PARKING SPACES: 12
LOWER LIVING: 2,007 SQ. FT.
GARAGE: 2,573 SQ. FT.
TOTAL LIVING: 11,883 SQ. FT.

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A4.1

SCALE: 1/8" = 1'-0"
DATE: 08/26/2015
PROJECT: 317043

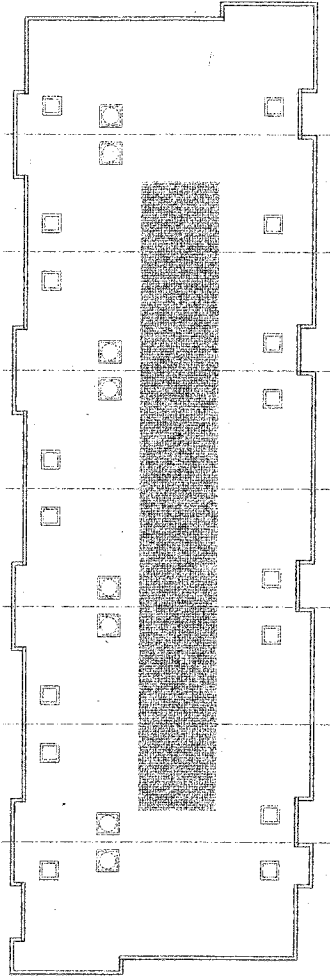
BUILDING 3 GROUND-MAIN

City Ventures

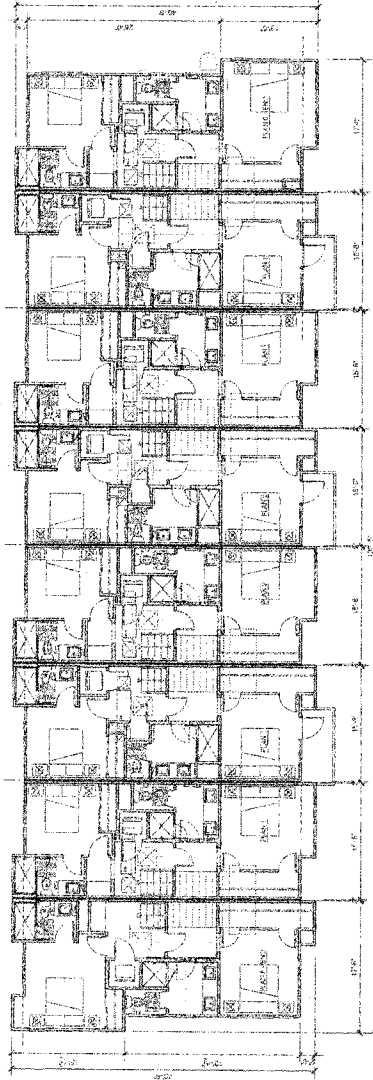


2:\317043_Oakland_4\Drawings\DR-SC\Drawings\Bldg 3\FP\317043_BLDG 3 FP.dwg, 10/5/2015 9:10:48 PM, Yaron

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ROOF PLAN
BUILDING 3



UPPER LEVEL
BUILDING 3

UPPER LIVING - 4,938 SQ. FT.

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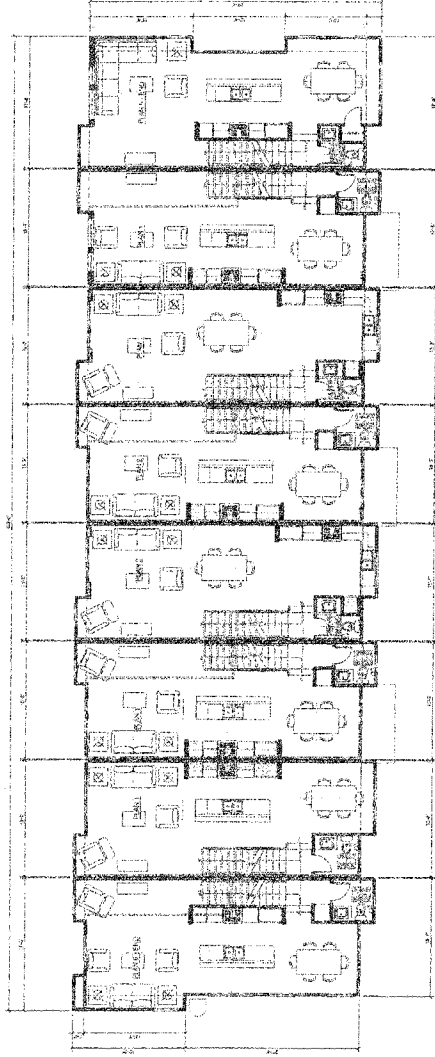
BUILDING 3 UPPER-ROOF
A4.2

SCALE: 1/8" = 1'-0"
DATE: 09/26/2015
PROJECT: 317043

T. 415-512-1300
F. 415-288-0288

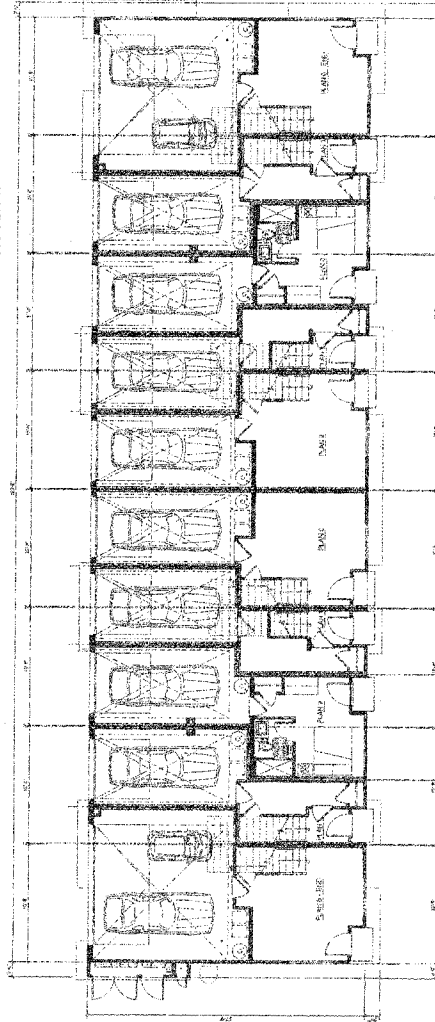
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MAIN LEVEL

MAIN LIVING: 4956 SQ. FT.



GROUND LEVEL

LOWER LIVING: 2,019 SQ. FT.
GARAGE: 2579 SQ. FT.
TOTAL LIVING: 11,945 SQ. FT.

NUMBER OF
PARKING SPACES: 12

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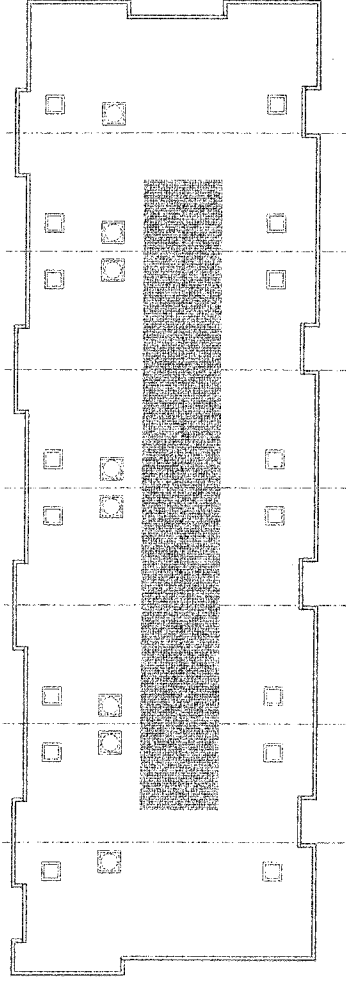
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BUILDING 4 GROUND-MAIN

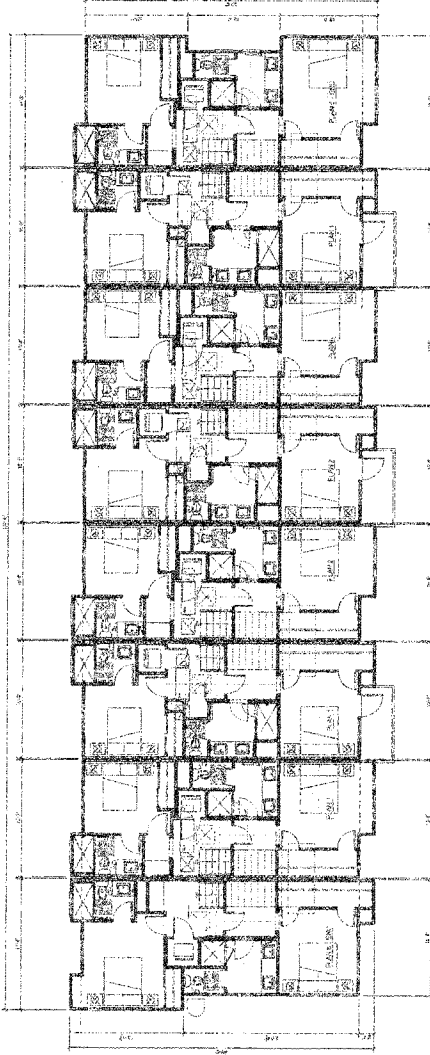
A5.1

SCALE: 1/8" = 1'-0"
DATE: 08/08/2015
PROJECT: 317043



ROOF PLAN

BUILDING 4



UPPER LEVEL

BUILDING 4

UPPER LIVING: 4966 SQ. FT.

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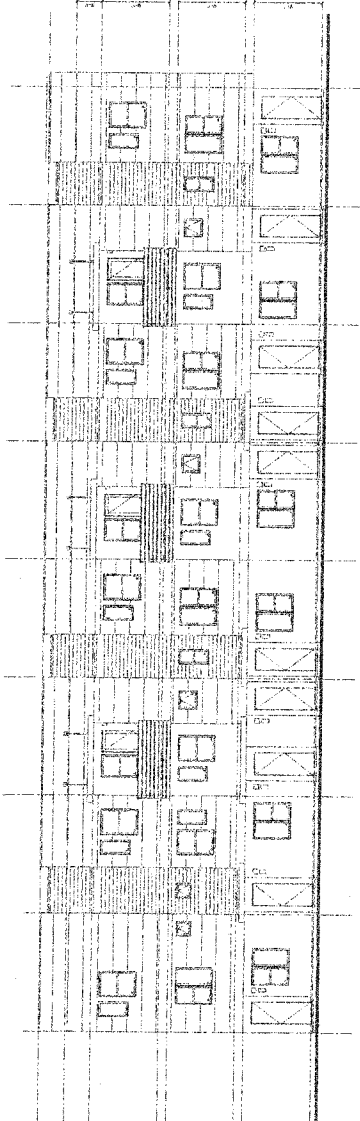
BUILDING 4 UPPER-ROOF

A5.2

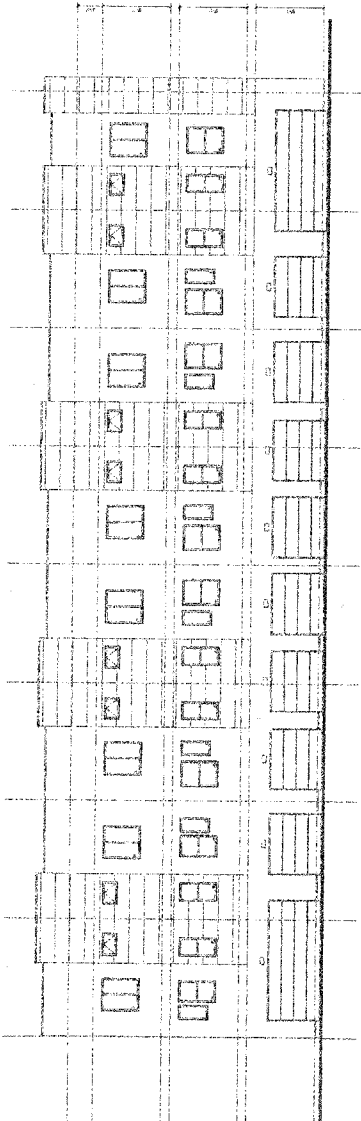
SCALE: 1/8" = 1'-0"
DATE: 09/28/2015
PROJECT: 317043

ELEVATION NOTES

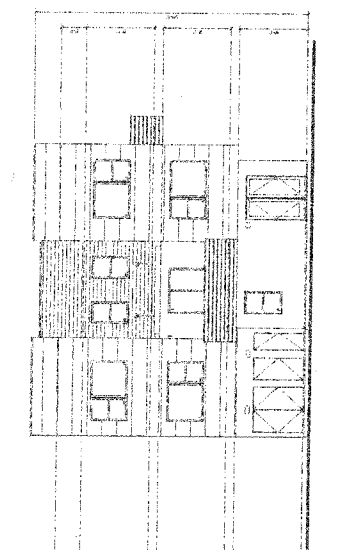
1. FINISHES
 1A. PAINTED STEEL BRACE
 1B. STEEL
 2. FINISH
 3. FINISH
 4. CONCRETE BRIDGE
 5. ROOFING
 6. ROOFING
 7. ROOFING
 8. DRAINAGE
 9. DRAINAGE
 10. DRAINAGE
 11. DRAINAGE



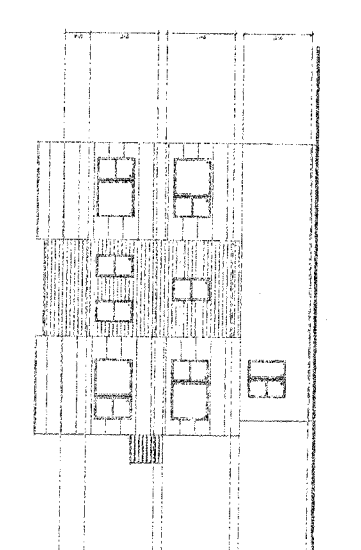
FRONT ELEVATION
BUILDING 4



REAR ELEVATION
BUILDING 4



LEFT ELEVATION
BUILDING 4



RIGHT ELEVATION
BUILDING 4



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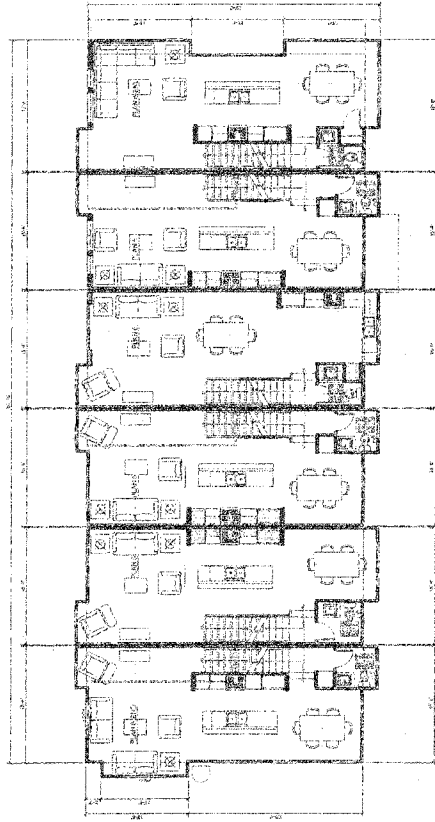
T 415-512-1970
F 415-288-0288

BUILDING 4 ELEVATIONS

A5.3

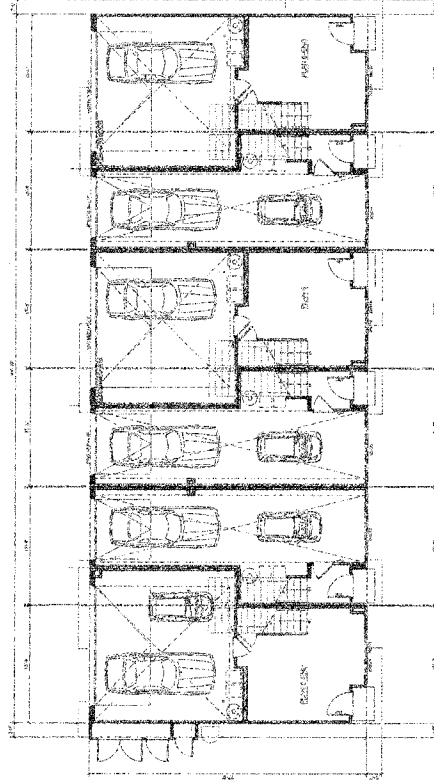
SCALE: 1/8" = 1'-0"
DATE: 06/26/2015
PROJECT: 317043

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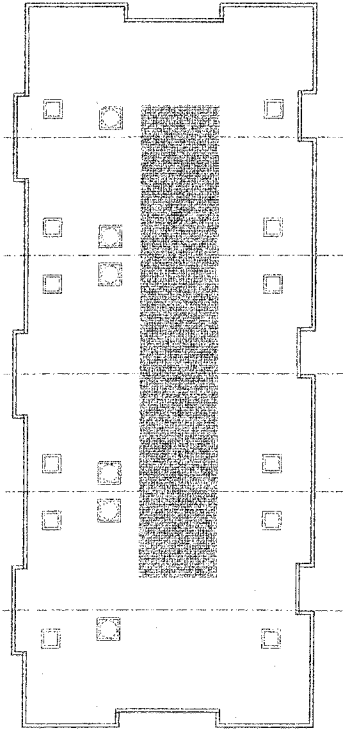
MAIN LEVEL

MAIN LIVING: 3752 SQ. FT.



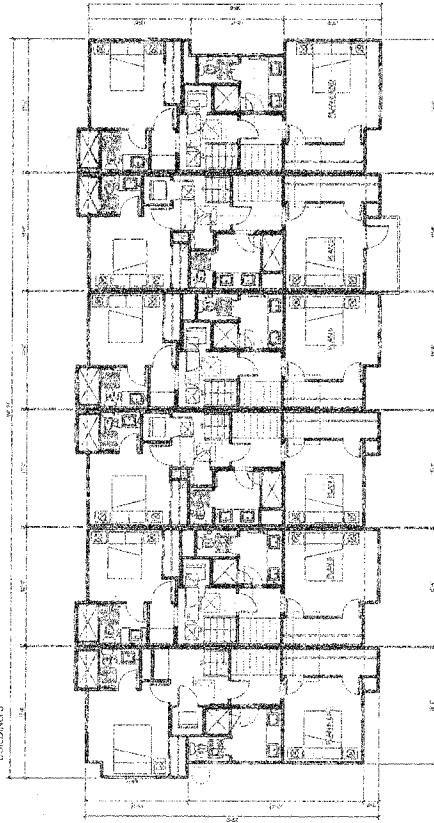
GROUND LEVEL

NUMBER OF
PARKING SPACES: 12
LOWER LIVING: 1,303 SQ. FT.
GARAGE: 2445 SQ. FT.
TOTAL LIVING: 8,501 SQ. FT.



ROOF PLAN

BUILDING 5



UPPER LEVEL

UPPER LIVING: 3746 SQ. FT.

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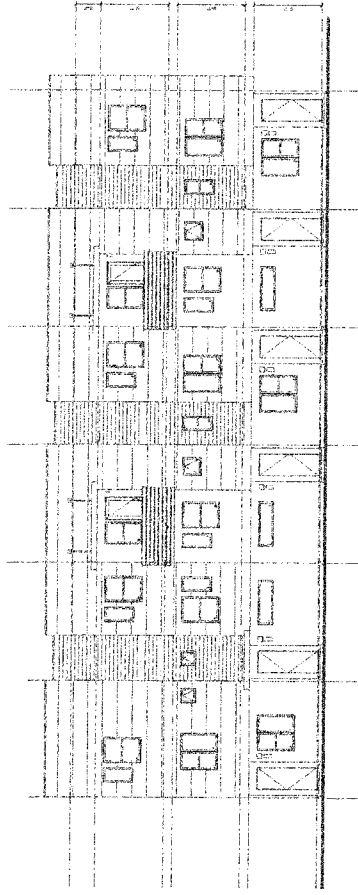
BUILDING 5 GROUND-MAIN-UPPER-ROOF

A6.1

SCALE: 1/8" = 1'-0"
DATE: 09/06/2015
PROJECT: 317043

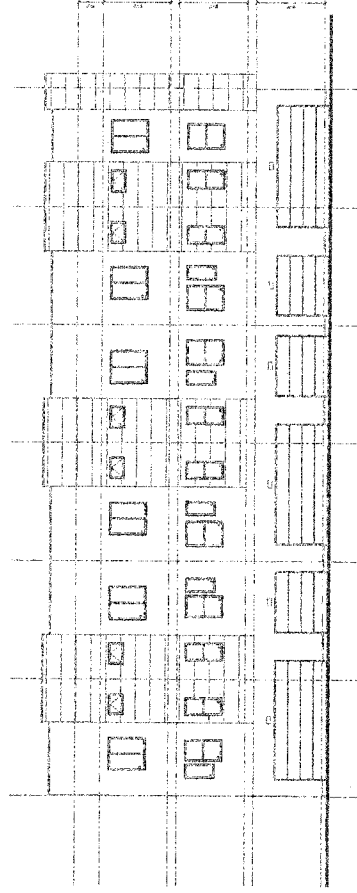
ELEVATION NOTES

1. FINISHES
2. MATERIALS
3. WINDOW SIZES
4. WINDOW TYPES
5. WINDOW PLACEMENT
6. WINDOW FINISHES
7. WINDOW SIZES
8. WINDOW TYPES
9. WINDOW PLACEMENT
10. WINDOW FINISHES



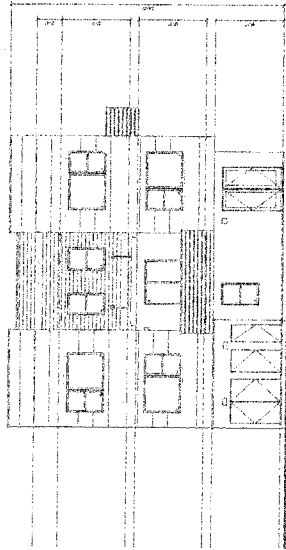
FRONT ELEVATION

BUILDING 5



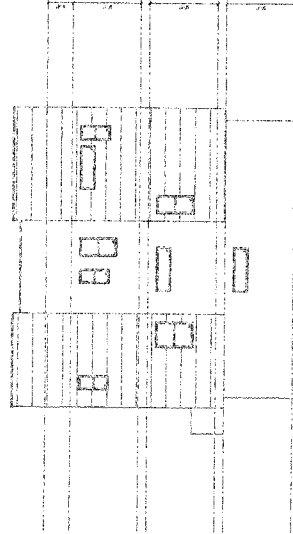
REAR ELEVATION

BUILDING 5



LEFT ELEVATION

BUILDING 5



RIGHT ELEVATION

BUILDING 5

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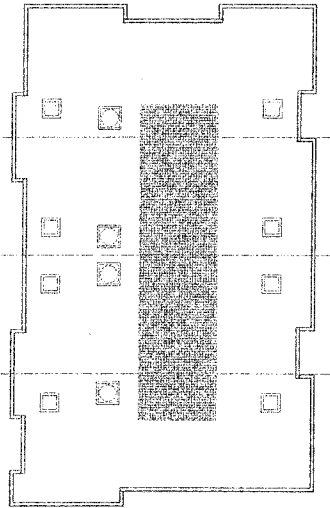
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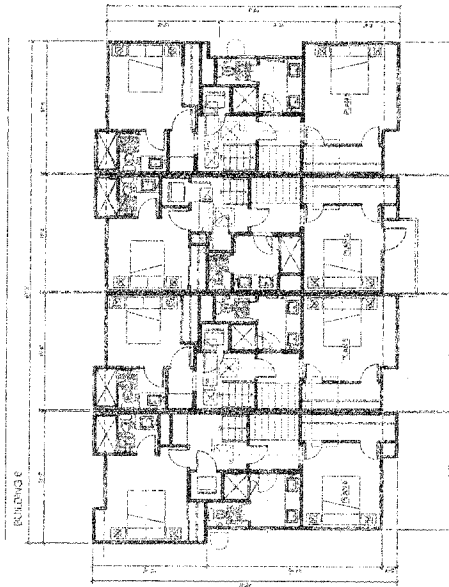
BUILDING 5 ELEVATIONS
A6.2

SCALE 1/8" = 1'-0"
DATE 09/28/2015
PROJECT 317043

2:\317043_cad\dwg\41\dwg\04-50\dwg\Buildings\Bldg 5 ELEV\317043_BLDG 5 ELEV.dwg 10/5/2015 3:12:38 PM jaym

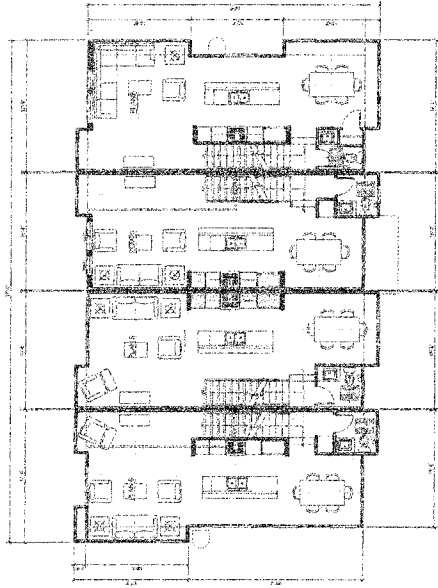


ROOF PLAN

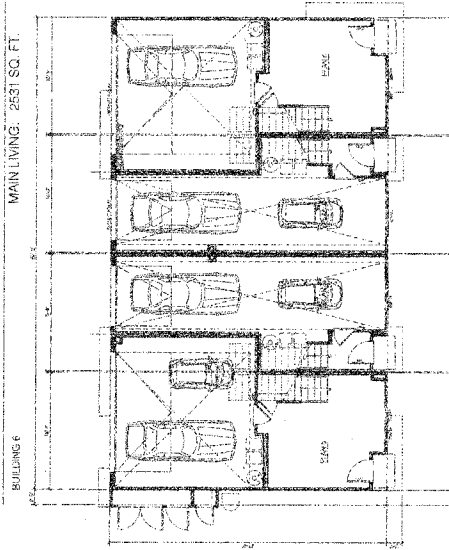


UPPER LEVEL

BUILDING 6
UPPER LIVING: 2520 SQ. FT.



MAIN LEVEL



GROUND LEVEL

BUILDING 6
NUMBER OF
PARKING SPACES: 8
LOWER LIVING: 668 SQ. FT.
GARAGE: 1628 SQ. FT.
TOTAL LIVING: 8,723 SQ. FT.

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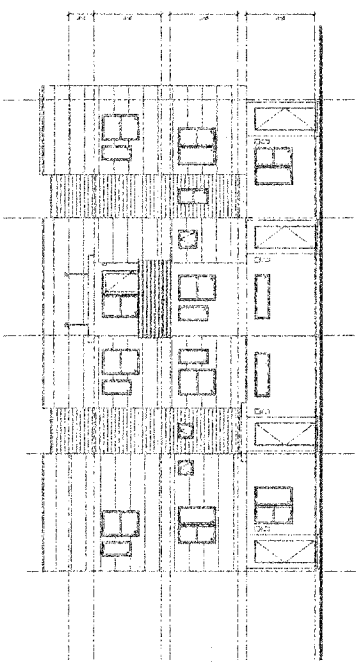
BUILDING 6 GROUND-MAIN-UPPER-ROOF

A7.1

SCALE: 1/8" = 1'-0"
DATE: 09/20/15
PROJECT: 317043

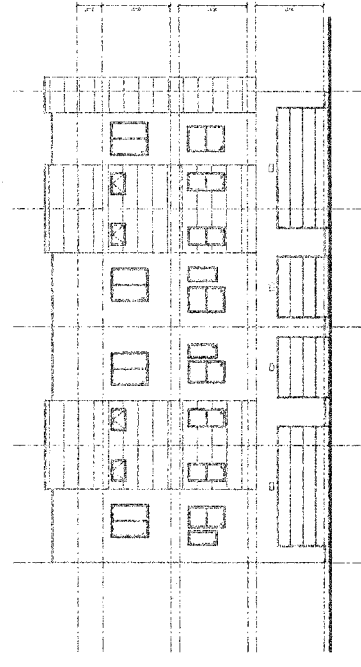
ELEVATION NOTES

- 1. FINISHES: EXTERIOR: STUCCO, INTERIOR: GYP. BOARD
- 2. EXTERIOR: STUCCO, INTERIOR: GYP. BOARD
- 3. EXTERIOR: STUCCO, INTERIOR: GYP. BOARD
- 4. EXTERIOR: STUCCO, INTERIOR: GYP. BOARD
- 5. EXTERIOR: STUCCO, INTERIOR: GYP. BOARD
- 6. EXTERIOR: STUCCO, INTERIOR: GYP. BOARD
- 7. EXTERIOR: STUCCO, INTERIOR: GYP. BOARD
- 8. EXTERIOR: STUCCO, INTERIOR: GYP. BOARD



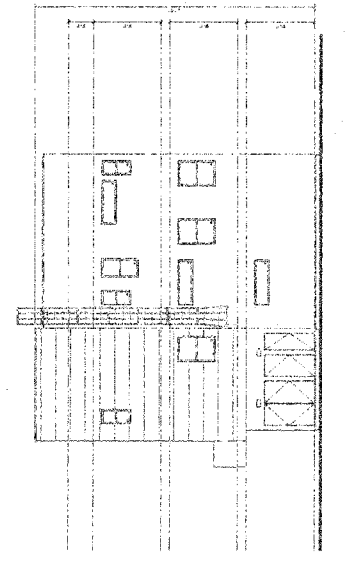
FRONT ELEVATION

BUILDING 6



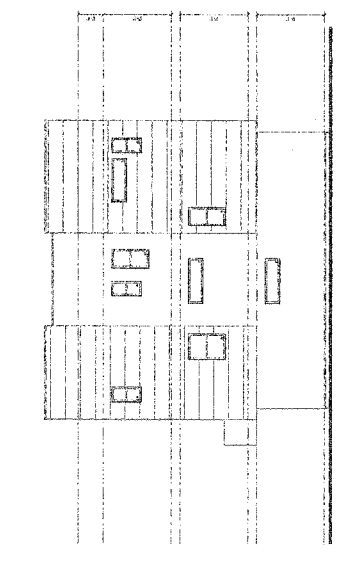
REAR ELEVATION

BUILDING 6



LEFT ELEVATION

BUILDING 6



RIGHT ELEVATION

BUILDING 6

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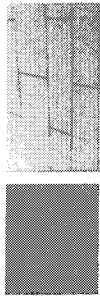


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BUILDING 6 ELEVATIONS
A7.2

SCALE: 1/8" = 1'-0"
DATE: 09/02/2015
PROJECT: 317043



1. Cypresside
SW 7076



2. Painted Current Board

3. Heavenly White
SW 6553



4. Something Blue
SW 6800



5. Cedar
SW 6900



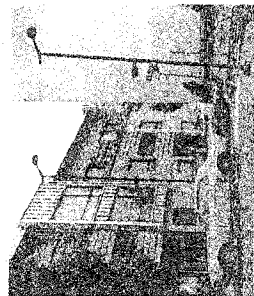
6. Sluice



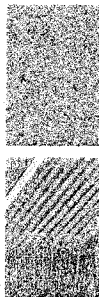
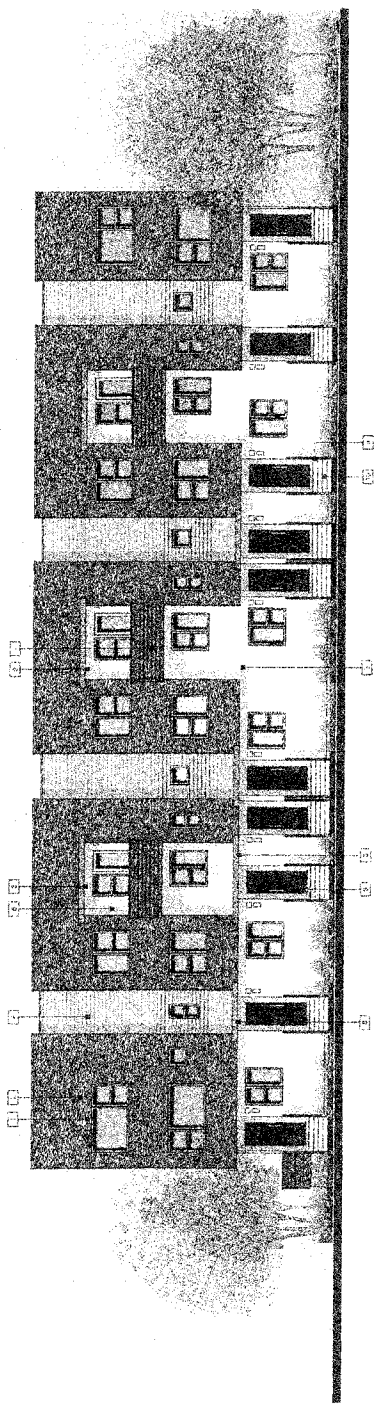
7. Conceptual Balcony



8. Conceptual Canopy



Lighting



9. Conceptual Sloop Wall to Concrete



Conceptual Foundation Planting



Conceptual Streetscape Planting



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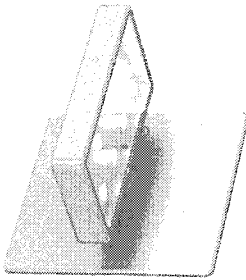
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MATERIAL AND COLOR BOARD 1

A8.1

SCALE: NTS
DATE: 09/28/2015
PROJECT: 317043



EON WALL FIXTURE

Proposed garage door light fixture

- Lumiere
- White or Black Finish
- LED

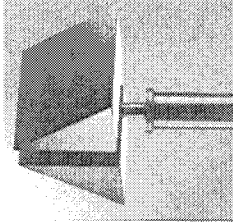


TUBE ARCHITECTURAL MODEL

Proposed garage door light fixture

- WAC Lighting
- White or Black Finish
- LED

ON SITE BUILDING MOUNTED



SHAPER POST TOP 982

Proposed for Pedestrian Walks and Courtyards

- Cooper Lighting
- Black Finish

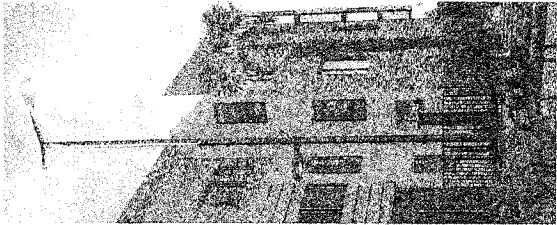


BK NITE STAR II

Proposed uplighting for specimen oak trees

- BK Lighting
- Black Finish
- LED

ON SITE OPEN SPACE

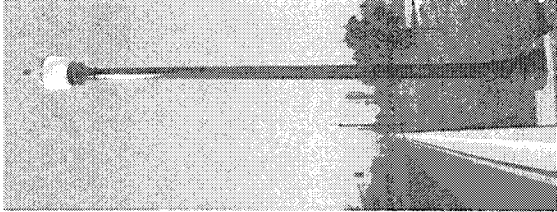


ICON SLIDE LUMINAIRE

Proposed for Wood Street, Prescott Street, and Pullman Way.

- Cooper Lighting
- Dark Green Finish
- LED
- Currently used on Wood Street and 14th Street
- Approved by City of Oakland Street Light Design Manual issued by PWA 2010

OFF SITE



WASHINGTON LUMINAIRE

Proposed for 18th Street and Pocket Park*

- Lumie
- Black Finish
- Currently used on Mandela Parkway in dark green finish
- Approved by City of Oakland Street Light Design Manual issued by PWA 2010

*Alternate for this location would be Icon Slide Luminaire—fixture was selected to be contextual adjacent to the train station

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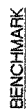
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MATERIAL AND COLOR BOARD 1

A8.2

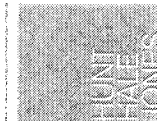
SCALE: NTS
DATE: 09/28/2015
PROJECT: 317043



THE CITY OF OAKLAND STREET MEASUREMENT, 237E - OAKLAND - 1953, AT THE INTERSECTION OF WOOD STREET AND 10TH STREET, ELEVATION 10.44 FEET. CITY OF OAKLAND DATUM, WAS USED AS THE BENCHMARK TO ESTABLISH THE ELEVATIONS SHOWN ON THIS SURVEY.

BASIS OF BEARINGS

THE BEARING OF N41°06'31"E, ON THE LINE BETWEEN CITY OF OAKLAND, STREET ALIGNMENT MONUMENTS AT THE INTERSECTION OF WOOD STREET AND 15TH STREET, AND WOOD STREET AND 10TH STREET, WAS USED AS THE BASIS OF EVIDENCE FOR THIS SURVEY. THIS BEARING WAS TAKEN FROM PRELIMINARY CALIFORNIA RECORD OF SURVEY MONUMENTATION MAP DATED FOR STATE ROUTE 880 ALAMEDA COUNTY, RECORD OF SURVEY NO. 182.



 **SANDIS**
CIVIL ENGINEERS
SURVEYORS
PLANNERS

465 Maple Street
Oakland, California
94612-3138
Phone: 415.763.8800
www.sandis.net



City Ventures

SIGNALHOUSE

CITY VENTURES

WOOD ST & 18TH ST

OAKLAND, CA

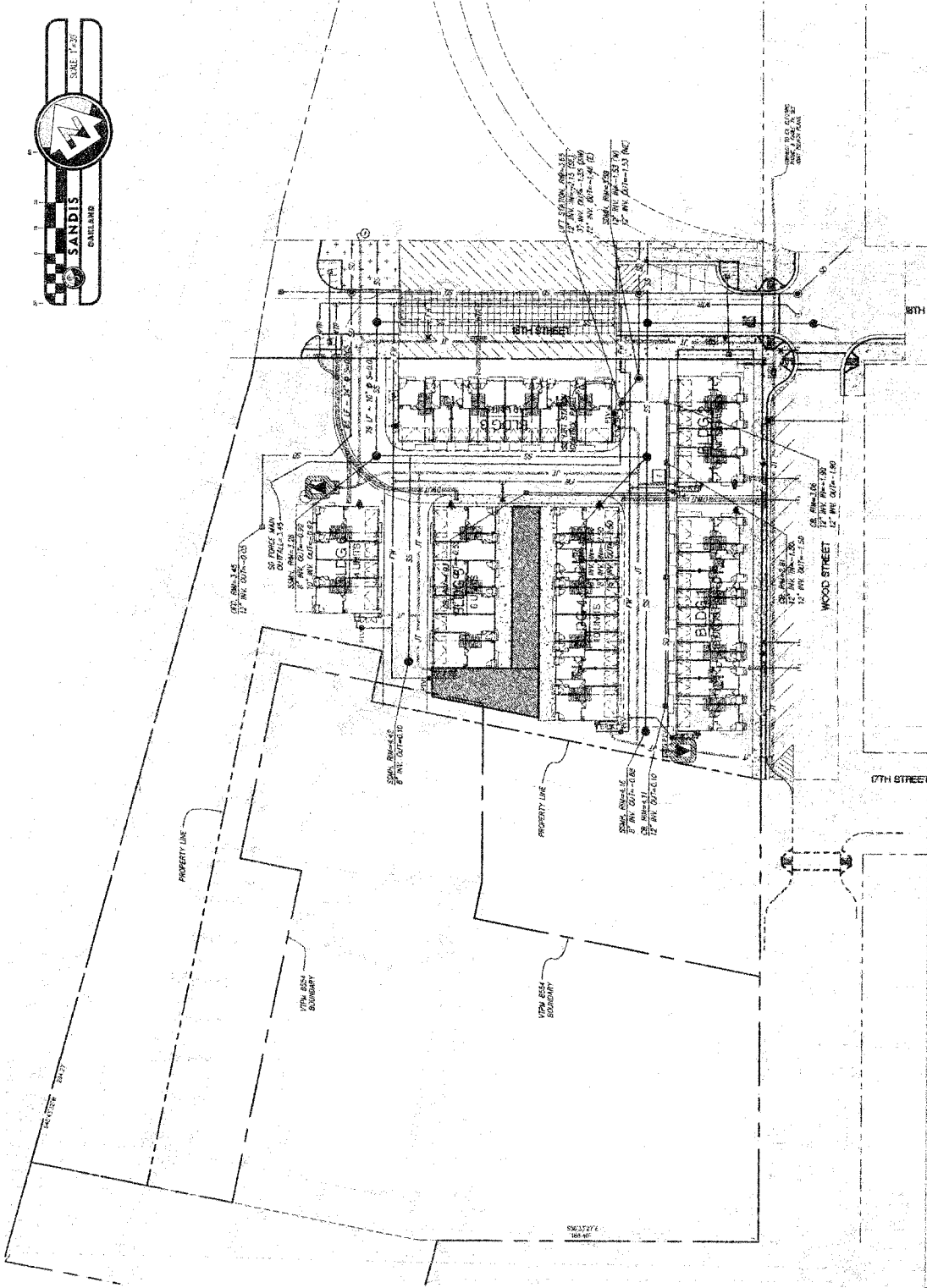
STANLEY HARTMAN & SONS, INC.

EXISTING CONDITIONS

C-0.01

SCALE: 1"=30'
DATE: 09.28.2015
PROJECT: 317043

EMAIL: info@openstax.org



SHEET NOTES

- 1. ONLY THROUGH INCLUDES: TELECOM, ELECTRICAL, AND SWEETENINGS.

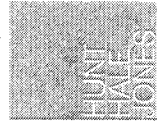
COORDINATION NOTES

- 1. DESIGN TEAM TO COORDINATE WITH LOCATIONS BETWEEN BUILDINGS 1 & 2.

GENERAL NOTES

- 1. WATER SYSTEM: WATER IMPROVEMENTS INCLUDE A 10\"/>
- 2. SANITARY SEWER SYSTEM: SANITARY SEWER IMPROVEMENTS INCLUDE A 10\"/>
- 3. STORM DRAIN SYSTEM: STORM DRAINAGE IMPROVEMENTS INCLUDE A 10\"/>
- 4. ELECTRICAL/TELECOM SYSTEMS: ELECTRICAL AND COMMUNICATION IMPROVEMENTS INCLUDE A 10\"/>

SIGNALHOUSE
CITY VENTURES
WOOD ST & 18TH ST
OAKLAND, CA



ONSITE UTILITY PLAN
C-2.01
SCALE: 1"=80'
DATE: 09.28.2015
PROJECT: 317043



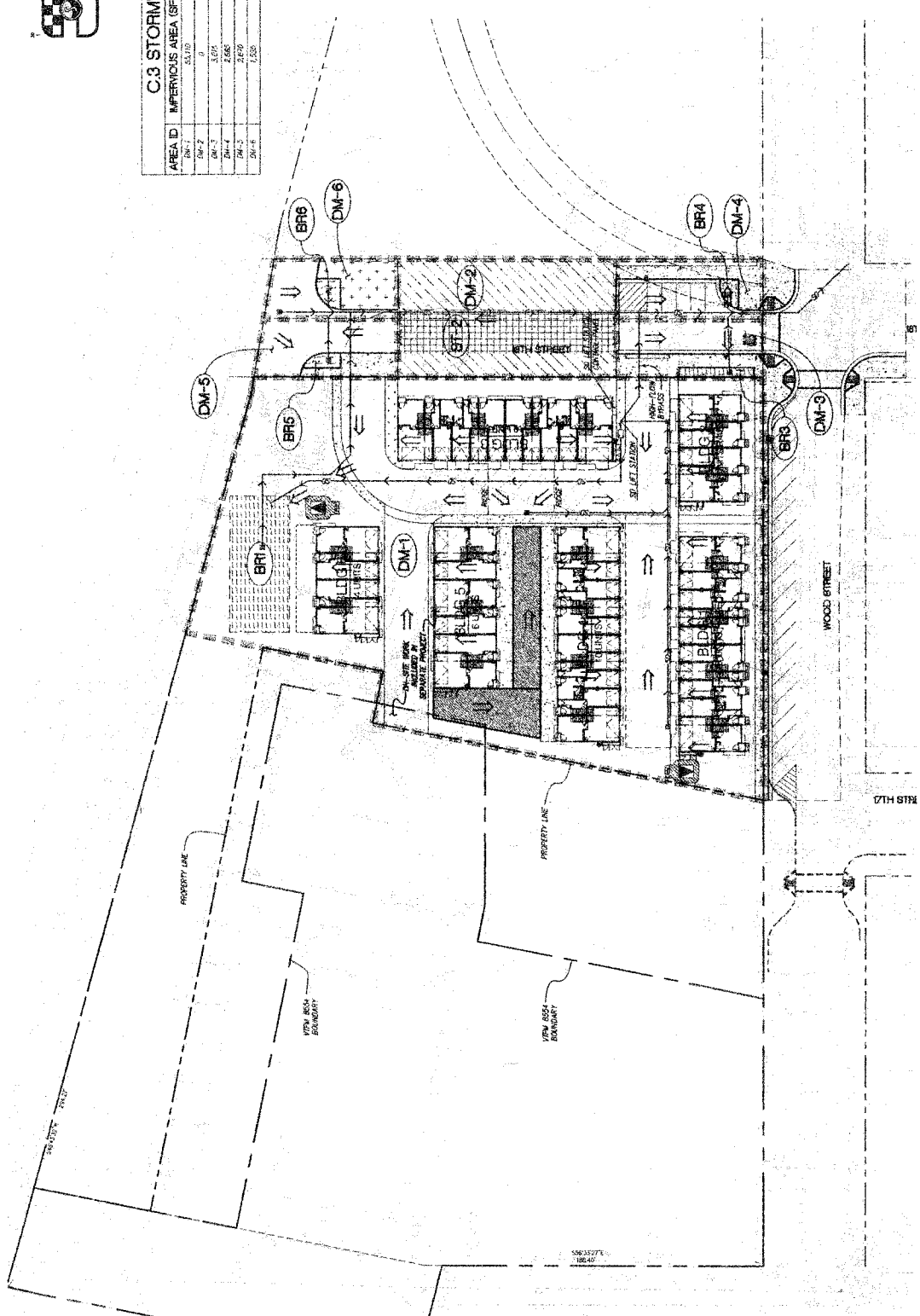
C-3 STORMWATER TREATMENT MEASURES				
AREA ID	INTERVIOUS AREA (SF)	REQUIRED BMP AREA (SQ)	BMP ID	BMP AREA PROVIDED (SF)
DM-1	53,100	2,700	BR1	2,800
DM-2	0	0	BR2	14,500 (SEE NOTE 4 BELOW)
DM-3	3,074	154	BR3	200
DM-4	2,685	135	BR4	144
DM-5	24,700	1,235	BR5	1,513
DM-6	1,520	76	BR6	200

GENERAL NOTES

- THIS PLAN PROVIDES METHODS FOR FULFILLING THE REQUIREMENTS FOR COMPLIANCE WITH THE CALIFORNIA WATER PROGRAM STORMWATER QUALITY CONTROL ACT (CWA).
- THE REQUIRED BMP AREA AND THE REQUIRED 1:1 RATIO ARE BASED ON THE 50 PERCENT OF EXISTING IMPERVIOUS AREA, RESULTING IN AN ALLOCATION OF 100 PERCENT OF THE REQUIRED BMP AREA TO THE EXISTING IMPERVIOUS AREA.
- WOOD STREET IS EXCLUDED FROM STORMWATER TREATMENT TREATMENT IS NOT REQUIRED FOR PARKING RESERVINGS WITHIN EXISTING LOT/LOT.
- DRAINAGE AREA DM-2 IS A SELF-TREATING PREVIOUS AREA.

LEGEND

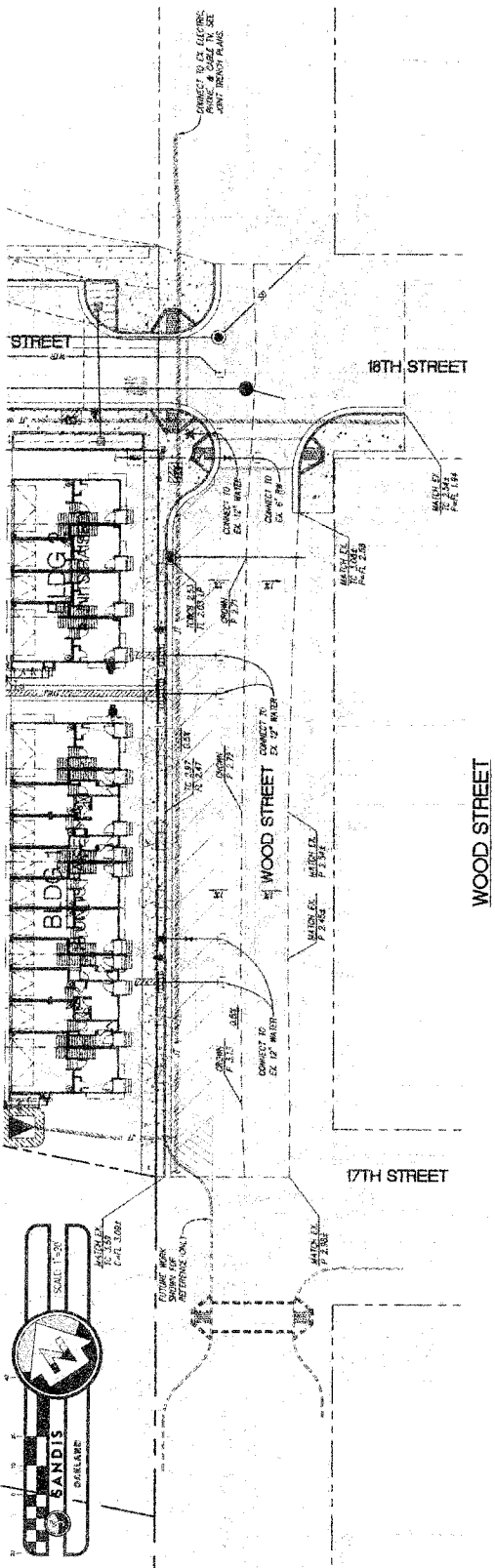
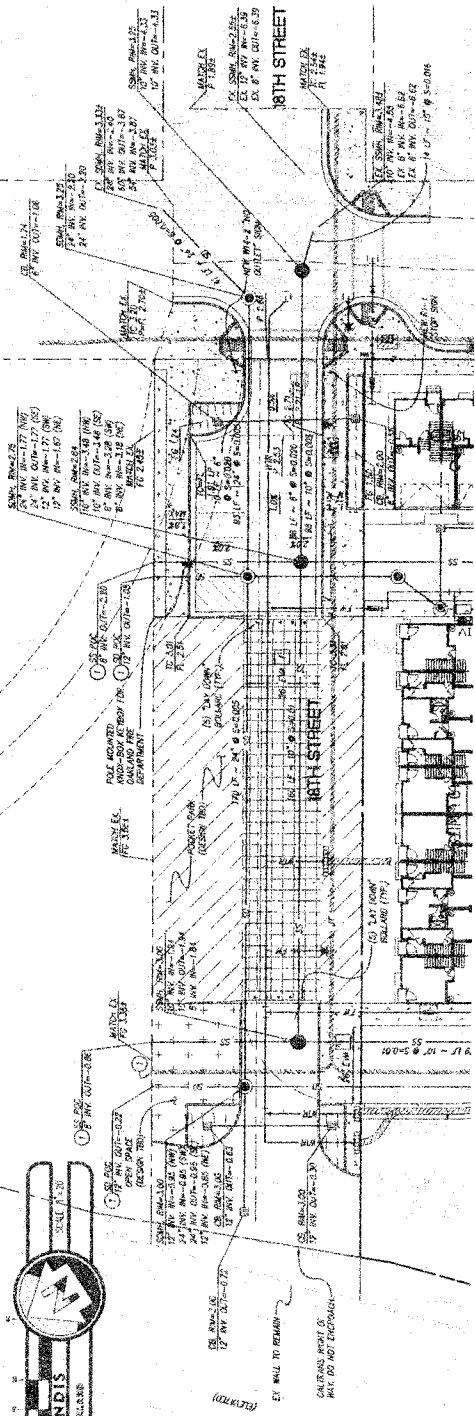
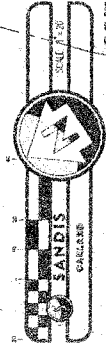
- BR# ROOF/PAVEMENT AREA
ST# SELF-TREATING AREA
DM# DRAINAGE MANAGEMENT AREA
- AC PAVEMENT
CONCRETE SETBACK
PERMEABLE TREATMENT AREA
COURTYARD CONCRETE
OPEN SPACE
GRASS/VEGETATION AREA LIMIT
FLOW DIRECTION



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OAKLAND, CA

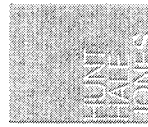
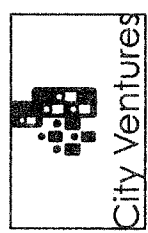


STORMWATER MANAGEMENT PLAN
C-3.01
SCALE: 1"=50'
DATE: 08-28-2015
PROJECT: 317043



- LEGEND**
- PROPERTY LINE
 - EXISTING LINE
 - PROPOSED LINE
 - AC PAVEMENT
 - CONCRETE CURB
 - STANDARD DUTILE S-1
 - PAVING
 - 800-RAINWATER AREA
 - OPEN SPACE
- COORDINATION NOTES**
- 1. ALL EXISTING UTILITY ACCESS MATERIAL SHALL BE ALL-WATER DRAINING MATERIAL WITH BEARING CAPACITY OF 75,000 POUNDS.
- SHEET NOTES**
- 1. SEE UTILITY FUTURE DEVELOPMENT

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OAKLAND, CA



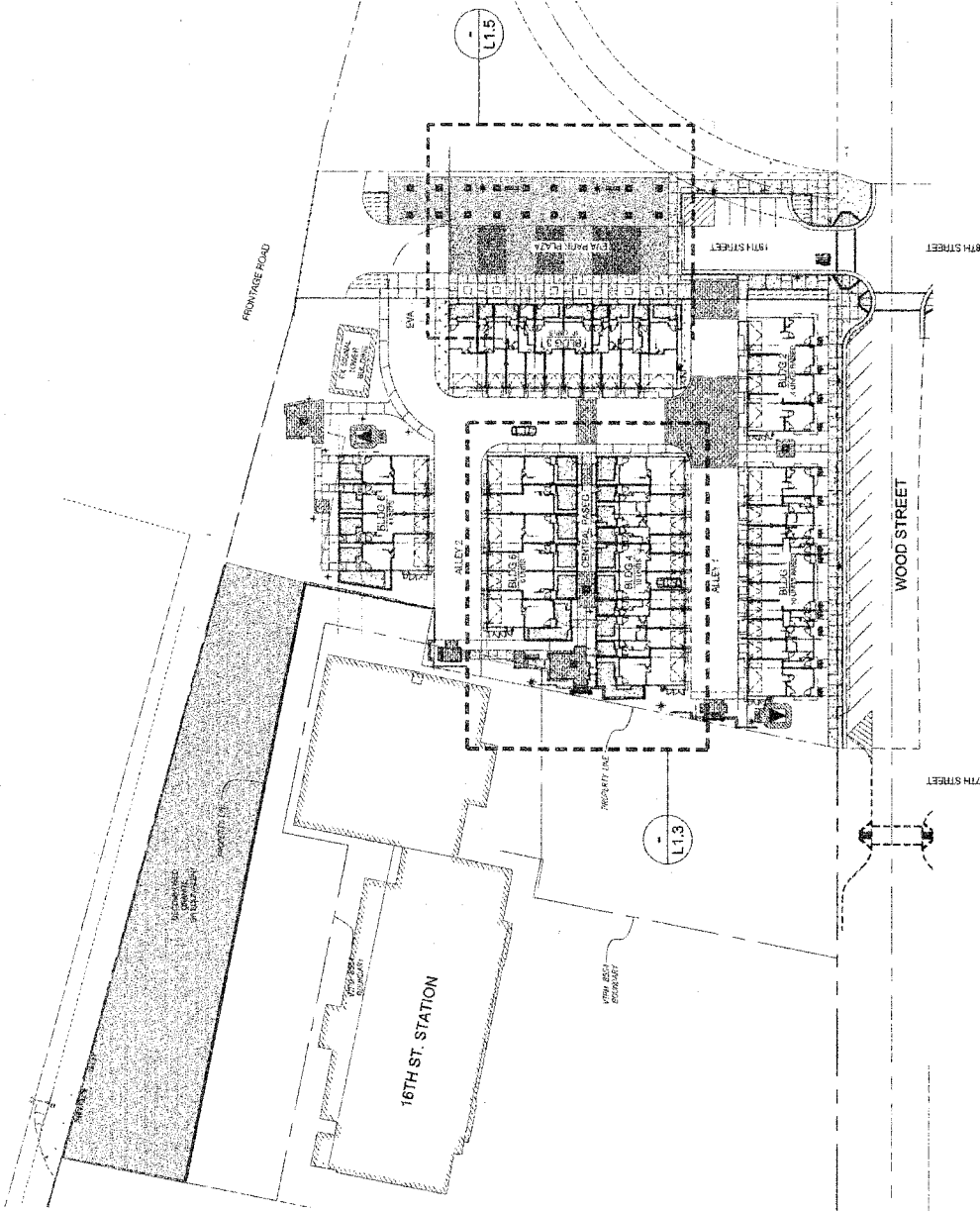
OFFSITE
GRADING AND UTILITY PLAN
C-4.01
SCALE: 1"=20'
DATE: 08/28/2015
PROJECT: 317043

- ALL THE PLANT MATERIALS ARE LOW WATER USE & DROUGHT TOLERANT SPECIES
- THE IRRIGATION SYSTEM IS INTENDED TO BE USED TO ESTABLISH NEW PLANT MATERIAL ONLY. ONCE THE PLANT MATERIAL HAD BECOME ESTABLISHED, THE IRRIGATION SYSTEM SHALL BE TURNED OFF.



MATERIALS LEGEND

- CONCRETE SIDEWALK
- DECORATIVE CONCRETE PAVING
- DECORATIVE PAVING
- DECOMPOSED GRANITE PAVING
- NO MOW/LOW WATER USE GRASS
- GRASS PAVE/GRAVEL PAVE (EVA)
- CONCRETE PLANTER
- BIKE RACK
- SEAT WALL
- BENCH
- STREET LIGHT
- BOLLARD LIGHT
- TREE WELL
- TREE GRATE
- WOOD FENCE PANEL
- LOW STUCCO WALL
- ARCH GATEWAY & PILASTERS
- PRIVATE PATIO FENCE



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LANDSCAPE MATERIALS PLAN

SCALE: 1" = 30'-0"

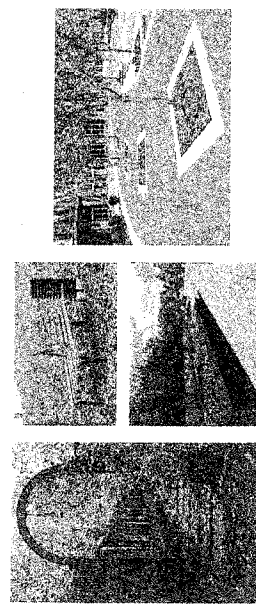
DATE: 10/05/15



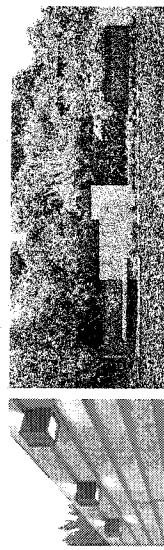
L1.2



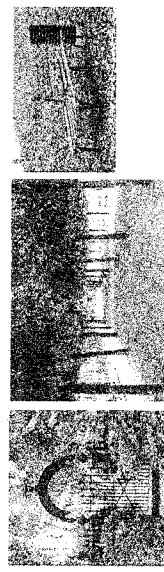
ALLEY 2 TERMINUS:
MATERIALS: DECORATIVE PAVERS, WOOD & METAL BENCHES
PLANTING: FLOWERING ACCENT ORNAMENTAL TREES IN DECOMPOSED GRANITE



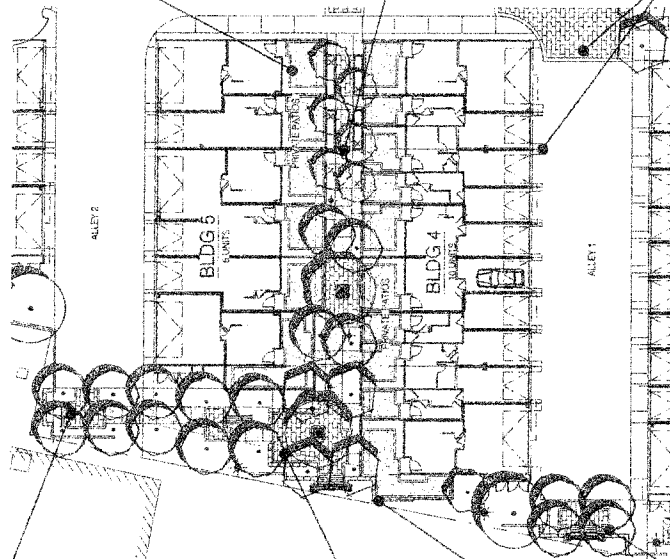
PRIVATE COURTYARD:
MATERIALS: DECORATIVE PAVERS, STEEL TREE GRATES, WOOD AND METAL BENCH
PLANTING: SINGLE SPREADER FLOWERING GRAPES, SINGLE TREE & MEDIUM EVERGREEN TREES AT CORNERS



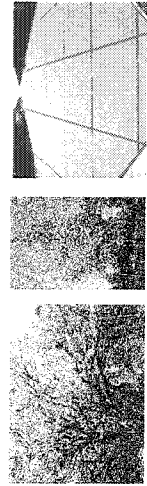
ACCENT WALLS & FENCE PANELS
MATERIALS: ALTERNATIVE LOW STUCCO WALL AND WOOD FENCING PANELS
PLANTING: ALTERNATING BUNCHES OF EVERGREEN FOUNDATION SHRUBS & FLOWERING VINES ON FENCING



PRIMARY GATEWAY AT ALLEY 1:
MATERIALS: STEEL ARCH GATEWAY & PLASTER, DECORATIVE PAVERS, WOOD & METAL BENCHES
PLANTING: FLOWERING ACCENT ORNAMENTAL TREES IN DECOMPOSED GRANITE



PRIVATE PATIOS:
MATERIALS: DECORATIVE SURF-RED CONCRETE PAVING, SIDEWALK SCREEN FENCING & 30" HIGH CONCRETE PLANTER
PLANTING: VERTICAL SCREEN SHRUBS AT SIDEWALKS AND IN PLANTERS, LOW ORNAMENTAL BASE SHRUBS AT PATIOS



PASEO:
MATERIALS: ALTERNATIVE DECORATIVE CONCRETE PAVING, 12" SQUARE PLASTER & CONCRETE PLANTERS
PLANTING: DECIDUOUS CO. LAMINAR TREES IN PLANTERS, SPREADING CANOPY SHADE TREE IN COURTYARD WITH FOUR FLOWERING SECONDARY COURTYARD ACCENT TREES



ALLEYS:
MATERIALS: DECORATIVE VERTICAL CONCRETE PAVING AT INTERSECTIONS AND CROSSWALKS
PLANTING: SHRUB STANDARDS IN DRIVEWAY POCKET WITH BASE PLANTING AT GROUND PLANE

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LANDSCAPE ENLARGEMENT-CENTRAL PASEO















SCALE: 1/16" = 1'-0"

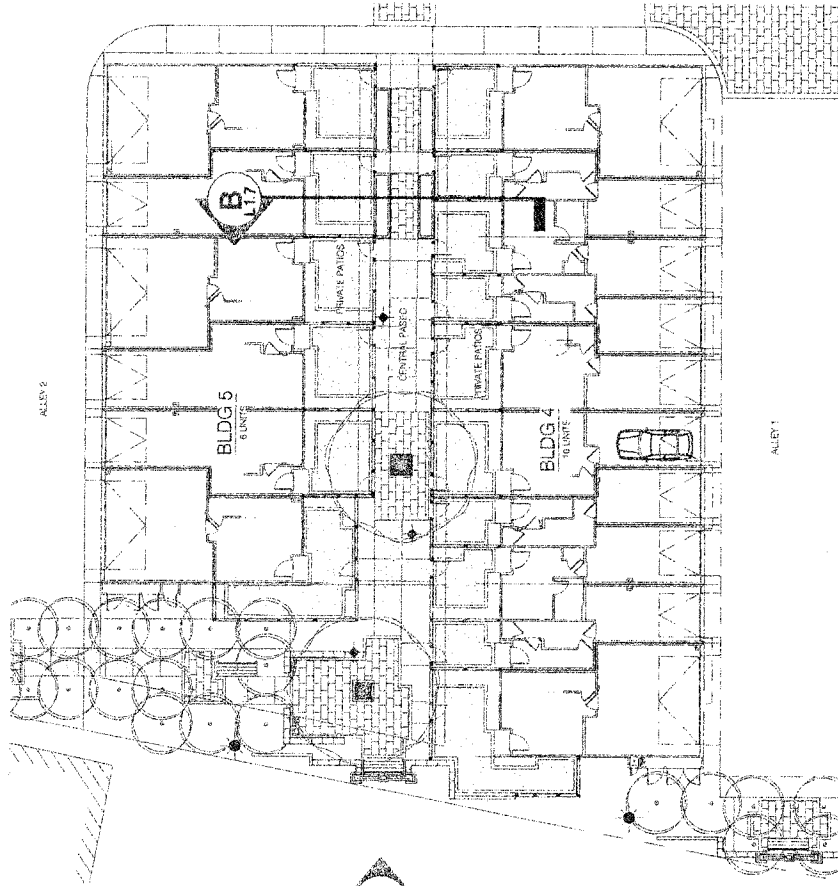
DATE: 10/05/15



L1.3

MATERIALS LEGEND

	CONCRETE ROADWALK
	DECORATIVE CONCRETE PAVING
	DECORATIVE CONCRETE PAVERS
	DECOMPOSED GRANITE PAVING
	CONCRETE PLANTER
	SEAT WALL
	BENCH
	POLE LIGHT
	BOLLARD LIGHT
	TREE SPOKE
	WOOD FENCE PANEL
	LOW STUDIO WALL
	ARCH GATEWAY & PLANTERS
	3' TALL PRIVATE PATIO FENCE

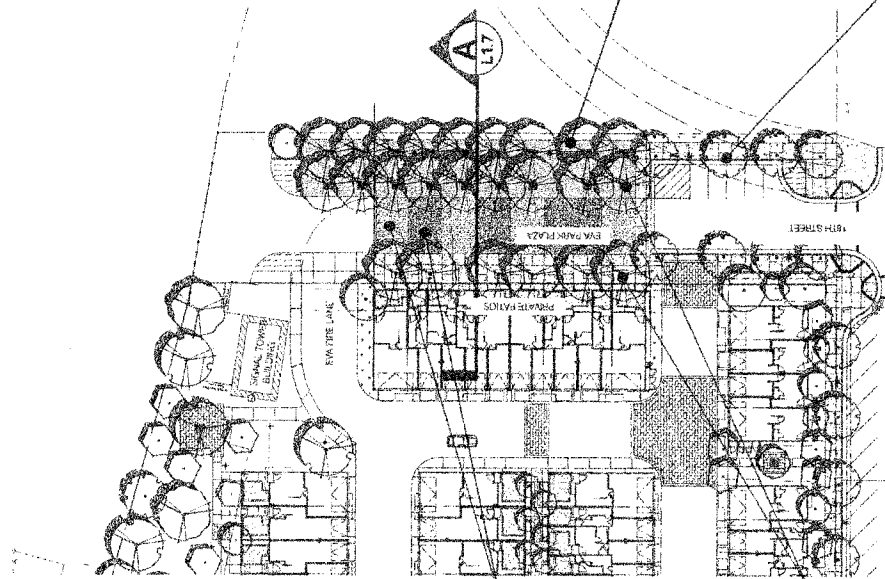


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OAKLAND, CALIFORNIA

MATERIALS ENLARGEMENT PLAN - CENTRAL PASEO
SCALE: 1" = 10'-0"
DATE: 10/05/15

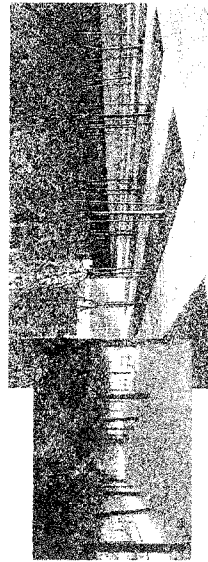
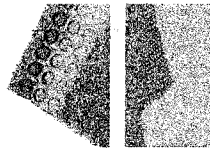


L1.4



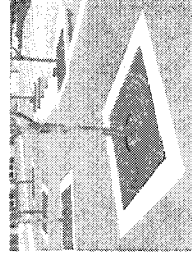
HARDSCAPE:

MATERIALS: ALTERNATING BANDS OF GRASS PAVE AND GRAVEL PAVE IN EVA



SOFTSCAPE:

PLANTING: ALTERNATING CO. JUNIPER, ORNAMENTAL TREES AND LARGE CANOPY SHADE TREES



SITE FURNITURE:

MATERIALS: WOOD AND METAL BRUSHED TREE GRATES



PARKING AREA:

MATERIALS: WASHINGTON LUMBER FOR 18TH STREET AND EVA PARK PLAZA
PLANTING: SHADE TREES IN TREE WELLS AT PARKING STALLS

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














LANDSCAPE ENLARGEMENT- EVA PARK/ PLAZA

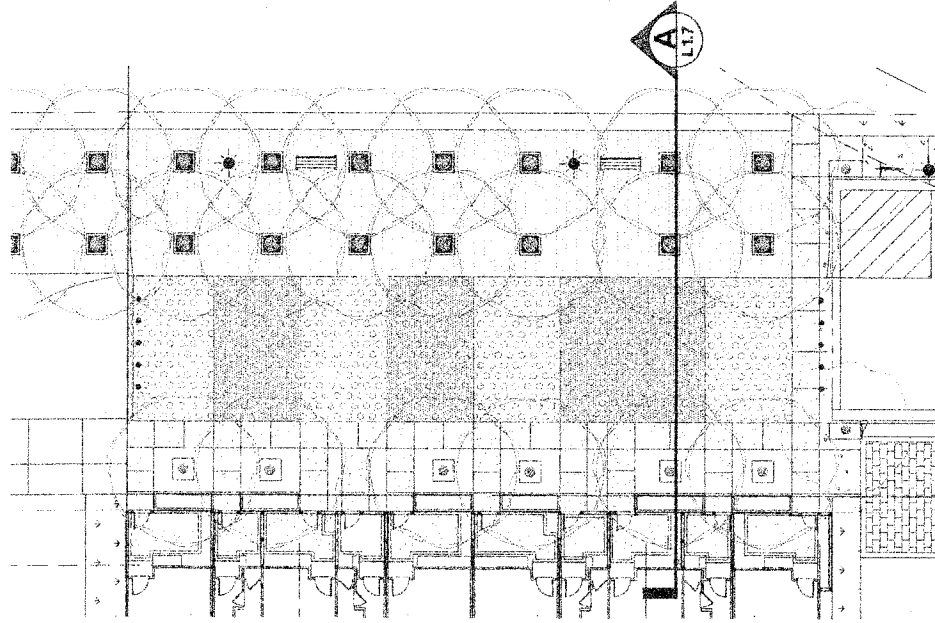
DATE: 10/05/15

SCALE: 1/16" = 1'-0"

L1.5

MATERIALS LEGEND

	CONCRETE SIDEWALK
	DECORATIVE CONCRETE PAVING IN PRIVATE PATIOS
	DECORATIVE CONCRETE PAVERS
	DECOMPOSED GRANITE PAVING ON GRADE
	DECOMPOSED GRANITE PAVING ON GRAVEL PAVE SUB-GRADE
	NO MOW/LOW WATER USE GRASS
	LOW WATER USE GRASS ON GRASS PAVE SUB-GRADE
	CONCRETE PLANTER
	BIKE RACK
	SEAT WALL
	BENCH
	POLE LIGHT
	TREE GRATE
	TREE WELL
	3' TALL PRIVATE PATIO FENCE
	"LAYDOWN" BOLLARDS PER CIVIL ENG PLANS



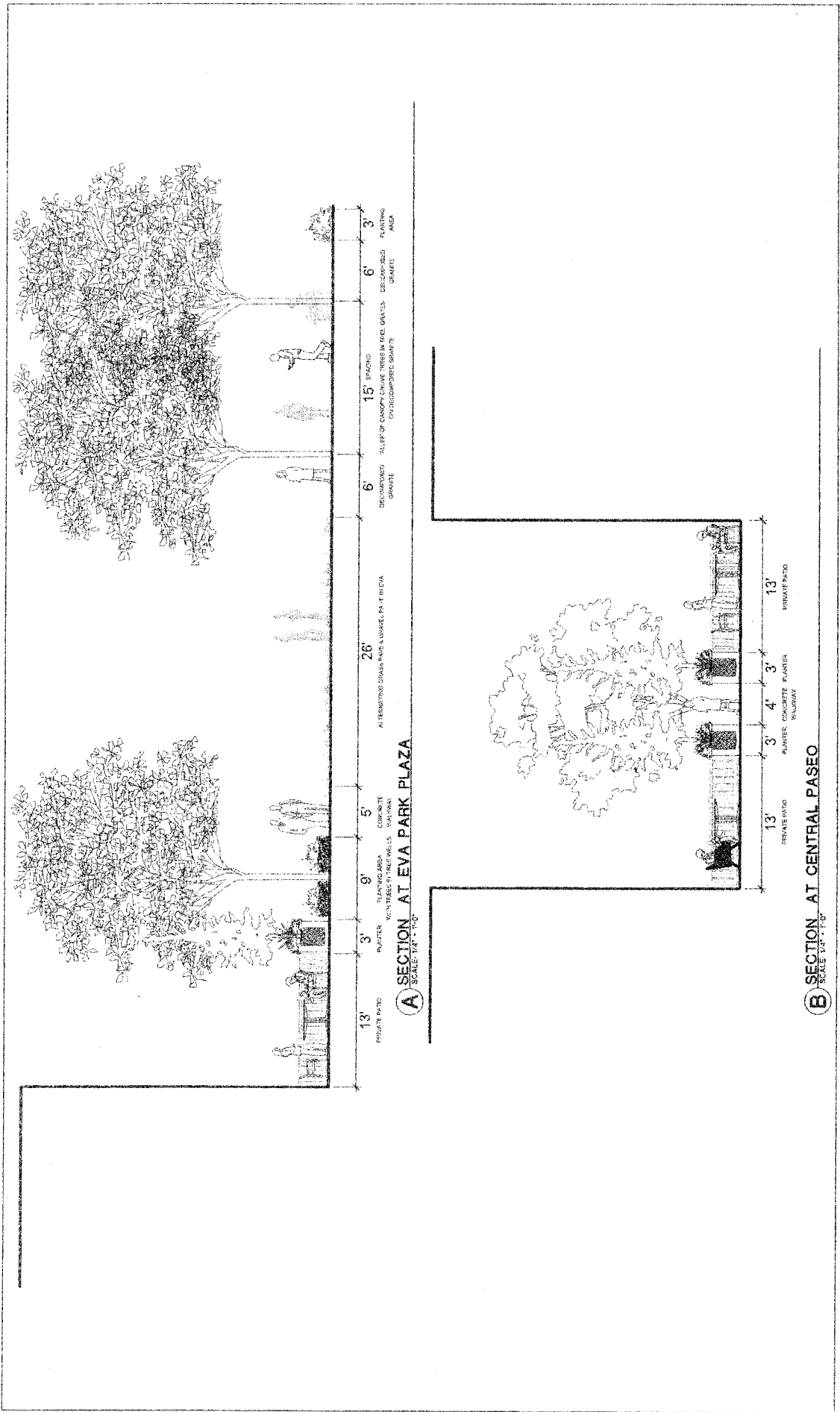
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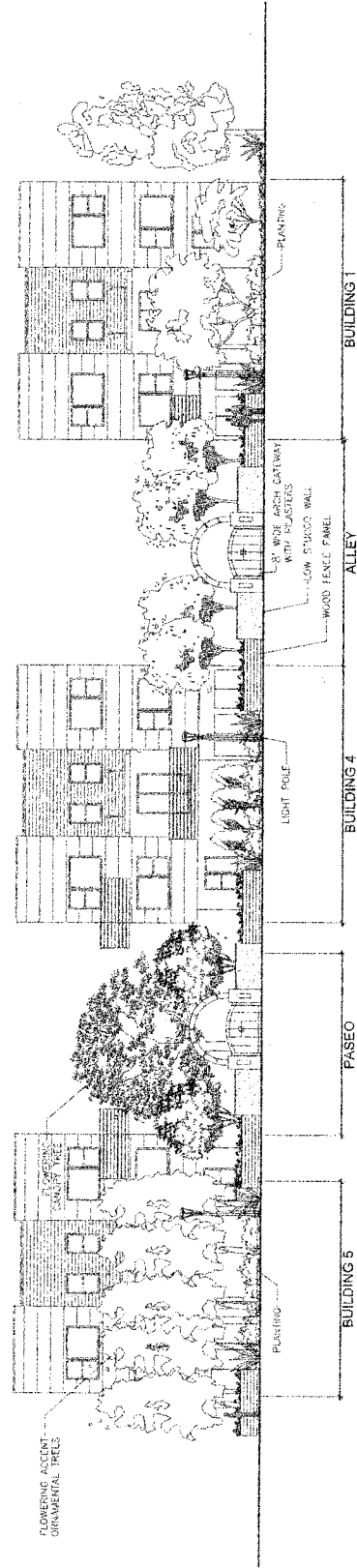
MATERIALS ENLARGEMENT PLAN - EVA PARK/PLAZA
SCALE: 1" = 10'-0"

DATE: 10/05/15



L1.6





A PLAZA ELEVATION
SCALE 1/8" = 1'-0"

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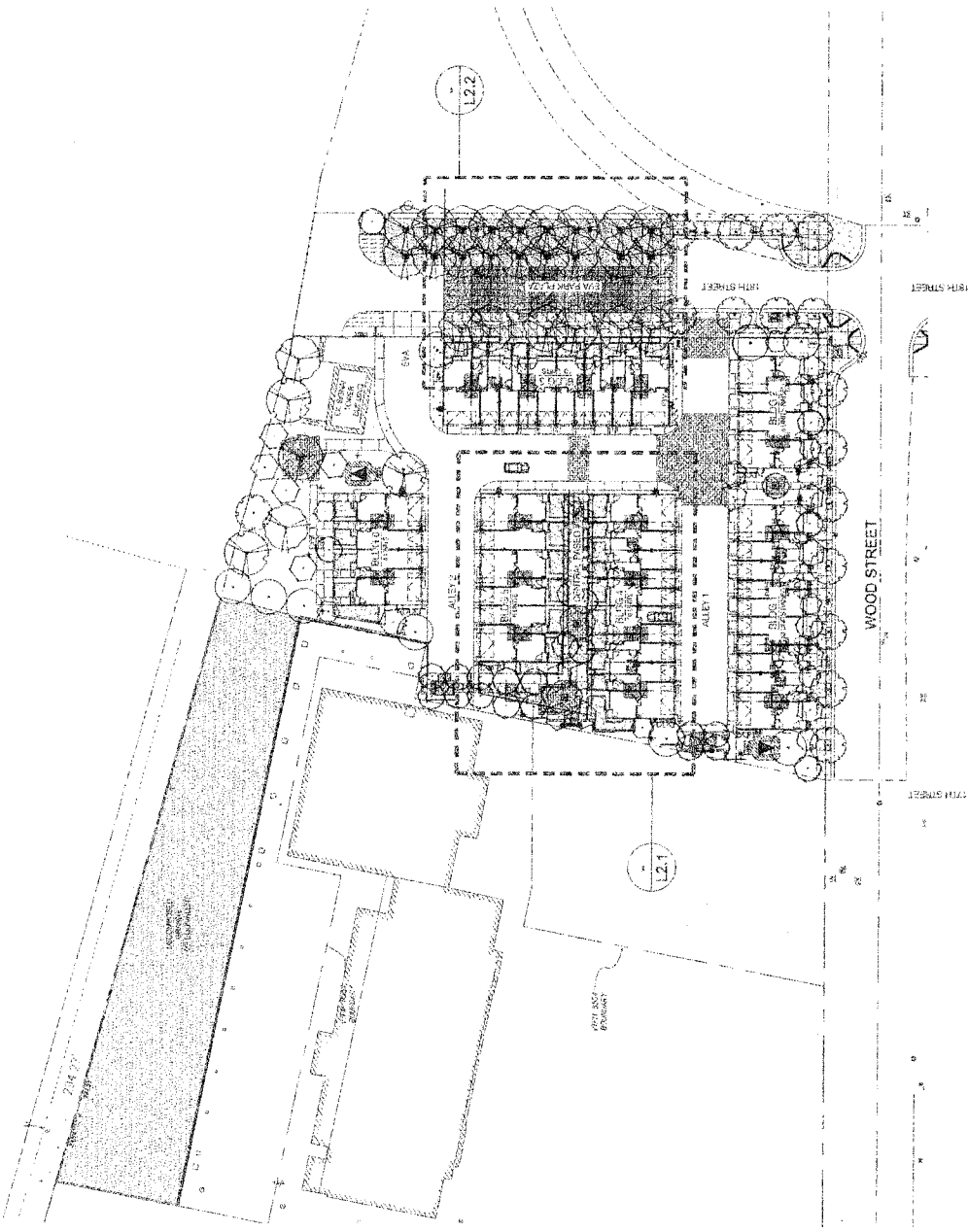
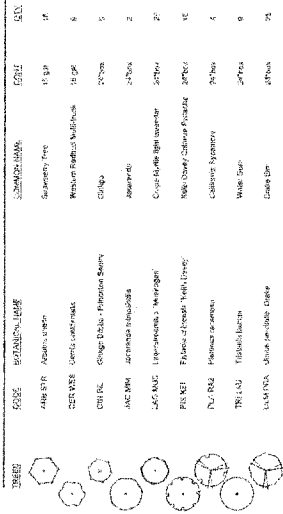
LANDSCAPE SECTIONS

SCALE: 1/4" = 1'-0"

DATE: 09/29/15

L1.8




























TREE PLANTING PLAN
SCALE: 1" = 30'-0"

DATE: 10/05/15



2.0



GENUS	SCOTT	REDAIRPLANE NAME	COMMON NAME	SCOTT	SET
	A08 STR	Avia 8	Stratford 700	10 gal	4
	CAR STR	Carex Redstart	Wilder Redstart Mustang	10 gal	1
	ONR DEL	Congress Blues + Redstart + Cherry	Orange	2000	6
	JAC WAT	Jeannette White	Jeannette	2000	8
	LGS MAG	Lepidoptera + Mustang	Congress High Lift Mustang	2000	17
	MR DEL	Plains Redstart + White Dove	Redstart + White Dove Mustang	2000	2
	TRR JAN	Tennessee	Wash. Conn.	2000	2
	VALM DEN	United States State	Dove-Git	2000	24
	CADRE	RED AIRPLANE NAME	COMMON NAME	SCOTT	SET
	ACW STR	Angus + White Dove	White Dove + Redstart	1 gal	290
	ANV TEL	Angelina + "White Dove"	White Dove + Mustang	5 gal	159
	BUR DEN	Burn + Mustang + Mustang + Green Dove	Green Dove + Mustang	5 gal	50
	CAS CAL	California California	Bull America	3 gal	13
	GHU LGL	Chickadee + Mustang + El Camp	Cape West	5 gal	1
	GGF RMV	Grass + Mustang + White Dove	Grass + Mustang	6 gal	12
	DRE BUC	Dove + White Dove	Mustang + White Dove	1 gal	312
	ESC COS	Escalante + "Contract"	Congress High Lift	5 gal	30
	GRE MOE	Grass + Mustang + Mustang	Grass + Mustang	5 gal	4
	HEN CAL	Hunter + "Contract"	Hunter	5 gal	2
	LRS TEL	Lepidoptera + Mustang	Texas + Mustang	5 gal	20
	LOR CHI	Lepidoptera + Mustang	Congress + Mustang	5 gal	6
	NRT COS	Northern + Mustang	Mustang + Mustang	1 gal	22
	NRT COS	Northern + Mustang	Mustang + Mustang	1 gal	40
	PHO ATR	Phoenix + Mustang + Mustang	Phoenix + Mustang + Mustang	6 gal	13
	PHO TEL	Phoenix + Mustang + Mustang	Phoenix + Mustang + Mustang	5 gal	17
	PRT COS	Phoenix + Mustang + Mustang	Phoenix + Mustang + Mustang	5 gal	29
	PRT WTE	Phoenix + Mustang + Mustang	Phoenix + Mustang + Mustang	1 gal	9
	PRR BAK	Phoenix + Mustang + Mustang	Phoenix + Mustang + Mustang	10 gal	2
	RRR MOE	Redstart + Mustang + Mustang	Redstart + Mustang + Mustang	10 gal	13
	ROR CAL	Redstart + Mustang + Mustang	Redstart + Mustang + Mustang	1 gal	53
	ROR ANP	Redstart + Mustang + Mustang	Redstart + Mustang + Mustang	1 gal	11
	ROR BZ	Redstart + Mustang + Mustang	Redstart + Mustang + Mustang	1 gal	8
	ROR BZ	Redstart + Mustang + Mustang	Redstart + Mustang + Mustang	1 gal	1
	ROR BZ	Redstart + Mustang + Mustang	Redstart + Mustang + Mustang	1 gal	1
	ROR BZ	Redstart + Mustang + Mustang	Redstart + Mustang + Mustang	1 gal	1
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	ROR BZ	Redstart + Mustang + Mustang	Redstart + Mustang + Mustang	1 gal	1
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	ROR BZ	Redstart + Mustang + Mustang	Redstart + Mustang + Mustang	1 gal	1
	ROR BZ	Redstart + Mustang + Mustang	Redstart + Mustang + Mustang	1 gal	1

INGERSOLL



PLANTING ENLARGEMENT PLAN

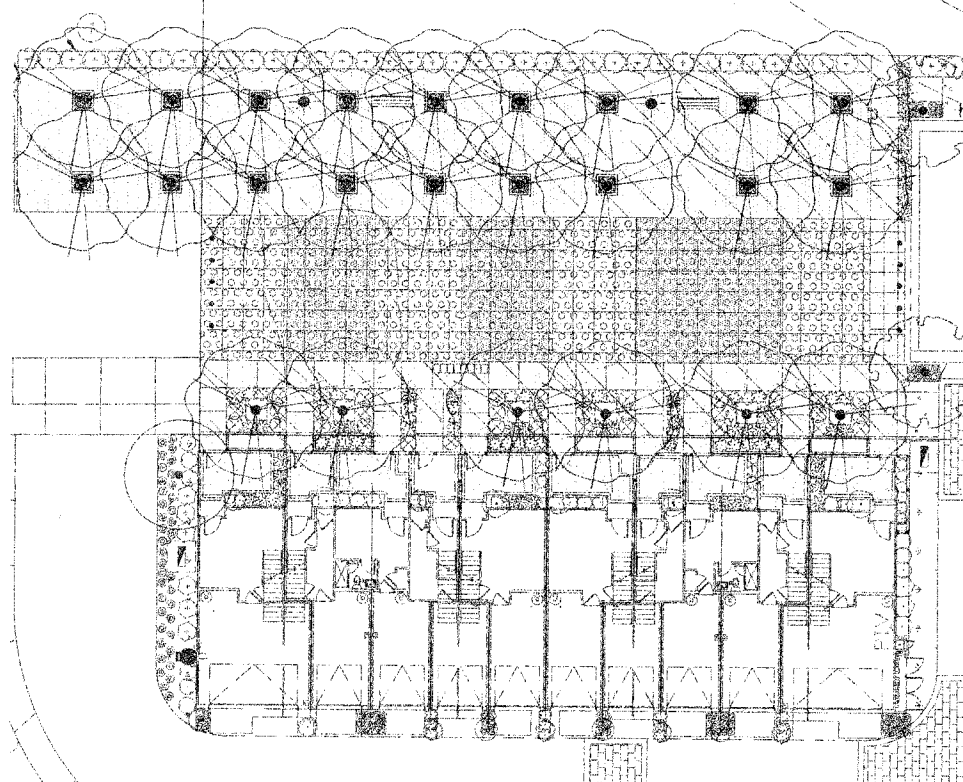
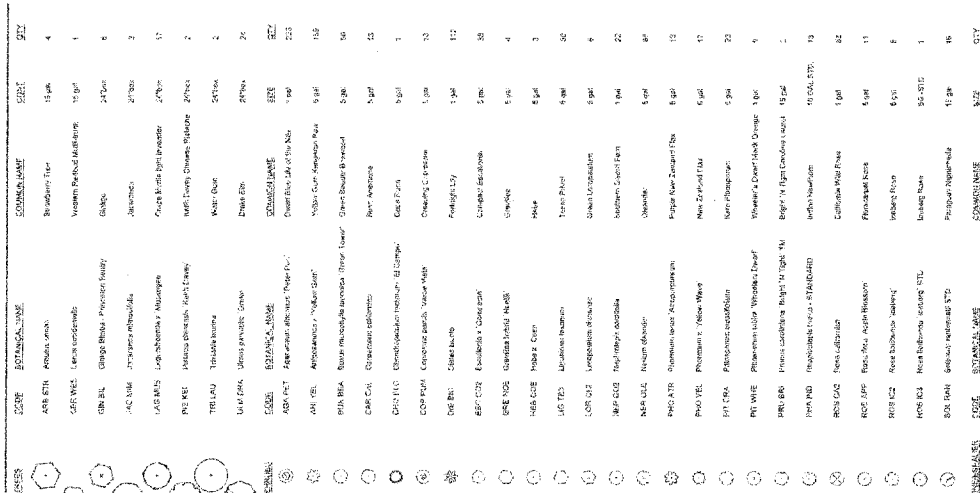
SCALE: 1" = 30'-0"

DATE: 10/05/15



L2.1





CITY VENTURES, INC.
Signalhouse
OAKLAND, CALIFORNIA

PLANTING ENLARGEMENT PLAN

DATE: 10/05/15

SCALE: 1" = 30'-0"



22



TREE PALETTE

BOTANICAL NAME	COMMON NAME
ARBUTUS UNEDO	STRANBERRY TREE
CERCIS OCCIDENTALIS	WESTERN REDBUD MULTI-TRUNK
GINKGO BILOBA - PRINCETON SENTRY	GINKGO
JACARANDA MIMOSIFOLIA	JACARANDA
LAGERSTROEMIA INDICA 'WUSKOCSE'	CRABE MYRTLE LIGHT LAVENDER
OLEA EUROPAEA 'SWAN HILL' TM	SWAN HILL OLIVE
PISTACHIA CHINENSIS 'KEITH DAVEY'	KEITH DAVEY CHINESE PISTACHE
PLATANUS RACEMOSA	CALIFORNIA SYCAMORE
PYRUS CALLERYANA 'CHANTICLEER'	CHANTICLEER PEAR
TRISTANIA LAURINA	WATER GUM
ULMUS PARVIFOLIA 'DRAKE'	DRAKE ELM



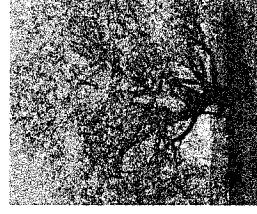
ARBUTUS UNEDO



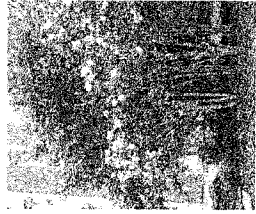
CERCIS OCCIDENTALIS



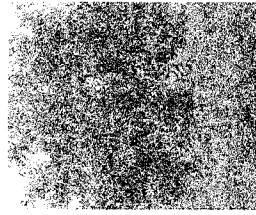
GINKGO BILOBA - PRINCETON SENTRY



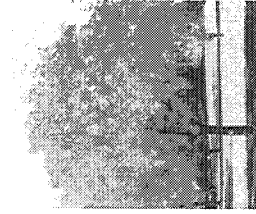
JACARANDA MIMOSIFOLIA



LAGERSTROEMIA INDICA 'WUSKOCSE'



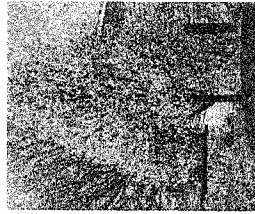
OLEA EUROPAEA 'SWAN HILL' TM



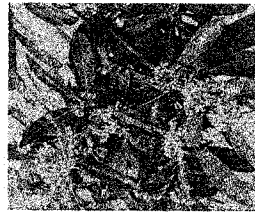
PISTACHIA CHINENSIS 'KEITH DAVEY'



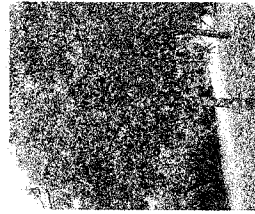
PLATANUS RACEMOSA



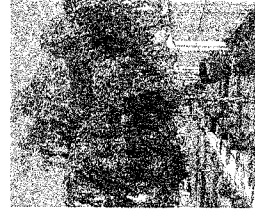
PYRUS CALLERYANA 'CHANTICLEER'



TRISTANIA LAURINA



TRISTANIA LAURINA



ULMUS PARVIFOLIA 'DRAKE'

CITY VENTURES, INC.
Signalhouse
OAKLAND, CALIFORNIA

TREE PALETTE & IMAGERY

SCALE: NTS

DATE: 09/29/15

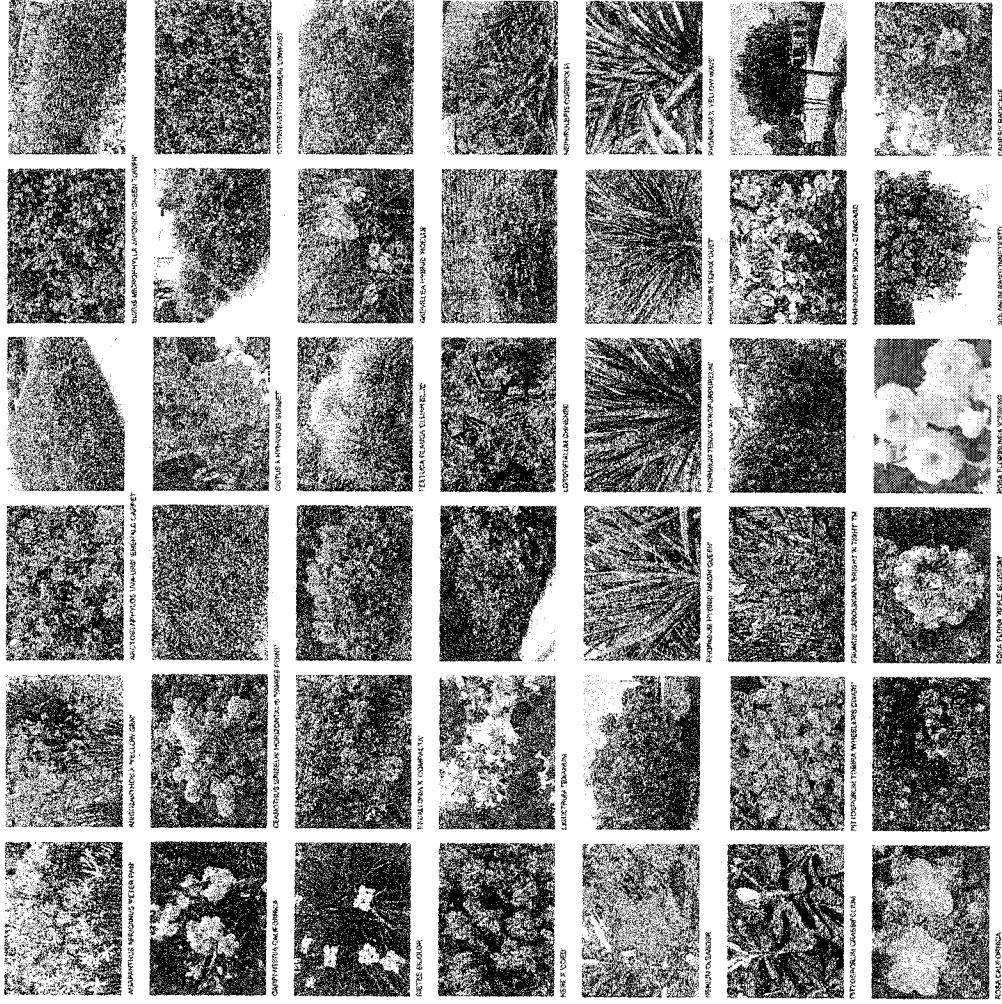
L3.0

SHRUB / CAMPODCOVER PALETTE

BOTANICAL NAME	COMMON NAME
AGAPANTHUS AFRICANUS 'PETER PAN'	SUN STRIPE AGAPANTHUS
ANISZANTHOS X 'YELLOW GEM'	YELLOW GEM KANGAROO PAW
ARCTOSTAPHYLOS UVA-URSI 'EMERALD CARPET'	EMERALD CARPET MANZANITA
BUXUS MICROPHYLLA 'JAPONICA GREEN TOWER'	GREEN BEAUTY BOXWOOD
CARPENTERIA CALIFORNICA 'CALIFORNICA'	BUSH ANEMONE
CEANOTHUS GRISEUS 'HORIZONTALIS YANKEE POINT'	CALIFORNIA LILAC
CISTUS X HYBRIDUS 'SUNSET'	MAGENTA ROCKROSE
COTONEASTER DAMMERI 'LOWFAST'	LOWFAST BEARBERRY COTONEASTER
DIETES ECCOLOR	FORTNIGHT LILY
ESCALLONIA X 'COMPACTA'	COMPACT ESCALLONIA
FESTUCA GLAUCA 'YELWAH BLUE'	BLUE FESCUE
GREVILLEA HYBRID 'NOBELIP'	GREVILLEA
HEBE X 'COED'	HEBE
LIGUSTRUM TEXANUM	TEXAS PRIVET
LOROPETALUM CHIRENSE	GREEN LOROPETALUM
NEPHROLEPIS CORDOLIA	SOUTHERN SWORD FERN
NERIUM OLEANDER	OLEANDER
PHORMIUM HYBRID 'MAORI QUEEN'	PHORMIUM HYBRID
PHORMIUM TEXAN ATROPURPUREUM'	PURPLE NEW ZEALAND FLAX
PHORMIUM TENAX 'DUET'	VARIEGATED NEW ZEALAND FLAX
PHORMIUM X 'YELLOW WAVE'	NEW ZEALAND FLAX
PITTOSPORUM CRASSIFOLIUM	KARO PITTOSPORUM
PITTOSPORUM YOSHINO WHEELERS DWARF	WHEELERS DWARF MOCK ORANGE
PRUNUS CAROLINIANA 'BRIGHT N TIGHT' TM	BRIGHT N TIGHT CAROLINA LAUREL
RHAPHIOLEPIS INDICA - STANDARD	INDIAN HAWTHORN
ROSA CALIFORNICA	CALIFORNIA WILD ROSE
ROSA FLORA 'APPLE BLOSSOM'	FLORACARPET ROSE
ROSA FLORIBUNDA 'ICEBERG'	ICEBERG ROSE
SOLANUM RANTONNETII - STANDARD	PARAGUAY NIGHTSHADE

VINE PALETTE

BOTANICAL NAME	COMMON NAME
CAMPISIS MACCANS	TRUMPETVINE



CITY VENTURES, INC.
Signalhouse
OAKLAND, CALIFORNIA

PLANT PALETTE & IMAGERY

SCALE: NTS

DATE: 09/29/15

L3.1



SIGNALHOUSE

WOOD STREET DEVELOPMENT AREA 6

Statement of Compliance with Wood Street Zoning District

The following statement addresses, in a narrative format, the way in which the project proposed for Wood Street Zoning District Development Area 6 complies with the applicable Development Standards and Design Guidelines of the Wood Street Zoning District.

Pursuant to Section 3.50 of the WSZD regulations, "Design Review shall be limited to a determination of whether or not the proposed design is in substantial compliance with the design guidelines specified in these Wood Street Zoning Regulations."

» DEVELOPMENT STANDARDS

5.20 Maximum Density

The density standards allow a maximum residential density of 79.33 dwelling units per acre (DU/A). The proposed project provides 22.22 DU/A, which is well under the 79.33 DU/A maximum density requirement.

5.21 Minimum Density

N/A. There is no minimum density requirement for Development Area 6.

5.22 Floor Area Ratio

N/A. The proposed project does not include any Non-Residential Uses.

5.23 Maximum Height

The maximum height allowed in Development Area 6 is 65 ft. The proposed building heights are 39'6", well below the allowed maximum.



5.24 Minimum Setbacks

Minimum Street Setbacks

The required minimum street setback from Wood Street is 10 ft. The proposed project provides a street setback of 13 ft at Wood Street, which is in excess of the 10 ft street setback required. The remainder of the site does not require a street setback.

Minimum Interior Setbacks

N/A. The minimum interior setback requirement does not pertain to the proposed project.

5.30 Building Frontage

The percentage of building frontage calculated for each location noted below exceeds the minimum standards set forth in Section 5.30.

<u>Location</u>	<u>Min Required</u>	<u>Proposed</u>
A. Wood Street Overlay Zone	75%	80%
C. 18th Street	50%	59%

The building frontages on Wood Street and 18th Street conform to the following requirements:

- A maximum of 5 ft. from the street line or setback line;
- Greater than 15 ft. in height;
- More than 20 ft. in depth; and
- Contains occupied space at the ground floor

5. 31 Building Frontage at Corner Locations on Wood Street

The corner of Wood Street and 18th Street, the only applicable corner within the proposed project area, has a continuous building frontage along Wood Street for 65 ft., well in excess of the minimum of 30 ft. It also has a continuous building frontage along the intersecting street line (18th Street) of 37 ft., exceeding the minimum of 20 ft. The qualifying building frontage:

- Is 39'6" in height, more than the minimum of 15 ft
- Is 37 ft in depth, more than minimum of 20 ft; and



- Contains occupied space at the ground floor.

The first pedestrian entry in Building 2 is located 22 ft away from intersecting street corner, which is within the 30 ft required.

5.32 Street Front Entries

Pedestrian entries to individual units at the street line or street setback line are provided at:

<u>Location</u>	<u>Max Allowed</u>	<u>Proposed</u>
Wood Street Overlay Zone	60 ft	15 ft
18th Street	100 ft	15 ft

All of the proposed project pedestrian entries in the areas noted above, in compliance with the applicable design standards:

- Are visible from the street;
- Are located within 20 ft of the street line;
- Provide direct path of travel to the public right of way;
- Are marked by building articulation; and
- Incorporate exterior lighting at point of entry.

5.33 Street Front Openings

The proposed ground floor building frontages located within the Wood Street Overlay Zone are in compliance with the following development standards:

- Ground level openings – door, window, or passageway is located within 25 ft apart, measured parallel to the street line
- The percentage of transparent openings, pedestrian entries, or a combination of the two exceed the minimum requirement of 25%
- The vertical distance between the window sill and the grade of lower subfloor is 3 ft.

5.34 Projections over the Street Line or Street Setback Line

Bay windows do not project over the street line or street setback line.



5.40 Usable Open Space for Residential Uses

The proposed project provides a mix of private and group usable open space. All open space areas comply with the specific requirements of the WSZD development standards and referenced the Planning Code.

The development standards for Development Area 6 require a minimum of 75 sf of usable open space per dwelling unit. The proposed project, which includes 44 homes, requires a minimum of 3,300 sf of total usable open space. The standards also require that at a minimum, 30% of the total usable open space be group space. As shown in the table below, the proposed project far exceeds the minimum open space requirements of WSZD.

	<u>Min Required</u>	<u>Proposed</u>
Usable Open Space	3,300 sq ft	6,949 sq ft
Group Open Space	990 sq ft	2,995 sq ft
Private Open Space	2,310 sq ft	3,964 sq ft

In addition, the proposed usable open spaces comply with the following WSZD standards:

- Each of the private usable open spaces are located directly adjacent to and are readily accessible from the living units served.
- The private usable open spaces vary between 35 sf and 214 sf, with dimensions that are above the 5 ft x 5 ft minimum.
- Each of the group usable open spaces have dimensions well above the minimum requirement of 10 ft x 10 ft.

5.41 Minimum Separation Between Opposite Walls on the Same Lot

The minimum required separation between opposition walls on the same lot is 12 ft and the maximum is 30 ft. The separation between facing buildings range from 18 ft to 36 ft.

5.50 Required Off-Street Parking

The development standard for residential uses is 1.1 parking spaces per dwelling unit, which requires a total of 49 spaces. The proposed project provides 62 private spaces in enclosed garages, yielding an average of 1.4 spaces per unit.



5.51 Dimensional Requirements for Off-Street Parking

All residential parking spaces range between 10 ft x 20 ft to 20 ft x 20 ft, exceeding the minimum of 8 ft x 17 ft. The proposed parking spaces also comply with subsections of Section 17.94 of the Oakland Planning Code.

5.52 Joint Use Parking

N/A. The proposed project does not utilize joint use parking.

5.53 Curb Cuts

The curb cuts at the 18th Street line are limited to a driveway that provides access to the interior streets for the residents and a secondary driveway for Emergency Vehicle Access only. The two curb cuts are located 130 ft apart from one another, far exceeding the development standard minimum of 44 ft.

Per the overriding requirements of the Oakland Fire Department, the above mentioned site entrances are 26 ft wide, greater than the 24 ft wide WSZD standard. In all other aspects, the site driveways and curb cuts conform to the WSZD development standards.

5.54 Off-Street Loading Requirements

N/A. The proposed project does not require off-street loading areas.

5.60 Location and Screening of Surface Parking

N/A. The proposed project provides all parking within enclosed private garages.

5.61 Shade Trees at Surface Parking Lots

N/A. There are no surface parking lots included in the proposed project.



5.62 Location and Screening of Tuck Under Parking

N/A. There is no tuck under parking in the proposed project.

5.63 Location and Screening of Parking Garages

N/A. There are no parking garages in the proposed project.

5.70 Limitations on Signs

All signage in the proposed project will meet the requirements of this development standard. The applicant will submit a complete signage package as a condition of approval prior to the issuance of a building permit.

5.80 16th Street Station and 16th Street Signal Tower

The development standards regarding the 16th Street Station are not applicable to the proposed project.

Per the development standards, no additions to the 16th Street Signal Tower are being made. The new townhomes are set back at a minimum of 33 ft from the signal tower, well above the required minimum distance of 15 ft.

5.90 16th Street Plaza

These development standards are not applicable to the proposed project.



» DESIGN GUIDELINES

6.20 Architectural Character

Proposed project creates a consistent architectural vocabulary that unifies the project as a whole, compliments existing residential homes, commercial buildings and other townhome projects that are being developed in the area. A combination of homes fronting onto existing streets and homes fronting mews and open spaces provide contrasting urban lifestyle options that are reinforced by the contemporary architectural style.

6.21 Pedestrian Connections

Per the general guidelines of WSZD, the proposed project provides multiple street facing openings and pedestrian connectivity to the existing neighborhood at several locations. Along Wood Street, the primary pedestrian connection is provided to the interior of the project. Additional pedestrian connections are provided from the 18th Street project entrance and the pocket park. Easy wayfinding and connections are provided throughout the interior of the project with opportunities to connect to the future train station plaza.

6.23 Building Massing

Per the general guidelines regarding building massing, the architecture of the proposed townhomes responds to the scale and massing of the adjacent residential development projects in the WSZD. The proposed townhomes are three stories in height, which is consistent with the existing neighborhood of residential and commercial buildings. Each building is articulated to express the urban townhome form of a more vertical articulated façade with movement between horizontal planes. A varied parapet height is incorporated to provide variety in the horizontal roof line, this change is emphasized with material changes.

6.24 Building Articulation

The proposed façade design exceeds the guidelines for building articulation through recesses and projections in the building plane. This articulation provides a rhythm to the façade in both vertical and horizontal planes along Wood Street and 18th Street.



6.25 Parking Garage Facades

N/A. The proposed project does not include a parking garage.

6.26 Balconies

The proposed balconies are designed as integrated elements of the building facades, and do not dominate facades facing street lines.

6.27 Awnings and Canopies

N/A. The proposed project does not include any awnings or canopies.

6.30 Windows

The proportion and patterning of windows reflect and reinforce the character of the buildings. Per the general guidelines, the window materials and construction will be of good quality and consistent with the other exterior materials. Security screens are not included in the proposed project.

6.31 Garage Doors

N/A. The project does not include parking garages containing three or more parking spaces.

6.32 Service Access

N/A. There are no ground level service doors or gates visible from and located within 20 ft of the street line.

6.33 Underground Utility Connections

Per the general guidelines and the WSZD Conditions of Approval, all utility connections in the proposed project will be underground.



6.34 Screening of Equipment

Where exterior equipment occurs it will be screened in the following method:

- Electrical Transformers: adjoining planting will reach a sufficient height to screen the equipment within three years.
- Roof Mounted equipment will be obscured by the use of parapets and will not be visible from the street.

6.35 Mechanical Penetrations at Facades and Roofs

Per the general guidelines, the mechanical penetrations which are visible on the street, such as appliance vents, will be coordinated and aligned with building features to present an organized appearance. The material finish of mechanical penetrations will match the adjacent surfaces.

6.36 Waste Handling Areas

The waste handling will be enclosed within each unit, including space for recycling. Waste handling will not occur within the Wood Street or Plaza Overlay Zones.

6.40 Exterior Materials

All exterior building elements will be high quality durable materials. Primary materials will include: painted cement board, stucco trim, wood siding, and cement board.

6.41 Exterior Color

The variations in building color are used to create a cohesive architectural expression and highlight articulations in form. Accent colors and materials are provided at unit entrances and where they enhance architectural expression.

6.42 Exterior Lighting

Exterior lighting of one foot-candle will be maintained along all designated paths of travel between dwelling units, buildings, site entrances, public streets, and open spaces.



6.50 Signage and Graphics

Signage will be coordinated with the design and character of the proposed project following the guidelines of this section. The applicant will submit a complete signage package prior to the issuance of a building permit.

6.60 Planting Areas

Planting areas have been designed to be attractive year round through provision of a variety of textures and seasonal color. The landscape design for the Wood Street Development creates a variety of social spaces with unique characters that address the scale of the home, the neighborhood, and the natural world.

Proposed project planting has been utilized in conformance with the following general guidelines of Wood Street Zoning District:

- Emphasis on both pedestrian and vehicular entries
- screened service areas
- buffers between private and group usable outdoor space
- improved appearance and usability of outdoor space.

The planting design incorporates principles of the "Bay Friendly Landscape Guidelines". The planting design minimizes turf/lawn areas and utilizes primarily drought tolerant and native plant species with select low and medium water-use ornamental plants that are adapted to the local climate. Plants are grouped by hydrozone, exposure, and local climatic conditions. The planting design allows for the plants to reach their natural, full-grown size and eliminates the need for excessive pruning or hedging.

Planting is mindful of best contemporary practices for storm water management, safety, and the importance of street life. The project is designed to be a good neighbor and in particular the planting does not interfere with the site lines or movement of motorists, pedestrians.

Planting at Street Frontage

The planting design as proposed maximizes planting in the required 10 ft setback at Wood Street while meeting the functional requirements of the proposed building type.



Planting in the required street setbacks do not exceed the required maximum of 36" in height to maintain sight lines for pedestrians and residents, with the exception for trees limbed to provide visibility through and below the canopy.

Streetscapes are composed of trees, foundation plantings, and vegetated flow through planters for storm water treatment. Street trees have been selected for long term performance as well as character.

All pots will have a minimum soil depth of 18".

Planting Materials

A variety of trees have been selected to provide a variation of heights, colors and canopy sizes. Street tree species will be reviewed/approved by the City Arborist. Vegetated swales will be planted with no-mow fescue with Perimeter shrubs that are adapted to bio-swale conditions. The plants have been selected utilizing the state of California's Model Water Efficient Landscape Ordinance Plant List, and Water Use Classification of Landscape Species (WUCOLS III). No plants are used that are considered invasive in the the region as listed by the CAL-IPC.

Irrigation

The irrigation for the proposed project will be designed for maximum efficiency and water conservation in the following ways:

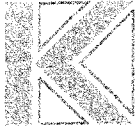
- Smart E.T. base irrigation controller w/ automatic, low gallon use drip system
- Water schedule adjustments daily based upon local site climatic conditions
- Rain sensor and shutoff device
- Low volume drip emitters at tree, shrub and ground
- Cover planting areas
- High efficiency "water smart" precision spray heads at turf block/EVA area for optimum application
- Flow reducers & flow sensors w/ automatic shut off in the event of a line break
- Separate hydrozone valve circuits for low, medium and high water use planting areas

The irrigation system will comply with the 2010 California Model Water Efficient Landscape Ordinance (WELO) as well as the June 2015 California state & locally adopted emergency water use regulations.



Maintenance

All landscaped areas shall be maintained in conformance with standards set forth in Section 17.124.020 of the Planning Code.



KITTELSON & ASSOCIATES, INC.

TRANSPORTATION ENGINEERING / PLANNING

610 SW Alder Street, Suite 700, Portland, OR 97205 P 503.228.5230 F 503.273.8169

August 31, 2015

Project #: 19392

Claire Han
City Ventures
444 Spear Street, Suite 200
San Francisco, CA 94105

RE: Signalhouse Project, Preliminary Trip Generation Assessment

Dear Claire,

City Ventures proposes to construct 44 new residential townhomes (including 38 two-bedroom units and six micro-units) on a 1.98 acre site in West Oakland (herein referred to as the "Signalhouse Project"). The Signalhouse Project site (Assessor's Parcel Number 18-310-13-1) is bounded to the west by Frontage Road and Interstate 880 (I-880) and to the east by Wood Street. The site covers the entire block between 17th Street and 18th Street and extends south to include the western portion of the block between 16th Street and 17th Street. The site is located in Wood Street Zoning District Development Area 6 in the *West Oakland Specific Plan Environmental Impact Report (WOSP EIR)* (certified June 2014). The proposed site plan is shown in *Appendix A*.

Based on guidance provided by City of Oakland staff on a previous analysis for the Oakland 2 - Icehouse Project, the following assessment focuses on a preliminary trip generation and distribution estimate and a site plan evaluation focusing on access and circulation for all modes. Based on this assessment, the vehicle-trip generation potential of the proposed Signalhouse Project falls within the *WOSP EIR* estimates for the development program and will not present additional traffic impacts than those found in the *WOSP EIR*. The Signalhouse Project would add a maximum of 10 vehicle-trips to intersections studied in the *WOSP EIR* and would represent less than one percent of peak hour traffic volumes at the significantly impacted intersections identified in the *WOSP EIR*. The following sections summarize the results of our assessment.

TRIP GENERATION AND DISTRIBUTION

Trip Generation

The trip generation estimates were developed for the Signalhouse Project based on the trip rates provided in the *Institute of Transportation Engineer's (ITE) Trip Generation Manual, 9th Edition (2012) (Trip Generation Manual)* and using methods consistent with the *City of Oakland Transportation Impact Study (TIS) Guidelines (2013) (TIS Guidelines)*. The trip generation comparisons for the daily, weekday AM peak hour and weekday PM peak hour are provided in Table 1, below.

Table 1: Trip Generation Estimate

Land Use	ITE Code	Units	Weekday Daily	Weekday AM Peak Hour			Weekday PM Peak Hour		
				In	Out	Total	In	Out	Total
Residential (Townhouse)	230 ¹	44 DU	256	3	17	20	15	8	23
Non-Auto Reduction (-3.1%) ²			-8	0	-1	-1	-1	0	-1
New Vehicle Trips			248	3	16	19	15	8	22

1 Trip Generation Manual – ITE Land Use 230: Residential Condominium/Townhouse

Daily Trip Rate: 5.81 trips/DU

Weekday AM Peak Hour Trip Rate: 0.44 trips/DU (17% in, 83% out)

Weekday PM Peak Hour Trip Rate: 0.52 trips/DU (67% in, 33% out)

2 The TIS Guidelines shows an 3.1% reduction for motor vehicle trips for dense suburban locations greater than 1.0 mile from rail/ferry stations.

The TIS Guidelines presents multimodal trip generation adjustment factors to apply to the ITE trip generation to calculate the number of trips generated by the project for each mode. These factors are based on observed travel data for Alameda County from the Metropolitan Transportation Commission's 2000 Bay Area Travel Survey, and differentiate between proximity to rail/ferry stations and surrounding residential density (people per square mile). As the Signalhouse Project is located within a dense suburban area more than one mile from a rail/ferry station (i.e., 1.1 mile from West Oakland BART station), a factor of 96.9 percent is applied to the ITE-estimated trip generation to calculate the number of new motor vehicle trips generated by the project.

As shown in Table 1, the Signalhouse Project is expected to generate approximately 234 net new daily motor vehicle trips, including 19 motor vehicle trips (3 inbound, 16 outbound) during the weekday AM peak hour, and 22 motor vehicle trips (15 inbound, 8 outbound) during the weekday PM peak hour. It should be noted that there are currently no existing active uses on the site and in order to present a more conservative trip generation estimate, no trip generation reduction factors have been applied for pass-by and diverted trips.

Trip Distribution and Assignment

The site-generated motor vehicle trips shown in Table 1 were distributed onto the roadway network based on existing travel patterns and the locations of major trip origins and destinations in the Oakland area. The following approximate trip distribution was assumed for the motor vehicle trips:

- 50 percent to and from the San Francisco area in the west via Grand Avenue,
- 15 percent to and from downtown Oakland in the southeast via 14th Street,
- 10 percent to and from the West Oakland BART station via Wood Street and 7th Street,
- 10 percent to and from I-980 to the north at 27th Street,
- 10 percent to and from I-880 to the south at Frontage Road and 7th Street, and
- 5 percent would come to and from the north via San Pablo Avenue and Hollis Street.

Based on this proposed trip distribution pattern, the trips generated by the Signalhouse Project were assigned to the nearby study intersections during the weekday AM and weekday PM peak hours.

WOSP EIR Study Intersections and Signalhouse Project Trips

Table 2 shows the intersections that were studied in the *WOSP EIR*, and indicates the intersections that were shown to be impacted in Existing plus Project Conditions and in 2035 plus Project Conditions, and the resulting level of significance after mitigation measures. The last column of the table shows the number of trips that would be added to the intersection with the Signalhouse Project.

As shown in Table 2, trips associated with the Signalhouse Project would be added to 16 of the 24 intersections studied in the *WOSP EIR*. There are 10 trips or less added to each intersection studied in the *WOSP EIR*.

The intersection of Mandela Parkway / West Grand Avenue was identified as a significant impact under the WOSP 2035 plus Project Conditions, and the Signalhouse Project is projecting to generate approximately 6 AM and 7 PM peak hour trips at this intersection. The *WOSP EIR* 2035 plus Project scenario projected this intersection to have a total entering volume of 7,092 trips in the AM and 5,239 trips in the PM peak hour. Relative to the projected 2035 total entering volume, the trips generated by the Signalhouse Project represent less than 1-percent of the 2035 total entering volume in the AM and PM peak hours at this intersection.

Table 2: WOSP EIR Impacts and Signalhouse Project Trips

WOSP EIR Study Intersections		WOSP EIR Existing plus Project Conditions		WOSP EIR 2035 plus Project Conditions		Trips Added by Signalhouse Project	
		Impact	Level of Significance	Impact	Level of Significance	AM Peak Hour	PM Peak Hour
1	Hollis Street/40th Street	PM	SU	AM and PM	SU (AM & PM)	1	0
2	San Pablo Avenue/40th Street	PM	SU	AM and PM	SU (AM & PM)	1	1
3	I-980 off-ramp/27th Street	--	--	--	--	0	3
4	I-980 on-ramp/27th Street	--	--	--	--	1	3
5	Maritime Street/West Grand Avenue	--	--	--	--	9	10
6	Frontage Road/West Grand Avenue	--	--	--	--	9	10
7	Mandela Parkway/West Grand Avenue	--	--	AM and PM	SU (AM & PM)	2	3
8	Adeline Street/West Grand Avenue	--	--	--	--	1	3
9	Market Street/West Grand Avenue	--	--	--	--	1	3
10	San Pablo Avenue/West Grand Avenue	--	--	--	--	1	3
11	Martin Luther King Jr. Way/West Grand Avenue	--	--	--	--	1	3
12	Northgate Avenue/West Grand Avenue	--	--	--	--	1	3
13	Broadway/West Grand Avenue	--	--	PM	LTS with MM	0	0
14	Harrison Street/West Grand Avenue	--	--	--	--	0	0
15	Adeline Street/18th Street	--	--	PM	LTS with MM	0	0
16	Market Street/18th Street	--	--	--	--	0	0
17	Adeline Street/14th Street	--	--	--	--	3	4
18	Adeline Street/12th Street	--	--	--	--	0	0
19	Frontage Road/7th Street	--	--	--	--	2	2
20	Mandela Parkway/7th Street	--	--	--	--	0	0
21	Adeline Street/7th Street	--	--	--	--	0	0
22	Market Street/7th Street	--	--	--	--	0	0
23	Market Street/5th Street/I-880 off-ramp	--	--	--	--	0	0
24	Adeline Street/5th Street	--	--	PM	LTS with MM	0	0

SITE PLAN EVALUATION

Appendix A shows the proposed site plan, dated August 10, 2015. The proposed site plan was analyzed for multimodal access and circulation.

Motor Vehicle Access and Circulation

Primary motor vehicle access to the site is via 18th Street which would be extended west of its current terminus at Wood Street. A secondary access driveway on 18th Street, west of the primary access point would be available for use by emergency vehicles only. The width of the internal streets for vehicles is 26 feet, which is sufficient width for two-way vehicle circulation and for maneuvering into the parking garages. The site plan has been designed to adequately accommodate garbage trucks, delivery vehicles, and emergency vehicles.

Each of the townhomes will have one or two parking spaces which would be located in individually accessible garages. A total of 62 private parking spaces would be provided on-site for residential use (equivalent to 1.4 spaces per unit). According to City of Oakland Municipal Code, 49 parking spaces are required for a project of this size. The Signalhouse Project exceeds minimum parking requirements. In addition to the parking provided for residential use, five perpendicular parking spaces on the north side of 18th Street west of Wood Street and 18 head-in diagonal parking spaces on the west side of Wood Street between 17th Street and 18th Street would be provided for visitors. On-street parking is also available on nearby streets (i.e., 18th Street, 17th Street, Wood Street, Willow Street, and Campbell Street).

Pedestrian Access and Circulation

Per the *TIS Guidelines*, 12.6% of the ITE-estimated trips generated will be walk trips and the Signalhouse Project will generate 32 daily walk-only trips. Including walk-to-transit trips, the Signalhouse Project will add 47 pedestrians (32 walk-only trips, 15 walk-to-transit trips) to the surrounding street network.

Primary pedestrian access to the site is provided via a 16-foot wide breezeway on Wood Street or via 18th Street. Pedestrian access to each unit would be provided via individual unit entrances accessible from Wood Street (Buildings 1 and 2), 18th Street (Building 3), or internal streets (Buildings 4, 5, and 6). It should be noted that Wood Street and 18th Street are designated as Neighborhood Routes in the *WOSP EIR*. This designation indicates the importance of pedestrian circulation and safety on these thoroughfares.

The Signalhouse Project includes the following sidewalk and frontage improvements in the public realm:

- New eight-foot wide sidewalks will be constructed on the west side of Wood Street (west side) between 17th Street and 18th Street and on both sides of 18th Street west of Wood Street.
- Bulbouts will be constructed on the north- and south-west corners with additional off-street improvements constructed on the north- and south-east corners at Wood Street / 18th Street.
- New directional curb ramps and marked crosswalks will be installed at all four corners at Wood Street / 18th Street.
- Landscaping and street trees will be provided along the Wood Street and 18th Street frontages.
- A pocket park will be provided on 18th Street.

These enhancements would increase pedestrian visibility, reduce pedestrian crossing distances, and generally improve conditions for pedestrians.

Bicycle Access and Circulation

Per the *TIS Guidelines*, 1.3% of the ITE-estimated trips rates will be bicycle trips and the Signalhouse Project will generate an estimated three daily bicycle trips.

Primary bicycle access to the site is provided via 18th Street. Although none are adjacent to the Project site, several existing or planned bicycle facilities are located in the vicinity of the Signalhouse Project. Currently, bike lanes are provided on Mandela Parkway and 14th Street (east of Mandela Parkway). According to the *WOSP EIR*, the proposed bikeway network includes an extension of the existing bike lanes on 14th Street to its western terminus near I-880 and installation of new bike lanes on Peralta Street.

City of Oakland Municipal Code requirement for bicycle parking are shown in Table 3. Based on the City of Oakland Municipal Code requirement for bicycle parking (Chapter 17.117), the Signalhouse Project will not be required to provide any long-term bicycle parking spaces and would be required to provide a minimum of two short-term bicycle parking spaces.

Table 3: Bicycle Parking Requirement

Land Use	Units	Long-Term Bicycle Parking Requirement		Short-Term Bicycle Parking Requirement	
		Code	Number Required	Code	Number Required
Residential	44 DU	None ¹	0	1 per 20 DU	3
Total		--	0	--	3

¹ No long-term bicycle parking is required for one-family dwelling.

Based on a review of the site plan, the Signalhouse Project would meet City of Oakland requirements for long- and short-term bicycle parking. Long-term bicycle parking will be provided within the individual garages of each of the residential units. A minimum of three short-term parking will be provided in bicycle racks placed in strategic locations throughout the development.

Transit Access

Per the *TIS Guidelines*, 6.0% of the ITE-estimated trips will be transit trips and the Signalhouse Project will generate an estimated 15 daily transit trips.

BART and Alameda Contra-Costa Transit (AC Transit) operate local and regional transit service with transit stops located near the Signalhouse Project.

BART provides regional rail service in the San Francisco Bay Area. The nearest BART station is located 1.1 mile away at the West Oakland BART station, which is served by the Dublin/Pleasanton-Daly City line, the Fremont-Daly City line, the Pittsburg/Baypoint-Millbrae line, and the Richmond-Millbrae line.

AC Transit provides local and regional transit service and operates in Western Alameda and Contra Costa Counties and provides Transbay service to San Francisco. The nearest AC Transit stop (Line 26) is 0.3 mile south of the Signalhouse Project. The following three AC Transit lines have stops within one mile of the site:

- Line 26 passes through West Oakland as it connects Emeryville to the Trestle Glen neighborhood in Oakland. The nearest stops are located 0.3 mile south of the Signalhouse Project at the northeast corner of Willow Street / 14th Street and the southeast corner of Wood Street / 12th Street. Line 26 operates from approximately 6:00 a.m. to 10:30 p.m. on the weekdays, with approximately 20-minute headways during the a.m. and p.m. peak hours and approximately 30-minute headways during the off-peak periods. On weekends, the hours of operations are the same and the headways are 30-minutes throughout the day.
- Line 31 passes through West Oakland as it connects the MacArthur, West Oakland, and 12th Street BART station and continues through the Webster/Posey Tube to Alameda City Hall West in the City of Alameda. The nearest stops are located 0.4 mile southeast of the Signalhouse Project at the southeast corner of Peralta Street / 17th Street (to MacArthur BART station) and the northwest corner of Peralta Street / 16th Street (to City of Alameda). Line 31 operates from approximately 5:45 a.m. to 11:00 p.m. on the weekdays, with approximately 30-minute headways all day. On weekends, Line 31 operates from approximately 5:30 a.m. to 11:30 p.m. with 30-minutes headways throughout the day.
- Line NL is a Transbay line that passes through West Oakland as it connects the Eastmont Transit Center to the Temporary Transbay Terminal in San Francisco. The nearest stop is located one mile north of the Signalhouse Project at the southwest corner of the West Grand Avenue / Mandela Parkway. Line NL operates from approximately 5:00 a.m. to 1:00 a.m. on the weekdays, with approximately 15-minute headways during the a.m. and p.m. peak hours and approximately 30-minute headways during the off-peak periods. On weekends, the hours of operations are the same and the headways are 30-minutes throughout the day.

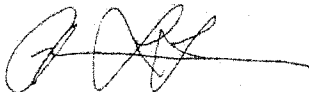
The *WOSP EIR* and *Emeryville Berkeley Oakland Transit Study (EBOTS)* identify several enhancements which would improve transit service in the vicinity of the Signalhouse Project. Planned/proposed improvements include an enhanced bus trunkline which is designed as a north-south route linking the cities of Oakland, Berkeley, and Emeryville. This route would connect transportation hubs (e.g., West Oakland BART station and Emeryville Amtrak) with frequent service. Both studies recommend upgrades to existing transit stops (e.g., seating, shelters, bike racks, lighting, and other amenities) to increase comfort and convenience for transit users. Additionally, the *WOSP EIR* proposes a future transit link (i.e., "The O"). This new transit link is envisioned as a community service similar to the Emery-Go-Round and Broadway "Free B". These enhancements would improve transit for all residents, employees, and visitors traveling to, from, and within West Oakland.

In addition, the proposed AC Transit Service Expansion Plan (SEP) shows the new route for Line 14 14th/High (identified as L17 in the SEP) running along Wood Street, between 7th Street and 14th Streets, serving Signalhouse and other WSZD residents. Service on Line 14 (L17) is to operate from 6:00 a.m. to 10:00 p.m. with 15-minute headways, during both peak- and off-peak periods. *Appendix B* shows the proposed route for Line 14.

The Signalhouse Project does not propose to change transit service to the area. Due to the marginal expected increase in ridership generated by the Signalhouse Project (i.e., 15 daily trips), the Signalhouse Project would not be expected to result in overcrowding on individual AC Transit lines or at nearby transit stops.

We hope that this letter has provided sufficient information regarding trip generation and the site plan review for the Signalhouse Project. Please do not hesitate to contact us with questions.

Sincerely,
KITTELSON & ASSOCIATES, INC.

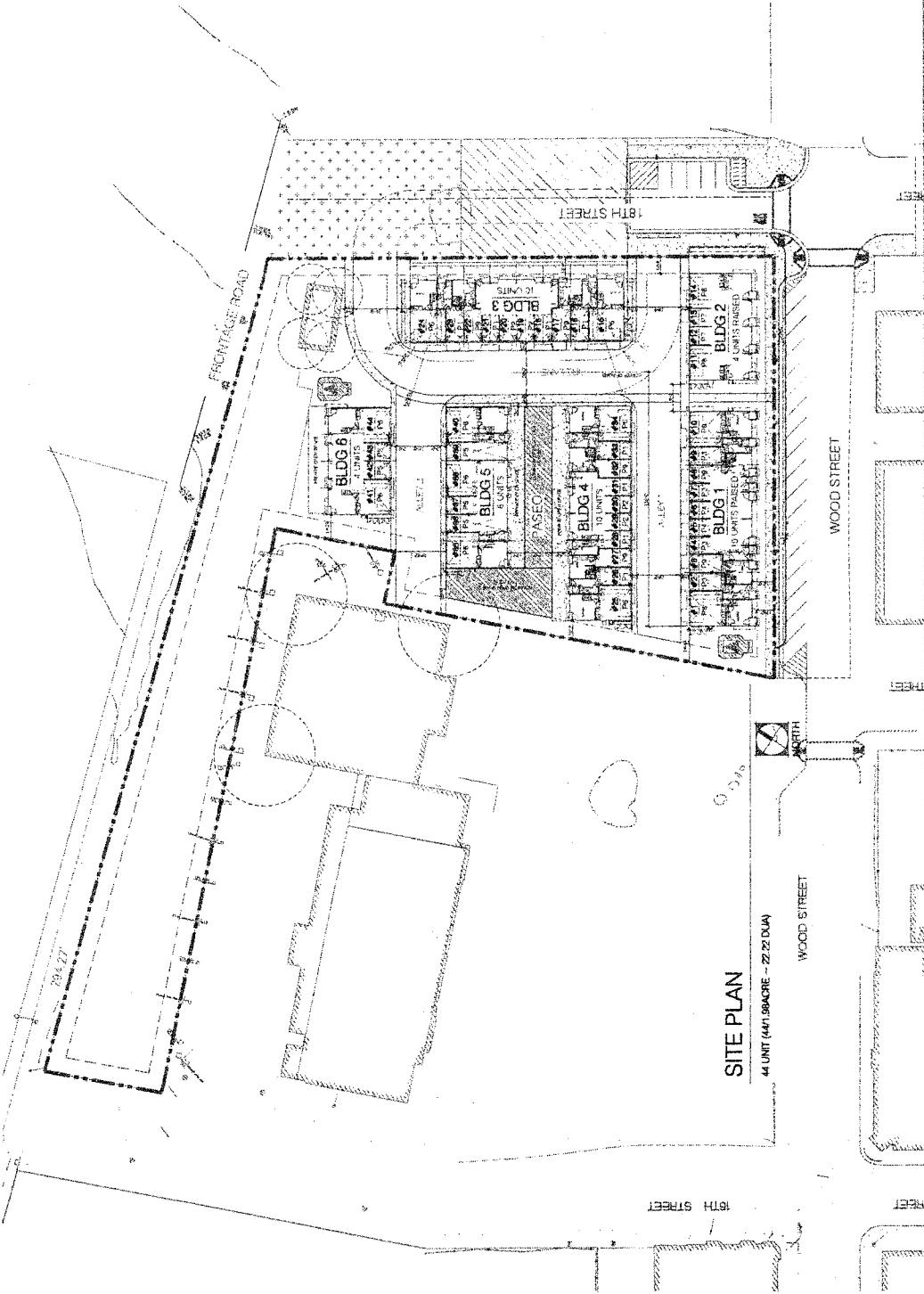


Amanda Leahy
Senior Planner



Ribeka Toda
Transportation Analyst

Appendix A
Site Plan



SITE PLAN

44 UNIT (441,984 SQ. FT. - 22.22 DUA)



SIGNALHOUSE
OAKLAND, CALIFORNIA

City Ventures



Architecture | Planning | Interiors
444 Spear Street, Suite 105
San Francisco, CA 94105
www.hunthalejones.com

SITE PLAN
SP
SCALE: 1"=30'
DATE: 08/10/2015
PROJECT: 317943

PLANTING AND WATER USE DESIGN INTENT STATEMENT

THE PLANTING DESIGN INCORPORATES PRINCIPLES INCLUDED IN THE "TRAFFIC-FRIENDLY LANDSCAPE GUIDELINES." THE PLANTING DESIGN MINIMIZES TURFLAWN AREAS, UTILIZES PRIMARILY DROUGHT-TOLERANT & NATIVE PLANT SPECIES WITH SELECT LOW AND MEDIUM GROWTH HABITS, AND EMPHASIZES PLANTING IN LOCAL CLIMATE. PLANTS ARE GROUPED BY HYDROZONE, EXPOSURE & LOCAL CLIMATIC CONDITIONS. THE PLANTING DESIGN ALLOWS FOR THE PLANTS TO REACH THEIR NATURAL, FULL-GROWN SIZE AND ELIMINATES THE NEED FOR EXCESSIVE PRUNING OR HEADING.

A VARIETY OF TREES HAVE BEEN SELECTED TO PROVIDE A VARIATION OF HEIGHTS, COLORS AND CANOPY SHAPES. STREET TREE SPECIES WILL BE REVIEWED/APPROVED BY THE CITY ARBORISTS.

VEGETATED SWALES WILL BE PLANTED WITH NO-MOW FESCUE WITH PERIMETER SHRUBS THAT ARE ADAPTED TO BIO-SWALE CONDITIONS. THE PLANTS HAVE BEEN SELECTED UTILIZING THE STATE OF CALIFORNIA'S MODEL WATER EFFICIENT LANDSCAPE ORDNANCE PLANT LIST. MINOR PLANTS ARE USED THAT ARE CONSIDERED INVASIVE IN THE REGION AS LISTED BY THE CALIF.

THE IRRIGATION WILL BE DESIGNED FOR MAXIMUM EFFICIENCY AND WATER CONSERVATION INCLUDING:

- SMART I.C.T. BASED IRRIGATION CONTROLLER W/ AUTOMATIC, LOW GALLON USE DRIP SYSTEM
 - WATER SCHEDULE ADJUSTMENTS DAILY BASED UPON LOCAL SITE CLIMATIC CONDITIONS
 - RAIN SENSOR AND SHUTOFF DEVICE
 - LOW VOLUME SPRINKLERS AT TREE, SHRUB AND GROUND COVER PLANTING AREAS
 - HIGH EFFICIENCY "WATER SMART" PRECISION SPRAY HEADS AT TURF BLOCKS/AREA FOR OPTIMUM APPLICATION
 - FLOW REDUCERS & FLOW SENSORS W/ AUTOMATIC SHUT OFF IN THE EVENT OF A LINE BREAK
 - SEPARATE HYDROZONE VALVE CIRCUITS FOR LOW, MEDIUM AND HIGH WATER USE PLANTING AREAS
- IRRIGATION SYSTEM WILL COMPLY TO 2010 CALIFORNIA MODEL WATER EFFICIENT LANDSCAPE ORDNANCE (MELD), INCLUDING THE JUNE 2013 CALIFORNIA STATE & LOCALLY ADOPTED EMERGENCY WATER USE REGULATIONS.
- ALL THE PLANT MATERIALS ARE LOW WATER USE &/OR DROUGHT TOLERANT SPECIES.
 - THE IRRIGATION SYSTEM IS INTENDED TO BE USED TO ESTABLISH NEW PLANT MATERIAL ONLY. ONCE THE PLANT MATERIAL HAS BECOME ESTABLISHED, THE IRRIGATION SYSTEM SHALL BE TURNED OFF.



CITY VENTURES, INC.
Signalhouse
OAKLAND, CALIFORNIA

SITE PLAN
SCALE: 1" = 30'-0"

DATE: 08/07/15



L1.0

Appendix B Proposed AC Transit
Service Expansion Plan, Line 17

Frequent Local

Plan|ACT

L17 14th/High

West Oakland - Fruitvale

Peak Headway	Base Headway	Span
15	15	6:00 am
		10:00 pm

Activity Centers:

- Downtown Oakland
- Laurel District
- Fruitvale Transit Village

Major Transit Hub Connections:

- W. Oakland BART
- 12th Street BART
- Fruitvale BART

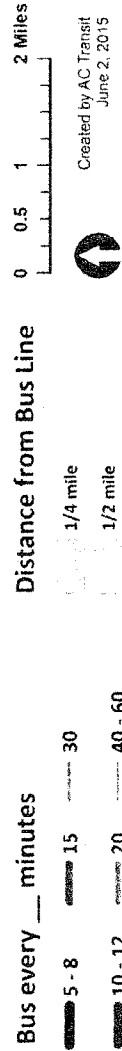
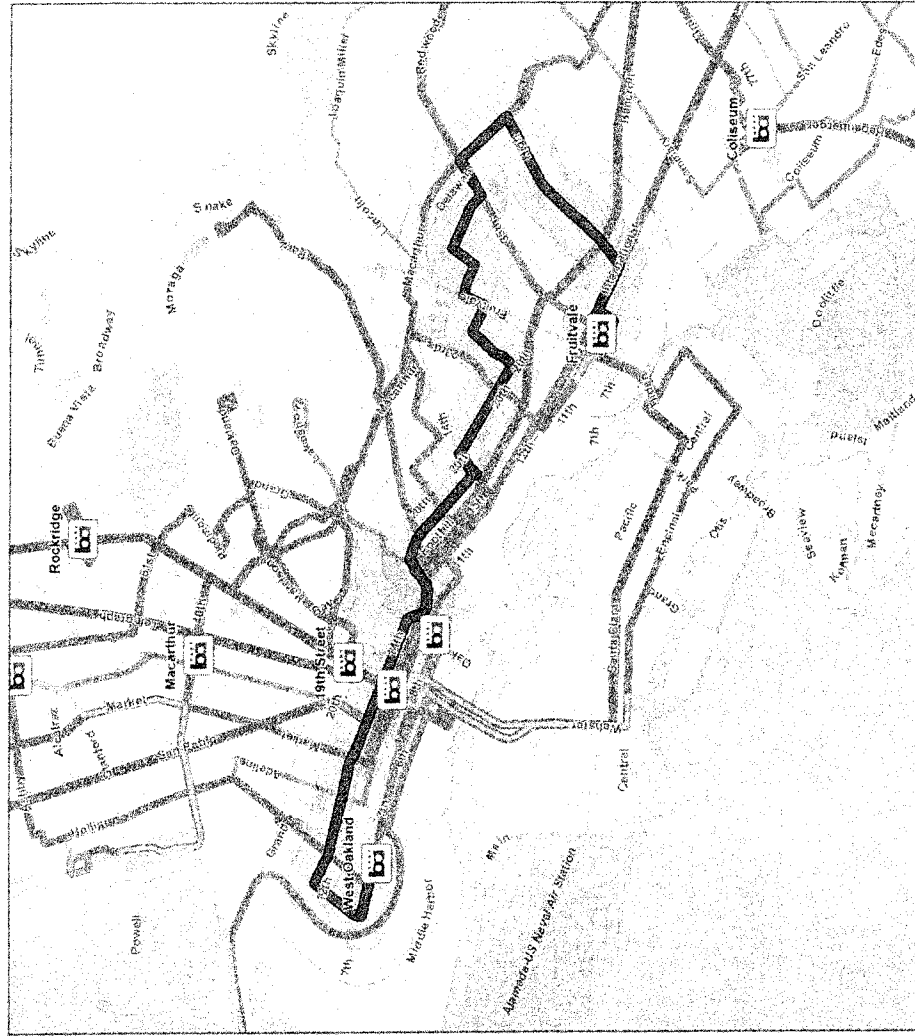
Frequent Network Connections:

- Ashby (L21)
- International Local (M7)*
- International Rapid (R2)*
- MacArthur (M5)
- Oakland/Park (L16)
- Shattuck/Solano (L20)
- Telegraph (M6)
- University (L18)
- 40 Foothill
- 51A Broadway/Santa Clara
- 54 35th Av
- 62 7th St/23rd Av
- 72M San Pablo/Macdonald
- 72R San Pablo Rapid
- 88 Sacramento

**Replaced by International Bus Rapid Transit (BRT), 2017*

North/East Route: Fruitvale BART, L 33rd, L San Leandro St, L High, L MacArthur, L 35th, R School, L Coolidge, R Brookdale, L Fruitvale, R, E 27th Ave, L 25th Ave, R, E 21st St, L 14th Ave, R, E 18th St, L Lakeshore, Into 1st Ave, Into 12th St Dam, R Into 14th St, L Wood, L 7th St, R Chester, L BART Roadway To W. Oakland BART

South/West Route: W. Oakland BART, L Mandala, L 7th St, R Wood, R 14th St, R Into 12th St Dam, L 1st Ave, Into Lakeshore Ave, R, E 18th St, L 14th Ave, R, E 21st St, L 25th Ave, R, E 27th St, L Fruitvale, R Brookdale, L Coolidge, R School, L 35th Ave, R MacArthur, R High, R San Leandro St, R 35th Ave, Into Fruitvale BART



24. Recordation of Mitigation Monitoring and Reporting Program and Conditions of Approval.

Prior to issuance of first demolition, grading or building permit.

The Project Sponsor shall execute and record with the Alameda County Recorder's Office a copy of the MMRP and Conditions of Approval for the project, on a form approved by the Planning and Zoning Division. Proof of recordation shall be provided to the Planning and Zoning Division.

FAIR SHARE IMPROVEMENTS

25. West Grand Avenue/Frontage Road Intersection.

Prior to issuance of the first building permit.

The Project Sponsor shall fund, on a fair share basis, the following improvements at the intersection of West Grand Avenue/frontage road:

- Revise the northbound frontage road lanes to provide:
 - one left-turn lane
 - one combination left-through lane
 - one through lane
 - one right-turn lane with overlap signal phasing (green arrow)
- Revise the southbound I-80 East Ramp lanes to provide:
 - one left-turn lane
 - one combination left-through lane
 - one through lane
 - one right-turn lane with overlap signal phasing (green arrow)
- Revise the eastbound West Grand Avenue lanes to provide:
 - one left-turn lane
 - one through lane
 - one combination through-right lane
- Revise the westbound West Grand Avenue lanes to provide:
 - one left-turn lane
 - two through lanes
 - one right-turn lane

The estimated amount of the Project Sponsor's contribution is \$1.596 million. Final determination of the Project Sponsor's contribution shall be based on a reasonable formula of the expected growth in traffic at the intersection. This formula shall be devised at the sole and complete discretion of the City of Oakland, and final cost estimates shall include right-of-way costs and all project support costs including design and engineering, construction oversight, preparation of plans and specifications, and detailed project cost estimates. The measured

growth in traffic is based on the traffic analysis in the EIR and the City has no obligation to fund any required improvements in the future. [WS MM TR-9.1]

26. West Grand Avenue/Mandela Parkway Intersection

Prior to issuance of the first certificate of building occupancy.

The Project Sponsor shall contribute its fair share of modifications at the West Grand Avenue/Mandela Parkway intersection estimated at \$180,000 (in combination with condition of approval #27, including design and engineering, construction oversight, preparation of plans and specifications and detailed project costs estimates.) The modifications at the intersection shall include providing protected left-turn signal phasing (left-turn green arrows) for the West Grand Avenue approaches to the intersection. [WS MM TR-9.2]

27. 7th Street/Mandela Parkway Intersection.

Prior to issuance of the first certificate of building occupancy.

The Project Sponsor shall contribute its fair share of modifications at the 7th Street/Mandela Parkway intersection estimated at \$180,000 (in combination with condition of approval #26, including design and engineering, construction oversight, preparation of plans and specifications and detailed project costs estimates). The modifications at the intersection shall include adding a northbound lane on the 3rd Street extension to provide one left-turn lane, one combination through-right turn lane, and protected left-turn signal phasing (left-turn green arrows) for all four approaches to the intersection. [WS MM TR-9.3]

28. West Grand Avenue/Maritime Street and 3rd Street/Market Street Intersections.

Prior to issuance of the first certificate of building occupancy.

As part of the cumulative growth of the OARB Area Redevelopment Plan, the Project Sponsor shall pay an amount equal to its fair share, estimated at \$180,000, as determined by the OARB Area Redevelopment Plan EIR, 2002, of future improvements at West Grand Avenue/Maritime Street and 3rd Street/Market Street intersections. [WS MM TR-9.4]

29. BART Train Capacity.

Prior to issuance of the first certificate of building occupancy.

The Project Sponsor shall participate in efforts to ensure that adequate BART train capacity will be available for riders to and from the Project Area, and fund BART train capacity improvements on a fair share basis. [WS MM TR-12.1]

30. West Oakland BART Station.

Prior to issuance of the first certificate of building occupancy.

The Project Sponsor shall participate in efforts to provide adequate fare gate capacity at the West Oakland BART Station to accommodate the Project. The City and the Project Sponsor shall provide detailed information regarding development to BART to enable BART to conduct a comprehensive fare gate capacity assessment at the West Oakland BART Station. Based on the results of that assessment, the Project Sponsor shall fund its fair share for adding one or more new fare gates at the West Oakland BART Station. *[This condition will be attached to the subdivision maps for Parcels 1, 2, 3, and 4 of VTPM 8551, Parcels 1 and 2 for VTPM 8552,*

Parcels 1, 2, and 3 for VTPM 8553, Parcel 3 of VTPM 8554, and Parcels 1 and 2 of VTPM 8555 only.] [WS MM TR-8.1].

31. Cul-de-Sac or other Turn-Arounds.

Prior to approval of Final Development Plan and specifications.

The Project Sponsors shall incorporate the design of a cul-de-sac or other appropriate turn-around at the end of 11th Street and at the end of the 18th and 20th Street extensions and construct these extensions in compliance with City of Oakland Design Standards. Appropriate turn-around designs would allow vehicles to return along 11th Street and enter Wood Street in a front-end-first manner. *[This condition will be attached to the subdivision maps for Parcels 1 and 2 of VTPM 8552, Parcel 3 of VTPM 8554, and Parcels 1 and 2 of VTPM 8555 only.] [WS MM TR-4.1]*

32. Underground Utilities.

Prior to issuance of a building permit.

The Project Sponsor shall submit plans for review and approval of the Planning and Zoning Division, Building Services Division and the Public Works Agency, and other relevant agencies as appropriate. The plans shall show all new electric and telephone facilities; fire alarm conduits; street light wiring; other wiring, conduits, and similar facilities placed underground by the developer from the Project Sponsor's structures to the point of service; and all electric and telephone facilities installed in accordance with standard specifications of the serving utilities.

33. Maintenance of Land Dedicated to Public.

Prior to recordation of the Final Map.

The Project Sponsor shall enter into a Maintenance Agreement in a form acceptable to the City Attorney, which shall be made binding on all successors and assigns and which obligates the owner(s) of each parcel included in the VTPM to pay, on a fair share basis, for the City's reasonable costs of maintaining the public access areas (also referred to as pocket parks), that are located between the terminus of 14th, 16th, 18th and 20th Streets and frontage road to be offered for dedication to the City. As used herein, "fair share" means dividing the number of residential units owned by the number of residential units built within Parcels 1, 2, 3 and 4 of VTPM 8551, Parcels 1 and 2 of VTPM 8552, Parcels 1, 2 and 3 of VTPM 8553, Parcel 3 of VTPM 8554, and Parcels 1 and 2 of VTPM 8555. Concurrently with the execution of the Maintenance Agreement, the Project Sponsor shall submit security in a form acceptable to the City Attorney (e.g., set-aside letter of credit) securing this obligation for a period of five years. Although the obligation is secured for five years only, the Maintenance Agreement will require an annual payment of the fair share amount for the life of the project. This covenant shall expire as to any streets or parks that are modified to meet City standards, as determined by the Planning Director. *[This condition will be attached to the subdivision maps for Parcels 1, 2, 3 and 4 of VTPM 8551, Parcels 1 and 2 of VTPM 8552, Parcels 1, 2 and 3 of VTPM 8553, Parcel 3 of VTPM 8554, and Parcels 1 and 2 of VTPM 8555.]*

55. Stabilization of Main Hall and Signal Tower.

Within three months of the effective date of the adoption of the condition of approval.

The Project Sponsor of Parcels 2 and 3 of VTPM 8554 shall, within three months of the effective date of the Wood Street Zoning District, take measures designed to preclude further deterioration of the Main Hall and the Signal Tower (both as defined in the notes and definitions on page 1) from rain and to exclude trespassers. Within 45 days of the effective date of the adoption of this condition of approval, the Project Sponsor shall submit to the Planning Director a description of the proposed measures. The Planning Director shall review, and may approve, disapprove, or modify the measures intended to eliminate deterioration or vandalism. These measures shall remain in place until the decision regarding reuse of the Main Hall is made. The facilities preserved and protected by this measure include the canopy at the Wood Street entrance to the Main Hall. *[This condition will be attached to the subdivision maps for Parcels 2 and 3 of VTPM 8554 only.]* [WS MM CR-2.3]

56. Interim Stabilization of Baggage Wing.

Within three months of the effective date of the adoption of the condition of approval.

The Project Sponsor of Parcel 2 of VTPM 8554 shall, within three months of the effective date of the Wood Street Zoning District, take measures designed to preclude further deterioration of the Baggage Wing from rain and to exclude trespassers, pending a decision from the Redevelopment Agency on whether to fund retention of the Baggage Wing. Within 45 days of the effective date of the adoption of the condition of approval, the Project Sponsor shall submit to the Planning Director a description of the proposed measures. The Planning Director shall review, and may approve, disapprove, or modify the measures intended to eliminate deterioration or vandalism. These measures shall remain in place until a decision from the Redevelopment Agency on whether to fund retention of the Baggage Wing is made. *[This condition will be attached to the subdivision map for Parcel 2 of VTPM 8554 only.]*

Effective
Date =
July 7, 2004

56A. Possible Agency Funding for Preservation and Restoration of Baggage Wing.

Prior to demolition or renovation of the Baggage Wing.

(a) To assist the Redevelopment Agency in its efforts to preserve and restore the Train Station, the Project Sponsor of Parcel 3 of VTPM 8554 ("BUILD") shall set aside the additional approximate .33 acre of land depicted on VTPM 8554 upon which the Baggage Wing is located (the "Baggage Wing Parcel") for a period of no less than eight months following the effective date of the Wood Street Zoning District, and shall include such parcel in the final map of Parcel 2 of VTPM 8554. During this period (1) BUILD and the Train Station Entity, defined in the General Notes and Definitions and described in Condition 59, shall negotiate in good faith to enter into an agreement for the Train Station Entity to purchase the Baggage Wing Parcel from BUILD within said eight-month period for the Purchase Price (defined below), (the "Baggage Wing Purchase Agreement") and (2) upon the receipt and acceptance by the Redevelopment Agency Board of a Business Plan, Financing Plan and Management Plan from the Train Station Entity, the Redevelopment Agency shall determine whether to make available the financial assistance necessary for the Train Station Entity to acquire the Baggage Wing Parcel and preserve the Baggage Wing, and the Train Station Entity and the Redevelopment Agency shall then negotiate in good faith to enter into an agreement for such financial assistance (the

“Baggage Wing Funding Agreement”). The Baggage Wing Funding Agreement shall (3) provide for Agency funding for the acquisition and preservation of the Baggage Wing and the Baggage Wing Parcel from available tax increment funds as such funds are received, under customary redevelopment agency terms and conditions; and (4) provide that the Train Station Entity shall seek funding from non-Agency and non-City sources (including state and federal programs, historic preservation tax credits, and private lenders) as appropriate for the acquisition, preservation, and restoration of the Baggage Wing and the Baggage Wing Parcel. For purposes of this condition, the term “available tax increment funds” shall mean the net tax increment revenues generated by the property in the Wood Street Project that are actually received by the Agency, excluding any pass-through payments to other entities, payments to the Low and Moderate Income Housing Fund, the Educational Revenue Augmentation Fund, or other set aside funds, or funds otherwise diverted from the Agency.

(b) In the event the Baggage Wing Purchase Agreement and the Baggage Wing Funding Agreement are executed within the eight month period, then:

- (1) BUILD will include the Baggage Wing in the materials, plans and information to be prepared in accordance with Conditions of Approval 58 and 59, BUILD will not make any alteration to the Baggage Wing that is not consistent with the preservation, rehabilitation, or reuse recommendations contained in the *OARB Area Redevelopment Plan* (as amended); the *City of Oakland General Plan* (as amended); the Wood Street Zoning District; and Secretary of the Interior’s Standards for the Treatment of Historic Buildings, and alterations shall be further restricted in accordance with any additional design standards, guidelines, or recommendations when the development plan, adopted pursuant to Mitigation Measure CR-2.5, becomes effective;
- (2) in accordance with Condition 59, BUILD will transfer title to the Baggage Wing Parcel to the Train Station Entity upon approval by the City Council, which approval shall be based upon the Council’s satisfaction of the progress in implementing Conditions 58, 59 and 60; and
- (3) the City will cooperate to develop and approve preliminary and final development plans for the remaining areas of Parcel 3 of VTPM 8554 such that the Project Sponsor will be able to build the residential units approved for development on such parcel, which cooperation may include, subject to appropriate environmental review, future approval of residential use of certain areas of the future adjacent right of way located in 18th street, provided that an equivalent area of public space is provided elsewhere within the Baggage Wing Parcel or the remaining areas of Parcel 3.

(c) For purposes of the above, the “Purchase Price” for the Baggage Wing Parcel shall be the lesser of (1) Fair Market Value, or (2) the Acquisition/Holding/Entitlement Costs. “Fair Market Value” shall mean the purchase price that an unrelated party negotiating at arm’s length would pay to purchase such property, taking into account all then current market factors,

including without limitation the quality, design, condition and location of the property including the extent and condition of the construction completed to date, if any, the amount of any and all liens, mortgages, and encumbrances against the property, required environmental remediation, and the value of the existing improvements to such party, and assuming the property was included with Parcel 3 of VTPM 8554. "Acquisition/Holding/Entitlement Costs" shall mean (1) the actual price initially paid for the Baggage Wing Parcel by BUILD, calculated on the basis of the overall purchase price paid by BUILD for developable acres within the Wood Street Zoning District and prorated on a square footage basis to the Baggage Wing Parcel, plus (2) actual costs incurred by BUILD in holding, maintaining and entitling the Baggage Wing Parcel (calculated on a developable square footage basis prorated to the Baggage Wing Parcel), including taxes, carrying costs (which shall be defined as the investment return BUILD's investor, CalPERS, has received on investments in similar projects in the CalPERS CURE program (California Urban Real Estate) since its inception in 1997, but not to exceed 10%), insurance, maintenance, and other out-of-pocket payments by BUILD to third parties for holding, maintaining and entitling the property, but not including BUILD's administrative or staff costs. The Acquisition/Holding/Entitlement Costs shall be determined by an independent cost certification obtained by BUILD.

(d) BUILD shall submit its determination of Acquisition/Holding/Entitlement Costs, along with the independent cost certification, and its determination of Fair Market Value to the Redevelopment Agency within three months of the City Council's approval of the District. If the Agency disputes BUILD's determination either of Fair Market Value or Acquisition/Holding/Entitlement Costs, or both, as contained in BUILD's notice, the Agency shall notify BUILD in writing within 30 calendar days of its receipt of BUILD's determination, which notice shall set forth the Agency's determination of the Fair Market Value and/or Acquisition/Holding/Entitlement Costs. The Agency and BUILD shall thereupon attempt to resolve their differences within 10 days following BUILD's receipt of the Agency's notice. If the Agency and BUILD cannot agree on Fair Market Value during such 10-day period, the Agency and BUILD shall each appoint an appraiser who shall be an M.A.I. and a California licensed appraiser experienced in appraising commercial and residential real estate in Alameda County, and give notice of such appointment to the other within 10 calendar days after the foregoing 10-day period. Such appraisers shall, within 30 calendar days after the appointment of the last of them to be appointed, complete their written determinations of Fair Market Value and furnish the same to the Agency and BUILD. Each party shall pay the fees and costs of the appraiser appointed by it. If the valuations vary by ten percent (10%) or less of the higher value, the Fair Market Value shall be the average of the two valuations. If the valuations vary by more than ten percent (10%) of the higher value, the two appraisers shall, within ten (10) calendar days after submission of the last appraisal report, appoint a third disinterested appraiser who shall be an M.A.I. and a California licensed appraiser with the experience described above. If the two appraisers are unable to agree in a timely manner on the selection of the third appraiser, then either appraiser, on behalf of both, may request appointment of such third disinterested M.A.I. appraiser by the presiding judge of the Superior Court of Alameda County. Such third appraiser shall, within 15 calendar days after appointment, make a determination of Fair Market Value by selecting one of the prior appraisals. The third appraiser shall have no right to select a Fair

Market Value other than as determined by one of the prior appraisals. If the Agency and BUILD cannot agree on Acquisition/Holding/Entitlement Costs during such 10-day period, the Agency and BUILD shall submit the issue to binding arbitration.

(e) If, after good faith negotiations, BUILD and the Train Station Entity have not entered into the Baggage Wing Purchase Agreement within the eight-month period, or the Agency and the Train Station Entity have not entered into the Baggage Wing Funding Agreement within the eight-month period, then the Agency or the Agency's designee shall have the option of purchasing the Baggage Wing Parcel for the Purchase Price. The Agency or its designee shall give written notice of its exercise of said option to BUILD (or the then-current owner of the Baggage Wing Parcel) within 60 calendar days of the end of the eight-month period.

(f) Should the Agency exercise the above option, BUILD shall deliver title to the Baggage Wing Parcel to the Agency free and clear of any junior liens, leases, mortgages, or encumbrances, except those liens, mortgages, or encumbrances that have been specifically approved by the Agency in writing. Escrow for the sale of the Baggage Wing Parcel shall close and BUILD shall execute and deliver to the Agency or its designee a grant deed or deeds to the Baggage Wing concurrent with the transfer of the Main Hall, in accordance with Condition 59, at which time the Purchase Price shall be paid by the Agency or its designee to BUILD. The Purchase Price shall be paid in the form of a promissory note executed by the Agency to BUILD in which the Agency pledges to pay the Purchase Price from available tax increment funds as such funds are received, along with interest at a rate equal to the prevailing Local Agency Investment Fund rate. The Agency shall be allowed to prepay the note without penalty. Prior to the close of escrow, BUILD shall take all necessary steps to ensure that a title company will be able to issue to the Agency or its designee, upon close of escrow, a standard CLTA owner's policy of title insurance, in an amount equal to the Purchase Price, showing title to the Baggage Wing Parcel vested in the Agency or its designee, with only the following exceptions:

- (1) Liens for property taxes not yet due and payable;
- (2) Any other lien or encumbrance approved in writing by the Agency in its sole discretion.

(g) Prior to the time for the Agency to provide notice of its election to exercise the option, the Agency or its designee and its agents may, upon reasonable notice to BUILD, enter the Baggage Wing Parcel for purposes of inspection, survey, tests, or other actions reasonably related to acquisition of the property by the Agency or its designee. The Agency or its designee shall indemnify and defend BUILD for any liability, claims or damages arising from such entry.

(h) The Agency may assign the option to purchase the Baggage Wing Parcel to any other entity in its sole discretion.

(i) If BUILD and the Train Station Entity have not entered into the Baggage Wing Purchase Agreement, or if BUILD and the Agency have not entered into the Baggage Wing

Funding Agreement, within the eight month period and any of them assert that one or the other of them has not negotiated the agreement in good faith, BUILD, the Agency or the Train Station Entity shall submit the issue to binding arbitration.

(j) If BUILD and the Agency have not entered into the Baggage Wing Funding Agreement within the eight month period because the Agency has declined to provide the funding from available tax increment funds necessary for the Train Station Entity to purchase and preserve the Baggage Wing Parcel as set forth above, and if the Agency or its designee has declined to exercise its option to purchase the Baggage Wing Parcel as set forth above, then BUILD shall have no further obligations under this condition. *[This condition will be attached to the subdivision map for Parcel 2 of VTPM 8554 only.]*

56B. Feasibility Determination If Funding Not Provided For Baggage Wing.
Prior to demolition or renovation of Baggage Wing.

In the event that the Baggage Wing Purchase Agreement and the Baggage Wing Funding Agreement described in Condition 56A are not executed within the eight month period, the Project Sponsor shall submit for City Council review application to demolish the Baggage Wing or its application to alter the Baggage Wing. Any alteration of the Baggage Wing shall be consistent with the preservation, rehabilitation, and reuse recommendations contained in the OARB Area Redevelopment Plan (as amended by action concurrent with approval of the Wood Street Zoning District), the City of Oakland General Plan (as amended by action concurrent with approval of the Wood Street Zoning District), the Wood Street Zoning District, and Secretary of the Interior's Standards for the Treatment of Historic Buildings. Upon review of detailed pro forma information (amount invested, return on equity, financing options) and any other information requested by the City Council, including CEQA compliance determination, financing requirements and other pertinent information, the City Council shall approve, deny, or conditionally approve the application.

57. Restriction on Alteration of the Main Hall and the Signal Tower.
Prior to demolition or renovation of any structures.

The Project Sponsor of Parcels 2 and 3 of VTPM 8554 shall not make any alteration to the Main Hall that is not consistent with the preservation, rehabilitation, or reuse recommendations contained in the *OARB Area Redevelopment Plan* (as amended); the *City of Oakland General Plan* (as amended); the Wood Street Zoning District; and Secretary of the Interior's Standards for the Treatment of Historic Buildings. Alterations shall be further restricted in accordance with any additional design standards, guidelines, or recommendations when the development plan, adopted pursuant to Mitigation Measure CR-2.5, becomes effective. *[This condition will be attached to the subdivision maps for Parcels 2 and 3 of VTPM 8554 only.]* [WS MM CR-2.4]

57A. Restriction on Alteration of the Elevated Platform Feasibility Study Area.
Prior to demolition of the elevated track platform adjacent to the Main Hall.

The Project Sponsor shall not make any alteration to the Elevated Platform Feasibility Study Area until the Project Sponsor has further pursued, with due diligence, the feasibility of retaining and preserving more width of the Elevated Platform Feasibility Study Area, than is shown in

FEIR Figure CR-4 and, in any event, no alteration shall be made prior to the approval of a preliminary development plan for Parcel 3 of VTPM 8554. The Project Sponsor shall include in its preliminary development plan application for Parcel 3 of VTPM 8554 additional evidence of the feasibility or infeasibility of retaining a greater width of the Elevated Platform Feasibility Study Area. The determination of the width to be retained and preserved shall be made by the City in connection with its consideration and approval of such preliminary development plan. The preliminary development plan shall include retention and preservation of as much width of the Elevated Platform Feasibility Study Area as the City determines is feasible. The precise location of the parcel line between Parcel 2 and Parcel 3 on the final map for Parcel 3 shall accommodate such determination. *[This condition will be attached to the subdivision map for Parcel 3 of VTPM 8554 only.]*

58. Application for Redevelopment Agency Funding Approval for Train Station Preservation, Rehabilitation, and Stabilization.

Within 12 months of the effective date of the adoption of this condition of approval.

Consistent with the *OARB Area Redevelopment Plan* goals as set out in Section 100 of that Plan, the Project Sponsor of Parcel 2 of VTPM 8554 or the Train Station Entity shall submit an application to the Redevelopment Agency of the City of Oakland (the "Agency") requesting that the Agency make available tax increment funds provided for in Section 502 of the *OARB Area Redevelopment Plan* for the preservation, rehabilitation, and stabilization of the Main Hall. In connection with such application, the property owner shall submit the following materials and information to the Agency:

- a. a finance plan demonstrating the prudent use of tax increment funds in restoring, preserving, and reusing the Main Hall, including a commitment by the property owner to maximize the leverage of the tax increment funds by seeking additional public funding, tax credits, private financing, and/or private philanthropic grants;
- b. a management plan demonstrating exemplary and continued stewardship of the Main Hall, with recognition of its cultural and historical importance to the City of Oakland and which is accountable to the goals and policies of the *OARB Area Redevelopment Plan* and the *City of Oakland General Plan*;
- c. a community participation plan providing for input by Oakland community members in decisions concerning the Main Hall's preservation and reuse;
- d. a development plan demonstrating that the proposed renovation and reuse of the Main Hall is consistent with the design standards, policies, and goals of the *OARB Area Redevelopment Plan* (as amended); the *City of Oakland General Plan* (as amended); and the Wood Street Zoning District; as well as with any other design criteria that the Agency determines is appropriate to meet said goals and policies; and
- e. a business plan that establishes a framework for the funding of rehabilitation efforts and identifies the grant source(s), the funding mechanisms and the budget for the work, as provided in Condition 59.

Not in limitation of the Project Sponsor's obligations in Conditions No. 95, 96 and 97, any Agency decision to fund all or any portion of the 16th Street Train Station, including either the Main Hall, Baggage Wing and/or Signal Tower shall be conditioned upon Project Sponsor's indemnification of the Agency and the City for any claims related to the construction, operation or maintenance of any and all projects using Agency funds.

[This condition will be attached to the subdivision map for Parcel 2 of VTPM 8554 only.]
[WS MM CR-2.5]

59. Rehabilitation and Reuse of Main Hall, Platform and Signal Tower.

Within six months of the effective date of the adoption of this condition of approval for establishment of a Train Station Entity to oversee the rehabilitation and reuse of the historic 16th Street Train Station; within twelve months of the effective date of the adoption of this condition of approval to prepare a business plan for the retention of historic resources and the reuse of the 16th Street Train Station.

Within six months of adoption of this condition of approval, the Project Sponsor will establish a Train Station Entity to oversee the rehabilitation and reuse of the historic 16th Street Train Station (as defined in the General Notes and Definitions), which will assure public access and include elements commemorating its historical significance, and within twelve months of the effective date of this condition of approval the Project Sponsor shall obtain City Council approval of such Train Station Entity. Within twelve months of adoption of this condition of approval, the Project Sponsor and/or the new Train Station Entity for the historic 16th Street Train Station shall prepare a business plan for the retention of historic resources and the reuse of the 16th Street Train Station. The business plan will establish a framework for the funding of rehabilitation efforts and identify the grant source(s) and the funding mechanisms for the work. The business plan will also establish the information needed for requesting tax increment financing and the timing and sequencing of such funding in relation to the phasing of the historic restoration efforts. Within two years of approval of the Wood Street Zoning District, the Project Sponsor will complete a schematic set of plans and specifications for the restoration of the 16th Street Train Station. The plans shall include an analysis of the feasibility of restoration and reuse of the structure and establish a budget for the project to demonstrate the viability of proposals related to possible use of historic resources and identify important details about how modifications to historic resources will be integrated into the final project. Upon receipt of Agency tax increment funds and other public and/or private funds in accordance with Condition of Approval #58, the Project Sponsor of Parcel 2 of VTPM 8554 will, within one year, diligently commence and pursue the completion, within seven years, in accordance with the plans and specifications for the restoration of the 16th Street Train Station, and rehabilitation of the facilities depicted for retention in Figure 2-4 of the Draft EIR of the proposed project, in accordance with the Secretary of the Interior's Standards for the Treatment of Historic Buildings, and in conformance with the following General Standards.

- (1) Any renovation, modification or addition to the 16th Street Train Station shall conform with the standards set forth in the Planning Code "Special regulations of designated landmarks."

(2) Any reuse of the 16th Street Train Station shall include stabilization and repair of exterior materials to improve the exterior appearance and to ensure a watertight building envelope. This rehabilitation shall include using salvaged materials to the extent feasible, and seismically strengthening and rehabilitating the exterior of the Main Hall, including the portions of the platform that are to be preserved. No additions to the structures would be permitted except as specified in the following standards:

(a) No addition to the existing 16th Street Train Station shall exceed a total building footprint greater than 20 percent of the existing structure to be retained.

(b) No addition to the existing 16th Street Train Station shall exceed the height of the north or south wings that flank the Main Hall (approximately 25 feet in height).

(c) No addition shall be made to either the primary façade facing the 16th Street Plaza or the southern façade, facing the 16th Street right of way or pocket park.

(d) No additions are permitted to the Signal Tower. Plaques shall be installed on the exterior façade of the station and the Signal Tower that identify their historic uses and include additional historical information. A display shall be created on the interior of the station using historic photos and documents to give a more complete history of the station and the Signal Tower.

The Project Sponsor of VTPM 8554 shall not transfer title of the retained portion of the 16th Street Train Station to the Train Station Entity unless and until the transfer is approved by the City Council, which shall be based upon Council's satisfaction of the progress in implementing Conditions 58, 59 and 60. Not in limitation of the Project Sponsor's obligations in Conditions No. 95, 96 and 97, in the agreement between the Project Sponsor and Agency to fund all or any portion of the 16th Street Train Station (as defined in the General Notes and Definitions), the Project Sponsor shall indemnify, in a form acceptable to the City Attorney, the Agency and the City for any claims related to the construction, operation or maintenance of any and all projects constructed by or at the direction of a Project Sponsor, using Agency funds.

Not in limitation of the Project Sponsor's obligations in Conditions No. 95, 96 and 97, any Agency decision to fund all or any portion of the 16th Street Train Station, including either the Main Hall, Baggage Wing and/or Signal Tower shall be conditioned upon Project Sponsor's or the Train Station Entity's indemnification of the Agency and the City for any claims related to the construction, operation or maintenance of any and all projects using Agency funds. ***[This condition will be attached to the subdivision maps for Parcels 2 and 3 of VTPM 8554 only.]***
[WS MM CR-2.6]

60. Reuse of the Main Hall

Prior to issuance of the first certificate of occupancy; upon approval of funding by the Redevelopment Agency as specified in WS MM CR-2.5.

Following the satisfaction of the prior Condition No. 59, the Project Sponsor of Parcel 2 of VTPM 8554 in the reuse of the Main Hall shall incorporate exhibit space commemorating the site's cultural history and its function as the end of the trans-continental railroad and the gateway arrival point in the West. The exhibit space could also serve as a venue for private and public events, facilitating greater exposure of persons to the historical significance of the station. Oral histories shall be recorded and made available to the extent feasible. The building would not be subjected to extensive night lighting. Reuse shall proceed according to the finance, management, community participation, and development plans submitted pursuant to Condition No. 58, as approved by the Redevelopment Agency, as well as any other design criteria that the City Planning Director determines is appropriate to meet the City's goals and policies. ***[This condition will be attached to the subdivision map for Parcel 2 of VTPM 8554 only.]*** [WS MM CR-2.7]

61. Enhancement of the Train Station Setting

Prior to issuance of certificate of building occupancy of the restored Main Hall or issuance of a certificate of occupancy for the 600th residential dwelling with the Project Area, whichever occurs first.

The Project Sponsor of Parcel 1 of VTPM 8554 shall construct and landscape the plaza area to provide an enhanced visual setting for the Main Hall, to provide a visual focus and view corridor, to increase public accessibility to the 16th Street Train Station, and to create a feature that recalls the historic use of the station. All these improvements shall be completed with private financing by the Project Sponsor; no public funds would be requested with respect to the Plaza. ***[This condition will be attached to the subdivision map for Parcel 1 of VTPM 8554 only.]*** [WS MM CR-2.8]

LANDSCAPING & TREE PERMIT

62. Installation of Landscaping and Bonding.

Prior to issuance of certificate of occupancy, unless bonded.

The Project Sponsor shall install all proposed landscaping indicated on the approved landscape plan prior to the issuance of a certificate of occupancy, unless bonded pursuant to the provisions of Section 17.124.50 of the Oakland Planning Code. The amount of such bond or cash deposit shall equal the greater of \$2,500 or the estimated cost of the required landscaping, based on a licensed contractor's bid.

63. Tree Removal Permit.

Prior to the removal of subject trees.

The Project Sponsor shall obtain a Tree Removal Permit prior to removing trees subject to Chapter 12.36 of the Oakland Municipal Code. [OARB MM 4.12-7]

64. Securing Future Residents' Acknowledgment of Potential Future Land Uses.

Prior to lease or sale of any unit and ongoing.

The Project Sponsor shall ensure that future residents sign a notice acknowledging that they are aware of and accept the possible noise levels related to Frontage Road located near the project site.

65. Reduced Water Usage.

Prior to issuance of the building permits for the mechanical system.

The Project Sponsor shall confer with East Bay Municipal Utility District (EBMUD) to examine incorporating water saving techniques such as dual piping for recycled water into the final design of the mixed-use project.

66. Internal Landscaping.

Prior to issuance of the certificate of occupancy.

The Project Sponsor shall ensure that internal landscaping conforms with City design standards as contained in the City Planning Code.

NOISE (OPERATIONAL) & OTHER NUISANCES

67. Operational Noise.

Ongoing.

Noise levels from the activity, property, or any mechanical equipment on site shall comply with the performance standards of Section 17.120 of the Oakland Planning Code and Section 8.18 of the Oakland Municipal Code. If noise levels exceed these standards, the activity causing the noise shall be abated until appropriate noise reduction measures have been installed and compliance verified by the Planning and Zoning Division and Building Services.

68. Compliance with Title 24.

Prior to issuance of certificate of occupancy.

The Project Sponsor shall implement acoustical techniques in compliance with Title 24 to ensure that noise levels in interior habitable spaces remain at or below 45 CNEL with all doors and windows closed.

68.A. Buffer Area Adjacent to California Waste Solutions Facility.

Ongoing.

The Project Sponsor of Parcel 1 of Vesting Tentative Parcel Map No. 8551 shall not develop residential units within said parcel, and the Project Sponsor of Parcel 1 of Vesting Tentative Parcel Map No. 8552 shall not convert the structure (known as the "Ice House") currently located on that parcel into residential units or otherwise develop residential units so long as California Waste Solutions is actively operating its plant in substantially the same manner as at the time of approval of this condition, including using the same recycling technologies and hours of operation and generating substantially the same truck traffic, odor, and noise, on the parcel identified as APN 006-0029-003-02 (located between 10th and 11th Streets, north of Pine Street). Notwithstanding the foregoing, residential units may be developed sooner if buffering measures are incorporated into the design of the residential development (such as increasing the distance between the CWS operations and future residential units, adding more landscaping, modifying

building orientation or access, and applying Title 24 noise abatement measures), which the Planning Director determines, after notice and a hearing, sufficiently reduce land use conflicts between the current CWS operations and future residential units. *[This condition will only apply to Parcel 1 of VTPM 8551 and Parcel 1 of VTPM 8552.]*

PARKING AND DRIVEWAYS

69. Closure of Parking Lot After Hours.

Ongoing.

The project is approved with more than ten on-site parking spaces, and the project site is located within the area designated; therefore, all openings to the parking lot used for the ingress or egress of motor vehicles shall be secured by an approved barrier chain and locking device within one (1) hour after the close of business and shall remain secured until one (1) hour prior to the opening of business, pursuant to the standards included in City Ordinance No. 12390, with access only granted to tenants and police.

70. Off-site Parking Agreement.

Prior to approval of any final development plan for which off-site parking is requested..

In the event that the project sponsor of any proposed development subject to these conditions shall request to satisfy required parking obligations off-site, and pursuant to Chapter 17.116 of the Oakland Planning Code, the Project Sponsor and the property owner(s) of off-site property located within 300 feet of the project boundary shall prepare, using forms provided by the Planning and Zoning Division, execute to the satisfaction of the City Attorney, and file with the Alameda County Recorder, a "Joint Parking Agreement" specifying that the required number of donor parking spaces shall be provided at the off-site property location to satisfy on-site parking requirements for the required number of parking stalls approved by the applicable permit. Said donor parking spaces shall be designated for said activity for the duration of operation of the activity approved by the permit.

71. Parking and Circulation Plan.

Prior to issuance of building permit and prior to final inspection.

The Project Sponsor shall submit a Parking and Traffic Circulation Plan for review and approval by the Planning and Zoning Division. This plan shall include wheel stops for all parking spaces, and pavement marking and striping that delineate the driveways and traffic paths to be used by the general public and deliveries. All wheel stops, pavement markings and striping, as approved by the Planning and Zoning Division shall be installed prior to final inspection.

72. Parking Lot Lighting.

Ongoing.

The exterior lighting fixtures which serve the parking area shall be equipped with daylight sensors or computerized time clocks that will automatically turn the lights on at dusk and off at sunrise, and that shall be adequately shielded to a point below the bulb and reflector, and that shall prevent unnecessary glare onto adjacent properties.

73. Parking Spaces

Ongoing.

The Project Sponsor shall include parking spaces in the Project Area as required by the Wood Street Zoning District.

74. Designated Parking.

Ongoing.

The Project Sponsor shall designate all on-site parking spaces consistent with the Oakland Municipal Code by marking, either with a small sign at the head of the parking stalls or stenciled lettering painted with the parking stalls, that read “resident-only,” or “employee-only” parking as appropriate.

PARKING & TRANSPORTATION MANAGEMENT

75. Transportation Demand Management.

Prior to issuance of the certificate of building occupancy; upon City adoption of a traffic demand management program in West Oakland.

The Project Sponsor shall distribute materials concerning the availability of public transit to initial Project residents, and prior to certificate of occupancy shall pay the fee adopted by the City on residential units to assist the City in implementing traffic demand management programs. [WS MM TR-10.1]

76. Shuttle Service.

Prior to approval of Final Development Plans and specifications; within three months following the issuance of a certificate of occupancy of the 300th residential dwelling with the Project Area; every two years thereafter until the Planning Director determines the shuttle service is no longer necessary.

The Project Sponsor shall provide or cause to be provided a public or private shuttle service between the Project Area and the West Oakland BART Station and incorporate shuttle stops into the final design. The Project Sponsor shall provide full funding for the shuttle service whether it is public or private. In the event the Project Sponsor elects to not use a private shuttle service, the Project Sponsor shall work with AC Transit and BART to design a public shuttle service and incorporate public transit stops into the final development plans in consultation with AC Transit. The shuttle or transit stops shall be located within the Project Area and would be dispersed such that Project residents would be no more than one-quarter mile from a shuttle or transit stop. Shuttle or transit stops at the existing AC transit bus stop on Wood Street by Parcel 3 of VTPM 8553, in front of the 16th Street Plaza (Parcel 1 of VTPM 8554), and on Wood Street at 20th Street by Parcel 1 of VTPM 8555 should be considered. The shuttle service would operate at 15-minute peak-hour headways during commute hours. The shuttle service shall be designed to meet City of Oakland standards, link with pedestrian access, and be reviewed for approval by the City.

The shuttle service shall be implemented within three months following the issuance of a Certificate of Occupancy of the 300th residential dwelling within the Project Area. At that time, the Project Sponsor, or its successor in interest, will fund operation and maintenance of the

shuttle. Thereafter, and every two years until such time as the Planning Director determines that the shuttle service is no longer necessary, the Project Sponsor or its successor shall report to the Planning Director on the amount of shuttle use by Project residents and occupants, and the availability of other means to reduce the use of private vehicles by Project residents and occupants. The Planning Director shall permit discontinuation of the shuttle service upon finding either that (a) the shuttle is not being used sufficiently to result in a substantial reduction in private vehicle use by Project residents and occupants, or (b) another means of reducing the use of private vehicles by Project residents and occupants would be feasible and cost the same or less than the shuttle, would create a greater reduction in private vehicle use than would the shuttle, and would result in a substantial reduction in private vehicle use by Project residents and occupants. If the Planning Director determines item (b), above, is the basis for discontinuing the shuttle service, then the Project Sponsor or its successor or their successors shall implement other means of reducing private automobile use by Project residents and occupants. [WS MM TR-10.2]

PUBLIC IMPROVEMENTS

77. Conformance with Vesting Tentative Parcel Maps.

Ongoing.

All public improvements shall be constructed in substantial conformance with the individual vesting tentative parcel maps submitted by the Project Sponsors and as specified in Condition of Approval Numbers 78 through 82.

78. Public Improvements – Vesting Parcel Map 8551.

Prior to the issuance of certificate of occupancy for development on each parcel.

Project Sponsor of Parcel 1 of VTPM No. 8551 shall construct or cause the construction of improvements to the extension of 10th Street, including the pocket park. Project Sponsor of Parcel 2 shall construct or cause the construction of improvements to the portion of 14th Street accessed from the frontage road. Project Sponsor of Parcel 3 shall construct or cause the construction of improvements to the existing 14th Street right of way, as well as the portion accessed from the frontage road, should its development precede parcel 2 of this map or Parcel 1 of Map 8553. Project Sponsor of Parcel 4 shall construct or cause the construction of improvements to 12th Street, Wood Street from 12th Street to 14th Street, and 14th Street should development on this parcel precede development of Parcel 2 or 3 of this map and Parcel 1 of Map 8553.

Except as otherwise provided in this condition, the street improvements referred to in this condition include complete street width, curb, gutter, sidewalk, and installation of utilities in accordance with the standards of the City of Oakland to the limits shown on VTPM 8551. Sidewalks on the opposite side of Wood Street Zoning District perimeter streets (i.e. outside the District) will get minor repairs only.

79. Public Improvements – Vesting Parcel Map 8552.

Prior to the issuance of certificate of occupancy for development on each parcel.

Project Sponsor of Parcel 1 of VTPM No. 8552 shall construct or cause the construction of improvements to 11th Street when the “Ice House” parcel is redeveloped. Project Sponsor of Parcel 2 shall construct or cause the construction of improvements to Pine Street between 11th Street and 12th Street.

Except as otherwise provided in this condition, the street improvements referred to in this condition include complete street width, curb, gutter, sidewalk, and installation of utilities in accordance with the standards of the City of Oakland to the limits shown on VTPM 8552. Sidewalks on the opposite side of Wood Street Zoning District perimeter streets (i.e. outside the District) will get minor repairs only.

80. Public Improvements – Vesting Parcel Map 8553.

Prior to the issuance of certificate of occupancy for development on each parcel.

The Project Sponsor of the first development project within VTPM 8553 shall construct all public improvements to 14th Street, 16th Street, and Wood Street between 14th Street and 16th Street, unless development has occurred on an adjacent parcel and the public improvements are already installed.

Except as otherwise provided in this condition, the street improvements referred to in this condition include complete street width, curb, gutter, sidewalk, and installation of utilities in accordance with the standards of the City of Oakland to the limits shown on VTPM 8553. Sidewalks on the opposite side of Wood Street Zoning District perimeter streets (i.e. outside the District) will get minor repairs only.

81. Public Improvements – Vesting Parcel Map 8554.

Prior to the issuance of certificate of occupancy for development on each parcel.

The Project Sponsor of the first to be developed of Parcel 1, 2 or 3 of VTPM No. 8554 shall construct or cause the construction of improvements to 16th Street. The Project Sponsor of Parcel 3 shall construct or cause the construction of improvements to the 16th Street Train Station Public Plaza on Parcel 1. The Project Sponsor of Parcel 3 shall construct or cause the construction of improvements to 18th Street. Improvements to Wood Street, between 16th and 17th Streets, will be constructed prior to the completion of Parcel 1 (plaza). Wood Street between 17th and 18th Streets will be constructed when Parcel 3 is developed.

Except as otherwise provided in this condition, the street improvements referred to in this condition include complete street width, curb, gutter, sidewalk, and installation of utilities in accordance with the standards of the City of Oakland to the limits shown on VTPM 8554. Sidewalks on the opposite side of Wood Street Zoning District perimeter streets (i.e. outside the District) will get minor repairs only.

82. Public Improvements – Vesting Parcel Map 8555.

Prior to the issuance of certificate of occupancy for development on each parcel.

The Project Sponsor of Parcel 1 of VTPM No. 8555 shall construct or cause the construction of improvements to Wood Street, from 18th Street to 20th Street, (unless preceded by Parcel 2 of VTPM 8555), 18th Street (unless preceded by Parcel 3 of Map 8554), and 20th Street if needed for access. The Project Sponsor of Parcel 2 of VTPM No. 8555 shall construct or cause to be constructed public improvements to Wood Street, from 20th Street to West Grand Avenue (unless it precedes the development of Parcel 1 of VTPM 8555, in which case I shall construct Wood Street from 18th Street to West Grand Avenue), and 20th Street, if not already installed by Project Sponsor of Parcel 1.

Except as otherwise provided in this condition, the street improvements referred to in this condition include complete street width, curb, gutter, sidewalk, and installation of utilities in accordance with the standards of the City of Oakland to the limits shown on VTPM 8555. Sidewalks on the opposite side of Wood Street Zoning District perimeter streets (i.e. outside the District) will get minor repairs only.

SHARED MAINTENANCE

83. Use and Maintenance Easement.

Prior to submittal of Final Map.

The Project Sponsor shall indicate on the Final Map a Use and Maintenance Easement reserved for all parcels to ensure the continued shared maintenance of the entire plaza (Parcel 1 on VTPM No. 8554) and the planned access road across Parcel 3 of VTPM No. 8554 and Parcels 1 and 2 of VTPM No. 8555.

84. Recordation of Agreement.

Prior to submittal of Final Map.

The Project Sponsor shall ensure that a Joint Maintenance Agreement in a form acceptable to the City Attorney is executed and recorded with the Alameda County Recorder concurrent with the recordation of the Parcel Map. Said agreement shall ensure the shared maintenance of the plaza (Parcel 1 on VTPM No. 8554) and the planned access road across Parcel 3 of VTPM No. 8554 and Parcels 1 and 2 of VTPM No. 8555. A copy of this document shall be submitted for review and approval by the Planning and Zoning Division prior to its execution.

SUBDIVISIONS

85. Recordation of Legal Descriptions.

Within sixty (60) days of the effective date of this approval.

The Project Sponsor shall record a written legal description of the new configuration of the parcels at the Alameda County Offices as part of the deed for the site; and shall provide evidence of recordation to the Planning and Zoning Division within 60 days of the effective date of this approval.



CITY OF OAKLAND

Community and Economic Development Agency, Planning & Zoning Division
250 Frank H. Ogawa Plaza, Suite 3315, Oakland, California, 94612-2032

May 29, 2009

Mr. Andy Getz
HFII, Ltd.
6450 Hollis Street
Emeryville, CA 94608

Fair Share Payments for Intersection Improvements Oakland Army Base Redevelopment Area

Dear Mr. Getz:

This letter is being sent to all property owners in the Oakland Army Base Wood Street Sub-Area, as shown on the attached map, which includes all of the Wood Street Development Project as well as three parcels to the south of the Wood Street Development Project.

When the Army Base Redevelopment Area was adopted, the Environmental Impact Report (EIR) prepared for the project identified a number of intersections that needed to be improved based on the expected amount of new traffic that would be passing through the intersections. The Mitigation Measures adopted in the EIR specified that the costs of improving these intersections would be shared among all property owners, including the Port of Oakland, the Oakland Redevelopment Agency, and the private property owners within the Sub-District. These Mitigation Measures were also adopted as part of the Wood Street Development EIR and included as Conditions of Approval 25, 26, 27 and 28 in the Wood Street Development Project.

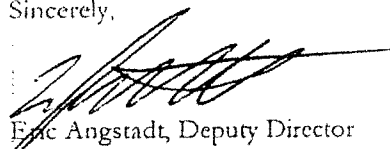
A consultant was hired by staff to calculate the fair share costs of funding these improvements. As shown in Table 2, attached, the Wood Street Sub-Area is responsible for \$162,196 of the intersection improvement costs.

Using the same method that was used for the formation of the Community Facilities District for the Wood Street Development Project, staff allocated the costs based on the number of acres for each approved parcel. Dividing the total improvement cost of \$162,196 by the total number of acres in the sub-area, 37.45, yielded a per acre cost of \$4,331. The allocation of the costs per property owner is shown in the attached table.

A property owner is required to pay this fair share cost only when a building permit is approved for **new construction** on each individual site. If no new development is proposed or approved, then the fair share cost does not need to be paid. To date, the only new project that has been completed and has paid its fair share is the Pacific Cannery Lofts.

If you have any questions about this information, please contact Marge Stanzione, Project Planner, at (510) 238-4932 or by email at mstanzione@oaklandnet.com.

Sincerely,


Eric Angstadt, Deputy Director
Community and Economic Development Agency
City of Oakland
250 Frank H. Ogawa Plaza, Suite 3315
Oakland, CA 94612-2032

ATTACHMENT D

**PROPERTY OWNERS MAILING LIST
FAIR SHARE INTERSECTION IMPROVEMENTS**

Wood Street Developers

Lydia Tan
BUILD West Oakland, LLC
345 Spear Street, Suite 700
San Francisco, CA 94105-1673

Ben Metcalf
Project Manager
BRIDGE Housing
345 Spear Street, Suite 700
San Francisco, CA 94105-1673

Andy Cost
Pulte Homes
Land Department-Bay Area Division
6210 Stoneridge Mall Road, 5th Floor
Pleasanton, CA 94588

Richard Holliday
Holliday Development
1500 Park Avenue, Suite 200
Emeryville, CA 94608

Andy Getz
HFH, Ltd.
6450 Hollis Street
Emeryville, CA 94608

David Truong Duong
California Waste Solutions
1820 10th Street
Oakland, CA 94607-1450

Vishav Bhushan
1755 16th Street
Oakland, CA 94607-1545

Al Auletta, Redevelopment Agency
Diana Downton, Housing
City of Oakland Redevelopment Agency
250 Frank H. Ogawa Plaza
Oakland, CA 94612

Remaining Parcels

Clyde D., Gail S. & Clar Mark Batavia
P.O. Box 217
Carnelian Bay, CA 96140-0217

State of California
P.O. Box 7444
San Francisco, CA 94120-7444

David Truong Duong
California Waste Solutions
1820 10th Street
Oakland, CA 94607-1450

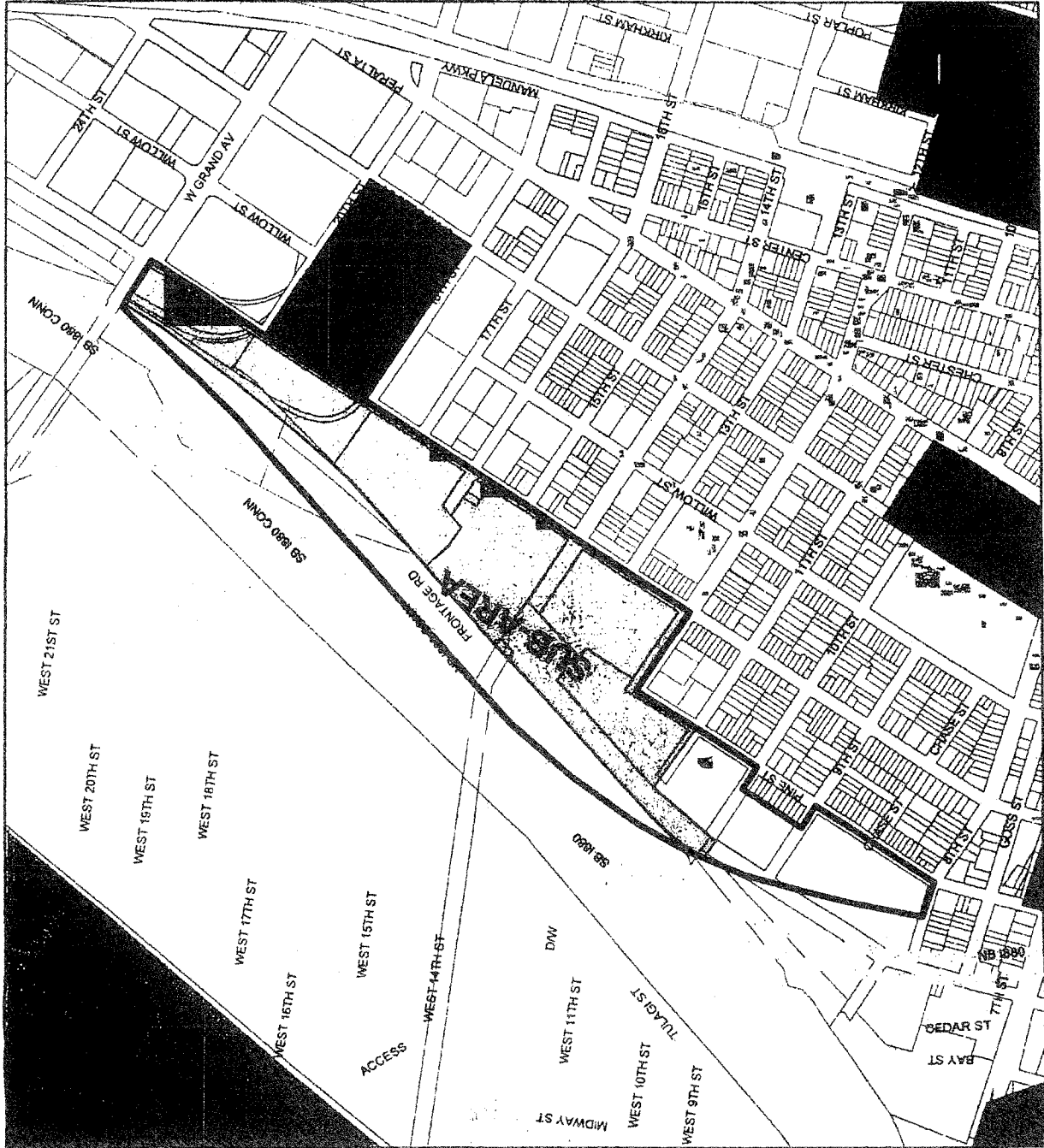
OAKLAND ARMY BASE REDEVELOPMENT AREA
FAIR SHARE CALCULATION
WOOD STREET SUB-AREA

PROPERTY OWNER	VTPM No.	No. Acres	Fair Share Cost/Acre	Share of Total Cost
Cal Waste Solutions	8551/1	0.78	\$4,331	\$3,378
BUILD	8551/2	2.52	\$4,331	\$10,914
Icehouse/Holliday	8552/1	0.94	\$4,331	\$4,071
Pacific Cannery Lofts	8552/2	2.74	\$4,331	\$11,867
14th Street Apts - BRIDGE	8551/3	1.56	\$4,331	\$6,756
Zephyr Gate - Pulte	8551/4	4.03	\$4,331	\$17,454
HFH Apts	8553/1	5.67	\$4,331	\$24,557
HFH Apts	8553/2	0.49	\$4,331	\$2,122
Bea's Hotel	8553/3	0.17	\$4,331	\$736
BUILD - Train Station Plaza	8554/1	0.75	\$4,331	\$3,248
BUILD - Train Station	8554/2	1.19	\$4,331	\$5,154
BUILD	8554/3	2.71	\$4,331	\$11,737
Oakland Redevel Agency	8555/1	2.65	\$4,331	\$11,477
Central Station LLC	8555/2	2.01	\$4,331	\$8,705
WOOD STREET DEVELOPMENT PROJECT		28.21		\$122,178
REMAINING PARCELS				
800 Cedar Street (State of CA)	006 004700100	5.49	\$4,331	\$23,777
1819 10th Street	006 004902501	1.6	\$4,331	\$6,930
1820 10th Street (Cal Waste)	006 002900302	2.15	\$4,331	\$9,312
SUB-TOTAL REMAINING		9.24		\$40,018
TOTAL		37.45	\$4,331	\$162,196

Table 2: Intersection Improvements - Fair-Share Cost Allocations (2009 with North Gateway Mixed Use)

	Total Cost	Cost Attributable to OARB Redevelopment	Split of OARB		Split of City Share				
			Port Share	City Share	North Gateway	West Gateway	Central Gateway	East Gateway	16th/Wood
W. Grand & Maritime	\$7,554,000	100% \$7,554,000	9% \$679,860	91% \$6,874,140	26% \$1,787,276	27% \$1,856,018	41% \$2,818,397	3% \$206,224	2% \$137,483
7th Street & Maritime	\$1,600,000	100% \$1,600,000	63% \$1,008,000	37% \$592,000	29% \$171,680	14% \$82,880	54% \$319,680	3% \$17,760	0% \$0
7th Street & I-880	\$50,000	66% \$33,000	55% \$18,150	45% \$14,850	14% \$2,079	34% \$5,049	24% \$3,564	0% \$0	27% \$4,010
3rd Street & Adeline	\$150,000	65% \$97,500	100% \$97,500	0% \$0	0% \$0	0% \$0	0% \$0	0% \$0	0% \$0
3rd Street & Market	\$150,000	53% \$79,500	74% \$58,830	26% \$20,670	0% \$0	0% \$0	0% \$0	0% \$0	100% \$20,670
12th Street & Brush	\$150,000	3% \$4,500	100% \$4,500	0% \$0	0% \$0	0% \$0	0% \$0	0% \$0	0% \$0
Total	\$9,654,000	97% \$9,368,500	20% \$1,866,840	80% \$7,501,660	26% \$1,961,035	26% \$1,943,947	42% \$3,141,641	3% \$223,984	2% \$162,162
Difference from 2007:	\$0	\$4,500	\$42,900	(\$38,400)	\$907,671	\$865,717	(\$2,046,069)	\$223,984	\$10,148
Additional Intersections:									
S. Auto Mall & Maritime	\$250,000	89% \$222,500	10% \$22,250	90% \$200,250	7% \$14,018	32% \$64,080	56% \$112,140	5% \$10,013	0% \$0
Parcel I & Maritime	\$250,000	100% \$250,000	12% \$30,000	88% \$220,000	8% \$17,600	42% \$92,400	51% \$112,200	0% \$0	0% \$0
Total	\$10,154,000	97% \$9,841,000	20% \$1,919,090	80% \$7,921,910	25% \$1,992,653	27% \$2,100,427	42% \$3,365,981	3% \$233,997	2% \$162,162
Difference from 2007:	\$500,000	\$477,000	\$95,150	\$381,850	\$939,288	\$1,022,197	(\$1,821,729)	\$233,997	\$10,148

Oakland Army Base Redevelopment, Sixteenth and Wood Street Sub-Area



Land Use Map

Legend

- Sixteenth & Wood Sub-Area
- Wood Street Development Project
- Freeways
- Parcels
- General Plan Landuse
- Hillside Residential
- Detached Unit Residential
- Mixed Housing Type Residential
- Urban Residential
- Neighborhood Center Mixed Use
- Community Commercial
- Housing and Business Mix
- Central Business District
- Regional Commercial
- Business Mix
- Gen Industrial/Transportation
- Institutional
- Resource Conservation Area
- Urban Open Space
- Estuary Plan Area

ATTACHMENT NO. 3 C



March 2005

