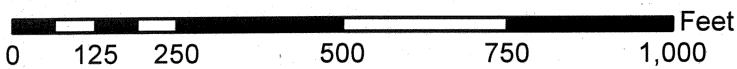
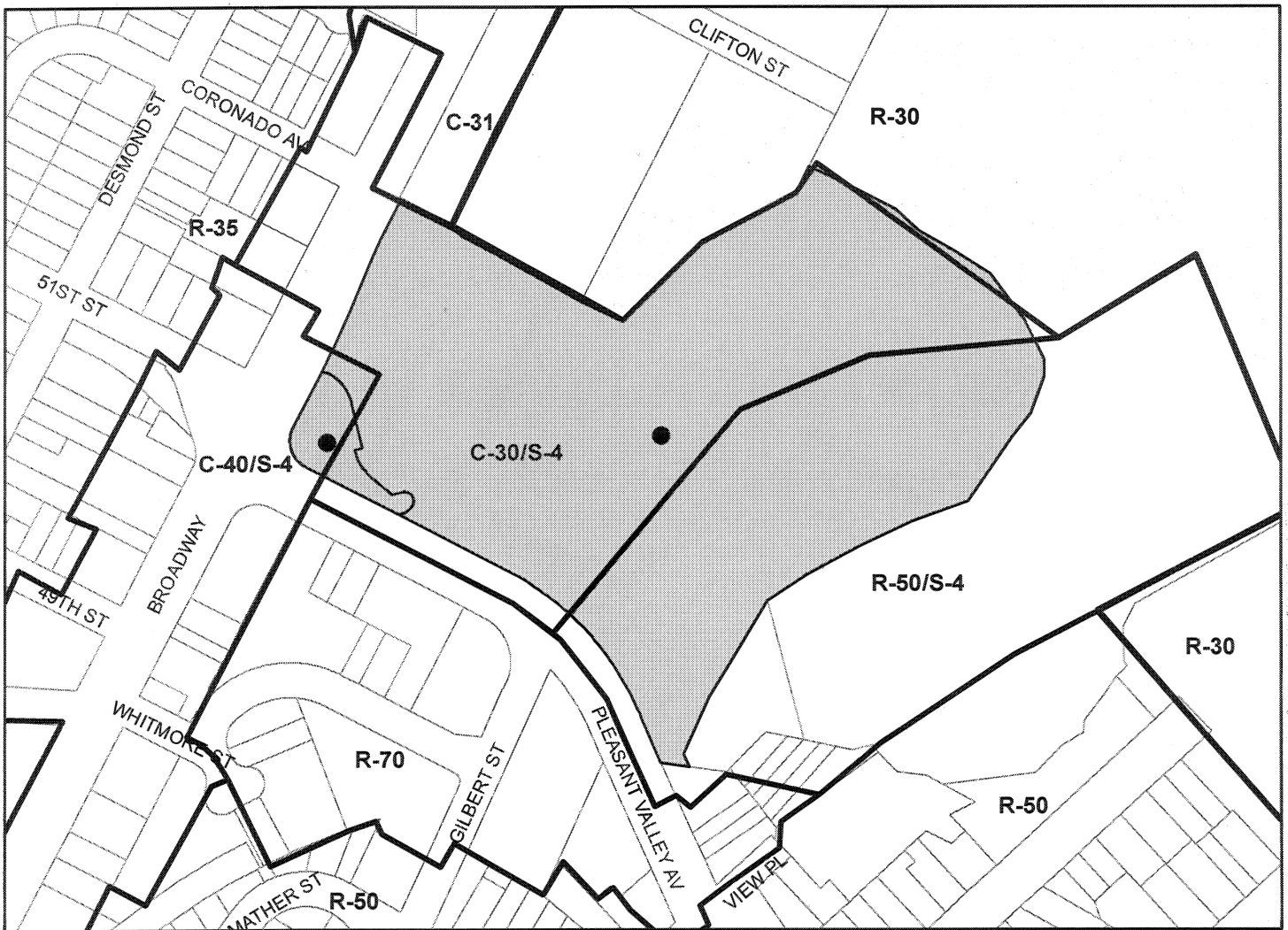


Project Name:	Safeway Redevelopment Project (Broadway @ Pleasant Valley Avenue)
Location:	5050-5100 Broadway (APN 014-1242-002-03 & 014-1242-005-07) (see map on reverse)
Proposal:	Redevelopment of the existing Rockridge Shopping Center, including demolition of all 185,500 square feet of existing buildings and construction of a new Safeway store and other retail, office, and restaurant space, totaling approximately 322,500 square feet with approximately 967 off-street parking spaces. Proposed modifications to streets in the project vicinity including Broadway/51 st Street/Pleasant Valley Avenue, Pleasant Valley Avenue/Gilbert Street, Broadway/Coronado Avenue, and Broadway/College Avenue intersections.
Applicant / Phone Number:	Property Development Centers / (925) 738-1239
Owner:	Alvin B. Chan, Inc.; Safeway, Inc. (ground lessee)
Planning Permits Required:	1) Regular Design Review for new construction; 2) Interim Conditional Use Permit to apply the policies of the General Plan to the portions of the site in the R-50 Zone; 3) Major Conditional Use Permit to allow a Drive-Through Nonresidential Facility; 4) Major Conditional Use Permit to allow alcoholic beverage sales in conjunction with a new General Food Sales Commercial Activity (new Safeway store); 5) Minor Variance to allow a building height up to 70 feet in the C-30 Zone where the maximum allowed height is 45 feet; and 6) Creek Protection Permit (Category IV) to allow development on a creekside property.
General Plan:	Community Commercial
Zoning:	C-30 District Thoroughfare Commercial Zone (portion of site); C-40 Community Thoroughfare Commercial Zone (portion of site); R-50 Medium Density Residential Zone (portion of site); S-4 Design Review Combining Zone (entire site)
Environmental Determination:	A Draft Environmental Impact Report (EIR) has been prepared. A Notice of Release/Availability for the Draft EIR was published on <u>January 11, 2013</u> . The public comment period on the Draft EIR ends on <u>February 25, 2013</u> .
Historic Status:	Not a Potential Designated Historic Property (PDHP); Survey rating: X
Service Delivery District:	2
City Council District:	1
Status:	Pending
Action to be Taken:	Receive comments on the Draft EIR and the project. No decision will be made on the project at this hearing.
Staff Recommendation:	Receive comments on the Draft EIR and the project.
For Further Information:	Contact case planner Darin Ranelletti at (510) 238-3663 or by email at dranelletti@oaklandnet.com .

CITY OF OAKLAND PLANNING COMMISSION



Case File: CMDV09-135; CP09-090; ER09-007
Applicant: Property Development Centers
Address: 5050-5100 Broadway
Zone: C-40/S-4, C-30/S-4, R-50/S-4

SUMMARY

The applicant proposes to redevelop the existing Rockridge Shopping Center¹ located at Broadway and Pleasant Valley Avenue, currently anchored by Safeway and CVS Pharmacy (formerly Long's Drugs), with a new commercial development containing a Safeway store and other commercial space totaling approximately 322,500 square feet of floor area.

Pursuant to the California Environmental Quality Act (CEQA), a Draft Environmental Impact Report (EIR) has been prepared for the project.² The Draft EIR was released for public review on January 11, 2013, beginning a 45-day public comment period. The public comment period on the Draft EIR ends on February 25, 2013. The purpose of the February 20th hearing is to take comments on the Draft EIR and the project. No decision will be made on the project at this hearing. After all comments are received, a Final EIR will be prepared and the Planning Commission will consider certification of the Final EIR and render a decision on the project at a later meeting date to be scheduled.

PROPERTY DESCRIPTION

The project site is approximately 15.4 acres in size. The irregular-shaped site is bounded by Broadway to the west, Pleasant Valley Avenue to the south, a reservoir and recently remodeled commercial building (occupied by AAA) to the east, and the California College of the Arts, multi-family residential apartment buildings, and the Claremont Country Club to the north.

The site is currently occupied by the Rockridge Shopping Center, a single-story shopping center that contains approximately 185,000 square feet of floor area. The shopping center is anchored by Safeway and CVS Pharmacy (formerly Long's Drugs) and includes a surface parking lot containing a total of 615 off-street parking spaces.

PROJECT DESCRIPTION

The project involves the redevelopment of the existing Rockridge Shopping Center, including the demolition of all 185,500 square feet of existing buildings on the site and the construction of a new Safeway store and other retail, office, and restaurant space, totaling approximately 322,500 square feet of commercial space (293,200 square feet of gross leasable floor area and an additional 29,300 square feet of common space). A total of approximately 967 off-street parking spaces are proposed. Parking would be located in surface parking lots, on the rooftop of the new Safeway store, and in a three-level parking garage located above commercial space. The current project drawings are attached (see **Attachment A**).

The applicant also proposes modifications to streets in the project vicinity including the following:

- Broadway between College Avenue and 49th Street would be reduced from three through lanes to two through lanes in each direction and new bike lanes would be installed.
- A new signal would be installed at the intersection of Broadway and Coronado Avenue to provide left turns into and out of the project site.

¹ The Rockridge Shopping Center is distinct from the Safeway store located at College Avenue and Claremont Avenue in the Rockridge commercial district which is typically referred to as the "College Avenue Safeway."

² The Draft EIR was previously provided to each member of the Planning Commission and is also available at the City Planning offices at 250 Frank H. Ogawa Plaza, 2nd floor Zoning Counter, and on the City's website at: <http://www2.oaklandnet.com/Government/o/PBN/OurServices/Application/DOWD009157>.

- The southbound approach to the Broadway/51st Street/Pleasant Valley Avenue would be modified to provide two left-turn lanes onto Pleasant Valley Avenue.
- The eastbound approach to the Pleasant Valley Avenue/Gilbert Street intersection would be modified to provide two left-turn lanes into the project site.

A complete description of the proposed modifications to transportation facilities is included in the Transportation chapter of the Draft EIR beginning on page 4.11-39.

Construction Phasing

The project would be constructed in two phases over a period of approximately 20 months. Phase 1 would involve demolition of the existing CVS Pharmacy and adjacent stores and the construction of the new Safeway store and adjacent buildings in the northeast portion of the site. Phase 2 would involve demolition of the existing Safeway store, adjacent stores, and the existing bank building on the corner of Broadway and Pleasant Valley Avenue, the construction of new buildings located in the southwest portion of the site along Broadway and Pleasant Valley Avenue, and the proposed street modifications. The proposed phasing would allow the existing Safeway store to remain open while the new Safeway store is constructed.

PREVIOUS HEARINGS, PUBLIC COMMENTS AND REVISIONS TO PROJECT

The Planning Commission previously reviewed the project in July 2009 during the public comment period on the Notice of Preparation (NOP) for the EIR and the Design Review Committee reviewed the project in January 2011. At these meetings and in written comments the public has made a variety of comments on the project. Some members of the public support the project, particularly as a way to achieve City economic development goals. Other members of the public have expressed concerns that the project lacks a housing component and is too auto-oriented and suburban in character for an urban location.

At the Design Review Committee meeting in 2011 the Committee voted to forward the project to the full Planning Commission for decision without the need for the project to return to the Committee. At the meeting the Committee expressed a mix of support and concerns for the project. In response to public and Committee concerns, the applicant submitted revisions to the project. **Attachment B** contains the project site plan for the original project reviewed by the Planning Commission in 2009, the revised project reviewed by the Design Review Committee in 2011, and the current project showing the evolution of the proposed site plan. **Attachment C** contains the Broadway and Pleasant Valley Avenue architectural elevations for the 2011 project and the current project. **Attachment D** contains the concerns expressed by the Design Review Committee members in 2011 and the applicant's responses.

The primary recent design modifications incorporated into the current project (compared to the 2011 project) are the following:

- Pleasant Valley Avenue Façade: The service corridor serving the commercial spaces along Pleasant Valley Avenue was relocated below ground thereby allowing more transparent windows along Pleasant Valley Avenue increasing the visual appeal and interaction between the sidewalk and the commercial spaces.
- Architecture: The architecture of the project has been modified so that the exterior elevations are more refined and less chaotic.

- Pleasant Valley Avenue Bank Building: The bank building at the corner of the project entrance on Pleasant Valley Avenue (Building D) has been relocated closer to the street to increase its street presence and reduce the visibility of parking.
- Relationship to Reservoir: A new restaurant building with an outdoor seating area facing the reservoir has been introduced (Building E) and a new pedestrian path has been added to create a direct pedestrian link from Broadway to the reservoir. These modifications are intended to increase the visual relationship between the site and the reservoir thereby enhancing the reservoir's position as a site amenity.
- Rooftop Parking: The parking on the roof of the new Safeway (Building A) has been connected to the parking structure located over the commercial space in the central portion of the site (Buildings H and J) thereby increasing site circulation efficiency.
- Pleasant Valley Avenue Entrance: The number of inbound vehicle lanes at the primary vehicle entrance to the site on Pleasant Valley Avenue (at Gilbert Street) has increased from two to three in order to reduce the potential for vehicle queues backing up onto Pleasant Valley Avenue.

GENERAL PLAN ANALYSIS

The General Plan land use classification for the site is Community Commercial. According to the General Plan, the intent and desired character of the Community Commercial is the following:

The Community Commercial classification is intended to identify, create, maintain, and enhance areas suitable for a wide variety of commercial and institutional operations along the City's major corridors and in shopping districts or centers. Community Commercial areas may include neighborhood center uses and larger scale retail and commercial uses, such as auto related businesses, business and personal services, health services and medical uses, education facilities, and entertainment uses. Community Commercial areas can be complemented by the addition of urban residential development and compatible mixed use development.³

Broadway is a designated Key Corridor in the General Plan and the Broadway and Pleasant Valley Avenue/51st Street area is a designated Neighborhood Activity Center. Key Corridors are envisioned as mixed-use environments with concentrations of commercial and civic uses. Neighborhood Activity Centers are considered the focal point of a community with diverse business, civic, and social activities supported and strengthened by surrounding housing.

The Upper Broadway/College Avenue area, which includes the project site, is a target area for community and economic development in the General Plan. In this area, the key objectives are to "reduce loitering, improve traffic safety, code compliance, street maintenance, and development of vacant and underutilized properties."⁴

The Land Use chapter of the Draft EIR, beginning on page 4.9-7, identifies the General Plan objectives and policies relevant to the project and presents information to support a determination that the project could generally be considered consistent with the General Plan in that the project would be consistent with the Community Commercial land use classification, improve the economic viability of the shopping

³ Oakland General Plan, Land Use and Transportation Element, p. 150

⁴ Oakland General Plan, Land Use and Transportation Element, pp. 222-223.

center, create a neighborhood activity center, be compatible with neighboring properties, and enhance pedestrian, bicycle and transit facilities.

ZONING ANALYSIS

The zoning applicable to the project is the zoning in effect at the time the project's application was deemed complete in 2010. The applicable zoning is discussed below.

The site is split into three different base zoning districts. The southwestern corner of the site, roughly equal to the location of the existing Chase bank building, is located in the C-40 Community Thoroughfare Commercial Zone. The central portion of the site is located in the C-30 District Thoroughfare Commercial Zone. The eastern portion of the site is located in the R-50 Medium Density Residential Zone. The entire site is located in the S-4 Design Review Combining Zone.

The portion of the site located in the R-50 Zone would not allow the commercial uses proposed by the project. The R-50 Zone conflicts with the Community Commercial General Plan designation for the site. Pursuant to the City's Guidelines for Determining Project Conformity with the General Plan and Zoning Regulations, an interim conditional use permit would be required to apply the policies of the General Plan to the portion of the site in the R-50 Zone. In May 2009 the City made a determination in conjunction with the review of a proposal to remodel the former Emil Villa's restaurant building at 1982 Pleasant Valley Avenue (now occupied by AAA) that in reviewing such an interim conditional use permit at the site, the City would apply the standards of the C-30/S-4 Zone as the "best-fit" zone, meaning the C-30/S-4 Zone contains the most appropriate zoning standards to use when reviewing the proposal because the standards best implement the policies of the Community Commercial General Plan designation at the site. The S-4 Zone is an overlay zone that requires design review for the construction and alteration of buildings.

The project complies with all the development standards in the zoning regulations except for the height of a portion of the project located in the C-30 Zone. The maximum allowed building height in the C-30 Zone is 45 feet. The three-story parking structure located over ground-floor commercial space located in the central interior portion of the site has a maximum proposed height of 66 feet. A variance would be required to allow the proposed height. The parking structure is located well away from surrounding neighbors in the interior of the site and the height of the structure would roughly equal the ground elevation of the neighboring buildings to the north due to the change in topography. For these reasons, a variance for the proposed building height may be acceptable.

Subsequent to the project's application being deemed complete in 2010, the City adopted new citywide residential and commercial zoning which became effective in 2011. Under the new zoning, which is not applicable to the project because the new zoning became effective after the project's application was deemed complete, the project site is located in the CC-2 Community Commercial-2 Zone.

ENVIRONMENTAL DETERMINATION

The Draft EIR was released for public review on January 11, 2013, beginning a 45-day public comment period. The public comment period on the Draft EIR ends on February 25, 2013. The purpose of the February 20th hearing is to take comments on the Draft EIR and the project. No decision will be made on the project at this hearing. All comments received will be considered by the City prior to finalizing the EIR and making a decision on the project. Comments on the Draft EIR should focus on the sufficiency of the EIR in discussing possible impacts on the physical environment, ways in which potential adverse effects might be minimized, and alternatives to the project in light of the EIR's purpose to provide useful and accurate information about such factors. Comments on the Draft EIR may be made at the February 20th public hearing or in writing to the Planning and Zoning Division. Comments must be received prior

to the comment period deadline (4:00 p.m. on February 25, 2013). After all comments are received, a Final EIR will be prepared and the Planning Commission will consider certification of the Final EIR and render a decision on the project at a later meeting date to be scheduled.

The Executive Summary chapter of the Draft EIR (Chapter 2) is attached to this report (see **Attachment E**). All of the project's potentially significant impacts can be reduced to less-than-significant levels with the incorporation of the identified mitigation measures and the City's Standard Conditions of Approval, except for the following potential transportation impacts which are considered significant and unavoidable:

- Broadway/51st Street/Pleasant Valley Avenue (Impacts Trans-5 and Trans-10): In year 2015 the project would degrade intersection operations from level-of-service (LOS) D to LOS E during the weekday PM peak hour and increase delay for the critical eastbound through movement by more than six seconds during the Saturday midday peak hour. In year 2035 the project would increase the volume-to-capacity (v/c) ratio for the intersection by 0.01 or more and the critical movement v/c ratio for the eastbound left, eastbound through, westbound left, northbound through, and southbound left movements by 0.02 or more during the weekday PM peak hour, and it would increase the v/c ratio for the intersection by 0.01 or more and the critical movement v/c ratio for the eastbound left, eastbound through, and northbound through movements by 0.02 or more during the Saturday midday peak hour.

To mitigate the above impacts, an additional vehicle lane would be required on both 51st Street and Pleasant Valley Avenue. The mitigation would reduce the impact in 2015 to a less-than-significant level but would not reduce the impact to a less-than-significant level in 2035. Accommodating the additional vehicle lane would substantially increase the pedestrian crossing distance which would conflict with City policy concerning pedestrian safety and comfort, including the Public Transit and Alternative Modes Policy (i.e., the "Transit-First Policy") and the Pedestrian Master Plan, and would also degrade pedestrian safety by increasing pedestrian exposure to vehicles. Due to these potential significant impacts, the mitigation is considered infeasible.

Traffic operations at the intersection could be improved, though not to a less-than-significant level, by providing additional vehicle travel lanes on Broadway. However, there is insufficient existing vehicle right-of-way to accommodate new vehicle lanes on Broadway so the street would need to be widened which would require obtaining additional right-of-way from adjacent private property containing existing buildings/structures/improvements, and/or removing bicycle lanes, on-street parking, or medians which would conflict with City policy concerning pedestrian and bicycle safety. Due to these potential significant impacts, the mitigation is considered infeasible.

- Howe Street/Pleasant Valley Avenue (Impacts Trans-3, Trans-8, and Trans-13): Under Existing Plus Project, year 2015, and year 2035 conditions, the project would add more than 10 vehicle trips during the weekday PM and Saturday midday peak hours and meet the peak hour signal warrant. To mitigate these impacts, one of the following measures would be required: signalize the intersection; prohibit on-street parking along northbound Howe Street to allow right-turning vehicles to bypass queued left-turning vehicles; or prohibit left-turns from Howe Street to westbound Pleasant Valley Avenue during peak commute periods.

Signalizing the intersection would allow easier vehicle access between Howe Street and Pleasant Valley Avenue, which may encourage cut-through vehicles to use Howe Street and Pleasant Valley as an alternative to the congested Broadway and Piedmont Avenue corridors.

Considering that this segment of Howe Street is primarily residential, a potential increase in cut-through traffic would be undesirable. Prohibiting on-street parking to allow right-turning vehicles would be undesirable because parking is at or near capacity on weekday evenings. Prohibiting left-turns from Howe Street onto Pleasant Valley Avenue during peak commute periods would divert traffic from Howe Street to other streets such as Piedmont Avenue or Montgomery Street. Vehicles diverted to Piedmont Avenue would increase the potential significant impact at the Piedmont Avenue/Pleasant Valley Avenue intersection (described below). Due to these potential significant impacts, these mitigation measures are considered infeasible.

- Piedmont Avenue/Pleasant Valley Avenue (Impact Trans-14): In year 2035 the project would increase the v/c ratio for the critical westbound and northbound movements by 0.02 or more. The impact could be mitigated by installing a new left-turn lane on the northbound Piedmont Avenue approach. There is insufficient existing vehicle right-of-way to accommodate a new left-turn lane on Piedmont Avenue so the street would need to be widened which would require obtaining additional right-of-way from adjacent private property containing existing buildings/structures/improvements, and/or removing on-street parking and planned bicycle lanes. Prohibiting on-street parking would be undesirable because parking is at or near capacity. Removing planned bicycle lanes would conflict with City policy promoting bicycle safety and facilities, including the Public Transit and Alternative Modes Policy (i.e., the “Transit-First Policy”) and the Bicycle Master Plan. Due to these potential significant impacts, the mitigation is considered infeasible.

As required by CEQA, the Draft EIR includes an analysis of a range of alternatives to the project which would feasibly attain most of the applicant’s basic objectives and avoid or substantially lessen any of the significant effects of the project. The alternatives include the following:

- Alternative 1 (No Project) assumes the existing Safeway store remains in its existing location and there is no redevelopment of the site apart from a possible remodeling of the Safeway store;
- Alternative 2 (Safeway Relocation) assumes the relocation of the Safeway store to the current CVS Pharmacy space but retains the remainder of the shopping center in its current condition;
- Alternative 3 (Reduced Project) assumes the proposed redevelopment of the shopping center except with less commercial space by removing the proposed upper-floor space;
- Alternative 4 (Concept with Commercial Emphasis) is based on a concept presented by the Rockridge Community Planning Council (RCPC) during the scoping process for the EIR; and
- Alternative 5 (Concept with Residential Emphasis) is based on a concept presented by Urbanists for a Livable Temescal-Rockridge Area (ULTRA) during the scoping process for the EIR.

KEY ISSUES AND IMPACTS

Below is a summary of the key issues and impacts related to the project.

Proposed Land Uses

The proposal involves commercial uses only. Some members of the public have recommended including residential units in the project. Both the General Plan and the zoning would allow residential units at the

site. Safeway has a ground lease on the site and the lease prohibits using the site for residential purposes. New joint living and working quarters (i.e., “live/work” units) would not be allowed by the zoning. The General Plan is silent on live/work units.

Site Design

Some members of the public have criticized the proposal for being too suburban in character and too auto-oriented. In response, the current proposal (compared to the original 2009 proposal) brings additional building massing to Broadway and Pleasant Valley Avenue to create a more urban street edge and introduces a new internal “street” intended to function like an urban shopping street. In addition, in the most recent set of revisions the service corridor serving the commercial spaces along Pleasant Valley Avenue was relocated below ground thereby allowing more transparent windows along Pleasant Valley Avenue increasing the visual appeal and interaction between the sidewalk and the commercial spaces. The surface parking lot in front of the new Safeway store remains. The new site layout generally divides the site into two areas characterized by the more urban pedestrian-oriented western area and the more suburban auto-oriented eastern area.

Significant and Unavoidable Transportation Impacts

As discussed above in the Environmental Determination section of this report, the Draft EIR identifies significant and unavoidable transportation impacts at the following intersections: Broadway/51st Street/Pleasant Valley Avenue; Howe Street/Pleasant Valley Avenue; and Piedmont Avenue/Pleasant Valley Avenue. Due to these significant and unavoidable impacts, under CEQA the City can only approve the project if there are economic, legal, social, technological, or other benefits of the project that outweigh the significant and unavoidable environmental impacts.

Broadway/College Avenue Intersection

The project would generate additional automobiles, bicycles, and pedestrians at the Broadway/College Avenue intersection. The intersection currently does not provide a crosswalk on the south approach on Broadway. In addition, vehicles on southbound College Avenue turn right onto Broadway at high speeds due to the angle that College Avenue intersects Broadway. These vehicles may potentially conflict with pedestrians crossing College Avenue or vehicles turning left from northbound Broadway into the existing Wendy’s restaurant. The Draft EIR recommends that the intersection be modified so that College Avenue intersects Broadway at a right angle. **Appendix F** contains a conceptual plan for this modification. The modification would improve conditions for pedestrians and bicyclists by reducing the size of the intersection, reducing the speed of automobiles on southbound College Avenue turning right onto Broadway, and providing a crosswalk across Broadway on the south approach. The modification would not result in a significant impact to automobile congestion. However, large trucks would continue to not be able to turn right from southbound Broadway onto northbound College Avenue, motor vehicles including buses on southbound College Avenue would turn right onto southbound Broadway at reduced speeds increasing vehicle delay, and vehicles on northbound Broadway would not be able to turn left into the existing Wendy’s restaurant, however, they would be able to make a U-turn at College Avenue.

Economic Development Implications

The City has developed a Retail Enhancement Strategy to improve the retail sector in Oakland. The Strategy identifies Broadway and 51st Street/Pleasant Valley Avenue as one of the priority retail nodes where retail enhancement efforts should be focused. The June 2008 Implementation Plan for the Strategy states the following:

The Rockridge Shopping Center underutilizes a rare commodity in Oakland: a retail site over 10 acres in a prime location. Today the center operates as a suburban retail solution in a key urban location...More intensive use of this site could provide an expansion opportunity for the supermarket and also meet the City's strategic goal of expanding its supply of comparison retail stores.⁵

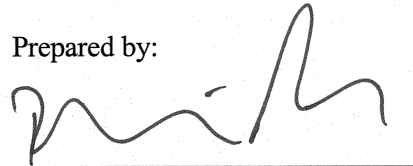
The Implementation Plan includes the following strategies for enhancing the Broadway and 51st/Pleasant Valley node (along with the nearby retail node surrounding Oakland Technical High School):

- ❖ Major development changes in these nodes provide an opportunity to redevelop the pattern of land use to one that is less auto-oriented, and supports creation of a pedestrian environment that serves the adjacent neighborhoods.
- ❖ Incorporate a viable comparison goods component into large scale retail development in this node.
- ❖ Modify traffic and circulation patterns to facilitate pedestrian circulation.⁶

CONCLUSION

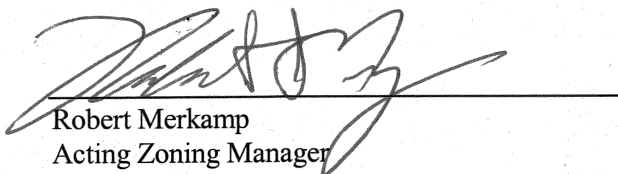
Staff recommends that the Planning Commission take public testimony on the Draft EIR and the project and provide feedback to staff on the adequacy of the Draft EIR and the key project issues.

Prepared by:



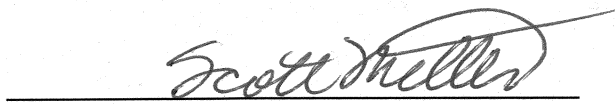
Darin Ranelletti
Planner III

Approved by:



Robert Merkamp
Acting Zoning Manager

Approved for forwarding to the
City Planning Commission:



Scott Miller
Interim Planning and Zoning Director

⁵ Oakland Retail Enhancement Strategy: Implementation Plan, June 2008, p. 44

⁶ Oakland Retail Enhancement Strategy: Implementation Plan, June 2008, pp. 44-45

ATTACHMENT A

Current Project Drawings
(dated November 21, 2012; received November 27, 2012)

OAKLAND, CALIFORNIA

DR-0	COVER SHEET
DR-1	SITE PLAN w/ AERIAL (Main Bldg. Level)
DR-2	SITE PLAN w/ AERIAL (2nd, 3rd & 4th Levels)
DR-3	SITE PLAN w/ DATA & Tree Survey
DR-3a	SITE PLAN (Upper Floor Level)
DR-4	SITE SECTIONS
DR-5	EXTERIOR ELEVATIONS
DR-6	EXTERIOR ELEVATIONS
DR-7	EXTERIOR ELEVATIONS
DR-8a	EXTERIOR ELEVATIONS
DR-8b	EXTERIOR ELEVATIONS
DR-8c	EXTERIOR ELEVATIONS
DR-9	EXISTING SITE PLAN
DR-10	AERIAL OF EXISTING SITE PLAN
DR-11	EXISTING SITE PHOTOS
DR-12	EXISTING CONTEXTUAL VIEWS
DR-13	EXISTING CONTEXTUAL VIEWS
DR-14a	DEMO AND PHASING PLANS
DR-14b	DEMO AND PHASING PLANS
DR-15	EXISTING AERIAL PERSPECTIVES
DR-16	BICYCLE CONNECTIVITY PLAN
DR-17	PEDESTRIAN CONNECTIVITY PLAN
DR-18	PERSPECTIVE RENDERINGS
DR-19	PERSPECTIVE RENDERINGS
DR-20	PERSPECTIVE RENDERINGS
DR-21	PERSPECTIVE RENDERINGS
DR-22	PERSPECTIVE RENDERINGS
DR-23a	PERSPECTIVE RENDERINGS
DR-23b	PERSPECTIVE RENDERINGS
DR-23c	PERSPECTIVE RENDERINGS
DR-24	SHADOW STUDY
DR-24	BUILDING & SITE LIGHTING PLAN
DR-25	EASEMENT / PROPERTY LINE ADJUSTMENTS
DR-26	COLOR AND MATERIAL SCHEDULE
DR-27	TRUCK SERVICE ROUTE
DR-28	LANDSCAPE PLAN (OVERALL)
LS-1	LANDSCAPE PLAN
LS-2	LANDSCAPE PLAN
LS-3	LANDSCAPE PLAN
C1.0	PRELIMINARY GRADING PLAN
C2.0	PRELIM. POST-CONST. STORMWATER MANAGEMENT PLAN
C3.0	CREEK PROTECTION PLAN
01	TOPOGRAPHIC SURVEY
02	TOPOGRAPHIC SURVEY
03	TOPOGRAPHIC SURVEY
04	TOPOGRAPHIC SURVEY

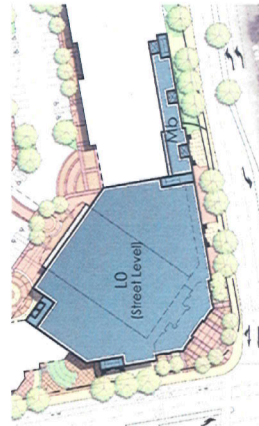


**BENNER
STANGE
ASSOCIATES**
ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
SUITE B-130
LAKE OSWEGO, OR 97035
(503) 670-0234
FAX (503) 670-0235
bsa@bsarch.com

DR O



1 SITE PLAN & AERIAL (MAIN BUILDING LEVEL)
SCALE: NTS



2 LOWER LEVEL RETAIL (BLDG - LO)
SCALE: NTS

Dev. Review
November 21, 2012

51ST & Broadway
Store No. 3132
Oakland, California

Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

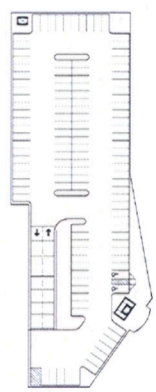
BENNER STANGE ASSOCIATES ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
LAKE OSWEGO, OR 97035
TEL: (503) 265-0000
FAX: (503) 265-0055
bsa@bsaarch.com
bsaarch.com

DR 1

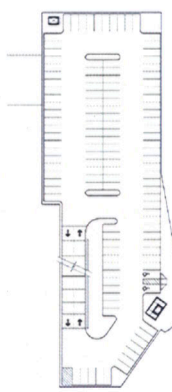
BSAA
Architecture & Planning



1 SITE PLAN & AERIAL (UPPER BUILDING LEVEL 2) SCALE: NTS



3 PARKING UPPER DECK LEVEL 4 SCALE: NTS



2 PARKING UPPER DECK LEVEL 3 SCALE: NTS

Dev. Review
November 21, 2012

DR
2



BENNER
STANGE
ASSOCIATES
ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
LANE OSWEGO, OR 97035
(503) 670-0234
FAX: (503) 670-0234
bsaa@bsaarch.com

51ST & Broadway
Store No. 3132
Oakland, California

**Property
Development
Centers**
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

SITE DATA

Site Area: 15.28 ACRES 668,409 S.F.

Major A: (Sidewalk)	65,013 S.F.
Major B1: (Retail)	8,179 S.F.
Major B2: (Garden Center)	4,998 S.F.
Major C1a: (Retail)	6,867 S.F.
Major C1b: (Retail)	8,835 S.F.
Major C2: (Office)	8,426 S.F.
Major D: (Bank - Two Story)	8,426 S.F.
Major E: (Restaurant - Includes 1,800 S.F. Patio)	4,695 S.F.
Major F: (Restaurant)	2,913 S.F.
Major G: (Major Retail - Upper Level)	10,494 S.F.
Major G2: (Major Retail)	9,944 S.F.
Major H: (Retail)	14,310 S.F.
Major I: (Retail)	15,351 S.F.
Major J: (Includes 2,330 S.F. of Restaurant)	22,903 S.F.
Major K: (Retail - Street Level)	9,483 S.F.
Major L1a: (Restaurant - Includes 2,107 S.F. Patio)	10,820 S.F.
Major L1b: (Retail)	6,684 S.F.
Major L2a: (Retail - Upper Level)	21,620 S.F.
Major L2b: (Retail - Upper Level)	14,833 S.F.
Major N: (Major Retail)	7,577 S.F.
Major O: (Retail - Upper Level)	7,203 S.F.
Major P: (Retail)	2,203 S.F.
Total Existing GLA (Net):	212,233 S.F.
Major N: (Gross)	220 S.F.
Total Existing GLA (Gross):	212,453 S.F.

Required Parking:
Standard Parking Provided: 531 SPACES
Compulsory Parking Provided: 86 SPACES
Compulsory Provided: 8,895
Total Proposed Parking: 617 SPACES

Parking Ratio: 3.30 SPACES/1,000 S.F.
Total Landscape Coverage: 2% 11,000 S.F.

Parking Ratio Requirements:
Office: 2.5 per 1,000
Retail: 1.66 per 1,000

Protected Tree Proposed Removal / within 10 Feet of Construction Activity			
Trees not proposed for removal but located within 10 feet of construction activity			
#	Species	dbh	dhb
20	Platanus Borensis	9.2	345
20	Platanus Borensis	9.2	345
218	Platanus Borensis	13	586
581	Ulmus Parvifolia	12	581
587	Ulmus Parvifolia	11.9	593
587	Platanus Borensis	9.4	594
587	Platanus Borensis	18.2	18.2
588	Pinus Badiata	21.3	16.6
590	Pinus Badiata	16.6	16.6
592	Pinus Badiata	16.6	16.6
593	Pinus Badiata	16.6	16.6
595	Pinus Badiata	16.6	16.6

LEGEND

+ TREE TO BE PROTECTED PER SITE REVISION

+ TREE TO REMAIN

o TREE TO BE REMOVED

Note: SEE SHEET DR-25 FOR BUILDING AND PARKING LOT LIGHTING PLANS.

NORTH



1 SITE PLAN
SCALE: 60' = 1" = 10'

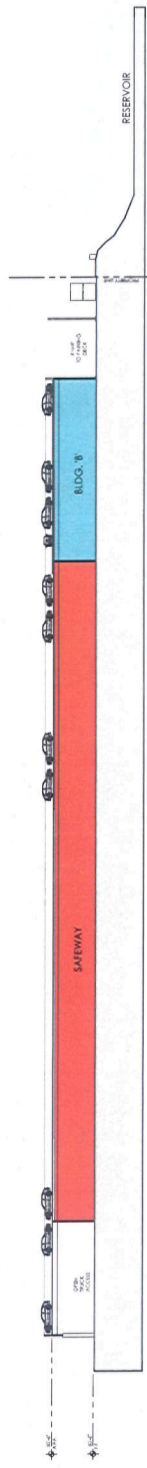
51ST & BROADWAY
Store No. 3132
Oakland, California

Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

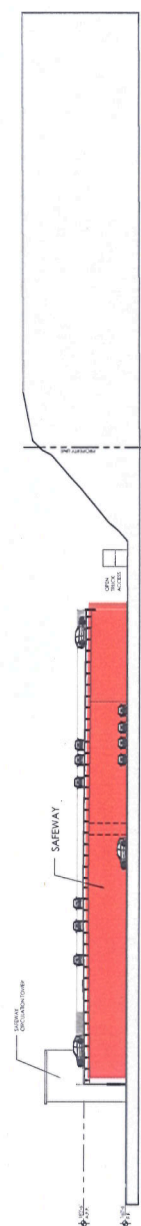
BENNER STANGE ASSOCIATES ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
SUITE 200
LAKE OSWEGO, OR 97035
(503) 670-0234
FAX: (503) 670-0235
bsa@benstange.com
Architects & Planners

DR 3

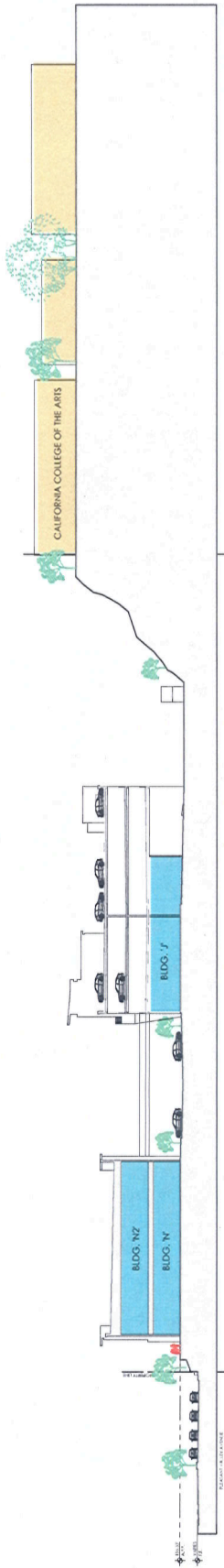
Dev. Review
November 21, 2012



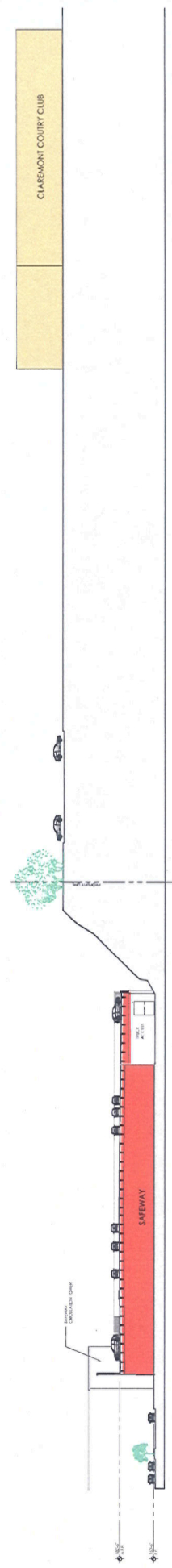
1 SITE SECTION A - A
SCALE: 1" = 30'-0"



2 SITE SECTION B - B
SCALE: 1" = 30'-0"



3 SITE SECTION C - C
SCALE: 1" = 30'-0"



4 SITE SECTION D - D
SCALE: 1" = 30'-0"

Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925.738.1200
Fax: 925.467.2861

51ST & Broadway
Store No. 3132
Oakland, California

Dev. Review
November 21, 2012

BENNER STANGE ASSOCIATES ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
LAKE OSWEGO, OR 97035
TEL: 503.670.0000
FAX: 503.670.0035
bsa@bsaarch.com

BSAA
Architecture & Planning

DR
4

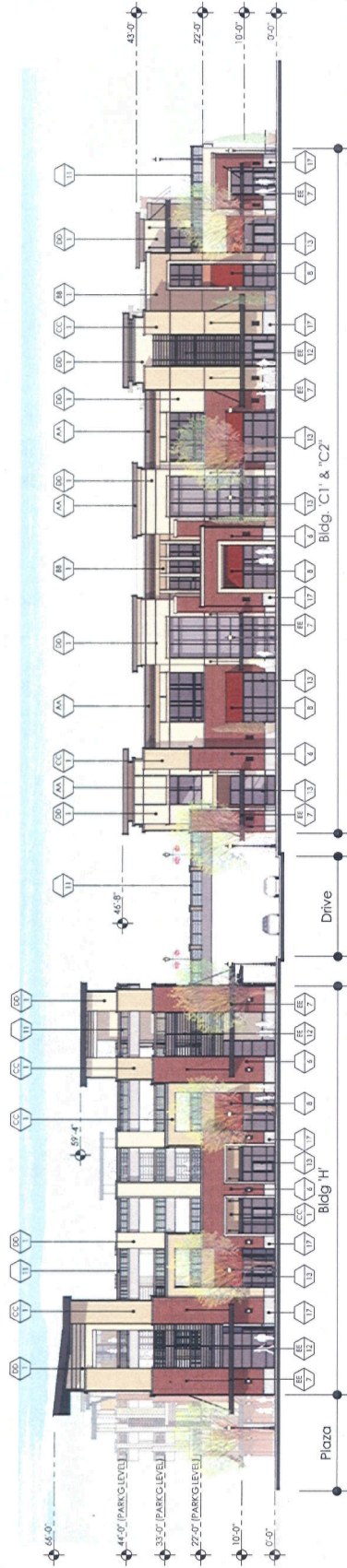
EXTERIOR ELEVATIONS

MATERIAL AND COLOR

AA	TUN EDWARDS 154 GARGOYLE	AA	TOT 1784 ARROW WOOD	AA	ELIZABETH MOUNTAIN LUGG-DURANGO	AA	MISC. METAL
BB	TUN EDWARDS DEC-36 DEER GRAY	BB	TOT 1784 BLACK BORDEAUX	BB	CUSTOM-BELT METAL ROOF KNAR 300 - STORM GRAY	BB	ALUMINUM STOREFRONT - BLACK
CC	TUN EDWARDS DE-4172 BENGALOW TAUTE	CC	TOT 1784 1/2 "ORISMAN GLASS	CC	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	CC	SPANDREL GLAZING
DD	TUN EDWARDS DEC-36 DEER GRAY	DD	TOT 1784 1/2 "ORISMAN GLASS	DD	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	DD	RECAST CONCRETE
EE	TUN EDWARDS DEC-36 DEER GRAY	EE	TOT 1784 1/2 "ORISMAN GLASS	EE	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	EE	CONCRETE - PAINTED (SHADOW II)
FF	TUN EDWARDS DEC-36 DEER GRAY	FF	TOT 1784 1/2 "ORISMAN GLASS	FF	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	FF	CONCRETE - NATURAL COLORED F
GG	TUN EDWARDS DEC-36 DEER GRAY	GG	TOT 1784 1/2 "ORISMAN GLASS	GG	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	GG	EXTERIOR LIGHT FIXTURE
HH	TUN EDWARDS DEC-36 DEER GRAY	HH	TOT 1784 1/2 "ORISMAN GLASS	HH	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	HH	SCISSOR
II	TUN EDWARDS DEC-36 DEER GRAY	II	TOT 1784 1/2 "ORISMAN GLASS	II	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	II	
JJ	TUN EDWARDS DEC-36 DEER GRAY	JJ	TOT 1784 1/2 "ORISMAN GLASS	JJ	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	JJ	
KK	TUN EDWARDS DEC-36 DEER GRAY	KK	TOT 1784 1/2 "ORISMAN GLASS	KK	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	KK	
LL	TUN EDWARDS DEC-36 DEER GRAY	LL	TOT 1784 1/2 "ORISMAN GLASS	LL	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	LL	
MM	TUN EDWARDS DEC-36 DEER GRAY	MM	TOT 1784 1/2 "ORISMAN GLASS	MM	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	MM	
NN	TUN EDWARDS DEC-36 DEER GRAY	NN	TOT 1784 1/2 "ORISMAN GLASS	NN	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	NN	
OO	TUN EDWARDS DEC-36 DEER GRAY	OO	TOT 1784 1/2 "ORISMAN GLASS	OO	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	OO	
PP	TUN EDWARDS DEC-36 DEER GRAY	PP	TOT 1784 1/2 "ORISMAN GLASS	PP	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	PP	
QQ	TUN EDWARDS DEC-36 DEER GRAY	QQ	TOT 1784 1/2 "ORISMAN GLASS	QQ	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	QQ	
RR	TUN EDWARDS DEC-36 DEER GRAY	RR	TOT 1784 1/2 "ORISMAN GLASS	RR	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	RR	
SS	TUN EDWARDS DEC-36 DEER GRAY	SS	TOT 1784 1/2 "ORISMAN GLASS	SS	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	SS	
TT	TUN EDWARDS DEC-36 DEER GRAY	TT	TOT 1784 1/2 "ORISMAN GLASS	TT	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	TT	
UU	TUN EDWARDS DEC-36 DEER GRAY	UU	TOT 1784 1/2 "ORISMAN GLASS	UU	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	UU	
VV	TUN EDWARDS DEC-36 DEER GRAY	VV	TOT 1784 1/2 "ORISMAN GLASS	VV	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	VV	
WW	TUN EDWARDS DEC-36 DEER GRAY	WW	TOT 1784 1/2 "ORISMAN GLASS	WW	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	WW	
XX	TUN EDWARDS DEC-36 DEER GRAY	XX	TOT 1784 1/2 "ORISMAN GLASS	XX	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	XX	
YY	TUN EDWARDS DEC-36 DEER GRAY	YY	TOT 1784 1/2 "ORISMAN GLASS	YY	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	YY	
ZZ	TUN EDWARDS DEC-36 DEER GRAY	ZZ	TOT 1784 1/2 "ORISMAN GLASS	ZZ	CUSTOM-BELT METAL KNAR 300 - STORM GRAY	ZZ	

SIGNAGE: BUILDING SIGNAGE IS NOT A PART OF THIS SUBMITTAL. A SEPARATE APPLICATION WILL BE SUBMITTED AT A LATER DATE.

3 BLDG. 'C' EXTERIOR SOUTH ELEVATION
SCALE: 1/16" = 1'-0"

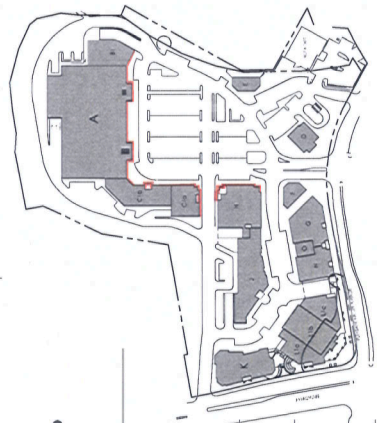


2 BLDG. 'H' & 'C' EXTERIOR EAST ELEVATION
SCALE: 1/16" = 1'-0"



1 SAFeway & Bldg. 'B' EXTERIOR SOUTH ELEVATION
SCALE: 1/16" = 1'-0"

Project Map



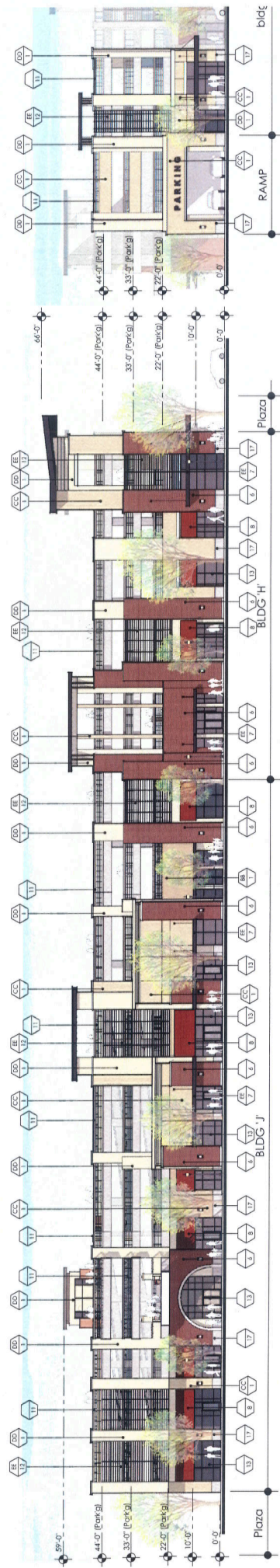
Dev. Review
November 21, 2012

DR
5

BSAA
Architecture & Planning
BENNER STANGE ASSOCIATES ARCHITECTS, INC.
5000 S.W. NEPAULIS RD.
LAKE OSWEGO, OR 97035
TEL: (503) 670-0235
FAX: (503) 670-0235
bsaarchitect.com

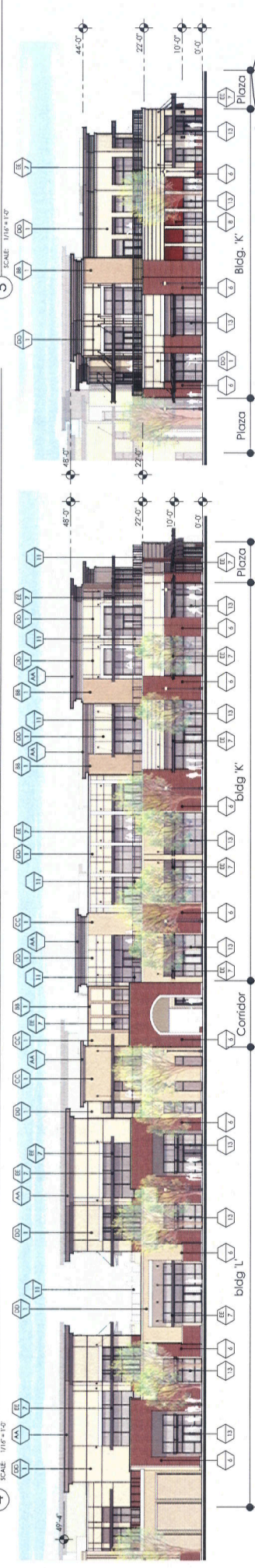
51st & Broadway
Store No. 3132
Oakland, California

Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925-738-1200
Fax: 925-467-2861



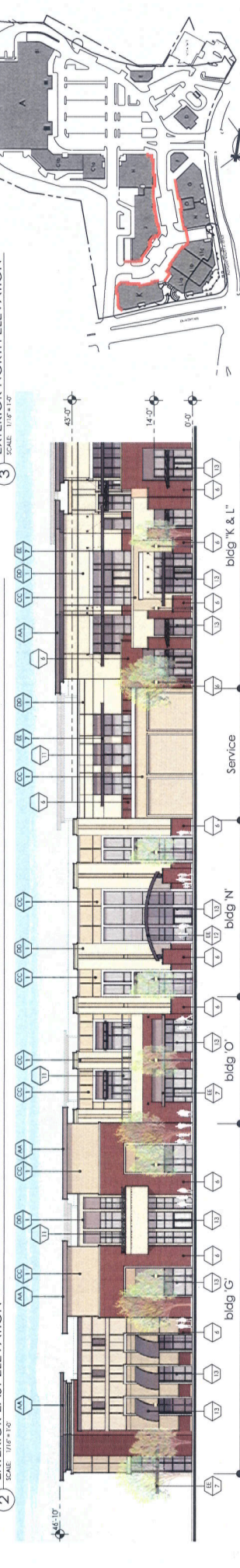
4 BLDG. 'J', 'K' & 'H' EXTERIOR SOUTH ELEVATION
SCALE: 1/16" = 1'-0"

5 BLDG. 'J', 'K' & 'H' EXTERIOR NORTH ELEVATION
SCALE: 1/16" = 1'-0"



2 BLDG. 'N' & 'V' EXTERIOR EAST ELEVATION
SCALE: 1/16" = 1'-0"

3 BLDG. 'K' EXTERIOR NORTH ELEVATION
SCALE: 1/16" = 1'-0"



1 BLDG. 'F', 'G', 'O' & 'N' EXTERIOR NORTH ELEVATION
SCALE: 1/16" = 1'-0"

BUILDING COLOR & MATERIALS
SEE SHEET DR5 & DR7

Project Map

Dev. Review
November 21, 2012



Property
Development
Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94568
Phone: 925.738.1200
Fax: 925.457.2861

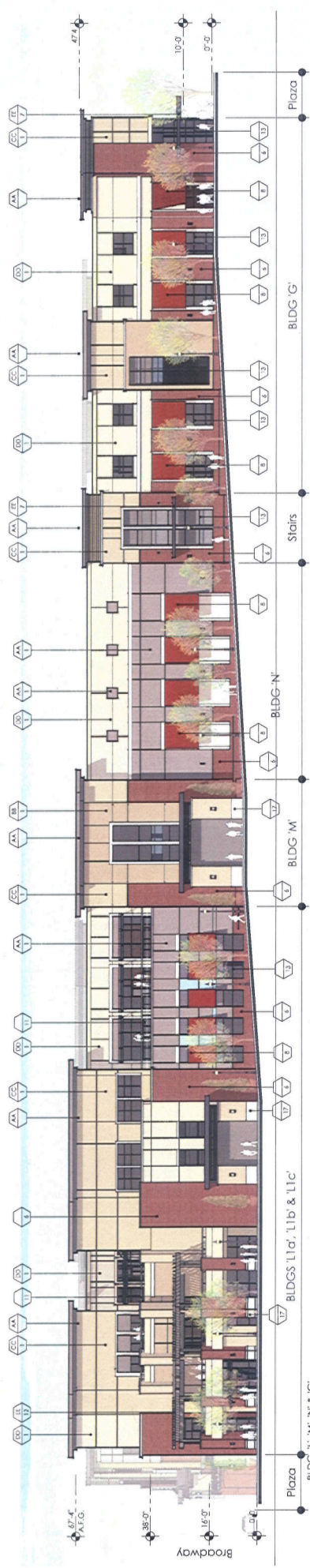
51ST & Broadway
Store No. 3132
Oakland, California

SIGNAGE: BUILDING SIGNAGE IS NOT A PART OF THIS SUBMITTAL. A SEPARATE APPLICATION WILL BE SUBMITTED AT A LATER DATE.

**BENNER
SIENGE
ASSOCIATES
ARCHITECTS, INC.**
5200 S.W. MEADOWS ROAD
SUITE 8130
LAKE OSWEGO, OR 97035
PHONE: 503.265.1300
FAX: 503.265.2335
bsa@bsaarch.com

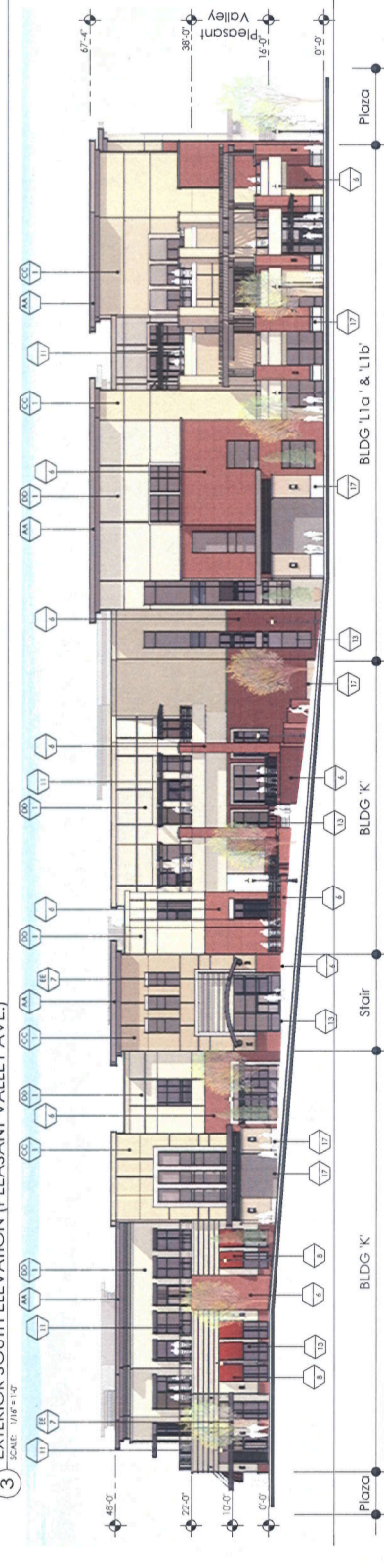


DR
6

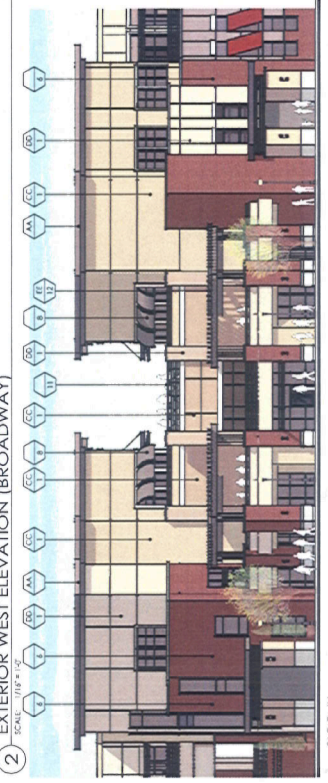


3 BLDG. 'L', 'M', 'N' & 'G'
EXTERIOR SOUTH ELEVATION (PLEASANT VALLEY AVE.)
SCALE: 1/16" = 1'-0"

SIGNAGE: BUILDING SIGNAGE IS NOT A PART OF THIS SUBMITTAL. A SEPARATE APPLICATION WILL BE SUBMITTED AT A LATER DATE.



2 BLDG. 'K' & 'L'
EXTERIOR WEST ELEVATION (BROADWAY)
SCALE: 1/16" = 1'-0"



1 BLDG. 'L'
EXTERIOR ELEVATION AT INTERSECTION
SCALE: 1/16" = 1'-0"



Project Map

Dev. Review
November 21, 2012

DR
7

BENNER STANGE ASSOCIATES
ARCHITECTS
5203 S.W. MEADOWS RD.
SUITE 8-130
LAKE OSWEGO, OR 97034
PHONE: (503) 674-0234
FAX: (503) 674-0235
bennerstange.com

BSAA
Architecture & Planning

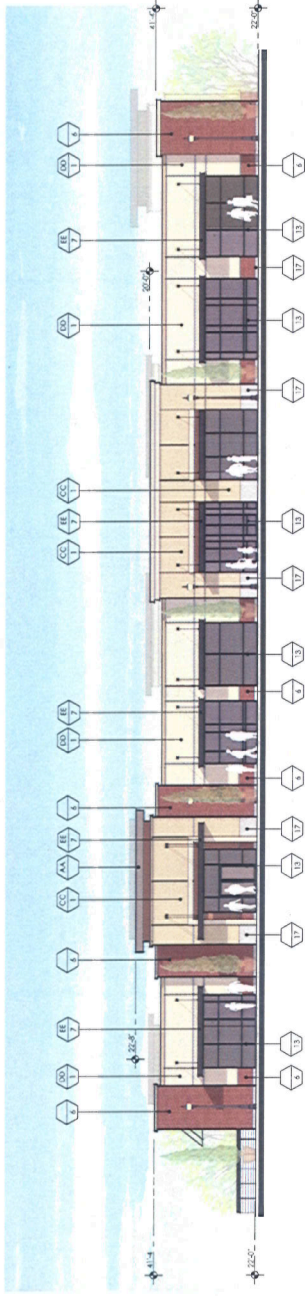
51ST & Broadway
Store No. 3132
Oakland, California

Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

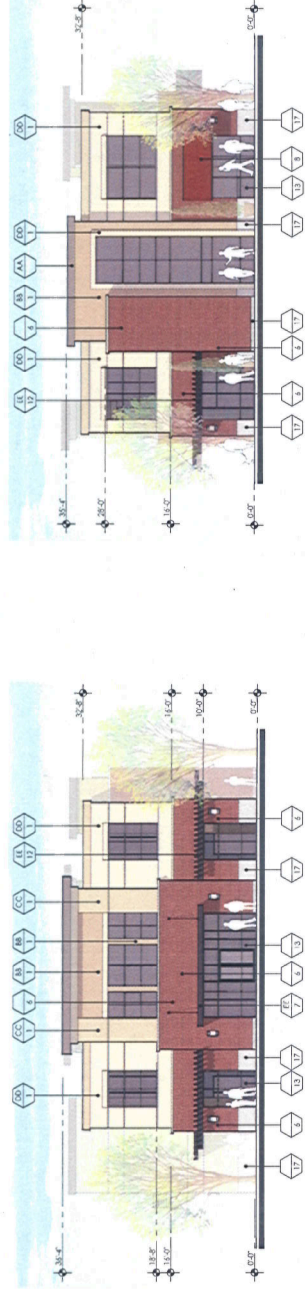
EXTERIOR ELEVATIONS

MATERIAL AND COLOR

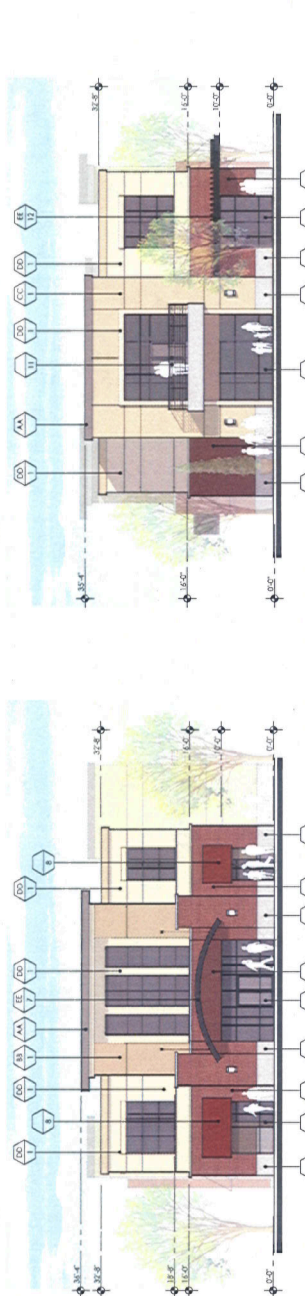
AA	DINN EDWARDS 1546 GARGOYLE	1	STUCCO
BB	DINN EDWARDS 1546 DESERT GRAY	2	METAL PANEING
CC	DINN EDWARDS 1546 BUNGALOW FAIR	3	SPUT FACED C.M.U. PAINTED
DD	DINN EDWARDS 1546 GRANITE BRIDGE	4	SANDWICH FACED C.M.U. PAINTED
EE	DINN EDWARDS 1546 DEER 187 BLACK	5	METAL CANOPY
FF	ICI A1831 CAMEL TAN	6	METAL ANVING
GG	ICI A1879 SQUARE DANCE	7	METAL TRELLIS
HH	BENJAMIN MOORE 2114.30 DESERT SHADOWS	8	MASONRY
II	ICI A1743 AFTERNOON TEA	9	PRECAST CONCRETE
JJ	ICI A2006 CHARCOAL SLATE	10	SPANREL GLAZING
KK	ICI A1943 WEST COAST GREY	11	NOT USED
LL	ICI A1784 ARROW WOOD	12	EXTERIOR LIGHT FIXTURE
MM	ICI A6531 AUTUMNAL CHESTNUT	13	METAL RAIL
NN	ICI A1745 WHISPER BUFF	14	MISC. METAL
OO	BASALITE D113 GROUND FACE	15	ALUMINUM STOREFRONT
PP	ROCK-IT LEDGESTONE	16	EXISTING
QQ	LEDGESTONE Mountain Ledge - Durango	17	NATURAL LEDGESTONE
RR	ALUMINUM STOREFRONT - BLACK ANODIZED	18	CONCRETE
SS	CUSTOM-BILT METAL ROOF KYNAR 500		
TT	STORM GRAY		
UU	CUSTOM-BILT METAL ANVINGS KYNAR 500		
VV	MIDNIGHT BRONZE		



1 BLDG 'C2' WEST ELEVATION
SCALE: 3/32\"/>



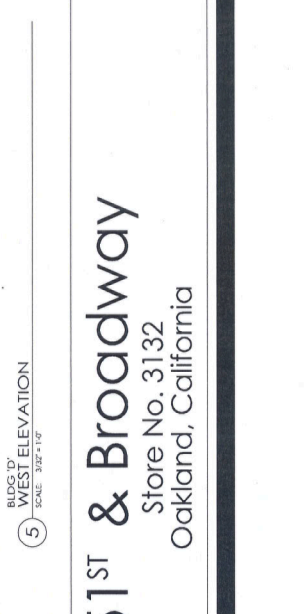
2 BLDG 'D' SOUTH ELEVATION
SCALE: 3/32\"/>



4 BLDG 'D' NORTH ELEVATION
SCALE: 3/32\"/>



3 BLDG 'D' EAST ELEVATION
SCALE: 3/32\"/>



5 BLDG 'D' WEST ELEVATION
SCALE: 3/32\"/>



Project Map

Dev. Review
November 21, 2012

51ST & Broadway
Store No. 3132
Oakland, California

Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925/738 1202
Fax: 925/467 2861

BENNER STANGE ASSOCIATES ARCHITECTS, INC.
3000 S.W. MEADOWS BLVD., SUITE 2100
LAKE OSWEGO, OR 97035
TEL: (503) 670-0235
FAX: (503) 670-0235
bsa@bsaarch.com
bsa-llbbsaarch.com

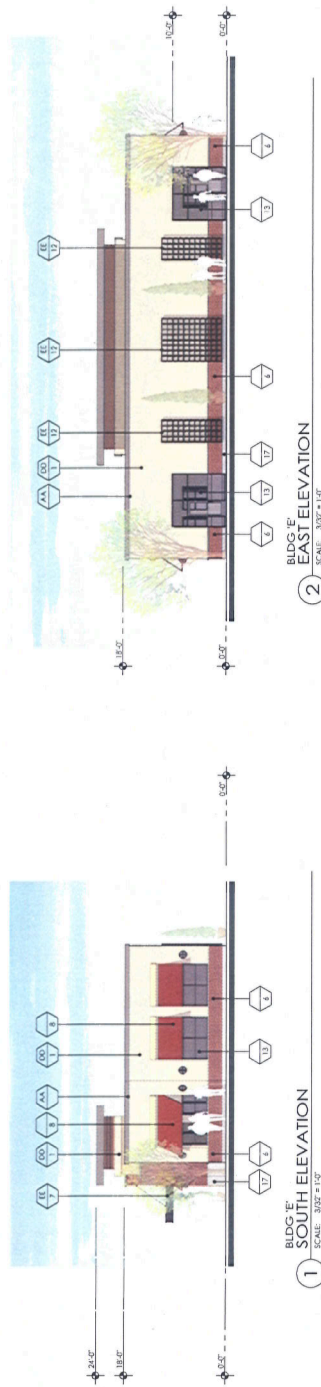
DR
8A



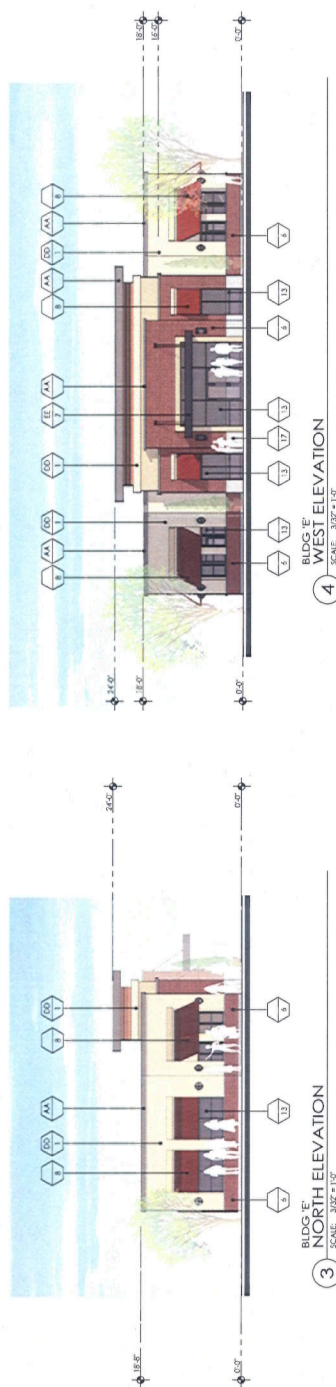
EXTERIOR ELEVATIONS

MATERIAL AND COLOR

1A	DUNN EDWARDS 1546 CARCOTITE	1	STUCCO
1B	DUNN EDWARDS DEC-760 DESERT GRAY	2	METAL PANELING
2A	DUNN EDWARDS DE-6172 BUNGALOW TAUPE	3	SPLIT FACED C.M.U. PAINTED
2B	DUNN EDWARDS HC-83 GRANITE BEIGE	4	SMOOTH FACED C.M.U. PAINTED
3	DUNN EDWARDS DEA-187 BLACK	5	METAL CANOPY
4	ICI A1524 CAMEL TAN	6	METAL AWNING
5A	ICI A1627 SQUARE DANCE	7	METAL TRELLIS
5B	BENJAMIN MOORE 2114-30 DESERT SHADOWS	8	MASONRY
6	ICI A1763 AFTERNOON TEA	9	PRECAST CONCRETE
7	ICI A2026 CHARCOAL SLATE	10	SPANDREL GLAZING
8	ICI A1903 WEST COAST GREY	11	NOT USED
9A	ICI A1794 ARROW WOOD	12	EXTERIOR LIGHT FIXTURE
9B	ICI A0531 AUTUMNAL CHESTNUT	13	METAL RAIL
10	ICI A1165 WHISPER BUFF	14	MISC. METAL
11	BASAUIE D113 GROUND FACE	15	ALUMINUM STOREFRONT
12	ROCKIT LEDGESTONE	16	EXISTING
13	Safeway Lifestyle Shards	17	NATURAL LEDGESTONE
14	TELOORADO MOUNTAIN LEDGE - DURANGO	18	CONCRETE
15	ALUMINUM STOREFRONT - BLACK ANODIZED	19	
16	CUSTOM-BUILT METAL ROOF KYNAR 500		
17	STORM GRAY		
18	CUSTOM-BUILT METAL AWNINGS KYNAR 500		
19	MOONLIGHT BRONZE		



2 EAST ELEVATION
SCALE: 3/32" = 1'-0"



3 NORTH ELEVATION
SCALE: 3/32" = 1'-0"



Project Map

Dev. Review
November 21, 2012

DR
8B

BENNER STANGE ASSOCIATES ARCHITECTS, INC.
5200 S.W. MEADOWS BLVD., SUITE 8100
LAKE OSWEGO, OR 97035
TEL: (503) 270-0235
FAX: (503) 270-0235
bsa@bsaarch.com

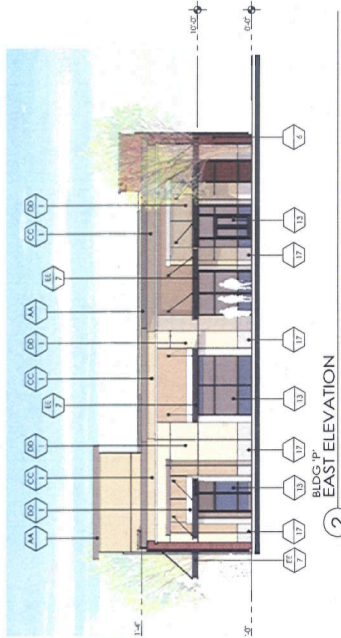
Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

51ST & Broadway
Store No. 3132
Oakland, California

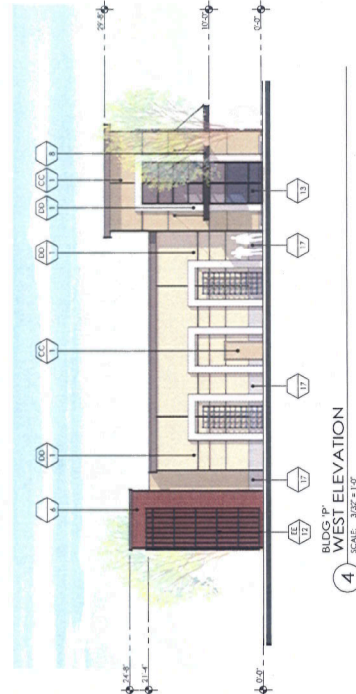
EXTERIOR ELEVATIONS

MATERIAL AND COLOR

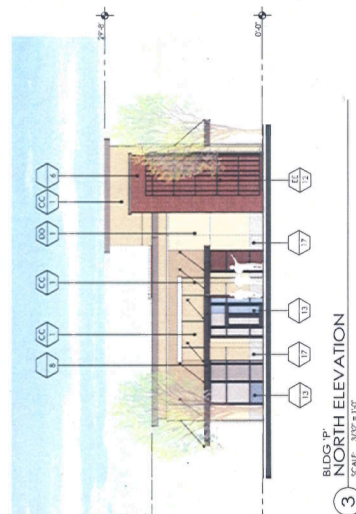
AA	DINN EDWARDS 1546 GARGOYLE	1	STUCCO
AB	DINN EDWARDS DEC-760 DESERT GRAY	2	METAL PANELING
AC	DINN EDWARDS DE-6172 BUNGALOW TAUPE	3	SPLIT FACED C.A.U. PAINTED
AD	DINN EDWARDS HC-83 GRANITE BEIGE	4	SMOOTH FACED C.A.U. PAINTED
AE	DINN EDWARDS DEA-187 BLACK	5	METAL CANOPY
AF	ICF A1824 CAMEL TAN	6	METAL ANNING
AG	ICF A1679 SQUARE DANCE	7	METAL TRELLIS
AH	BENJAMIN MOORE 2114-30 DESERT SHADOWS	8	MASONRY
AI	ICF A1749 AFTERNOON TEA	9	PRECAST CONCRETE
AJ	ICF A2026 CHARCOAL SLATE	10	SPANFIRE GLAZING
AK	ICF A1743 WEST COAST GREY	11	NOT USED
AL	ICF A1784 AIRFLOW WOOD	12	EXTERIOR LIGHT FIXTURE
AM	ICF A2031 AUTUMNAL CHESTNUT	13	METAL RAIL
AN	ICF A1745 WHISPER BUFF	14	MISC. METAL
AO	BALAUDE D113 GROUND FACE	15	ALUMINUM STOREFRONT
AP	ROCK-IT LEDGESTONE Slovenly Lifestyle Stonework	16	EXISTING
AQ	ELDOBADO MOUNTAIN LEDGE - DURANGO	17	NATURAL LEDGESTONE
AR	ALUMINUM STOREFRONT - BLACK ANODIZED	18	CONCRETE
AS	CUSTOM-BILT METAL ROOF KYNAR 500 STORM GRAY		
AT	CUSTOM-BILT METAL AWNINGS KYNAR 500 MOONLIGHT BIONE		



BLDG 1P
SOUTH ELEVATION
SCALE: 3/32\"/>



BLDG 1P
WEST ELEVATION
SCALE: 3/32\"/>



BLDG 1P
NORTH ELEVATION
SCALE: 3/32\"/>



Project Map

Dev. Review
November 21, 2012

51ST & Broadway
Store No. 3132
Oakland, California

Property Development Centers
5918 Stoneridge Mall Road
Emeryville, CA 94608
Phone: 925.738.1202
Fax: 925.467.2861

BENNER STANGE ASSOCIATES
ARCHITECTS, INC.
3000 34TH AVENUE, SUITE 8-130
LAKE OREGON, OR 97030
PHONE: 503.670.0234
FAX: 503.670.0235
bsa@bsacorp.com
bsa-llc.com

DR
8C

51ST & Broadway

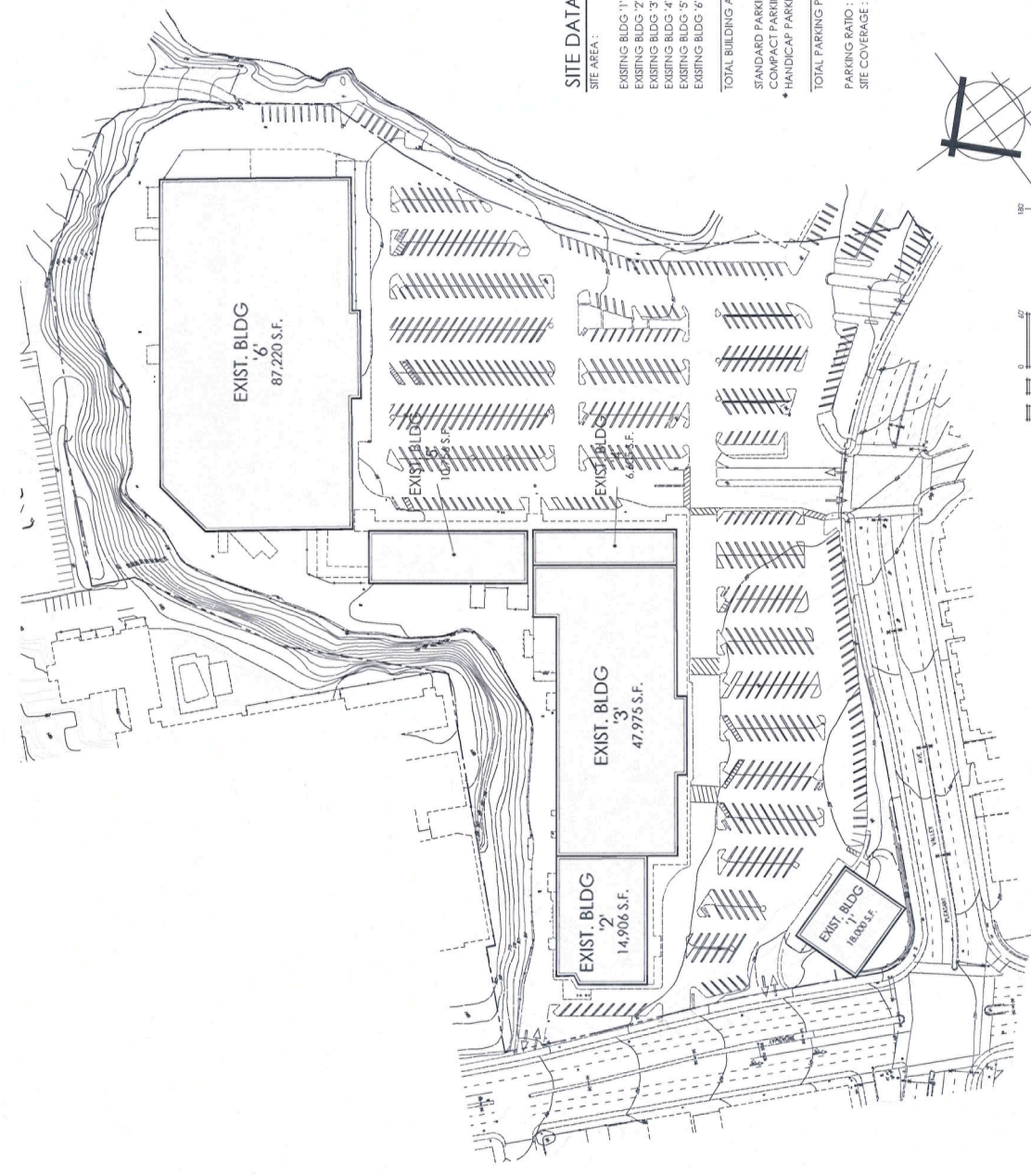
Store No. 3132
 Oakland, California

Dev. Review
 November 21, 2012

BENNER STANGE ASSOCIATES ARCHITECTS, INC.
 5200 S.W. MEADOWS RD.
 SUITE 200
 LAKE OSWEGO, OR 97035
 (503) 265-0255
 benstange.com

BSAA
 Architecture & Planning

DR
 9



SITE DATA :

SITE AREA :	15.49 ACRES	670,824 S.F.
EXISTING BLDG '1' 3 STORY		18,000 S.F.
EXISTING BLDG '2'		14,906 S.F.
EXISTING BLDG '3'		47,975 S.F.
EXISTING BLDG '4'		6,665 S.F.
EXISTING BLDG '5'		10,756 S.F.
EXISTING BLDG '6'		87,220 S.F.
TOTAL BUILDING AREA :		185,462 S.F.
STANDARD PARKING PROVIDED :		650 SPACES
COMPACT PARKING PROVIDED :		0 SPACES
HANDICAP PARKING PROVIDED :		17 SPACES
TOTAL PARKING PROVIDED :		667 SPACES
PARKING RATIO :		3.60 SPACES / 1000 S.F.
SITE COVERAGE :		28%

1 EXISTING SITE PLAN
 SCALE: 1" = 40'



1 AERIAL OF EXISTING SITE PLAN

SCALE: 60' = 1"=0"



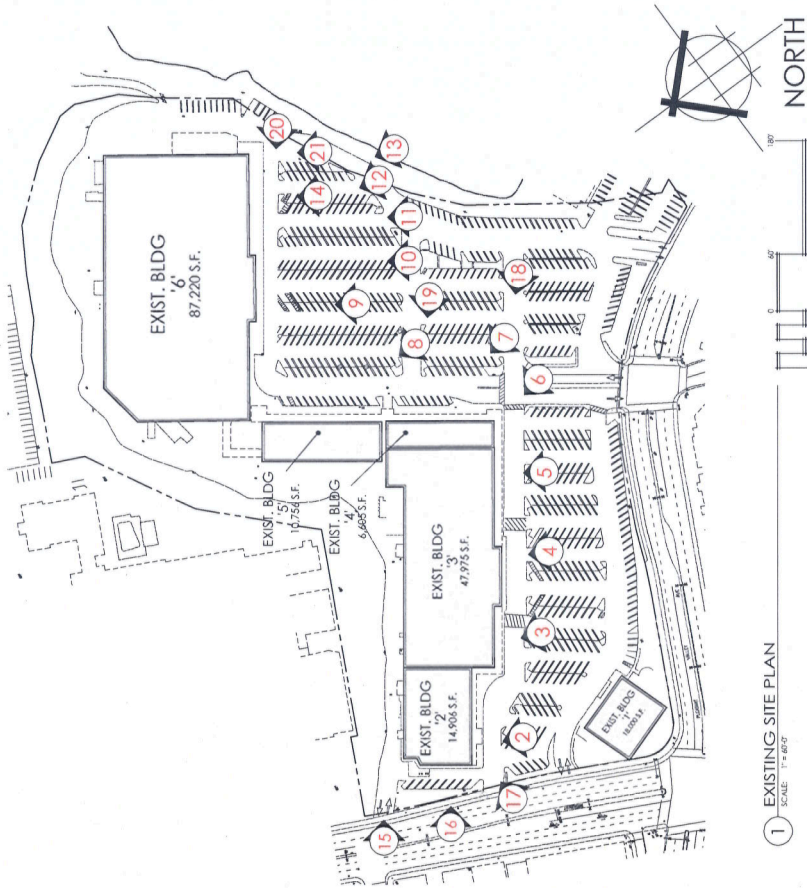
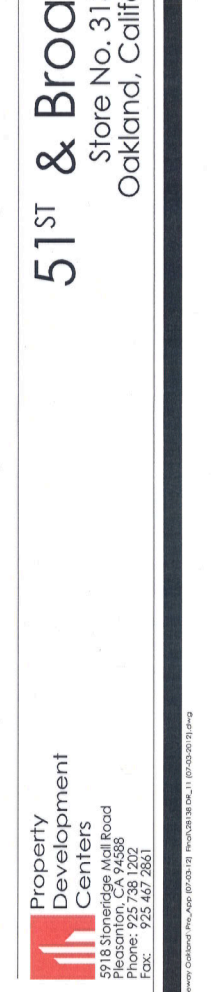
Dev. Review
November 21, 2012

**Property
Development
Centers**
5918 Stoneridge Mall Road
Dublin, CA 94568
Phone: 925.738.1202
Fax: 925.467.2861

51ST & Broadway
Store No. 3132
Oakland, California

**BENNER
STANGE
ASSOCIATES**
ARCHITECTS, INC.
3200 3RD AVENUE, SUITE 8-130
LAKE OSWEGO, OR 97034
PHONE: (503) 670-0234
FAX: (503) 670-0235
bsa@benstange.com

**DR
10**



Dev. Review
November 21, 2012

51st & Broadway

Store No. 3132
Oakland, California

Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

BENNER STANGE ASSOCIATES
ARCHITECTS
5200 S.W. MEADOWS RD.
SUITE B-130
LAKE OSWEGO, OR 97034
FAX (503) 670-0234
bennerstange.com

DR
11



① VIEW FROM BROADWAY LOOKING SOUTHEAST
N.E.S.



② VIEW FROM PLEASANT VALLEY AVE. LOOKING SOUTHWEST
N.E.S.



③ VIEW FROM PLEASANT VALLEY AVENUE LOOKING NORTHEAST
N.E.S.



④ VIEW FROM BROADWAY LOOKING NORTHWEST
N.E.S.

**Property
Development
Centers**
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925.738.1202
Fax: 925.467.2861

51ST & Broadway
Store No. 3132
Oakland, California

Dev. Review
November 21, 2012

**BENNER
STANGE
ASSOCIATES**
ARCHITECTS, INC.
520313 W. MEADOWS
SUITE 100
LAKE OSWEGO, OR 97035
Tel: 503.265.1000
Fax: 503.265.0235
bsa@bsaarch.com
Architect/Engineer & Planning

DR
12



① VIEW FROM BROADWAY LOOKING NORTHWEST
N.T.S.



② VIEW OF WATER RESERVOIR LOOKING NORTHEAST
N.T.S.

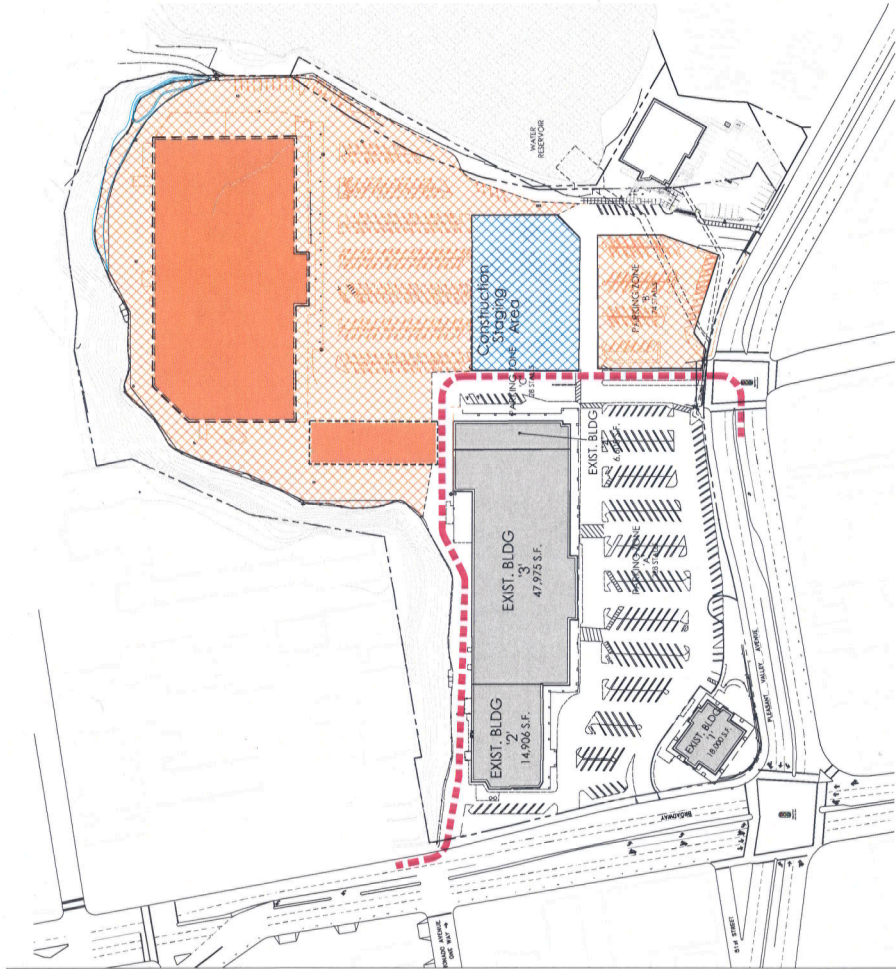
**Property
Development
Centers**
5918 Stoneridge Mall Road
Pleasanton, CA 94586
Phone: 925 467 2801
Fax: 925 467 2861

51ST & Broadway
Store No. 3132
Oakland, California

**BENNER
STANGE
ASSOCIATES**
ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
LAKE OSWEGO, OR 97035
TEL: (503) 265-0255
FAX: (503) 265-0256
bsa@bsaarch.com
BSAA
Architecture & Planning

DR
13

Dev. Review
November 21, 2012



DEMO DEMO ZONE EXISTING STAGING SERVICE ROUTE

DEMO AND PHASE PLANS



Dev. Review
November 21, 2012

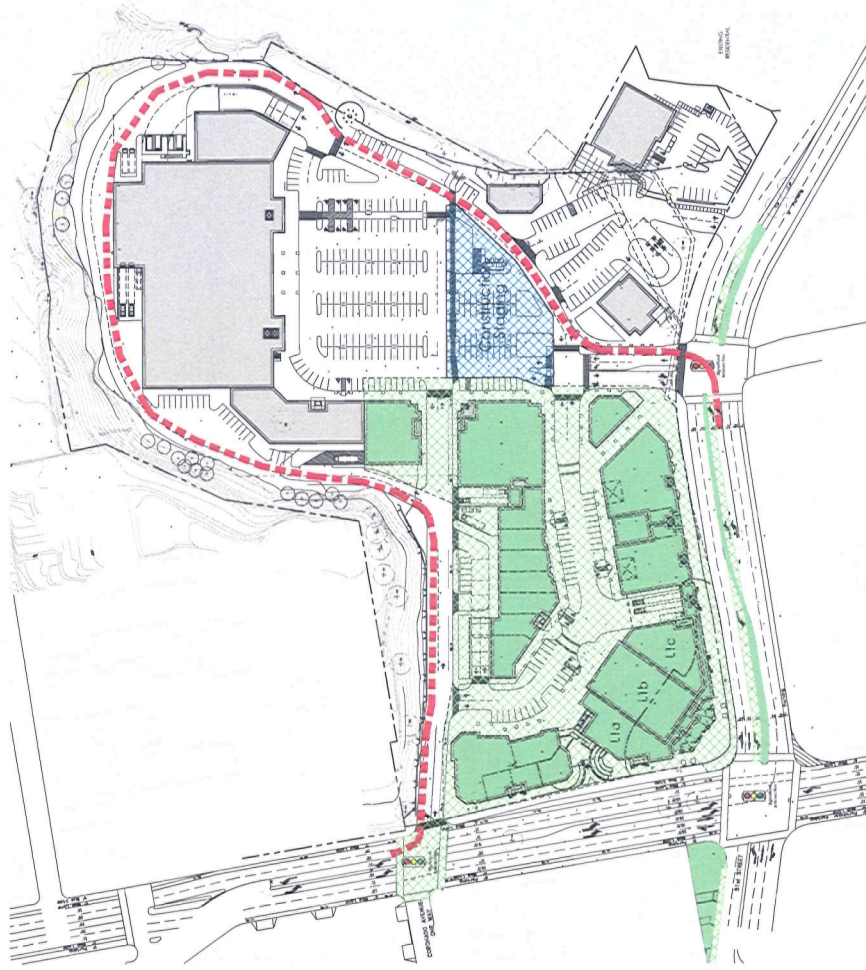
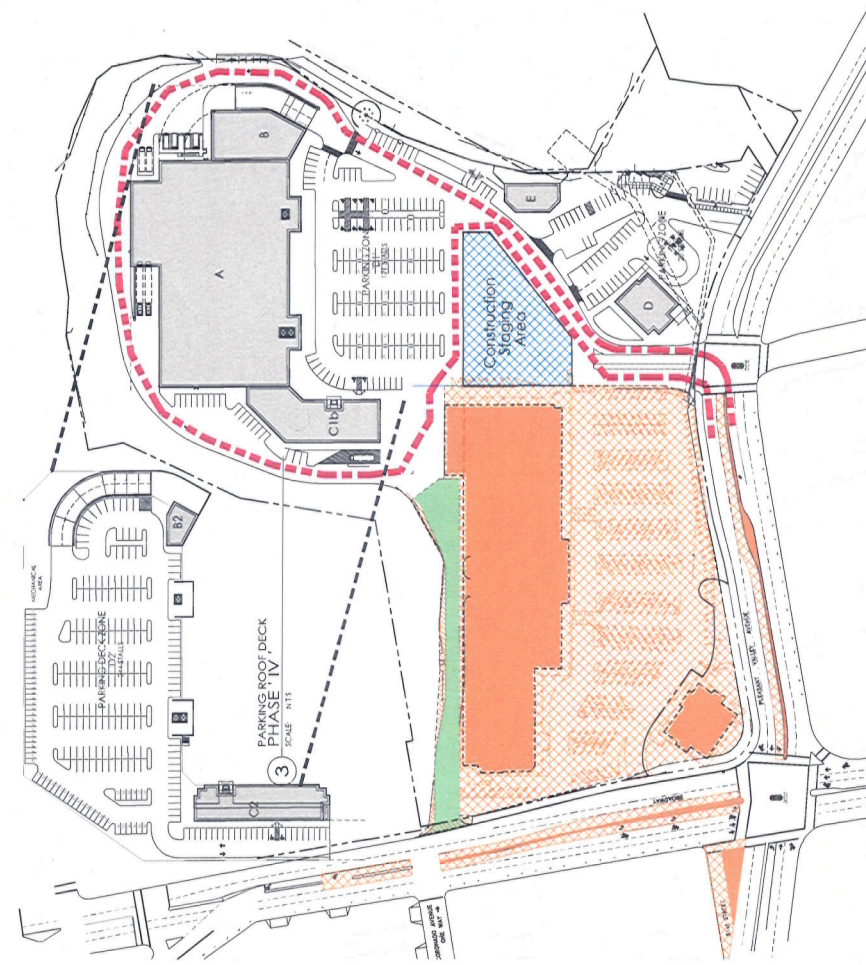
Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 739 1222
Fax: 925 467 2861

51ST & Broadway
Store No. 3132
Oakland, California

BENNER STANGE ASSOCIATES ARCHITECTS, INC.
5000 S.W. MEADOWS RD.
SUITE 100
LAKE OSWEGO, OR 97035
PHONE: 503 650-0334
FAX: 503 650-0334
bsaia@bsaia.com
bsaiaarch.com

BSAA
Architecture & Planning

DR
14a



DEMO AND PHASE PLANS



Dev. Review
November 21, 2012

Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 735 1202
Fax: 925 467 2861



51ST & Broadway
Store No. 3132
Oakland, California



BENNER STANGE ASSOCIATES
ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
LAKE OSWEGO, OR 97035
PHONE: 503 270 0234
FAX: 503 270 0234
bsa@bsaarch.com

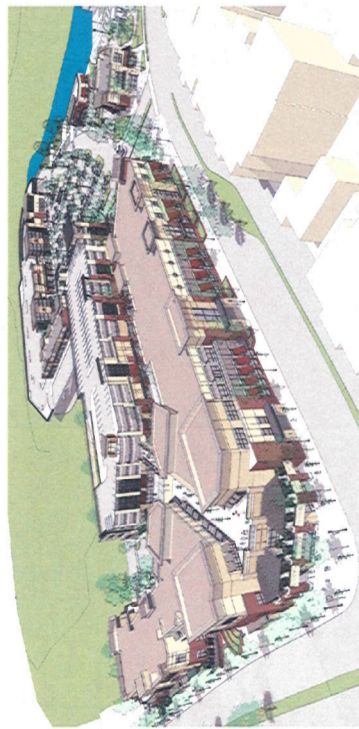
DR 14b



① EXISTING VIEW FROM BROADWAY LOOKING EAST
N.E.S.



② EXISTING VIEW FROM PLEASANT VALLEY LOOKING NORTH
N.E.S.



③ PROPOSED VIEW FROM BROADWAY LOOKING EAST
N.E.S.



④ PROPOSED VIEW FROM PLEASANT VALLEY LOOKING NORTH
N.E.S.

**Property
Development
Centers**
5918 Stoneridge Mall Road
Pleasanton, CA 94566
Phone: 925.738.1200
Fax: 925.467.2861

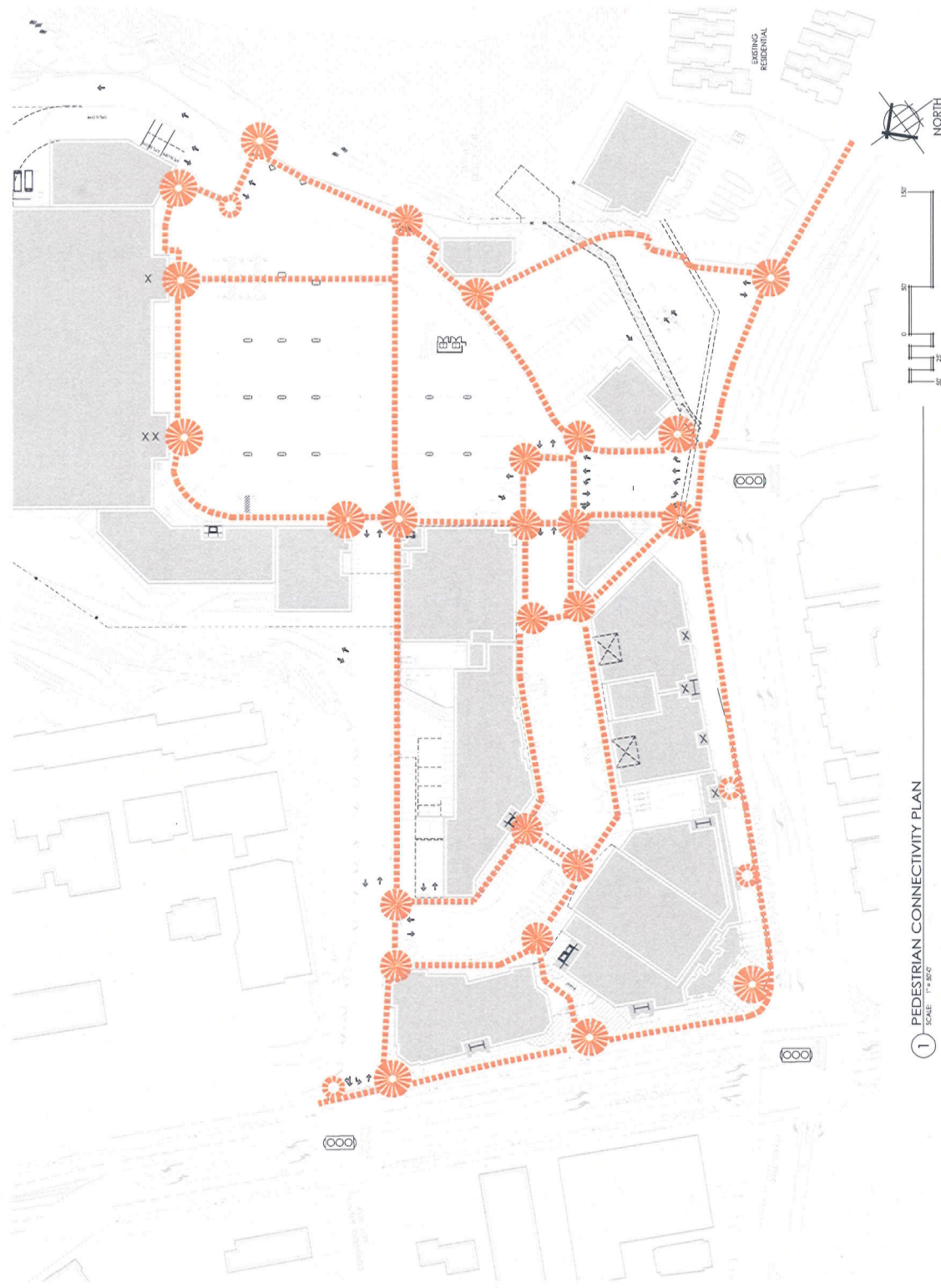
51ST & Broadway
Store No. 3132
Oakland, California

**BENNER
STANGE
ASSOCIATES
ARCHITECTS, INC.**
5000 S.W. MESSERSmith
LAKE OSWEGO, OR 97035
Phone: 503.670.0335
Fax: 503.670.0335
bsaa@bsaaarch.com



**DR
15**

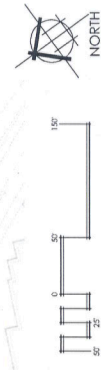
Dev. Review
November 21, 2012



INTERNAL PEDESTRIAN ROUTES

PEDESTRIAN PATH CONNECTIONS

① PEDESTRIAN CONNECTIVITY PLAN
SCALE: 1" = 50'



Property Development Centers
5918 Stoneridge Mall Road
Emeryville, CA 94608
Phone: 925.738.1202
Fax: 925.467.2861

51ST & Broadway
Store No. 3132
Oakland, California

Dev. Review
November 21, 2012

BENNER STANGE ASSOCIATES ARCHITECTS, INC.
5000 S.W. MADEIRA AVENUE, SUITE 8-130
LAKE OSWEGO, OR 97035
PHONE: (503) 670-0200
FAX: (503) 670-0235
bsa@bsaarch.com

BSAA
Architecture & Planning

DR 17



① PROPOSED BROADWAY ENTRY AERIAL VIEW
N.T.S.



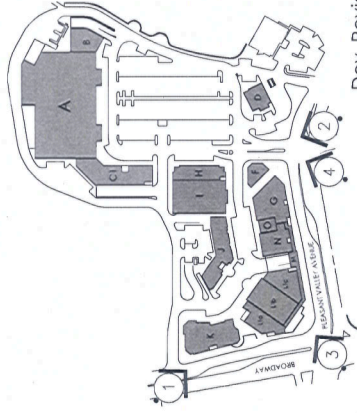
② PROPOSED PLEASANT VALLEY MAIN ENTRY AERIAL VIEW
N.T.S.



③ PROPOSED BROADWAY & PLEASANT VALLEY AERIAL VIEW
N.T.S.



④ PROPOSED PLEASANT VALLEY MAIN ENTRY VIEW
N.T.S.



Dev. Review
November 21, 2012

**Property
Development
Centers**
5918 Stoneridge Mall Road
Pleasanton, CA 94566
Phone: 925.738.1202
Fax: 925.467.2861

51ST & Broadway
Store No. 3132
Oakland, California

**BENNER
STANGE
ASSOCIATES**
ARCHITECTS, INC.
5200 S.W. MEADOWS DRIVE, SUITE B-130
LAKE OSWEGO, OR 97035
PHONE: 503.261.0000
FAX: 503.270.0235
bsa@bsaarch.com
Architecture & Planning

**DR
18**



② PROPOSED INTERIOR AERIAL VIEW
N.E.S.



① PROPOSED INTERIOR AERIAL VIEW
N.E.S.



④ PROPOSED INTERIOR AERIAL VIEW
N.E.S.



③ PROPOSED INTERIOR AERIAL VIEW
N.E.S.



Dev. Review
November 21, 2012

51ST & Broadway

Store No. 3132
Oakland, California

**Property
Development
Centers**
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 763 1201
Fax: 925 467 2881

**BENNER
STANGE
ASSOCIATES, INC.**
ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
LAKE OSWEGO, OR 97035
TEL: 503 670-2251
FAX: 503 670-2255
bsa@bsaarch.com
Architecture & Planning

DR
19



Dev. Review
November 21, 2012



**Property
Development
Centers**

5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

51ST & Broadway
Store No. 3132
Oakland, California

**BENNER
STANGE
ASSOCIATES
ARCHITECTS, INC.**
5200 S.W. MEADOWS RD.
SUITE B-130
LAKE OSWEGO, OR 97035
(503) 670-0224
FAX (503) 670-0235
bsa@bsarch.com

DR 20



① PROPOSED VIEW AT BUILDING 'C' & 'H'
N.T.S.



② PROPOSED VIEW AT BUILDING 'C'
N.T.S.



③ PROPOSED VIEW AT SAFEWAY
N.T.S.



④ PROPOSED VIEW AT BUILDING 'D'
N.T.S.



Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2881

51ST & Broadway
Store No. 3132
Oakland, California

BENNER STANGE ASSOCIATES ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
LAKE OSWEGO, OR 97035
PHONE: 503 265-0222
FAX: 503 265-0234
bsa@bsaarch.com
bsa@bsaarch.com

DR 21

Dev. Review
November 21, 2012



1 PROPOSED VIEW-AT BUILDING 'E'
N.T.S.



2 PROPOSED INTERIOR VIEW
N.T.S.



3 PROPOSED INTERIOR VIEW
N.T.S.



4 PROPOSED INTERIOR VIEW
N.T.S.



Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 729 7201
Fax: 925 467 2881

51ST & Broadway
Store No. 3132
Oakland, California

BENNER STANGE ASSOCIATES, INC.
ARCHITECTS, INC.
5200 S.W. MEADOWS DRIVE, SUITE 100
LAKE OSWEGO, OR 97035
Phone: 503 670-0235
Fax: 503 670-0235
bsa@bsaarch.com
bsaarch.com

DR 22

Dev. Review
November 21, 2012



① PROPOSED INTERIOR VIEW
N.E.S.



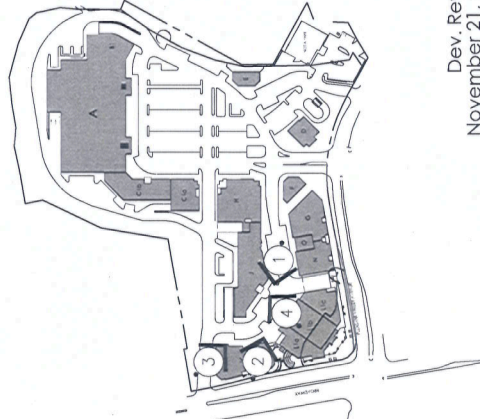
② PROPOSED INTERIOR VIEW
N.E.S.



③ PROPOSED INTERIOR VIEW
N.E.S.



④ PROPOSED INTERIOR VIEW
N.E.S.



Dev. Review
November 21, 2012

**Property
Development
Centers**
25918 Stoneridge Mall Road
Philastrum 94588
Phone: 925 738 1202
Fax: 925 467 2861

51ST & Broadway
Store No. 3132
Oakland, California

**BENNER
STANGE
ASSOCIATES**
ARCHITECTS, INC.
5500 S.W. MEADOWS
SUITE B-100
LAKE OSWEGO, OR 97035
Phone: 503 265-1234
Fax: 503 265-0235
bsa@bsaarch.com

**DR
23a**



1 PROPOSED VIEW FROM SAFEWAY PARKING DECK
N.E.S.



2 PROPOSED INTERIOR VIEW
N.E.S.



3 PROPOSED VIEW OF BUILDING 'K'
N.E.S.



4 PROPOSED VIEW OF BUILDING 'K'
N.E.S.



Property Development Centers
5718 Stoneridge Mall Road
Pleasanton, CA 94586
Phone: 925 467 2801
Fax: 925 467 2801

51ST & Broadway
Store No. 3132
Oakland, California

BENNER STANGE ASSOCIATES ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
LAKE OSWEGO, OR 97035
TEL: (503) 265-0235
FAX: (503) 265-0235
bsa@bsaarch.com
bsaarch.com

DR 23b

Dev. Review
November 21, 2012



1 PROPOSED VIEW AT BUILDING 'K' & 'L'
N.E.S.



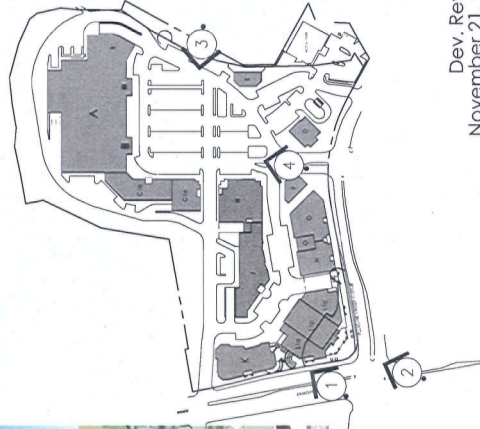
2 PROPOSED VIEW AT BUILDING 'I'
N.E.S.



3 PROPOSED INTERIOR VIEW
N.E.S.



4 PROPOSED VIEW LOOKING AT SAFEWAY
N.E.S.



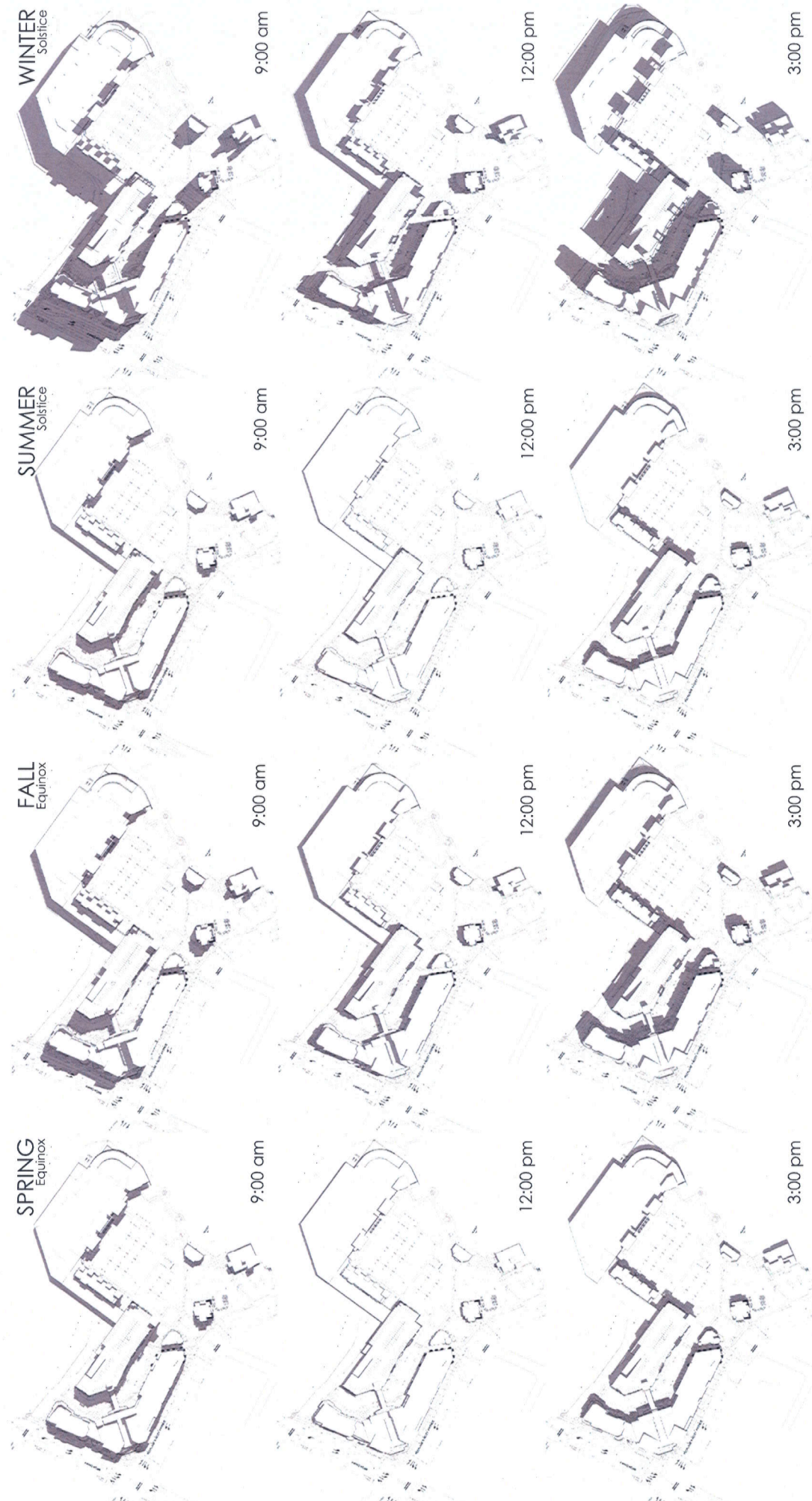
**Property
Development
Centers**
5918 Stoneridge Mall Road
Pleasanton, CA 94566
Phone: 925.738.1200
Fax: 925.467.2861

51ST & Broadway
Store No. 3132
Oakland, California

Dev. Review
November 21, 2012

**BENNER
STANGE
ASSOCIATES
ARCHITECTS, INC.**
5200 S.W. MEADOWS RD.
SUITE 100
LAKE OSWEGO, OR 97035
PHONE: 503.674.0235
FAX: 503.674.0236
bsa@bsaarch.com
Architecture & Planning

**DR
23C**



① SHADOW DIAGRAMS
N.E.E.

Dev. Review
November 21, 2012

DR 24
BSAA
Architectural & Planning

BENNER STANGE ASSOCIATES ARCHITECTS, INC.
5200 S.W. MEADOWS RD., SUITE 200
LAKE OSWEGO, OR 97035
(503) 676-0034
Fax: (503) 676-0034
bsa@bsaarch.com

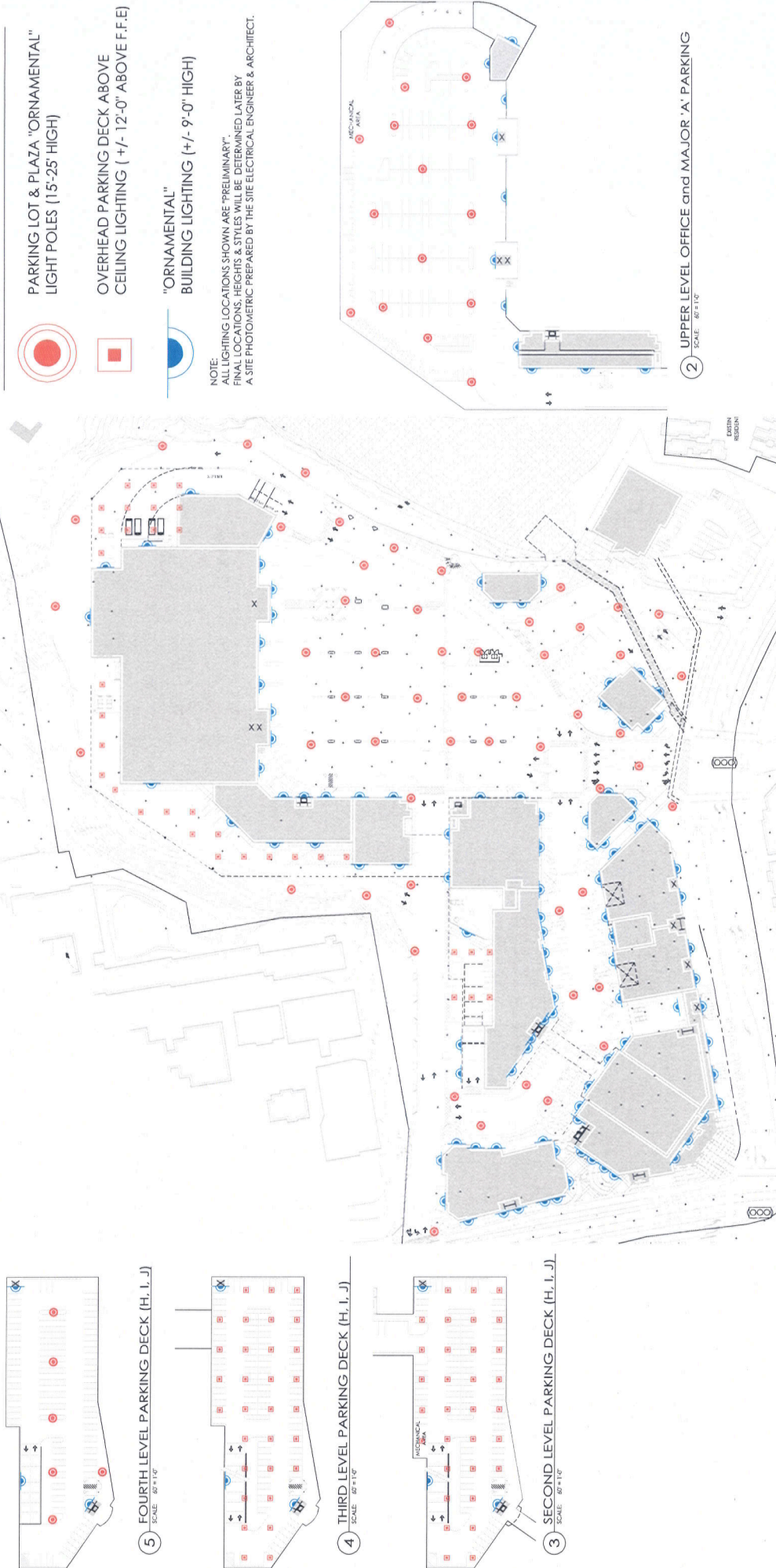
51ST & Broadway

Store No. 3132
Oakland, California

Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 729 7072
Fax: 925 467 2861

PARKING LOT & PLAZA "ORNAMENTAL" LIGHT POLES (15'-25' HIGH)	OVERHEAD PARKING DECK ABOVE CEILING LIGHTING (+/- 12'-0" ABOVE F.F.E)	"ORNAMENTAL" BUILDING LIGHTING (+/- 9'-0" HIGH)

NOTE:
ALL LIGHTING LOCATIONS SHOWN ARE "PRELIMINARY".
FINAL LOCATIONS, HEIGHTS & STYLES WILL BE DETERMINED LATER BY
A SITE PHOTOMETRIC PREPARED BY THE SITE ELECTRICAL ENGINEER & ARCHITECT.



1 SITE & BUILDING LIGHTING PLAN
SCALE: 1" = 60'-0"

2 UPPER LEVEL OFFICE and MAJOR 'A' PARKING
SCALE: 60' = 1"0"

Dev. Review
November 21, 2012



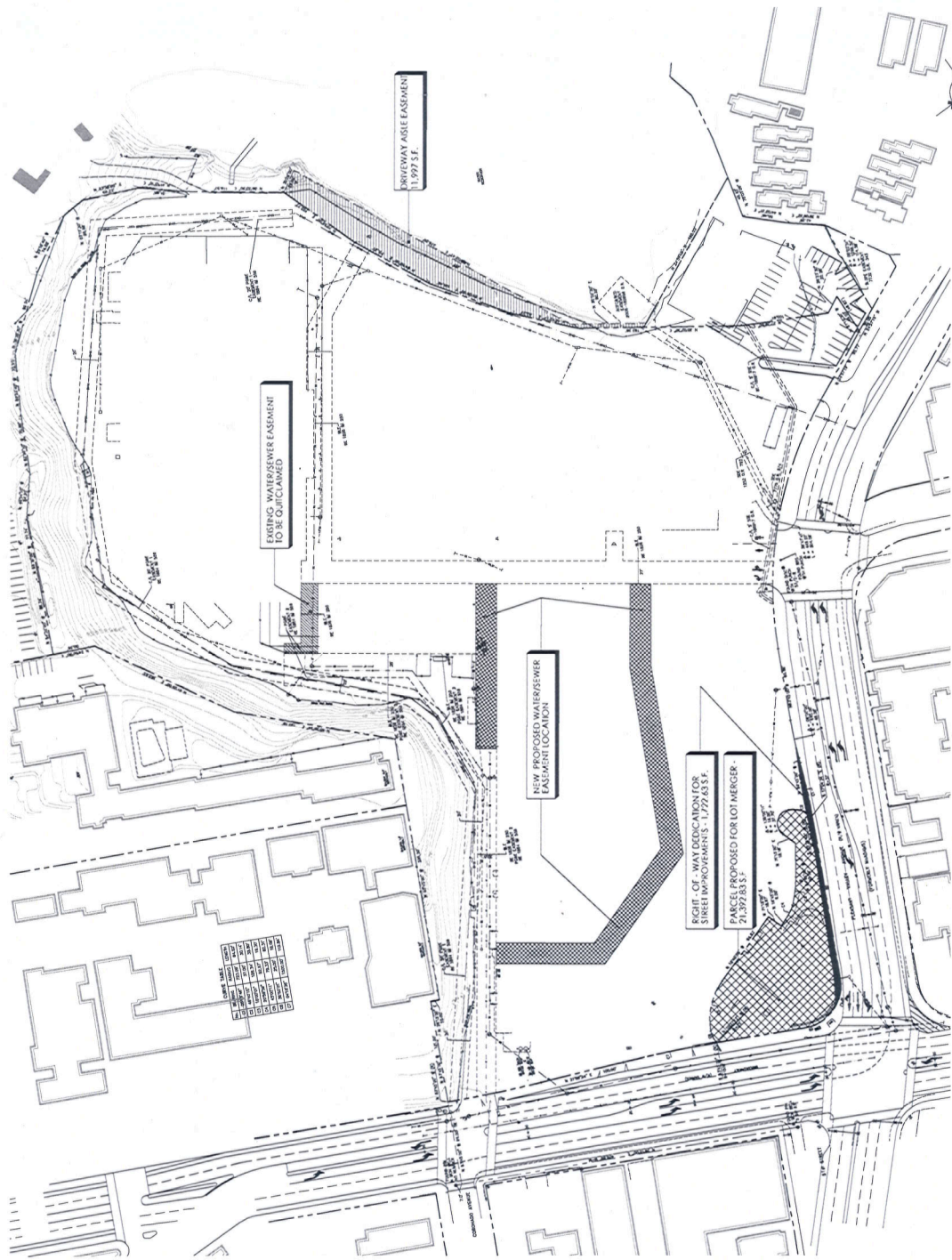
**Property
Development
Centers**

5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

51ST & Broadway
Store No. 3132
Oakland, California

**BENNER
STANGE
ASSOCIATES**
ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
SUITE B-130
LAKE OSWEGO, OR 97035
(503) 670-0234
Fax (503) 670-0235
bsa@bsaarch.com

DR 25



PROPOSED EASEMENT and PROPERTY LINE ADJUSTMENT DEDICATION

Dev. Review
November 21, 2012

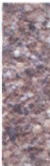
51ST & Broadway
Store No. 3132
Oakland, California

Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

BENNER STANGE ASSOCIATES
ARCHITECTS & ENGINEERS
5200 S.W. MILDENBURY RD.
LAKE OSWEGO, OR 97031
PHONE: (503) 670-0234
FAX: (503) 670-0235
bsaassociates.com

DR 26

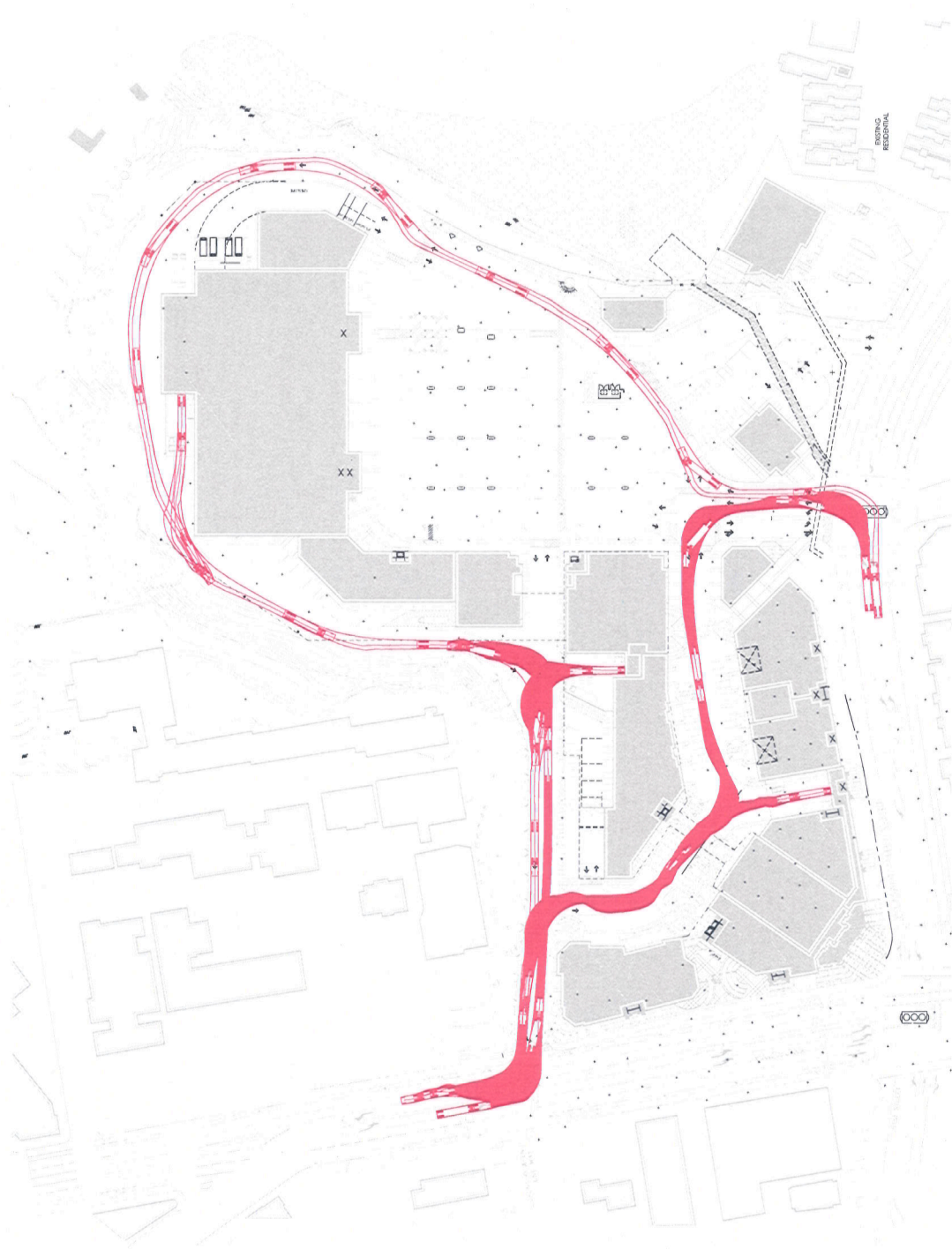
Oakland, California
COLOR/ MATERIAL SCHEDULE
July 27, 2010

B8		'BASALITE' D375 GROUND FACE
S5		'BASALITE' D113 SPLIT FACE
TT		'BASALITE' D113 GROUND FACE
UU		'BASALITE' D390 SPLIT FACE
VV		'BASALITE' D389 SPLIT FACE
WW		'H.C. MUDGOH' 1183 BURNT ROSE
XX		'ROCK IT' LEDGESTONE Salseway Lifestyle Standard
YY		'ELUDORADY' MOUNTAIN LEDGE - DURANGO
ZZ		'CUSTOM-BILT' METAL ROOF KYNAR 500 - STORM GREY
AZ		'CUSTOM-BILT' METAL AWNINGS KYNAR 500 - MIDNIGHT BRONZE
BZ		BLACK ANODIZED

51ST & Broadway
Store No. 3132
Oakland, California

**BENNER
STANGE
ASSOCIATES**
ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
SUITE B-130
LAKE OSWEGO, OR 97035
(503) 276-0234
FAX (503) 276-0235
bsa@bsarch.com

DR 27



① SITE PLAN - TRUCK SERVICE ROUTE
SCALE: 8" = 1' ±



Dev. Review
November 21, 2012

51ST & Broadway

Store No. 3132
Oakland, California

Property Development Centers
25118 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925.738.1202
Fax: 925.467.2861

BENNER STANGE ASSOCIATES ARCHITECTS, INC.
5200 S.W. MEADOWS DRIVE, SUITE 8-130
LAKE OSWEGO, OR 97035
PHONE: (503) 261-1000
FAX: (503) 270-0235
bsa@bsaarch.com

DR 28

LANDSCAPE LEGEND

[illegible]

BIORETENTION AREA

[illegible]

Proposed Shrubs

Existing tree to remain

Existing Protected Tree to remain

Protected Tree to be removed

1 LANDSCAPE PLAN - MAIN LEVEL
SCALE: 60' = 1"=0"

Dev. Review
November 21, 2012

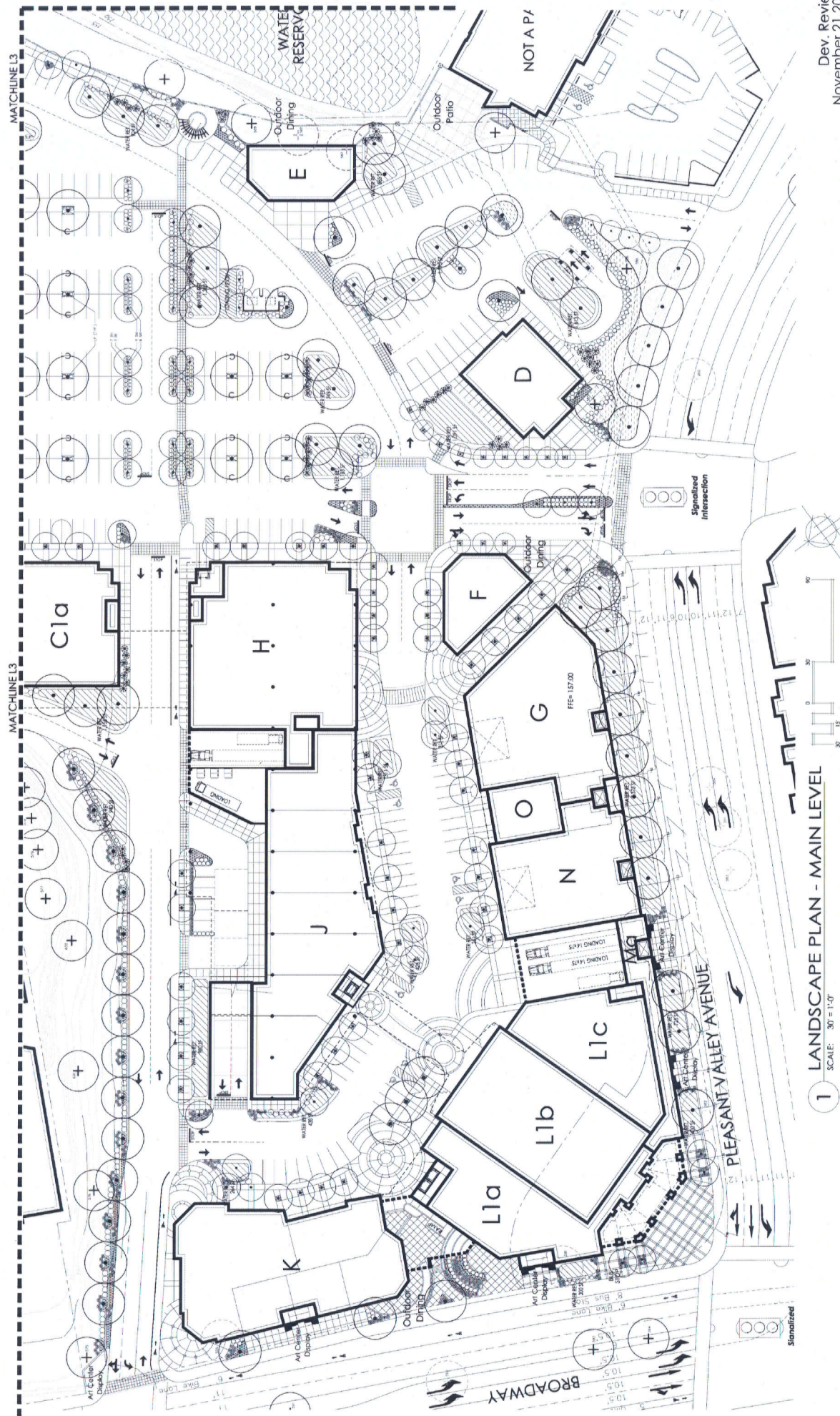
**Property
Development
Centers**

5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

CHRISTOPHER FRESHLEY
LANDSCAPE ARCHITECT

**BENNER
STANGE
ASSOCIATES**
ARCHITECTS, INC.
5200 S.W. MEADOWS RD.
SUITE B-130
LAKE OSWEGO, OR 97035
(503) 670-0234
FAX (503) 670-0235
bsa@bsarch.com

LS1



1 LANDSCAPE PLAN - MAIN LEVEL
SCALE: 30' = 1"

Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

51ST & Broadway
Store No. 3132
Oakland, California

**CHRISTOPHER FRESHLEY
LANDSCAPE ARCHITECT**
1000 14TH STREET, SUITE 200, OAKLAND, CA 94612
PHONE: (510) 763-1100
FAX: (510) 763-1101
WWW.CFARCHITECT.COM

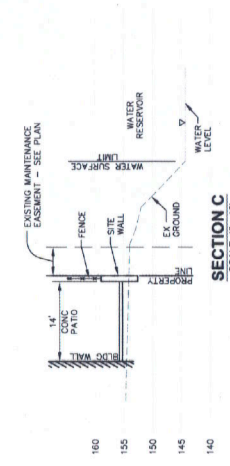
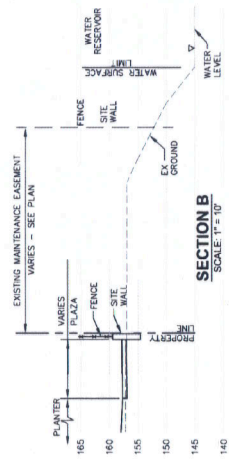
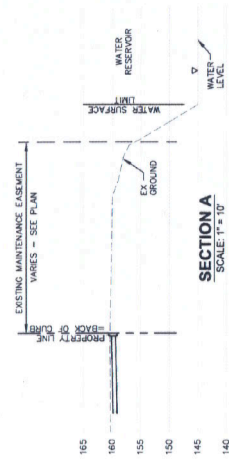


BENNER STANER ASSOCIATES ARCHITECTS INC.
5200 S.W. MEADOWS RD., SUITE B, 130
LAKE OREGON, OREGON 97034
PHONE: (503) 670-0234
FAX: (503) 670-0235
WWW.BSAA-ARCH.COM



LS 2

Dev. Review
November 21, 2012



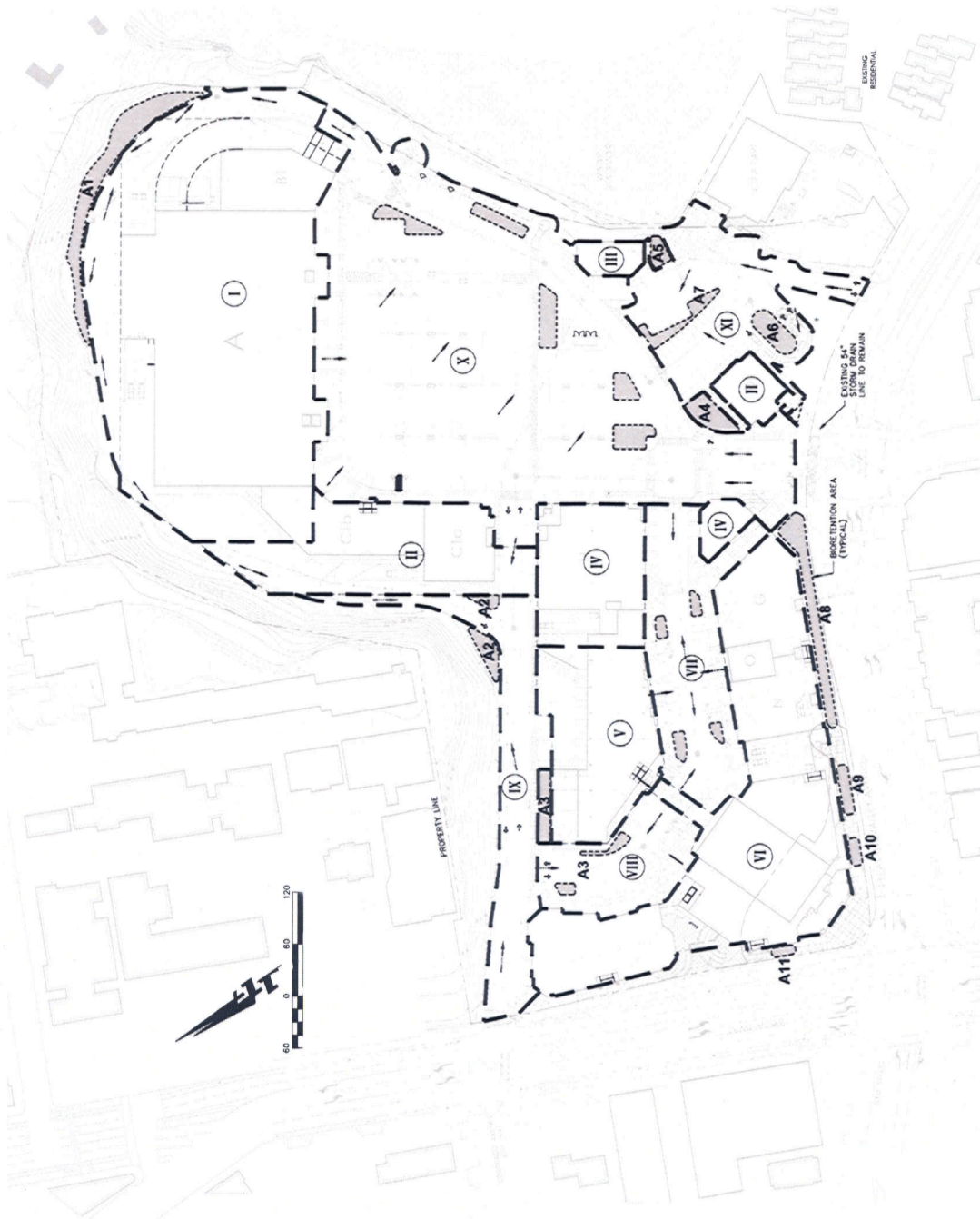
Dev. Review
November 21, 2012

BKF
C1.0
4670 Willow Road, Suite 250
Pleasanton, CA 94566
(925) 396-7700
(925) 396-7799 (Fax)

PRELIMINARY GRADING PLAN

51ST & Broadway
Store No. 3132
Oakland, California

Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

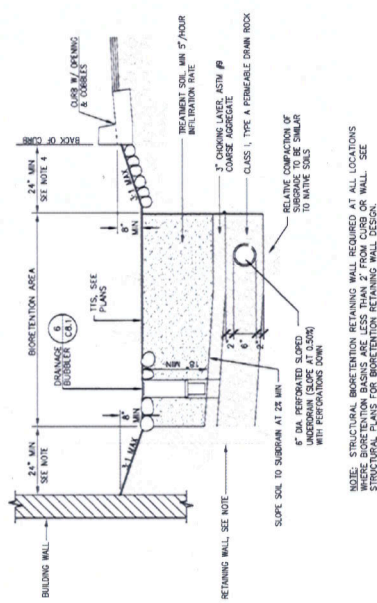


LEGEND:
 BIORETENTION AREA

NOTES:

- ON-SITE STORM DRAIN INLETS SHALL BE CLEARLY MARKED WITH THE WORDS "NO DUMPING! FLOWS TO BAY" OR EQUIVALENT, USING METHODS APPROVED BY THE CITY OF OAKLAND.
- BIORETENTION AREA IS SIZED TO TREAT 4% OF DRAINAGE AREA.

ON-SITE STORMWATER TREATMENT DATA				
DRAINAGE AREA TO BE TREATED			BMP EFFECTIVE TREATMENT AREA	
Drainage Area Designation	Description	Total Area (sf)	Required (sf)	BMP Treatment Method
I	Strip 1 & 2 Roadway Deck & Drive Aisle	125,000	5,000	At
II	Strip 3 & 4 Roadway Deck	125,000	5,000	At
III	Strip 5 & 6 Roadway Deck	125,000	5,000	At
IV	Strip 7 & 8 Roadway Deck	125,000	5,000	At
V	Strip 9 & 10 Roadway Deck	125,000	5,000	At
VI	Strip 11 & 12 Roadway Deck	125,000	5,000	At
VII	Strip 13 & 14 Roadway Deck	125,000	5,000	At
VIII	Strip 15 & 16 Roadway Deck	125,000	5,000	At
IX	Strip 17 & 18 Roadway Deck	125,000	5,000	At
X	Strip 19 & 20 Roadway Deck	125,000	5,000	At
1	Parking Lot	16,100	644	Filtering Media
2	Parking Lot	22,776	1,001	At
3	Parking Lot	22,776	1,001	At
4	Parking Lot	22,776	1,001	At
5	Parking Lot	22,776	1,001	At
6	Parking Lot	22,776	1,001	At
7	Parking Lot	22,776	1,001	At
8	Parking Lot	22,776	1,001	At
9	Parking Lot	22,776	1,001	At
10	Parking Lot	22,776	1,001	At
11	Parking Lot	22,776	1,001	At
12	Parking Lot	22,776	1,001	At
13	Parking Lot	22,776	1,001	At
14	Parking Lot	22,776	1,001	At
15	Parking Lot	22,776	1,001	At
16	Parking Lot	22,776	1,001	At
17	Parking Lot	22,776	1,001	At
18	Parking Lot	22,776	1,001	At
19	Parking Lot	22,776	1,001	At
20	Parking Lot	22,776	1,001	At
21	Parking Lot	22,776	1,001	At
22	Parking Lot	22,776	1,001	At
23	Parking Lot	22,776	1,001	At
24	Parking Lot	22,776	1,001	At
25	Parking Lot	22,776	1,001	At
26	Parking Lot	22,776	1,001	At
27	Parking Lot	22,776	1,001	At
28	Parking Lot	22,776	1,001	At
29	Parking Lot	22,776	1,001	At
30	Parking Lot	22,776	1,001	At
31	Parking Lot	22,776	1,001	At
32	Parking Lot	22,776	1,001	At
33	Parking Lot	22,776	1,001	At
34	Parking Lot	22,776	1,001	At
35	Parking Lot	22,776	1,001	At
36	Parking Lot	22,776	1,001	At
37	Parking Lot	22,776	1,001	At
38	Parking Lot	22,776	1,001	At
39	Parking Lot	22,776	1,001	At
40	Parking Lot	22,776	1,001	At
41	Parking Lot	22,776	1,001	At
42	Parking Lot	22,776	1,001	At
43	Parking Lot	22,776	1,001	At
44	Parking Lot	22,776	1,001	At
45	Parking Lot	22,776	1,001	At
46	Parking Lot	22,776	1,001	At
47	Parking Lot	22,776	1,001	At
48	Parking Lot	22,776	1,001	At
49	Parking Lot	22,776	1,001	At
50	Parking Lot	22,776	1,001	At
51	Parking Lot	22,776	1,001	At
52	Parking Lot	22,776	1,001	At
53	Parking Lot	22,776	1,001	At
54	Parking Lot	22,776	1,001	At
55	Parking Lot	22,776	1,001	At
56	Parking Lot	22,776	1,001	At
57	Parking Lot	22,776	1,001	At
58	Parking Lot	22,776	1,001	At
59	Parking Lot	22,776	1,001	At
60	Parking Lot	22,776	1,001	At
61	Parking Lot	22,776	1,001	At
62	Parking Lot	22,776	1,001	At
63	Parking Lot	22,776	1,001	At
64	Parking Lot	22,776	1,001	At
65	Parking Lot	22,776	1,001	At
66	Parking Lot	22,776	1,001	At
67	Parking Lot	22,776	1,001	At
68	Parking Lot	22,776	1,001	At
69	Parking Lot	22,776	1,001	At
70	Parking Lot	22,776	1,001	At
71	Parking Lot	22,776	1,001	At
72	Parking Lot	22,776	1,001	At
73	Parking Lot	22,776	1,001	At
74	Parking Lot	22,776	1,001	At
75	Parking Lot	22,776	1,001	At
76	Parking Lot	22,776	1,001	At
77	Parking Lot	22,776	1,001	At
78	Parking Lot	22,776	1,001	At
79	Parking Lot	22,776	1,001	At
80	Parking Lot	22,776	1,001	At
81	Parking Lot	22,776	1,001	At
82	Parking Lot	22,776	1,001	At
83	Parking Lot	22,776	1,001	At
84	Parking Lot	22,776	1,001	At
85	Parking Lot	22,776	1,001	At
86	Parking Lot	22,776	1,001	At
87	Parking Lot	22,776	1,001	At
88	Parking Lot	22,776	1,001	At
89	Parking Lot	22,776	1,001	At
90	Parking Lot	22,776	1,001	At
91	Parking Lot	22,776	1,001	At
92	Parking Lot	22,776	1,001	At
93	Parking Lot	22,776	1,001	At
94	Parking Lot	22,776	1,001	At
95	Parking Lot	22,776	1,001	At
96	Parking Lot	22,776	1,001	At
97	Parking Lot	22,776	1,001	At
98	Parking Lot	22,776	1,001	At
99	Parking Lot	22,776	1,001	At
100	Parking Lot	22,776	1,001	At
TOTAL		28,887	24,971	



BIORETENTION AREA

PRELIMINARY POST-CONSTRUCTION STORMWATER MANAGEMENT PLAN

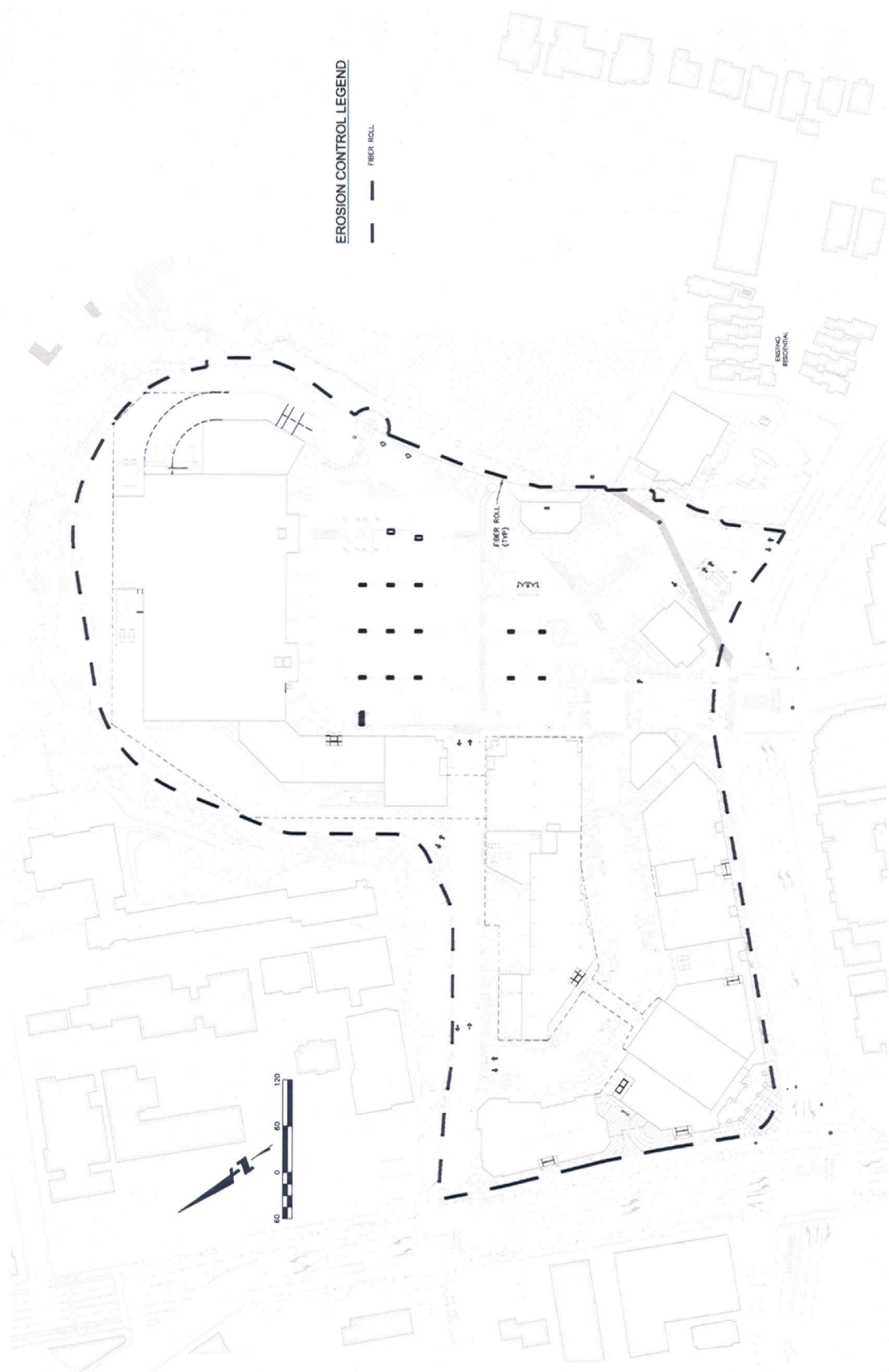
Dev. Review
November 21, 2012

BKF
 4670 Willow Road, Suite 250
 Pleasanton, CA 94566
 (925) 366-7700
 (925) 366-7799 (Fax)

Property Development Centers
 5918 Stoneridge Mall Road
 Pleasanton, CA 94588
 Phone: 925.738.1202
 Fax: 925.467.2861

51ST & Broadway
 Store No. 3132
 Oakland, California

JUNE 27, 2012
 C2.0



Dev. Review
November 21, 2012

BKF
C3.0

4670 Willow Road, Suite 250
Pleasanton, CA 94566
Phone: (925) 395-7700
(925) 395-7799 (Fax)

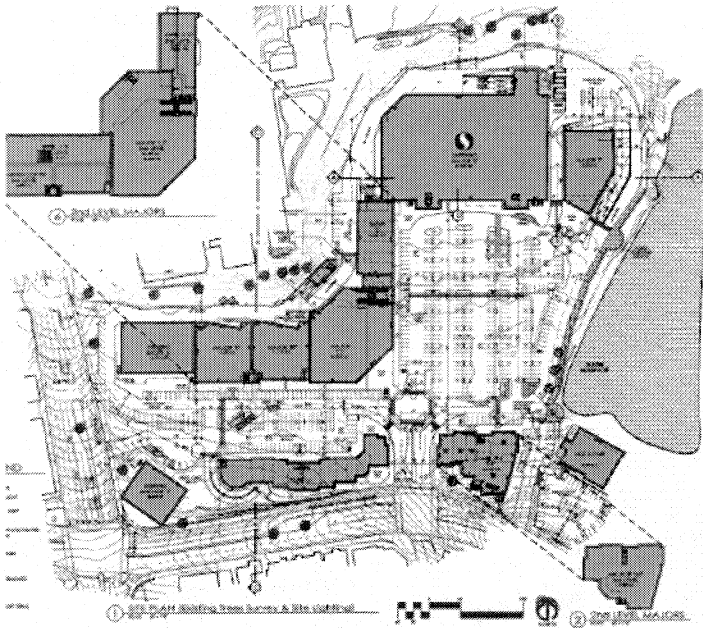
CREEK PROTECTION PLAN

51ST & Broadway
Store No. 3132
Oakland, California

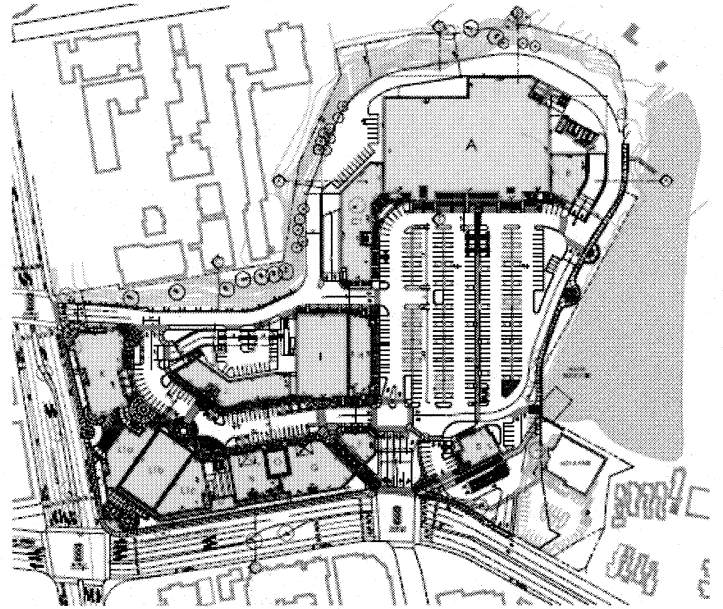
Property Development Centers
5918 Stoneridge Mall Road
Pleasanton, CA 94588
Phone: 925 738 1202
Fax: 925 467 2861

ATTACHMENT B

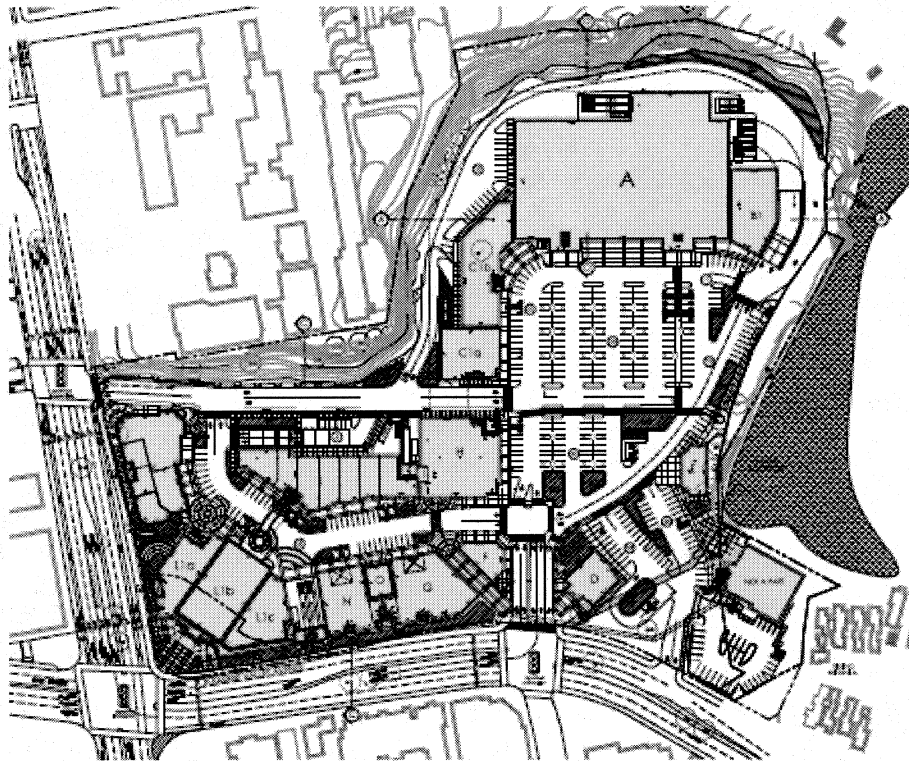
Project Site Plans
2009 Project vs. 2011 Project vs. Current Project



**2009 Original Project
Planning Commission**



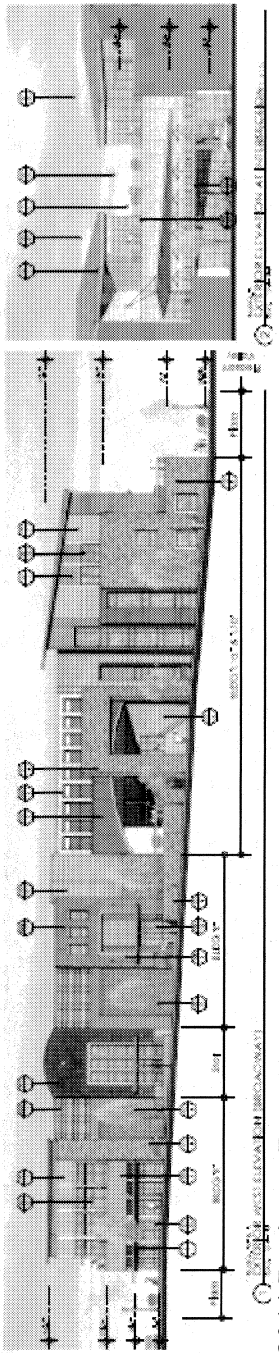
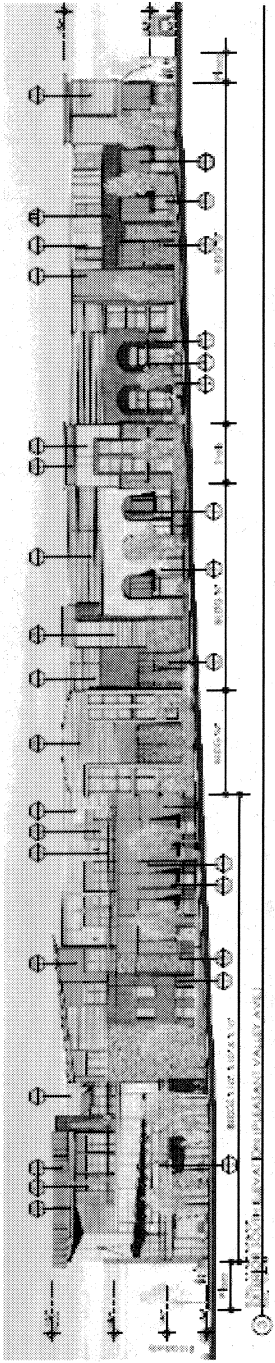
**2011 Project
Design Review Committee**



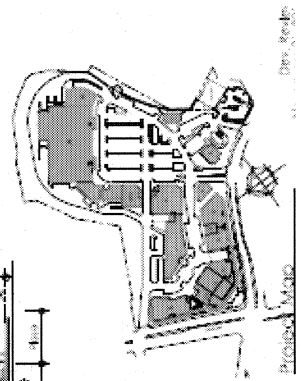
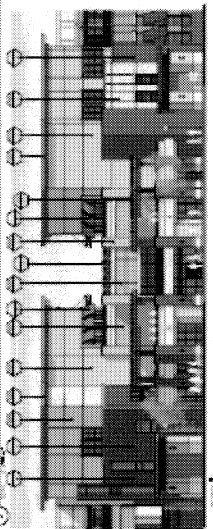
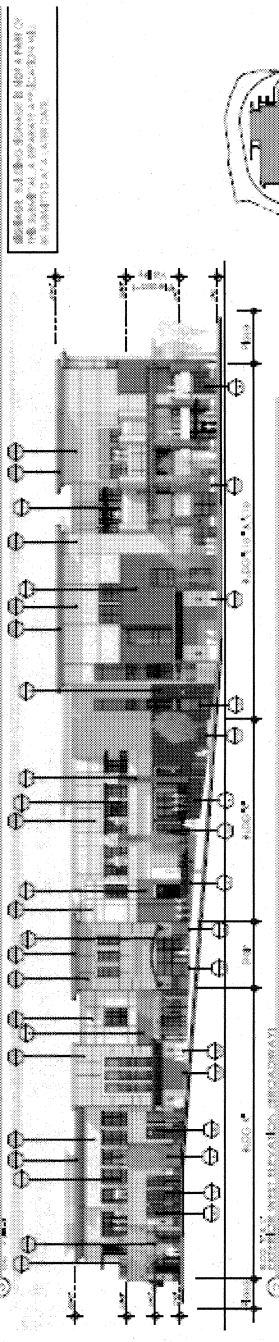
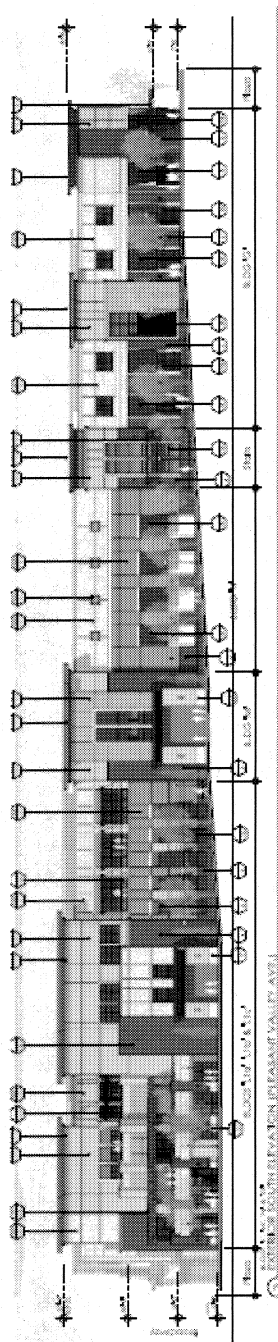
Current Project

ATTACHMENT C

Project Elevations 2011 Project vs. Current Project



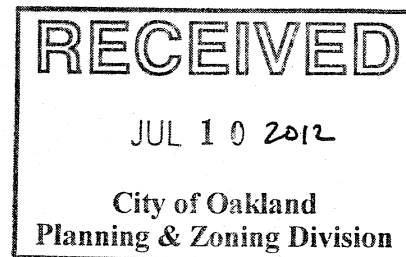
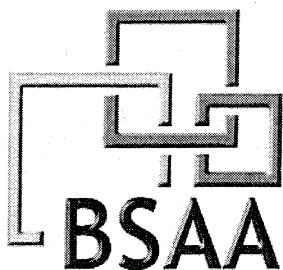
2011 Project – Broadway and Pleasant Valley Avenue Elevations



Current Project – Broadway and Pleasant Valley Avenue Elevations

ATTACHMENT D

Design Review Committee Comments and Applicant Responses



July 3, 2012

Mr. Darin Ranelletti, Planner III
City of Oakland, Planning and Zoning Division
250 Frank H. Ogawa Plaza, Suite 3315
Oakland, California 94612

**RE: Safeway Redevelopment Project (Broadway @ Pleasant Valley)
Design Review Committee Comments (January 26, 2011)
Needing Applicant Response**

Dear Darin:

We have responded to each of the Commissioners comments and including the updated Site Plan SP-84 Data and SP-84 Overall (attached separately), dated October 20, 2011, that corresponds and reflects the responses as stated:

Commissioner Colbruno

1. Consider traffic circle/roundabout shown on alternative design by community groups.
Consideration was made for the possibility of utilizing a traffic circle/roundabout as an alternative, but with the required diameter size for the roundabout would provide less queuing for the oncoming traffic from Pleasant Valley Avenue, than what is proposed presently.
2. Concerned about bicyclist and bus safety on busy streets.
The revised Site Plan SP-84 dated October 20, 2011, shows provisions of proposed northbound and southbound bike lanes along Broadway that would be part of the off-site improvements. A specified bike lane was added at the main site entry off Broadway, at the new proposed traffic signal, providing a protective zone for bicyclist. Addition of bike racks for short term and long term are provided throughout the site. Proposed transit bus stop, northbound on Broadway, joining the pedestrian main plaza entry will provide added safety with public activity.
3. Enhance relationship to adjacent pond; consider pond a project amenity; consider features such as a dog walk, pedestrian pathway, and/or café-restaurant adjacent to pond.

The proposed site plan, has taken this into consideration was complimented by an enhance pathway, trellised gathering benches facing the reflective reservoir and shear backdrop cliff beyond. Provisions included with barrier landscaping along the extended pedestrian pathway from Broadway, as same from pathways from Pleasant Valley Avenue. Proposed embellish protective barrier between pathway and reservoir cliff edge with bench like stone ledged base with open curved capped grate pickets. In addition, café'-restaurant is proposed along this feature edge, that includes the provision of outdoor dining experience.

Commissioner Gibbs

4. Address and better utilize pond.

As well received, please see response Item #3.

5. Concerned about "false front" on Pleasant Valley Ave; better utilize Pleasant Valley edge; consider using Pleasant Valley edge as terrace; make sure sound from project along Pleasant Valley does not leak out onto adjacent street.

Stated in Comment Item #11 – moving the delivery service core to the sub-grade, provides storefront frontage along the face of Pleasant Valley Avenue, provisions of tenant street store entry.

6. Consider providing shuttle service to BART.

Proposed provisions of locating bus stop off Broadway and the pedestrian connection to the bus stop and site have been provided. Further provisions, if necessary, to provide shuttle service to BART, if feasible, could be considered.

7. Incorporate on-site public art.

The proposed site plan incorporates provisions for Art Center display to encompass display window boxes within building structure frontages, strategically located along the street pedestrian walkway of Broadway and Pleasant Valley Avenue. In addition, on the northeastern corner of the main site entry on Broadway, adjacent to the Art Center, a proposed sculptural display patio. PD Centers has taken the next step in acquiring a professional designer with expertise in public interactive coordination within development.

Commissioner Zayas-Mart

8. Design doesn't fit community.

Broadway truly defines urbanizium of Oakland and as stated from previous responses to comments #11 and #12, reveals the means to continue the community feel of being urban. Pleasant Valley Avenue would accomplish the same; with a down scale envisions that leads to the existing establish suburban neighborhoods, today.

9. Architecture too chaotic.

The architecture has been streamlined; simplified to create identity to individual retail users. Trend today is to open the door for individualization from the retailers to place their preferences, through architect, expression, character and individuality, as historically in the urban settings of downtown. Governed by parties to respect the neighbors be means of design guidelines and creating competitiveness that enhances quality at/during that given time period.

10. Better civic sensibility needed.

Listening to citizens of their community, including City of Oakland's Design Commission, as this project neighbors, addressing concerns, envisions and desires, deserves respect to support the success of this proposed project. As for all proposed developments, there will be limits, compromise and understanding to reach an agreeable viable reality to support the development to proceed.

The site plan provides the sensibility by addressing the needs of pedestrian and bicyclist, with accommodations that would include the enhancement and experience the given water feature. The majority of vehicular required parking shows to being screened and segregated through use of parking structures. Knowingly, that the urbanizing equates to density of which requires a formulated requirement for vehicular parking. Provisions of community art display centers to support the art community. The community requesting a landscape nursery within the center, as the original Long's Drugstore provided, where PD Center is presently coordinating with a tenant that will fulfill this request, as reflected on the latest site plans.

11. Project too internally focused.

The present site plan reflects having tenants facing their storefront entry along Broadway, enhanced pedestrian entry from public bus stop transit system. The provisions of outdoor dining will enable interaction of public activity.

The initial site plan provided a delivery service core to service Building 'N' and 'G' along the street face off Pleasant Valley Avenue, blocking potential storefront provisions. In relocating the delivery service core to the hidden street level (sub-grade) along Pleasant Valley Avenue, will provide the major retailers of having storefront facing the street.

12. "False front" along Pleasant Valley Ave. is problematic.

As stated in Comment Item #11 – moving the delivery service core to the sub-grade, provides storefront frontage along the face of Pleasant Valley Avenue, provisions of tenant storefront entry off of Pleasant Valley Avenue.

13. Loading dock along internal street is not pedestrian-friendly.

Accommodating the operation of individual tenant delivery service requirements was achieved by utilizing a central loading dock service center for deliveries through multi-tenant means. This will require coordination from both tenants and the property management, including a program restricting deliveries during business hours. Enhancing the plaza walkway and closure to the delivery dock areas will screen majority of any existence to having such operation. The enclosed delivery center with also enable to control not just visual, but acoustically and emission ventilation.

14. Internal street is too wide and too meandering.

The internal street width dimension, from building to building, ranges on the average, from 90 feet to 101 feet. Similar to Downtown Broadway urban setting street right-of-way is approximately 100 feet in width. The intent of the width is to provide pedestrian plazas and provide natural sunlight, as possible, where shade comes into play with storefronts that are on the north facing of the internal street.

15. Pedestrian access cut from Broadway into project leads to parking garage entry – problematic.

Pedestrian access as shown from Broadway was taken into consideration of conforming terraced grades and extensive storefront frontages and plazas as pedestrian will experience when approaching the center. The parking garage entry plays just as an important role, by providing the vehicular user a direct access to the center core, as a welcoming visual experience when entering as a pedestrian.

16. Celebrate pond.

As well received, please see response Comment Item #3.

17. Internal street leads to surface parking – problematic.

Perhaps that may be the case, but from a smaller scale, as majority of the parking is provided through utilization of parking structures.

18. Traffic on Pleasant Valley Ave. needs to be slowed down / calmed.

The proposed traffic signal on Broadway, will give provisions of another main vehicular entry to this site, which was non-existing at its present condition. Intended to reduce the traffic demand off from Pleasant Valley Avenue will hopefully decrease the traffic demand, in return giving a sense of calmness.

19. Improve interaction with Pleasant Valley Ave.; take advantage of grade change to provide entrances onto Pleasant Valley on ground floor and entrances to internal project on upper floor; add on-street parking on Pleasant Valley without removing traffic lanes.

As stated in Item #11 – moving the delivery service core to the sub-grade, provides storefront frontage along the face of Pleasant Valley Avenue, provisions of tenant storefront entry off Pleasant Valley. The addition of on-street parking on Pleasant Valley without removing traffic lanes would be problematic via right-of-way dedication.

20. Proposed Chase bank building should not be positioned at an angle to the street.

The proposed Chase Bank building is situated against the existing overflow storm drain easement that forms an angle from the reservoir to tie-in within Pleasant Valley Avenue at the main vehicular entry, for the reservoir. The latest Site Plans represents in moving Chase Bank forward towards the main entry drive, creating a pedestrian plaza entry for Chase Bank. Positioning provides the screening of vehicular parking away from Pleasant Valley, including the Drive-Thru Banking sitting below street level, screened by grades and existing matured trees. The angle position seems to compliment the adjoining angle building faces and enhance the window to the center.

Upon your review, if you require further information or clarification, please do not hesitate in contacting us.

Sincerely,

L. Owen Chrisman

Benner Stange Associates, Architects, Inc.

ATTACHMENT E

Executive Summary from Draft EIR (Chapter 2)

Executive Summary

Project Overview

Property Development Centers, Inc. (an affiliate of Safeway, Inc.), proposes to redevelop the existing Rockridge Shopping Center, including the demolition of all 185,500 square feet of existing buildings on the site. Removed buildings would be replaced with construction of a new “Lifestyle”¹ Safeway store along with other retail, office and restaurant space, resulting in a total of approximately 322,500 square feet of new commercial building space (293,200 square feet of gross leasable floor area and an additional 29,300 square feet of common space). This represents an increase of approximately 137,000 square feet over existing development on the site. The applicant also proposes modifications to the adjacent streets and public rights-of-way to improve access and circulation for all travel modes and to provide new signalized left-turn access onto Broadway.

In early 2009, Property Development Centers, Inc. submitted an application to the City of Oakland for environmental review of the Project. On June 25, 2009 the City of Oakland issued a Notice of Preparation, determining that a project-level EIR would be the appropriate document to analyze the potential environmental effects of the proposed Project under CEQA. This EIR addresses environmental topics pertaining to Aesthetics; Air Quality; Biological Resources; Cultural Resources; Geology and Soils; Greenhouse Gas Emissions; Hazards and Hazardous Materials; Hydrology and Water Quality; Land Use, Plans and Policies; Noise and Vibration; Transportation, Circulation and Parking; Utilities and Public Services; and other environmental effects found to be less than significant.

Site Location

The 15.4-acre Project site is located on the northeast corner of the intersection of Pleasant Valley Avenue and Broadway in the City of Oakland, Alameda County, California.

The Project site is currently designated on the General Plan Land Use and Transportation Element (LUTE) Diagram as Community Commercial. The Project is consistent with this Oakland General Plan land use designation.

The effective zoning designation of the Project site is split into three different zoning districts.² The southwestern corner of the site, roughly equal to the location of the Chase Bank building, has an effective zoning of C-40 Community Thoroughfare Commercial. The central portion of the site has an effective

¹ The “Lifestyle” Safeway store is part of a corporate branding campaign intended to differentiate these stores from competitors as a more upscale shopping experience. Generally, these types of stores are designed with a more inviting decor with warm ambiance and subdued lighting, and containing special elements such as sushi and olive bars and the addition of in-store coffee kiosks. Many Safeway store locations are being converted to the “Lifestyle” format.

² The applicable zoning for the Project is the zoning that was in effect at the time the Project application was deemed complete in 2010.

zoning of C-30 District Thoroughfare Commercial. The eastern portion of the site has an effective zoning of R-50 Medium Density Residential.

Key Components of the Project

The Project would be constructed in two phases over approximately 20 months. Construction is anticipated to begin in July 2013 and end in March 2015. Project phasing is intended to enable the shopping center to remain operational and economically viable throughout the construction period, to capitalize on the current opportunity to move the Safeway grocery store into the current CVS Pharmacy site soon after the current CVS lease expires, and to match future phase development to meet both current and expected future retail market demands.

Buildings

At completion, the Project would include demolition of the entire 185,500 square feet of the existing 1 story shopping center. The shopping center would be replaced by an approximately 322,500 square-foot³ new shopping center anchored by an approximately 65,000 square-foot new Safeway store. The new buildings would range in height from 1 to 4 stories. The new Safeway would be a single story building, but with high ceilings it would appear to be 2 stories in height.

Parking

The Project proposes a total of 967 off-street parking spaces, including 851 standard spaces, 30 designated handicap spaces and 86 designated compact spaces. Parking would be located in surface parking lots and along drive aisles throughout the site, on a rooftop parking lot over the Safeway store and adjacent buildings, and in a centralized parking garage with three levels of parking over ground floor retail space.

Pedestrian and Bicycle Access

The Project proposes a substantially expanded pedestrian and bicycle network for the site, including:

- A continuous sidewalk that connects with small plazas ringed the entire site, separated only at the two vehicle entry points,
- Separated pedestrian and vehicle access provided at each of the entry points into the site, as well as a new pedestrian connection on Broadway near the Pleasant Valley Avenue/Broadway intersection,
- A number of routes leading pedestrians to the new Safeway store from Pleasant Valley Avenue, and
- Two routes that would lead pedestrians into the site from the Broadway/Coronado Avenue intersection.

The pedestrian and bicycle routes would interconnect a number of plazas. The two main plazas would be located along Broadway at the Pleasant Valley Avenue intersection and just north of the intersection, connecting through the buildings at this location. The internal street would also have a number of smaller plazas and wider sidewalks for outdoor cafes and public seating. The landscaped edge near the quarry pond would have two smaller plazas which serve as scenic outlooks over the pond.

³ Of the total 322,536 square feet within the proposed new shopping center, 293,233 square feet would be gross leasable floor area and approximately 29,303 square feet would be common space (e.g., stairs and loading corridors)

Vehicle Access

The current shopping center has three vehicle access points along Broadway. Under the proposed Project, the two most southerly vehicle access points would be eliminated, and the intersection at Coronado Avenue would be converted to a signalized intersection providing full turning movements with 1 inbound and 2 outbound lanes. The current shopping center also has two vehicle access points along Pleasant Valley Avenue. These access points would remain where they currently exist, but the main entry would be realigned and re-stripped to provide 3 inbound lanes and 2 outbound lanes.

Off-Site Roadway Modifications

The Project also proposes a number of roadway modifications on Broadway and 51st Street/Pleasant Valley Avenue to generally improve access and circulation for all travel modes and to specifically provide signalized left-turn access on Broadway to and from the Project site. Off-site roadway modifications proposed as part of the Project include the following.

- Broadway would be reduced from three through lanes to two through lanes in each direction between College Avenue and 49th Street.
- Class 2 bicycle lanes would be provided on both sides of Broadway between College Avenue and just south of 51st Street/Pleasant Valley Avenue.
- The Project driveway on Broadway opposite Coronado Avenue would be signalized to provide left turns in and out of the Project site. The proposed signal would be coordinated with the existing signals on Broadway at 45th Street, 51st Street/Pleasant Valley Avenue, College Avenue, and Broadway Terrace. The intersection would provide an exclusive left-turn lane from southbound Broadway to the Project site. The proposed signal would also provide a protected pedestrian crossing connecting the residential neighborhood west of Broadway to the Project site.
- The provision for the southbound left-turn lane from Broadway into the Project site would require the elimination of the existing median break that provides access to Wendy's Restaurant from northbound Broadway. As such, the northbound left-turn lane on Broadway at College Avenue would be modified to provide left-turn access into the existing Wendy's Restaurant on the opposite side of Broadway from the Project site.
- The Broadway/51st Street/Pleasant Valley Avenue intersection would be modified to increase vehicle capacity, to provide a six-foot wide median pedestrian refuge island, and to provide more efficient and safer signal operations.
- The Gilbert Street/Project Driveway/Pleasant Valley Avenue intersection would also be modified to provide additional turn lanes and the intersection signal equipment would be upgraded to provide protected phasing for the westbound Pleasant Valley Avenue left-turn movement.
- The locations of several bus stops would be moved from the near side to the far side of (i.e., from before to after) the intersection at northbound Broadway and Pleasant Valley Avenue, at eastbound 51st Street/Pleasant Valley Avenue at Broadway, and at eastbound Pleasant Valley Avenue at Gilbert Street.

The proposed modifications along Broadway can be accommodated within the existing curb-to-curb right-of-way. Providing a second left-turn lane from eastbound Pleasant Valley Avenue into the Project site would require widening Pleasant Valley Avenue by an additional 1 to 4 feet along the Project frontage.

Public Agency Approvals

This EIR is intended to cover all approvals necessary to implement the Project. These approvals include, but are not limited to the following.

City of Oakland

- Approval of an Interim Conditional Use Permit to allow for commercial use in the R-50 Medium Density Residential Zone pursuant to Chapter 17.01 of the Oakland Planning Code;
- Design Review pursuant to Chapter 17.136 of the Oakland Planning Code;
- Zoning variances (if required);
- Approval of a Category IV Creek Protection Permit for exterior development and work that may include earthwork, landscape walls, fences, patios, decks, private drainage improvements, irrigation systems and trenching conducted within the 20 foot setback from the top of bank of the adjacent watercourse (the quarry pond) pursuant to Chapter 13.16 of the Oakland Municipal Code;
- Approval of a Conditional Use Permit (for any drive-through facilities or alcohol sales);
- Approval of a Subdivision Map (or lot line adjustment);
- Tree removal permits pursuant to the City's Protected Trees Ordinance (Chapter 12.36 of the Oakland Municipal Code);
- Encroachment permits for work within and close to public rights-of-way (Chapter 12.08 of the Oakland Municipal Code); and
- Demolition permits, grading permits, and building permits.

Other Agencies Whose Approval May be Required

- Bay Area Air Quality Management District (BAAQMD) – Granting of permits for stationary source air emissions and compliance with Regulation 2, Rule 1 for all portable construction equipment subject to that rule.
- East Bay Municipal Utilities District (EBMUD) – Granting new water service connections and meters.
- State Water Resources Control Board (SWRCB) – Acceptance of Notice of Intent to obtain coverage under the General Construction Activity Storm Water Permit.
- San Francisco Bay Regional Water Quality Control Board (RWQCB) – Water quality certification under Section 401 of the Clean Water Act may be necessary for landscaping adjacent to the quarry pond.
- California Department of Fish and Game (CDFG) – A Streambed Alteration Agreement pursuant to California Fish and Game Code Sections 1600–1616 may be necessary for landscaping adjacent to the quarry pond.

Summary of Impacts and Mitigation Measures

The following **Table 2-1: Summary of Impacts and Mitigation Measures** provides a summary of potential environmental impacts, applicable Standard Conditions of Approval, recommended mitigation measures, and the resulting level of significance after implementation of all mitigation measures. For a more complete discussion of potential impacts and recommended mitigation measures, please refer to the specific discussions in the respective individual chapters of this Draft EIR.

Significant and Unavoidable Impacts

For purposes of this EIR, the following impacts are considered significant and unavoidable. Although mitigation measures consisting of physical modifications to intersection operations have been identified, such modifications would adversely affect other travel modes and conflict with City policy concerning pedestrian and bicyclist safety and comfort, therefore resulting in secondary impacts. Traffic operations at these intersections could be further improved by providing additional automobile travel lanes. However, such modifications cannot be accommodated within the existing automobile right-of-way and would require additional right-of-way and/or loss of bicycle lanes, on-street parking, or medians, and are therefore considered to be infeasible.

Broadway/51st Street/Pleasant Valley Avenue (Intersection #7)

- **Impact Trans-5:** The Project would degrade intersection operations from LOS D to LOS E during the weekday PM peak hour at the Broadway/51st Street/Pleasant Valley Avenue (#7) intersection under 2015 Conditions. The proposed Project would also add traffic that would increase delay for the critical eastbound through movement by more than six seconds during the Saturday midday peak hour, which the intersection would operate at LOS E regardless of the proposed Project.
- **Impact Trans-10:** The Project would increase the volume-to-capacity (v/c) ratio for the intersection by 0.01 or more, and the critical movement v/c ratio for the eastbound left, eastbound through, westbound left, northbound through, and the southbound left movements by 0.02 or more during the weekday PM peak hour, and it would increase v/c ratio for the intersection by 0.01 or more and the critical movement v/c ratio for the eastbound left, eastbound through, and, northbound through movements by 0.02 or more during the Saturday midday peak hour at the Broadway/51st Street/Pleasant Valley Avenue (#7) intersection under 2035 Conditions, which would operate at LOS F regardless of the Project.

Howe Street/Pleasant Valley Avenue Intersection (Intersection #19)

- **Impact Trans-3, -8 and -13:** The proposed Project would add more than 10 trips to the Howe Street/Pleasant Valley Avenue intersection during the weekday PM and Saturday midday peak hours under Existing plus Project conditions, 2015 Plus Project conditions, and 2035 Plus Project conditions. The intersection would meet the peak hour signal warrant during both time periods.

Piedmont Avenue/Pleasant Valley Avenue (Intersection #20)

- **Impact Trans-14:** The Project would increase the volume-to-capacity (v/c) ratio for the intersection by 0.01 or more, and the critical movement v/c ratio for the eastbound, westbound, and northbound movements by 0.02 or more during the weekday PM, Saturday midday, and Saturday PM peak hours at the Piedmont Avenue/Pleasant Valley Avenue (#20) intersection under 2035 Conditions, which would operate at LOS F regardless of the Project.

Recommended Conditions of Approval

Although not required by CEQA, certain “recommendations” are included in this EIR, and summarized in **Table 2-2**. These recommendations are not necessary to address or mitigate any significant environmental impacts of the Project under CEQA, but are recommended by City staff to address effects of the Project. These recommendations will be considered by decision makers during the course of Project review and may be imposed as Project-Specific Conditions of Approval.

It is not yet known which of these recommendations may be implemented and if so whether it would be as part of the Project or independent of the Project. The environmental consequences of each

recommendation have been considered and none of the recommendations would result in any significant impacts under CEQA.

Alternatives

Chapter 5 presents an analysis of a range of reasonable alternatives to the Project. The following alternatives were analyzed:

- Alternative 1: No Project
- Alternative 2: Safeway Relocation
- Alternative 3: Reduced Project
- Alternative 4: Concept with Commercial Emphasis (RCPC Plan)
- Alternative 5: Concept with Residential Emphasis (ULTRA Plan)

As required by the CEQA Guidelines, an alternative site location was considered but eliminated from further evaluation in this EIR because it would not meet the basic project objectives and would likely result in similar traffic impacts at intersections in the vicinity of any alternative site.

Alternative 1: No Project would be the environmentally superior alternative. Alternative 5: Concept with Residential Emphasis (ULTRA Plan) would be considered environmentally superior in the absence of the No Project alternative. This alternative would generate fewer vehicle trips as compared to all other alternatives (other than “no project” alternatives) as evaluated in this EIR. However, Alternative 5 would also not achieve many of the basic Project objectives.

Areas of Public Concern

The following topics were raised in comments received in response to the June 25, 2009 Notice of Preparation (NOP) of this EIR and at the July 15, 2009 EIR scoping session held before the City’s Planning Commission. Each of these topics is addressed in this EIR. Issues of concern (including some non-CEQA issues) include, but are not limited to, the following:

- Aesthetics
 - Overall visual character of site
 - Street frontage character on Broadway and Pleasant Valley Avenue
 - Auto-centric nature of proposed site layout
 - Opportunity for enhancement of quarry pond as site and community amenity
 - Blight and urban decay
- Air Quality
 - Construction period dust
 - Human health risks
- Biological Resources
 - Wildlife habitat in quarry pond
- Geology and Soils
 - Stability of slope at rear of site
- Greenhouse Gas Emissions

- Hydrology and Water Quality
 - Water quality of quarry pond
- Land Use, Plans and Policies
 - Proposed development density, mix of uses and site layout may not be sufficiently urban in character, integrated with surrounding neighborhoods, or supportive of alternative modes of travel
 - Socioeconomic impacts
- Transportation, Circulation and Parking
 - Auto-centric nature of proposed site design
 - Need for safer and more convenient pedestrian and bicycle access
 - Adequacy and appropriateness of parking supply
 - Local and regional traffic congestion
- Utilities and Public Services
 - Demand on public services
- Alternatives
 - Community amenities
 - Mixed-use development
 - Housing
 - Continued street grid

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
Aesthetics		
Impact Aesth-1: Views from the Project site have not been identified as scenic vistas or important visual resources in the Oakland General Plan or by a regulatory agency with jurisdiction over the site. As a result, development of the Project would not significantly alter scenic vistas.	None needed	No impact
Impact Aesth-2: No scenic resources have been formally identified at the Project site, and development of the Project would have no adverse effects on any formally-identified scenic resources.	None needed SCA Aesth-2: Tree Removal Permit and SCA Aesth-3: Tree Replacement Plantings	Less than Significant
Impact Aesth-3: The visual character of the Project site and its surroundings would change as a result of the Project, but the general character of the site would remain as a commercial shopping center. The Project would not substantially degrade but rather would improve the existing visual character and quality of the site and its surroundings.	None needed	No Impact
Impact Aesth-4: Lighting at the site would be modified as part of the proposed Project, but stores and parking areas at the site would still be illuminated in a manner similar to what is currently observed at the site.	None needed SCA Aesth-1: <i>Lighting Plan</i>	Less than Significant
Impact Aesth-5: No structures or landscape improvement proposed by the Project would at any time create substantial shadows beyond the Project site and thus would not interfere with any off-site solar collectors or generate shadows that would fall on any public space.	None required	No Impact
Impact Aesth-6: Structures proposed at the	None required	No Impact

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
Project site would not generate shadows that would fall on any historic resources.		
Impact Aesth-7: The Project would not fundamentally conflict with any policies or regulations of the General Plan, Planning Code or Uniform Building Code that address appropriate provisions of adequate light for various types of land uses.	None required	Less than Significant
Impact Aesth-8: Given the limited height of proposed structures at the Project site and the site's location, wind modeling is not necessary and there would be no wind-related impacts associated with the proposed Project.	None required	No Impact
Impact Aesth-9: Considering market conditions, retail sales leakage, existing regulatory controls that address blight, and diverted sales due to the Project, the Project would not cause business closures, long term vacancies and physical deterioration of properties. Therefore, the Project would not result in significant urban decay impacts.	None required	Less than Significant
Cumulative Impact Aesth-10: Implementation of the Project, combined with other past, present, existing, pending and reasonably foreseeable projects that would be visible in the vicinity of the Project site would not result in significant adverse changes to existing visual character, views, light and glare or shadow.	None required	Less than Significant
Air Quality		
Impact Air-1: During construction, the proposed Project would generate fugitive dust from demolition, grading, hauling and construction activities.	None required SCA Air-1: Construction-Related Air Pollution Controls (Dust and Equipment Emissions) SCA Air-2: Asbestos Removal in Structures	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
Impact Air-2: During construction, the proposed Project would generate regional ozone precursor emissions and regional particulate matter emissions from construction equipment exhaust. However, Project-related construction emissions would not generate emissions of criteria air pollutants that would exceed the City's thresholds of significance.	None required SCA Air-1: Construction-Related Air Pollution Controls	Less than Significant
Impact Air-3: The proposed Project's construction-related emissions would not result in the estimated cancer risk, chronic health index, acute health index or annual average $PM_{2.5}$ concentration levels exceeding the individual source significance threshold.	None required SCA Air-1: Construction-Related Air Pollution Controls	Less than Significant
Impact Air-4: Once complete and occupied, the proposed Project would generate emissions of criteria pollutants (ROG , NO_x and PM_{10}), primarily as a result of increased motor vehicle traffic and also from area source emissions. Project-related traffic emissions, combined with anticipated area source emissions, would not generate emissions of criteria air pollutants that would exceed the City's thresholds of significance.	None required SCA Trans-1: Parking and Traffic Management Plan	Less than Significant
Impact Air-5: The Project would include a back-up generator that would emit small amounts of toxic emissions.	None needed	Less than Significant
Impact Air-6: New vehicle trips associated with the proposed Project would add to carbon monoxide concentrations near streets that provide access to the Project site. The carbon monoxide emission levels associated with the Project's vehicle trips would not exceed the City's thresholds of significance.	None needed	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
Impact Air-7: The proposed Project would not frequently create substantial objectionable odors affecting a substantial number of people.	None needed	Less than Significant
Cumulative Impact Air-8: Since the Project would not result in a significant air quality impact, the Project would not result in a considerable contribution to a significant cumulative impact to air quality, and the cumulative impact would be considered less than significant.	None needed	Less than Significant
Cumulative Impact Air-9: The proposed Project's construction-related emissions and operation emissions would not lead to a cumulatively significant risk for cancer, chronic health, acute health or annual average $PM_{2.5}$ concentrations that would exceed the cumulative source significance thresholds.	None needed	Less than Significant
Biological Resources		
Impact Bio-1: Large trees and buildings within the Project site and its immediate vicinity provide potential nesting habitat for birds and roosting habitat for bats which could be disturbed during construction. The quarry pond adjacent to the Project site provides marginally suitable aquatic habitat for the western pond turtle and if present, pond turtles could be adversely affected by construction activities.	SCA Bio-1: Tree Removal During Breeding Season SCA Bio-1 Implementation: Roosting Bat Survey. A pre-construction survey for roosting bats should be performed by a qualified biologist within 30 days prior to any removal of trees or structures on the Project site. If no active roosts are found, then no further action would be warranted. If either a maternity roost or hibernacula (structures used by bats for hibernation) is present, the following minimization measures shall be implemented: a) If active maternity roosts or hibernacula are found in trees or structures which will be removed as part of Project construction, the Project should be redesigned to avoid the loss of the tree or structure occupied by the roost to the extent feasible. If an active maternity roost is located and the Project cannot be redesigned to avoid removal of the occupied tree or structure, demolition can commence before maternity colonies form (i.e., prior to March 1) or after young are volant (flying) (i.e., after July 31). Disturbance-free buffer zones as determined by a qualified biologist in coordination with CDFG shall be observed during the maternity roost season (March 1 through July 31). b) If a non-breeding bat hibernacula is found in a tree or structure scheduled for removal, the	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
	individuals should be safely evicted, under the direction of a qualified biologist (as determined by a memorandum of understanding [MOU] with CDFG), by opening the roosting area to allow air flow through the cavity. Demolition can then follow at least one night after initial disturbance for airflow. This action should allow bats to leave during darkness, thus increasing their chance of finding new roosts with a minimum of potential predation during daylight. Trees or structures with roosts that need to be removed will first be disturbed at dusk, just prior to removal that same evening, to allow bats to escape during the darker hours. Mitigation Measure Bio-1b: Western Pond Turtle Surveys: A western pond turtle survey should be conducted by a qualified biologist within two weeks prior to any disturbance or removal of upland vegetation around the quarry pond. If a turtle is found, it should be relocated out of harm's way in coordination with CDFG. a) If any turtles are encountered within the construction zone during construction, all work shall halt until the qualified biologist has determined whether it is a western pond turtle or some other species. If it is not a western pond turtle, work may continue. b) If a western pond turtle is found, the CDFG shall be notified regarding the presence of the western pond turtle and all work shall stop until additional exclusion measures have been defined and authorization to proceed is obtained from the CDFG. No person shall handle or otherwise harass any individual western pond turtle encountered during construction, with the exception of handling by the qualified biologist. A plan shall be developed in consultation with the CDFG to relocate the western pond turtle individuals to the nearest protected habitat outside the construction zone and to provide necessary on-site construction avoidance measures to prevent inadvertent take of this species. Mitigation Measure Bio-1c: Contractor Awareness. Contractor education should be conducted to make workers aware of measures being taken to protect resources on the site and to contribute to increased vigilance during their work. Before initiation of construction activities within close proximity to the quarry pond, all construction workers shall be trained by the qualified biologist regarding the potential presence of western pond turtle and the fact that this species is to be avoided, and if any turtles are seen, the job foreman must be notified and construction shall be halted until appropriate measures have been taken.	
Impact Bio-2: No wetlands or sensitive natural communities are present at the Project site such that they would be disturbed by Project construction or operation. However, landscape improvements at the edge of the Project site have the potential to adversely affect off-site wetland, riparian and sensitive natural communities.	None required SCA Bio-2: Creek Protection Plan SCA Bio-3: Regulatory Permits and Authorizations SCA Bio-4: Creek Monitoring SCA Bio-5: Creek Landscaping Plan	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
Impact Bio-3: Redevelopment of the Project site as proposed would not adversely affect wildlife movement or nursery sites.	None required	No Impact
Impact Bio-5: Redevelopment of the Project site as proposed would result in removal of four (4) “protected trees” to accommodate new buildings, five (5) protected trees within roadway medians, and two (2) non-protected Monterey pines for improved access to the adjacent quarry pond.	None required SCA Aesth-2: Tree Removal Permit, SCA Aesth-3: Tree Replacement Plantings, and SCA Aesth-4: Tree Protection During Construction	Less than Significant
Impact Bio-6: Although the proposed Project would be subject to the provisions of the City of Oakland Creek Protection Ordinance, there is nothing about the Project that would fundamentally conflict with elements of the ordinance intended to protect biological resources. The Project would not discharge a substantial amount of pollutants into the creek or watercourse, it would not significantly modify the natural flow of water, it would not deposit substantial amounts of new material into a creek or cause substantial bank erosion or instability, nor would it adversely impact a riparian corridor by significantly altering vegetation or wildlife habitat.	None required SCA Bio-2: Creek Protection Plan, SCA Bio-3: Regulatory Permits and Authorizations SCA Bio-4: Creek Monitoring SCA Bio-5: Creek Landscaping Plan	Less than Significant
Cumulative Impact Bio-7: The Project would not result in a significant cumulative impact on biological resources.	None required	Less than Significant
Cultural Resources		
Impact Cultural-1: The Project would not directly result in a substantial adverse change in the significance of a historical resource as defined in CEQA Guidelines §15064.5.	None needed	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
Impact Cultural-2: The Project would not cause a substantial adverse change in the significance of a known archaeological resource, nor would it directly or indirectly destroy a known unique paleontological resource or site, or unique geologic feature. It is possible that currently unknown archaeological or paleontological resources could be damaged during site grading and construction.	None required SCA Cultural-1: Archaeological Resources SCA Cultural-2: Paleontological Resources SCA Cultural-3: <i>Human Remains</i> SCA Cultural-5: Archaeological Resources – Sensitive Areas	Less than Significant
Cumulative Impact Cultural-3: Implementation of the Project would not adversely affect historic or cultural resources, thus it would similarly not combine with other past, present, existing, pending and reasonably foreseeable projects that may have cultural resource impacts.	None required	No Impact
Geology and Soils		
Impact Geo-1: The Project site is located in an area that would be subject to very strong ground shaking and potential liquefaction in a major seismic event.	None required SCA Geo-2: Soils Report	Less than Significant
Impact Geo-2: The cut slope at the Project site's northerly boundary shows evidence of erosion and fallen debris, and could potentially be susceptible to slides.	None required SCA Geo-2: Soils Report SCA Implementation: Catchment Structures. Pursuant to recommendations from the 2007 Kleinfelder Geotechnical Investigation, the Project applicant shall reconstruct the on-site catchment structures at the toe of the cut slope along the northerly site boundary. Detailed catchment structure designs shall be included in the required soils report.	Less than Significant
Impact Geo-3: Portions of the easterly side of the Project site near the quarry pond contain clayey soil with variable gravel content, potentially unsuitable as a sub-grade soil for building foundations.	None required SCA Geo-2: Soils Report SCA Implementation: Excavation of Unsuitable Soils. Pursuant to recommendations from the 2007 Kleinfelder Geotechnical Investigation, in the event that unsuitable soil is encountered during the construction phase, such soils should be excavated to a firm bottom and the resulting hole should	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
	be backfilled with engineered fill or lean mix concrete.	
Impact Geo-4: Site preparation and construction activity associated with the Project could result in soil erosion as the surface is disrupted.	None required SCA Geo-2: Soils Report	Less than Significant
Impact Geo-5: Soils samples taken at the Project site indicate that near-surface soils are considered to have a low potential for expansion.	None required SCA Geo-2: Soils Report	Less than Significant
Impact Geo-6: The Project site has been previously developed and there are no known wells, pits, swamps, mounds, tank vaults or unmarked sewer lines located below the surface of the site that would be disturbed as a result of the proposed redevelopment.	None needed	Less than Significant
Impact Geo-7: The Project site has been previously developed and there is no evidence to suggest that the site has been previously used as a landfill. Redevelopment of the Project site as proposed would not result in the placement of any structures above landfills.	None needed	No impact
Impact Geo-8: The Project site is currently served by municipal sewage systems, and redevelopment as proposed would continue to be served by these systems. The use of septic systems is not anticipated.	None needed	No Impact
Cumulative Impact Geo-9: Portions of Oakland are underlain by unstable geology and soil conditions, and cumulative development under these conditions could expose people or structures to substantial adverse effects. However, with required implementation of City of Oakland Standard Conditions of Approval, as	None needed SCA Geo-2: Soils Report	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
well as other applicable local and State laws and regulations, cumulative impacts related to unstable geology and soil conditions would remain less than significant.		
Greenhouse Gas Emissions		
Impact GHG-1: Construction and operation of the Project would not result in GHG emissions that exceed City thresholds of significance. Therefore, the Project would result in a less-than-considerable contribution to cumulative global climate change, and thus a less-than-significant impact.	None needed	Less than Significant
Impact GHG-2: Because the estimated GHG emissions of the Project would not exceed the City's numeric significance threshold as analyzed under Impact GHG-1, development and implementation of the Project would also comply with applicable plans, policies and regulations adopted for the purpose of reducing GHG emissions.	None needed	Less than Significant
Hazards and Hazardous Materials		
Impact Haz-1: No portion of the Project site is included on any list of hazardous materials sites compiled pursuant to Government Code Section 65962.5. Environmental Site Assessments prepared for the Project site do not indicate the presence of on-site soil or groundwater contamination at significant levels, and do not indicate that off-site contamination of soil or groundwater presents a concern to construction or operation of the Project. On-site building assessments do indicate that asbestos-containing materials are present in older portions of the shopping center.	None required SCA Haz-2: Environmental Site Assessment Reports/Remediation SCA Implementation: Soil Sampling. a. Soil and grab-groundwater samples shall be sought from along the sanitary sewer line further west, behind the existing Safeway store and toward Broadway. Based on the presence of groundwater within approximately 15 feet in depth at the former gas station at 5175 Broadway, it appears that field conditions may be more favorable for encountering groundwater closer to Broadway. Also, additional attempts to collect grab-groundwater samples could be made west of Boring SB-1. If grab-groundwater samples are successfully collected, then the laboratory results will also aid in evaluating the significance of the benzene detection at SB-2.	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
	b. Additional sampling activities for evidence of PCE impacts could be focused on the interior of the dry cleaning lease space. Further sampling across the site was not recommended because of the lack of laterally continuous groundwater, the lack of PCE in groundwater at SB-2 and SB-9, and the limited access along the sanitary sewer line behind the lessee spaces. c. These investigations shall be documented in a report which shall make recommendations for remedial action if appropriate and necessary, and shall be signed by a Registered Environmental Assessor, Professional Geologist, or Professional Engineer. SCA Haz-3: Radon or Vapor Intrusion from Soil or Groundwater Sources SCA Haz-10: Lead-Based Paint Remediation SCA Air-2: Asbestos Removal in Structures	
Impact Haz-2: Construction workers, future commercial tenants and shoppers at the Project site may be exposed to hazardous materials during site demolition and construction phases.	None required SCA Haz-7: Other Materials Classified as Hazardous Waste SCA Haz-10: Lead-Based Paint Remediation SCA Air-2: Asbestos Removal in Structures SCA Implementation: Asbestos Removal. a. The floor tile and mastic materials that were positive must be removed using floor abatement practices for asbestos in areas scheduled for renovation. All of the original and older floor tiles are considered asbestos containing material (ACM) due to the difficulty of separating and/or removing the asbestos containing mastic component. Any removal shall be performed using Wet methods, following all applicable regulatory guidelines. During the removal of any carpet floorings, areas of black mastic shall be treated as containing asbestos. b. The drywall materials that were positive must be removed using abatement practices for > 1% asbestos, in areas scheduled for renovation. All of the original or older gypsum board assemblies are considered asbestos containing construction material (ACCM), requiring the use of contractors, registered for asbestos-related work. Any removal shall be performed using Wet methods, following all applicable regulatory guidelines. c. The roofing materials that were positive must be removed using roofing abatement practices for asbestos, in areas scheduled for renovation. All of the roof cements are considered as asbestos containing material (ACM), due to the difficulty of separating and/or removing the asbestos containing mastic component. Any removal shall be performed using Wet methods, following all applicable regulatory guidelines.	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
	d. Renovation or demolition work in areas that are not specifically covered by this report shall be re-inspected prior to any disturbance of suspect materials.	
Impact Haz-3: The Project site is located within one-quarter mile of Oakland Technical High School and Emerson Elementary School.	None required SCA Haz-1: Phase I and/or Phase II Reports SCA Haz-2: Environmental Site Assessment Reports/Remediation	Less than Significant
Impact Haz-4: The project site is not located near any public airport, within an airport plan area or near a private airstrip.	None needed	No Impact
Impact Haz-5: With implementation of SCA Trans-2, the requirement to obtain an encroachment permit for work within street rights-of-way, and standard construction period notification requirements to first responders, potential Project impact related to interference with an emergency response plan or emergency evacuation plan would be less than significant.	None required SCA Trans-2: Construction Traffic and Parking	Less than Significant
Impact Haz-6: The Project site is located within a heavily urbanized portion of Oakland. There are no wild lands at the Project site and adjacent areas have been developed (e.g., as a college campus, a golf course and cemeteries) and would not pose a risk of wildland fires.	None needed	No Impact
Cumulative Impact Haz-7: Hazards and hazardous materials impacts are generally site-specific and/or have limited mobility. Thus, the Project would not be expected to have cumulatively considerable effects.	None needed	Less than Significant
Hydrology and Water Quality		
Impact Hydro-1: The Project site is already fully developed and/or paved, and is served with	None needed	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
water from the East Bay Municipal Utility District. Redevelopment of the Project site as proposed would not result in any change in existing groundwater recharge, and would not deplete groundwater resources.		
Impact Hydro-2: The Project site is not subject to potential flooding, and redevelopment of the Project site as proposed would not subject off-site areas to increased flood potential.	None needed	No Impact
Impact Hydro-3: The Project site currently has very little pervious surface and is almost entirely covered by buildings and paved areas. Redevelopment of the site as proposed would not substantially increase impervious surface area and thus would not increase stormwater runoff.	None needed	Less than Significant
Impact Hydro-4: Site preparation and construction activity associated with the proposed Project could result in soil erosion, which could have adverse effects on water quality. During site preparation and construction activity at the site, potentially significant soil erosion impacts could occur.	None required SCA Geo-1: Erosion and Sedimentation Control Plan	Less than Significant
Impact Hydro-5: Site preparation and construction activity associated with the proposed Project site could result in degradation of stormwater quality.	None required SCA Hydro-1: Stormwater Pollution Prevention Plan	Less than Significant
Impact Hydro-6: Operational activities such as vehicular use, landscaping maintenance and other operational activities could potentially introduce pollutants into stormwater runoff, resulting in degradation of downstream water quality.	None required SCA Hydro-2: Post-construction Stormwater Pollution Management Plan SCA Hydro-3: Maintenance Agreement for Stormwater Treatment Measures SCA Hydro-4: Erosion, Sedimentation, and Debris Control Measures	Less than Significant
Impact Hydro-7: Although the proposed Project would be subject to the provisions of the City of	None required	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
Oakland Creek Protection Ordinance, there is nothing about the Project that would fundamentally conflict with elements of the ordinance intended to protect hydrologic resources. The Project would not discharge a substantial amount of pollutants into the creek or watercourse, it would not significantly modify the natural flow of water, it would not deposit substantial amounts of new material into a creek or cause substantial bank erosion or instability, nor would it substantially endanger public or private property or threaten public health or safety.	SCA Bio-2: Creek Protection Plan SCA Bio-3: Regulatory Permits and Authorizations SCA Bio-4: Creek Monitoring SCA Bio-5: Creek Landscaping Plan	
Cumulative Impact Hydro-8: Implementation of the Project, combined with other past, present, existing, pending and reasonably foreseeable projects would not result in significant adverse changes to hydrology and/or water quality.	None needed	Less than Significant
Land Use		
Impact Land Use-1: The Project would redevelop the existing shopping center with a new shopping center, and would not result in the physical division of an existing community.	None needed	No Impact
Impact Land Use-2: The Project would not be incompatible with surrounding land uses.	None needed	Less than Significant
Impact Land Use-3: The Project would not result in a fundamental conflict with any applicable habitat conservation plan or natural community conservation plan.	None needed	No Impact
Noise		
Impact Noise-1: Noise generated by construction activities at the site would not be expected to	None required	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
violate the City of Oakland Noise Ordinance or violate the City of Oakland Noise Ordinance regarding nuisance of persistent construction-related noise, provided that standard construction noise controls are implemented at the site.	SCA Noise-1: Days/Hours of Construction Operation SCA Noise-2: Noise Control SCA Noise-3: Noise Complaint Procedures SCA Noise-5: Pile Driving and Other Extreme Noise Generators	
Impact Noise-2: The Project would not result in a substantial increase in the permanent outdoor ambient noise levels in the Project vicinity above levels existing without the Project.	None needed	No Impact
Impact Noise-3: The Project would not result in a conflict with land use compatibility guidelines used to determine the acceptability of noise for a commercial land use.	None needed	Less than Significant
Impact Noise-4: The Project's operation will not result in new or exacerbated operational noise levels that would exceed the City of Oakland Noise Ordinance (Oakland Planning Code Section 17.120.050) regarding operational noise.	None needed	Less than Significant
Impact Noise-5: Temporary project construction activities would not expose adjacent residences to groundborne vibration at levels that could cause cosmetic or structural damage to structures or improvements, and Project occupancy and operation would not generate groundborne vibration at levels that would be perceptible beyond the property boundaries.	None required SCA Noise-1: Days/Hours of Construction Operation SCA Noise-3: Noise Complaint Procedures SCA Noise-5: Pile Driving and Other Extreme Noise Generators	Less than Significant
Cumulative Impact Noise-6: Cumulative increases in noise within the vicinity of the Project area would not result in a 5 dBA L_{dn} permanent increase in ambient noise levels above noise levels without the Project, and the Project's contribution to the cumulative increase in noise would not result in a 3 dBA L_{dn} permanent	None needed	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
SafeWay Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
increase attributable to the Project.		
Traffic		
<i>Existing plus Project</i>		
Impact Trans-1: The proposed Project would degrade intersection operations from LOS D to LOS E during the Saturday PM peak hour at the signalized Shattuck Avenue/52nd Street intersection (#12).	Mitigation Measure Trans-1: Implement the following measures at the Shattuck Avenue/52nd Street intersection: a) Optimize signal timing parameters (i.e., adjust the allocation of green time for each intersection approach). b) Coordinate the signal timing changes at this intersection with the adjacent intersections that are in the same signal coordination group.	Less than Significant
Impact Trans-2: The signalized Telegraph Avenue/51st Street intersection currently operates at LOS E, even without increased traffic from the Project. The proposed Project would add traffic that would increase delay for the critical southbound left-turn movements by more than six seconds during the weekday PM peak hour.	Mitigation Measure Trans-2: Implement the following measures at the Telegraph Avenue/51st Street intersection: a) Optimize signal timing parameters (i.e., adjust the allocation of green time for each intersection approach). b) Coordinate the signal timing changes at this intersection with the adjacent intersections that are in the same signal coordination group.	Less than Significant
Impact Trans-3: The proposed Project would add more than 10 trips to the Howe Street/Pleasant Valley Avenue intersection during the weekday PM and Saturday midday peak hours under Existing plus Project conditions. The intersection would meet the peak hour signal warrant during both time periods.	Mitigation Measure Trans-3: Implementing one of the following measures at the Howe Street/Pleasant Valley Avenue intersection would reduce the impact to a less than significant level: a) Signalize the intersection, providing actuated operation with permitted left turns and coordinate the signal timings with the adjacent intersections that would be in the same signal coordination group. b) Prohibit on-street parking for about 80 feet along northbound Howe Street just south of Pleasant Valley Avenue to allow right-turning vehicles to bypass the queued left-turning vehicles. c) Prohibit the left-turn movement from Howe Street to westbound Pleasant Valley Avenue during the peak commute periods.	Because of secondary significant impacts associated with each of the identified mitigation measures, these measures are considered infeasible. Significant and Unavoidable
Impact Trans-4: The signalized Piedmont Avenue/Pleasant Valley Avenue intersection currently operates at LOS E, even without increased traffic from the Project. The proposed Project would add traffic that would increase average delay at this intersection by more than	Mitigation Measure Trans-4: Implement the following measures at the Piedmont Avenue/Pleasant Valley Avenue intersection: a) Convert signal control equipment from pre-timed to actuated-coordinated operations b) Optimize signal timing parameters (i.e., adjust the allocation of green time for each intersection approach)	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
four seconds during the weekday PM peak hour.	c) Coordinate the signal timing changes at this intersection with the adjacent intersections that are in the same signal coordination group.	
<i>2015 Intersection Impacts</i> Impact Trans-5: The proposed Project would degrade intersection operations from LOS D to LOS E during the weekday PM peak hour at the Broadway/51st Street/Pleasant Valley Avenue (#7) intersection under 2015 Conditions. The proposed Project would also add traffic that would increase delay for the critical eastbound through movement by more than six seconds during the Saturday midday peak hour, which the intersection would operate at LOS E regardless of the proposed Project	Mitigation Measure Trans-5: Implementation of the following measures at the Broadway/51st Street/Pleasant Valley Avenue intersection would reduce the impact to a less-than-significant level: a) Install a left-turn lane on the westbound Pleasant Valley Avenue approach. b) Install a left-turn lane on the eastbound 51st Street approach.	These modifications would conflict with City policy concerning pedestrian safety and comfort, therefore resulting in secondary impacts. For these reasons the mitigation is considered infeasible. Significant and Unavoidable
Impact Trans-6: The Shattuck Avenue/52nd Street intersection is projected to operate at LOS E under 2015 Conditions, even without increased traffic from the Project. The proposed Project would add traffic that would increase delay for the critical southbound through movement by more than six seconds during the Saturday PM peak hour, exceeding the City's threshold of significance.	Mitigation Measure Trans-6: Implement Mitigation Measure Trans-1.	Less than Significant
Impact Trans-7: The Telegraph Avenue/51st Street intersection is projected to operate at LOS E under 2015 Conditions, even without increased traffic from the Project. The proposed Project would add traffic that would increase delay for the critical southbound left-turn movement by more than six seconds during the weekday PM peak hour.	Mitigation Measure Trans-7: Implement Mitigation Measure Trans-2.	Less than Significant
Impact Trans-8: The proposed Project would add more than 10 trips to the Howe Street/Pleasant Valley Avenue (#19) intersection	Mitigation Measure Trans-8: Implement Mitigation Measure Trans-3	Because of secondary significant impacts associated with each of

Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
during the weekday PM and Saturday midday peak hours under 2015 Plus Project conditions. The intersection would meet the peak hour signal warrant during both time periods.		the identified mitigation measures, these measures are considered infeasible Significant and Unavoidable
Impact Trans-9: The proposed Project would degrade intersection operations from LOS E to LOS F during the weekday PM peak hour at the Piedmont Avenue/Pleasant Valley Avenue (#20) intersection under 2015 Conditions; the Project would also degrade the intersection operations during the Saturday midday and PM peak hour from LOS D to LOS E.	Mitigation Measure Trans-9: Implement Mitigation Measure Trans-4.	Less than Significant
<i>2035 Intersection Impacts</i> Impact Trans-10: The proposed Project would increase volume-to-capacity (v/c) ratio for the intersection by 0.01 or more, and the critical movement v/c ratio for the eastbound left, eastbound through, westbound left, northbound through, and the southbound left movements by 0.02 or more during the weekday PM peak hour, and it would increase v/c ratio for the intersection by 0.01 or more and the critical movement v/c ratio for the eastbound left, eastbound through, and, northbound through movements by 0.02 or more during the Saturday midday peak hour at the Broadway/51st Street/Pleasant Valley Avenue (#7) intersection under 2035 Conditions, which would operate at LOS F regardless of the Project.	Mitigation Measure Trans-10 Implement Mitigation Measure Trans-5.	Even with implementation of this mitigation measure, the impact would remain significant and unavoidable. In addition, these modifications would conflict with City policy concerning pedestrian safety and comfort, therefore resulting in secondary impacts. For these reasons the mitigation is considered infeasible. Significant and Unavoidable
Impact Trans-11: The proposed Project would increase intersection volume-to-capacity (v/c) ratio by 0.01 or more during the Saturday PM peak hour at the Shattuck Avenue/52nd Street (#12) intersection under 2035 Conditions, which would operate at LOS F regardless of the Project.	Mitigation Measure Trans-11: Implement Mitigation Measure Trans-1.	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
Impact Trans-12: The proposed Project would increase delay for the critical southbound left-turn movement by more than six seconds during the weekday PM peak hour at the Telegraph Avenue/51st Street (#15) intersection under 2035 Conditions, which would operate at LOS E regardless of the Project; the Project would also increase delay for the critical westbound and southbound movements by more than six seconds during the Saturday midday peak hour; the Project would also degrade the intersection during the Saturday PM peak hour from LOS D to LOS E.	Mitigation Measure Trans-12: Implement Mitigation Measure Trans-2.	Less than Significant
Impact Trans-13: The proposed Project would add more than 10 trips to the Howe Street/Pleasant Valley Avenue (#19) during the weekday PM, Saturday midday, and Saturday PM peak hours under 2035 Plus Project conditions. The intersection would meet the peak hour signal warrant during the three time periods.	Mitigation Measure Trans-13: Implement Mitigation Measure Trans-3	Because of secondary significant impacts associated with each of the identified mitigation measures, these measures are considered infeasible. Significant and Unavoidable
Impact Trans-14: The proposed Project would increase volume-to-capacity (v/c) ratio for the intersection by 0.01 or more, and the critical movement v/c ratio for the eastbound, westbound, and northbound movements by 0.02 or more during the weekday PM, Saturday midday, and Saturday PM peak hours at the Piedmont Avenue/Pleasant Valley Avenue (#20) intersection under 2035 Conditions, which would operate at LOS F regardless of the Project.	Mitigation Measure Trans-11: Implement the following measures at the Piedmont Avenue/Pleasant Valley Avenue intersection: a) Mitigation Measure Trans-4. b) Modify signal control equipment to provide lagging protected phasing in the northbound direction.	After implementation of this measure, the intersection impact would remain significant and unavoidable. No other feasible mitigation measures are available within the existing automobile right-of-way. Significant and Unavoidable
Impact Trans-15: The proposed Project would degrade intersection operations from LOS E to LOS F during the weekday PM peak hour at the Hudson Street/Manila Avenue/College Avenue	Mitigation Measure Trans-15: Implement the following measures at the Hudson Street/Manila Avenue/College Avenue intersection: a) Optimize signal timing parameters (i.e., adjust the allocation of green time for each intersection	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
SafeWay Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
(#24) intersection under 2035 Conditions.	approach). b) Coordinate the signal timing changes at this intersection with the adjacent intersections that are in the same signal coordination group.	
Impact Trans-16: The proposed Project would not cause congestion of regional significance on a roadway segment on the Congestion Management Program (CMP) and/or the Metropolitan Transportation System (MTS) evaluated per the requirements of the Land Use Analysis Program of the CMP.	None needed	Less than Significant
Impact Trans-17: The proposed Project would not substantially increased travel times for AC Transit buses.	None needed	Less than Significant
Impact Trans-18: The proposed Project would not directly or indirectly cause or expose roadway users (e.g., motorists, pedestrians, bus riders, bicyclists) to a permanent and substantial transportation hazard due to a new or existing physical design feature or incompatible uses.	None needed	Less than Significant
Impact Trans-19: The proposed Project would not generate substantial multi-modal traffic traveling across at-grade railroad crossings that cause or expose roadway users (e.g., motorists, pedestrians, bus riders, bicyclists) to a permanent and substantial transportation hazard.	None needed	Less than Significant
Impact Trans-20: The proposed Project would not directly or indirectly result in a permanent substantial decrease in pedestrian safety.	None needed	Less than Significant
Impact Trans-21: The proposed Project would not directly or indirectly result in a permanent substantial decrease in bus rider safety.	None needed	Less than Significant
Impact Trans-22: The proposed Project would	None needed	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
not directly or indirectly result in a permanent substantial decrease in bicyclist safety.		
Impact Trans-23: The proposed Project would not result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks.	None needed	Less than Significant
Impact Trans-24: The proposed Project would not fundamentally conflict with adopted City policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities adopted for the purpose of avoiding or mitigating an environmental effect and actually result in a physical change in the environment.	None needed	Less than Significant
Impact Trans-25: The proposed Project would result in a substantial, though temporary adverse effect on the circulation system during construction.	None required SCA Trans-1: Construction Traffic Management Plan SCA Implementation: Construction Traffic Management. The Construction Traffic Management Plan developed for the Project shall include the following: a) A set of comprehensive traffic control measures for motor vehicles, transit, bicycle, and pedestrian access and circulation during each phase of construction. b) A construction period parking management plan to ensure that parking demands for construction workers, site employees, and customers are accommodated during each phase of construction.	Less than Significant
Impact Trans-26: Neighborhood traffic intrusion would not exceed the capacity of affected residential streets, and would not result in a significant impact.	None needed	Less than Significant
Utilities and Public Services		
Impact Util-1: Although the Project will result in the construction of new storm water drainage facilities, the construction of these facilities would not cause significant environmental	None required SCA Util-2: Stormwater and Sewer SCA Air-1: Best Management Practices	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
effects.	SCA Geo-1: Erosion and Sedimentation Control Plan SCA Noise-1: Days/Hours of Construction Operation SCA Noise-2: <i>Noise Control</i> SCA Trans-1: Construction Traffic Management Plan	
Impact Util-2: The Project would not generate wastewater flows that would exceed the capacity of existing wastewater treatment facilities or necessitate the expansion of existing wastewater treatment facilities.	None needed	Less than Significant
Impact Util-3: Although the Project will result in the construction of new on-site wastewater collection infrastructure, the construction of such infrastructure would not cause significant environmental effects.	None required SCA Util-2: Stormwater and Sewer SCA Air-1: Best Management Practices SCA Geo-1: Erosion and Sedimentation Control Plan SCA Noise-1: Days/Hours of Construction Operation SCA Noise-2: <i>Noise Control</i> SCA Trans-1: Construction Traffic Management Plan	Less than Significant
Impact Util-4: The Project would not exceed water supplies available from existing entitlements and resources.	None needed	Less than Significant
Impact Util-5: Although the Project would result in the construction of certain new on-site water supply infrastructure, the construction of such infrastructure would not cause significant environmental effects.	None required SCA Air-1: Best Management Practices SCA Geo-1: Erosion and Sedimentation Control Plan SCA Noise-1: Days/Hours of Construction Operation SCA Noise-2: <i>Noise Control</i> SCA Trans-1: Construction Traffic Management Plan	Less than Significant
Impact Util-6: The amount of solid waste	None required	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
generated by the proposed Project would not exceed the capacity of the Davis Street Transfer Station or the Altamont Landfill and would not require the construction or expansion of landfill facilities.	SCA Util-1: Waste Reduction and Recycling	
Cumulative Impact Util-7: The Project, in combination with other known past, present, planned or reasonably anticipated future projects would not exceed existing or projected utility capacities.	None needed	Less than Significant
Other Less Than Significant Effects		
Impact Ag-1: The Project would not convert Prime Farmland, Unique Farmland or Farmland of Statewide Importance (Farmland) as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the Cal. Resources Agency to non-agricultural use.	None needed	No Impact
Impact Ag-2: The Project would not conflict with existing zoning for agricultural use, or a Williamson Act contract.	None needed	No Impact
Impact Ag-3: The Project would not involve any changes in the existing environment which, due to their location or nature, could result in conversion of farmland to non-agricultural use.	None needed	No Impact
Impact Min-1: The Project would not result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state.	None needed	No Impact
Impact Min-2: The Project would not result in loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan.	None needed	No Impact

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
Impact Pop-1: The Project would not induce substantial population growth in a manner not contemplated in the General Plan, either directly or indirectly.	None needed	No Impact
Impact Pop-2: The Project would not displace substantial numbers of existing housing or people, necessitating the construction of replacement housing elsewhere in excess of that contained in the City's Housing Element.	None needed	No Impact
Impact Pub Serv-1: The Project would not result in substantial adverse physical impacts associated with the provision of new or physically altered fire protection facilities in order to maintain acceptable service ratios, response times or other fire protection service performance objectives.	None required SCA Pub Serv-1: Fire Safety Phasing Plan	Less than Significant
Impact Pub Serv-2: The Project could result in an increase in calls for police protection services, but would not result in substantial adverse physical impacts associated with the provision of new or physically altered police facilities or the need for new or physically altered police facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other Oakland Police Department performance objectives	None needed	Less than Significant
Impact Pub Serv-3: The Project could result in new students for local schools, but would not require new or physically altered school facilities to maintain acceptable performance objectives.	None needed	No Impact
Impact Rec-1: The Project would not increase the use of existing neighborhood and regional parks or other recreational facilities such that	None needed	Less than Significant

**Table 2-1: Summary of Project Impacts, Standard Conditions of Approval, Mitigation Measures and Residual Impacts:
Safeway Redevelopment Project at Broadway and Pleasant Valley Avenue**

Potential Environmental Impacts	Mitigation Measures / Standard Conditions of Approval (SCA)	Resulting Level of Significance
substantial physical deterioration of the facility would occur or be accelerated.		
Impact Rec-2: The Project does not include recreational facilities nor does it require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment.	None needed	No Impact

Table 2-2: Summary of Non-CEQA Recommendations

Potential Effects	Recommendation
Vehicle, Pedestrian and Bicycle Safety <i>Broadway/College Avenue Intersection</i> The Project would generate additional automobiles, bicycles, and pedestrians at the Broadway/College Avenue intersection, which currently does not provide a crosswalk on the south approach. In addition, vehicles on southbound College Avenue turn right into Broadway at high speeds due to the angle that College Avenue intersects Broadway. These vehicles may conflict with pedestrians crossing College Avenue or vehicles turning left from northbound Broadway into Wendy's Restaurant.	Recommendation Trans-15a: Modify the Broadway/College Avenue intersection so that College Avenue would intersect Broadway at a right angle.
Internal Pedestrian Improvements The internal street in the western portion of the site provides a continuous commercial frontage and is intended as a pedestrian oriented street. The loading berths at Building "M" disrupt the pedestrian flow along the internal street and may result in potential conflicts when trucks are backing into and leaving the loading dock.	Recommendation Trans-17: Implement the following in order to improve pedestrian access, circulation, and safety: - Use different materials and/or striping patterns at all crosswalks within the site, including mid-block crossings, parking aisle crossings, and parking structure driveways. Also, consider using raised speed tables at crosswalks to reduce automobile speeds. - Ensure adequate sight distance is provided at all crosswalks, especially at midblock and parking structure driveways. - Potential options to improve pedestrian circulation and safety along the internal street near the loading berths at Building "M" include: - Allow trucks to load/unload along the internal street during non-peak periods. - Provide a pull-out on Pleasant Valley Avenue that would allow trucks to parallel park without interfering with automobile or bicycle flow along Pleasant Valley Avenue. This strategy would also require direct access between the uses on the south side of the internal street and Pleasant Valley Avenue. - Enlarge the existing loading berth adjacent to Building "J." This strategy would require material to be manually delivered to the uses south of the internal street. - Implement a loading management program at Building "M" loading berths to minimize disruptions to pedestrian activity. - Ensure that all pedestrian paths and sidewalks within the Project site have a minimum width of six feet (10 feet preferred). - Ensure that all pedestrian facilities provide pedestrian scale lighting.
Bus Rider Safety	Recommendation Trans-18: Provide a bus shelter at the bus stops on northbound and southbound Broadway north of Pleasant Valley Avenue/51st Street and on westbound Pleasant Valley Avenue west of Project driveway.

Table 2-2: Summary of Non-CEQA Recommendations

Bicycle Parking	Recommendation Trans-23: Implement the following improvements to bicycle parking: a) Locate long-term bicycle parking in the parking structures. b) Ensure the short-term bicycle parking on sidewalks do not block pedestrian circulation. c) Ensure that some short-term bicycle parking spaces can accommodate bicycles with trailers. d) Monitor the usage of long-term and short-term bicycle parking spaces and if necessary provide additional parking spaces. e) Provide shower and locker facilities in a central location that can be accessed by all site employees.
Parking Supply	Recommendation Trans-24: Implement the following strategies to reduce overall parking demand for the Project site and better manage the available parking supply: a) Implement a Transportation Demand Management (TDM) plan to encourage more Project employees to use other travel modes than driving as required by SCA Trans-1. b) Encourage employees to use the least convenient parking spaces such as parking spaces on the top deck of the parking structures and behind the buildings. c) Install an automated parking counting system including variable message signs to inform motorists of the number of parking spaces available in the structured parking facilities and reduce potential traffic circulation. d) Implement strategies to manage parking demand and supply during the peak December periods: • Provide attendant parking for employees and/or customers. Automobiles can park in the drive aisles with attendant parking and increase the overall parking capacity of the site. • Provide remote parking for site employees.
Truck Access and Circulation	Recommendation Trans-25: Implement a loading management program to ensure that truck deliveries for all Project buildings can be accommodated with minimal disruptions to pedestrian, bicycle, and automobile access, circulation and parking throughout the site. The loading management program should identify loading areas for all Project buildings and truck waiting areas when truck loading areas are occupied.

ATTACHMENT F

**Conceptual Improvements at Broadway/College Avenue Intersection
(Figure 4.11-23 from Draft EIR)**

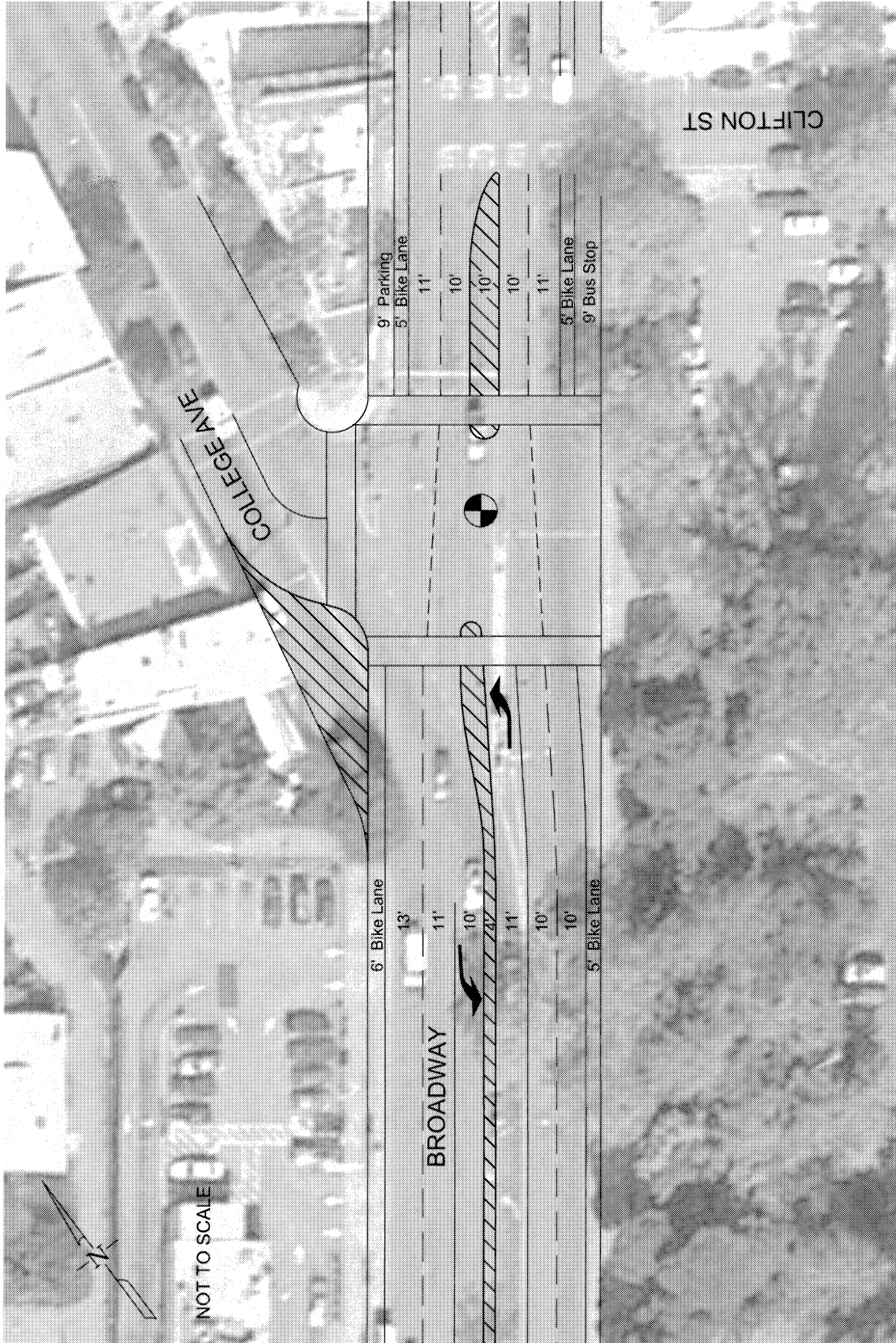


Figure 4.11-23
Conceptual Improvements at Broadway/College Avenue Intersection