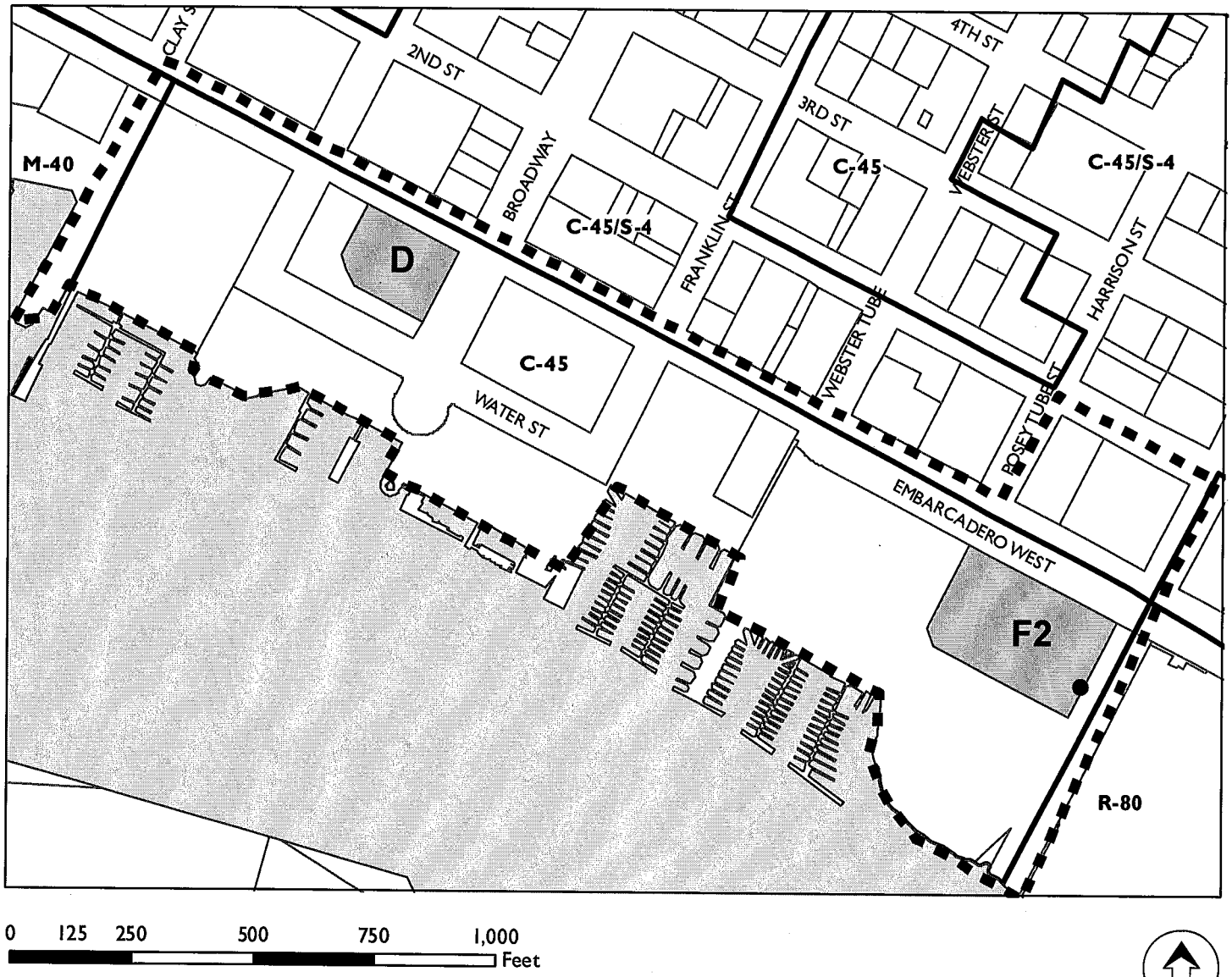
Location:	Jack London Square Development Project: Sites D at Broadway and F2 at Harrison Street (south of Embarcadero). (see map)
Assessor's Parcel Numbers:	018 041500101
Proposal:	Final Development Permits for Sites D and F2.
Applicant:	CIM Group, Sean Buran: p: (323) 860-1811
Contact Person:	CIM Group, Sean Buran: p: (323) 860-1811
Owner:	CIM Group
Case File Number:	PUD13170-PUDF-01 and -02
Planning Permits Required:	Final Development Permits for Sites D and F2 of Jack London Square Planned Unit Development Project; Minor variance to reduce parking.
General Plan:	Site D=Retail, Dining and Entertainment-1; and Site F2=Waterfront Commercial Recreation-1.
Zoning:	C-45 Community Shopping Commercial Zone
Environmental Determination:	Final EIR certified on March 17, 2004 by the Planning Commission; Addendum #1 approved on September 23, 2014.
Historic Status:	None for affected sites.
Service Delivery District:	I – Downtown/West Oakland/Harbor
City Council District:	3 – Lynette Gibson McElhaney
Date Filed:	June 30, 2016
Status:	Design Review Committee review on February 22, 2017.
Action to be Taken:	Consider approval of FDPs for Sites D and F2
Staff Recommendation:	Take public testimony, close the public hearing and consider decision.
Finality of Decision:	Appealable to City Council.
For Further Information:	Contact the case planner Catherine Payne at (510) 238-6168 or cpayne@oaklandnet.com

SUMMARY

CIM Group has submitted an application for Final Development Permits (FDPs) for Sites D and F2 of the Jack London District Planned Unit Development. The City of Oakland originally approved the nine-site, multi-phased development project known as "Jack London Square" in 2004. A revision to the PUD (and an accompanying General Plan Amendment) was approved for the project in 2014. Three sites have been constructed, and the applicant is currently seeking FDPs for two of the three remaining development opportunity sites in Jack London Square. In summary, the applicant proposes mid-rise residential mixed-use development, within the PUD development allowances, for both sites D (case file PUD13170-PUDF-01) and F2 (case file PUD13170-PUDF-02).

CITY OF OAKLAND PLANNING COMMISSION



Case File: PUD13170-PUDF-01 and -02
Applicant: CIM Group, Sean Buran
Address: Jack London Square Development Project: Sites D at Broadway and F2 at Harrison Street (south of Embarcadero
Zone: C-45

PROJECT SITE AND SURROUNDING AREA

The Jack London Square project site is a nine-site area located along the Oakland Estuary (at the southern terminus of Broadway) between Clay and Alice Streets. More specifically, Jack London Square project "Site D" is located immediately south of Embarcadero (and the Union Pacific right-of-way) on the west side of Broadway. Surrounding land uses include entertainment, dining and destination retail uses and the Jack London Square Marina to the south. "Site F2" is located immediately south of Embarcadero between Harrison and Alice Streets. Adjacent uses include the "Site F1" commercial building to the west, the Union Pacific right-of-way and "Site G" garage and commercial building and Amtrak passenger train station to the north, residential uses to the east, and a vacant lot ("Site F3", a planned hotel site) to the south.

PROJECT BACKGROUND

Project History

The City of Oakland approved the nine-site, multi-phased development project known as "Jack London Square" in 2004. The project is located on sites located throughout the Jack London District of Oakland, south of Interstate 880, and owned by the Port of Oakland (with the exception of Sites D and F2, which are owned by CIM Group). The project was subject to an Environmental Impact Report, Preliminary and Final Development Permits (and appeal), Major Conditional Use Permit (and appeal), Major Variance, Rezone, Development Agreement (and appeal), with final approvals for the land use entitlements granted by the Oakland City Council on June 15, 2004. The PUD was revised in 2014 to allow residential development options on Sites D and F2.

The adopted project is mixed-use development scheme that supports the retail, entertainment and dining uses in the project area, while providing needed complementary residential uses, as well. The project was subject to a high level of design scrutiny in both 2004 and 2014, with a concern for how buildings and land uses would relate to the waterfront, to the public spaces in Jack London Square (including the Bay Trail), and to nearby residential uses.

Since 2004, the project proponent has developed three sites: Sites "C", "G" and "F1". "Site C" is a commercial building that includes 16,000 square feet of above-ground floor office space and 16,000 square feet of retail, dining and entertainment space on the ground floor. "Site G" includes 1,086 parking spaces (although the site was only required to have 743 spaces), 30,000 square feet of vacant retail space on the ground floor, and a pedestrian bridge connecting the building to Jack London Square over the railroad ROW along Embarcadero. "Site F1" is a six-story building with an approximately 33,000 square-foot footprint, and encompasses a total of 191,000 square feet; there is a restaurant located on the ground floor and mostly occupied office uses on the upper floors.

The uses for sites approved in 2014 for sites D and F2 allow for up to 666 dwelling units between both sites. Specifically, approvals for each parcels include:

- Site D: The approved use is for up to 167 dwelling units, and up to 190,000 square feet of retail and office uses, including a theatre. The approved maximum building height is 179 feet.
- Site F2: The approved use is for up 370 dwelling units, up to 149,000 square feet of retail and office uses, and up to 550 parking spaces. The approved maximum building height is 293 feet.

Design Review Committee

The Design Review Committee of the Planning Commission (DRC) reviewed the proposed project at their regularly scheduled meeting on February 22, 2017. In general, the DRC and only public speaker were very supportive of the project. In fact, Commissioner Nagraj stated that the Site F2 proposal is amongst the best design schemes he has reviewed during his tenure as a Planning Commissioner. A summary of comments includes the following:

- Site F2:
  - Dark color on Embarcadero façade could be lightened and reflect materials used throughout the project, instead of introducing an entirely new material.
  - Ground floor along Embarcadero should be enlivened with lighting, storefront-style display windows and otherwise made less imposing and monotonous for pedestrians.
  - Water Street should be fully designed and implemented as part of the Site F2 improvements.

- Site D:
  - The base color is very dark. A brighter, lighter color would make more of a landmark statement at this important site.
  - The applicant should explore smooth, applied materials, such as a smooth cementitious board.
  - The applicant should consider eliminating parking on Site D to improve pedestrian circulation at the base of Broadway.
  - Reference to industrial and warehouse district may not be geographically relevant. The Warehouse District is closer the Interstate 880 freeway.
    - Note: Staff believes the Site D design references nearby Port activities. The angled roof line, deep soffits, cantilevers, and slightly offset grid reference the cranes and stacked shipping containers at the Port.

PROJECT DESCRIPTION

The proposed project is two Final Development Permits related to the Jack London District Planned Unit Development (JLD PUD). The proposed FDPs are for mixed-use residential development on Sites D and F2. In summary, the proposed development schemes are as follows:

Site D: Site D is located at the base of Broadway on the northwest corner of Broadway and Embarcadero. The proposed project includes the following features:

- 85-foot building height (eight stories);
- 15-20-foot ground floor retail height;
- 135 dwelling units
- 2470 sf retail
- 36 on-site parking spaces (including three spaces for the commercial square footage). It should be noted that Site G is not owned by the applicant although it is leased to the applicant and is included in the PUD. However, parking in Site G may not be available in the future. The applicant is requesting consideration of a minor variance from parking requirements to address the fact that available spaces at Site G may not be specifically dedicated to the residential uses in the future (although they will remain available for those uses on a first-come, first-served basis).
- 1 on-site loading space

Site F2: Site F2 is located between Harrison and Alice Streets, and between Embarcadero and Water Street. The proposed project includes the following features:

- 85-foot building height (eight stories)
- 338 dwelling units
- 2,562 square-foot ground-floor retail space
- 86 on-site parking spaces (with the balance of required spaces provided in the existing Site G garage, located on 2nd Street). It should be noted that Site G is not owned by the applicant although it is leased to the applicant and is included in the PUD. However, parking in Site G may not be available in the future. The applicant is requesting consideration of a minor variance from parking requirements to address the fact that available spaces at Site G may not be specifically dedicated to the residential uses in the future (although they will remain available for those uses on a first-come, first-served basis).
- 1 on-site loading space (consistent with approved variance)

GENERAL PLAN ANALYSIS

Existing General Plan Land Use Classifications

The Site D General Plan land use classification is Retail, Dining, Entertainment Phase 2 (RDE-2). The intent of this classification is to "enhance and intensify Lower Broadway as an active pedestrian-oriented entertainment district that can help to create stronger activity and pedestrian linkages with downtown Oakland, Old Oakland, and Chinatown." (EPP, page 132) With regards to desired character, "Future development in this area should be primarily retail, restaurant, entertainment, hotel, upper level office, cultural, parks public open space, and any other use that is complementary to active public-oriented ground-level uses." (EPP, p. 132) The maximum FAR is 7.0, and the district allows 125 dwelling units per gross acre. The RDE-2 designation allows residential uses and emphasizes pedestrian-oriented development with active public-oriented uses on the ground floor.

The Site F2 General Plan land use classification is Mixed Use District (MUD). The intent of his classification is to "Encourage the development of nontraditional higher density housing (work/live, lofts, artist studios) within a context of commercial and light industrial/manufacturing uses." (EPP, p.133) With regards to desired character, "Future development in this area should be primarily light industrial, warehousing, wholesale, retail, restaurant, office, residential, work/live, loft units, parks, and public open spaces with manufacturing, assembly, and other uses that are compatible with adjacent uses." (EPP, p. 133) The maximum FAR is 5.0, and the district allows up to 125 dwelling units per gross acre.

The General Plan land use designations for the proposed project were amended (changed) in 2014 to accommodate the proposed project. The proposed project is part of an approved PUD previously found to be consistent with and support the General Plan.

ZONING ANALYSIS

Zoning District Analysis

Both Sites D and F2 are zoned C-45 Community Shopping Commercial Zone (C-45 zone). The zoning regulations are consistent with the General Plan land use designations and would allow the proposed project. The intent of the C-45 zone is to "create, preserve, and enhance areas with a wide range of both retail and wholesale establishments serving both long and short term needs in compact locations oriented toward pedestrian comparison shopping, and is typically appropriate to commercial clusters near intersections of major thoroughfares." (Oakland Planning Code Section 17.56.010) The outright permitted residential density is one dwelling unit per 300 square feet of lot area. The maximum FAR is 7.0 (and may be exceeded by 10 percent on any corner lot).

The following table compares the proposed project with the C-45 development standards:

Zoning Criteria	C-45/PUD Development Standards	Site D Proposal	Site F2	Comments
Land Use	Includes permanent residential	Permanent Residential	Permanent Residential	Complies
~~Density~~	~~1 unit /300 s.f. lot area~~	NA	NA	Density does not apply in Estuary Plan area if GP FAR not exceeded
Front Yard	0'	0' and greater	0' and greater	Complies
Street Side Yard	0'	0' and greater	0' and greater	Complies
Interior Side Yard	0' unless if opposite living room window, then 8' plus 2' additional for each story above ground level	0' and greater	0' and greater	Complies
Courts	Required opposite legally required windows	Courts provided	Courts provided	Complies
Rear Yard	0'	0' and greater	0' and greater	Complies
Building Height	179' on D; 293' on F2	8 stories (85')	8 stories (85')	Complies
Open Space	150 s.f. group/unit (private space reduces requirement by 50%)	To Be Determined	To Be Determined	PUD allows relaxation of standards; see staff recommendation in staff report
Parking	0.25 space/residential unit	36 spaces	86 spaces	Minor variance required
Loading	<50 k sf = 0 >50k sf = 1 >200k sf = 2	1 provided	1 provided	Complies (minor variance approved in 2014 for location)
Bicycle Parking	Long-term: 1 per 4 units Short-term: 1 per 20 units	35 spaces	104	Does not comply; staff will ensure compliance prior to building permit issuance
Recycling Space	2 cubic feet of space per unit	245 cf	26 cy	Does not comply; staff will ensure compliance prior to building permit issuance

PUD Analysis

The proposed revisions to the PUD include the addition of residential options for Sites D and F2 and the removal of the cap on office uses for the same sites. The PUD allows permitted density and FAR to be applied in an additive manner throughout the PUD area. This means that the available FAR from one PUD site can be added to another site located within the same PUD. The proposed residential options rely on this calculation to maximize the allowable density and FAR for Sites D and F2, as shown below.

Similarly, the PUD provides parking throughout the larger PUD area in order to satisfy parking requirements for the various sites and land uses included in the PUD. In combination, dedicated residential parking on Sites D and F2, combined with the parking provided in the project garage on Site G, satisfies the quantity of parking required in the PUD. It should be noted that, due to the limitations of other applicable jurisdictions (including the Port of Oakland and State Lands), the project cannot specifically designate parking to the residential uses at a 1:1 ratio. The PUD only requires the adequate amount and ratio of parking, which is met. The applicant has applied for a zoning variance (see further discussion throughout this report) to address the limitation on dedicating an adequate amount of the available parking specifically to the residential use.

Site D

Site D PUD Requirement		Consistency of Site D Proposal	Requirements
Land Use	Up to 666 du/ 90k sf Retail/ 59k sf Office/41k sf theater	135 du/ up to 2,500 sf groundfloor retail	Complies
FAR	7.0 FAR (168,294 sf/886,723 sf sitewide*)	132,484 sf /3.5 FAR	Complies
Footprint	38k sf	Complies	Consistent with PUD
Height	179 feet	85'	Complies
Square footage	190k sf	132,484 sf	Complies
Parking	1:1 (district-wide)	.25/du onsite	Minor variance required; Parking quantity to meet PUD is provided in Site G garage on a first-come, first-served basis

Site F2

Site F2 PUD Requirement		Consistency of Site F2 Proposal	Requirements
Land Use	Up to 666 du/ 15k sf Retail/ 134k sf Office	338 du/ up to 2,600 sf groundfloor retail	Complies
FAR	5.0 FAR (73,847 sf/886,723 sf sitewide*)	341,702 sf/ 6.0 FAR	Complies
Footprint	57k sf	Complies	Complies
Height	293 feet	85'	Complies

Square footage	540,205 sf	341,702 sf	Complies
Parking	1:1 (district-wide)	.25/du onsite	Minor variance required; Parking quantity to meet PUD is provided in Site G garage on a first-come, first-served basis

ENVIRONMENTAL REVIEW

The planned Jack London District PUD, approved in 2004, fully evaluated environmental impacts of the project, in accordance with all applicable requirements. Specifically, the City of Oakland prepared and certified an Environmental Impact Report (EIR) under the California Environmental Quality Act (CEQA). The City Council certified the EIR for the existing project approvals on June 15, 2004. The EIR considered an envelope of development of up to 960,700 square feet of commercial uses.

In 2014, the City approved revisions to the PUD. At that time, in accordance with CEQA, the City reviewed and analyzed the proposed project changes and other relevant information to determine whether circumstances requiring the preparation of a subsequent or supplemental EIR exist. The project approved in 2014 allows development of up to 1,287,700 net new gross square feet (gsf) of commercial and residential uses (including up to 665 dwelling units and a "Maximum Commercial Scenario" that could develop up to 960,700 net new gsf of commercial uses--similar to the project analyzed in the 2004 EIR). Based upon substantial information, the City determined that none of those circumstances were present. Therefore, the appropriate CEQA documentation was an Addendum. An Addendum is appropriate when none of the circumstances that require a supplemental or subsequent EIR pursuant to CEQA Guidelines Section 15162 have occurred, specifically:

- There are no substantial changes proposed in the project which would result in new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

- There are no substantial changes with respect to project circumstances which would result in new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

- There is no new information of substantial importance which would result in new significant environmental effects, a substantial increase in the severity of previously identified significant effects, previously infeasible mitigation measures or alternatives now found to be feasible, or new mitigation measures or alternatives which are considerably different from previous ones that would substantially reduce environmental effects.

The Addendum was published and made publically available on May 9, 2014. The EIR and Addendum are provided under separate cover to the Planning Commission and are available to

the public at 250 Frank Ogawa Plaza, Suite 3315, Oakland CA 94612 during normal business hours and at http://www2.oaklandnet.com/government/o/PBN/OurServices/Application/DOWD009158 (see line item #19).

The specific Jack London District PUD action subject to CEQA compliance at this time is limited to FDPs for Sites D and F2. The FDPs are, by definition, design refinement and evolution of the approved project, and not changes to the approved project. The FDPs do not change the planned land use or conceptual design of the Jack London District PUD, as permitted under the PUD, and analyzed in the EIR and Addendum. Since there is no substantive change to the project and the project is already approved, there is no justification for preparing new analyses and/or studies of potential effects of the project on the environment. Therefore, no subsequent or supplemental environmental review is required under CEQA.

ISSUES

Development Agreement

The proposed FDPs are subject to the Jack London District PUD and to a Development Agreement (DA) between the Master Developer and the City of Oakland. The Development Agreement protects the project from the application of new fees and requirements legislated following adoption of the DA, such as the recently adopted Affordable Housing and Transportation and Capital Improvement Impact Fees. However, it should be noted that the planned project was initially an entirely commercial project; however, the 2014 revision included diversifying the land use mix to include much-needed residential uses. Although the planned project does not include affordable housing, it does include residential uses that will help alleviate the regional and local housing crisis by increasing supply at a desirable location.

Conditions of Approval

FDPs are subject to the adopted PUD Conditions of Approval, most recently revised in 2014. There are no proposed changes to the adopted Conditions of Approval at this time.

Open Space

In order to comply with the underlying zoning regulations, the project should provide 150 square feet of group open space per dwelling unit (or 75 square feet of private open space per unit). Although the project includes nicely designed open space for each site, the quantity does not meet the required minimum. The PUD regulations allow relaxation of development standards (such as the quantity or dimensions of required open space) for the purpose of designing an integrated development. Staff believes that an appropriate design solution is to include the public paseo between the F2 and F3 sites (essentially, Water Street) in the design and delivery of the open space requirement for the Sites D and F2 FDPs. As such, the paseo would be designed to provide a unique public access experience rich in furnishings, amenities and public art. At this

time, the applicant has provided images suggesting the design intent for Water Street. Staff recommends that the Planning Commission direct staff to work with the applicant to ensure delivery of Water Street improvements consistent with the design intent reflected in the plans.

Exterior Materials

In general, the proposed exterior materials, including the extensive and high-quality glazing, are acceptable to staff. However, one of the primary exterior materials proposed for both sites is stucco. Stucco, while appropriate to many single-family homes and certain styles of building, can appear cheap and wear quickly on larger, contemporary-style buildings. Staff is concerned that the material will negatively affect the appearance of both buildings, the building at Site D in particular. Site D is an important site for Oakland, located at the intersection of two primary arterials streets, Broadway and Embarcadero, and at the edge of the Oakland Estuary, a regional destination. Using high-quality, appropriate exterior materials at this location is critical to the success of the building as a landmark for Oakland. Staff recommends that the Planning Commission direct staff to work with the applicant to ensure high-quality exterior materials and to minimize the use and appearance of stucco as an exterior material, particularly on Site D.

D2 Site Color

The D2 site is well-designed with a dynamic roof line, dramatic soffit detailing and large expanses of glazing. However, the dark base color of the building is cool and recedes into the background. The D2 site is a gateway site in Oakland. The building at that site should have a signature, welcoming color scheme that reflects its location on the Oakland waterfront and near the Port of Oakland. Staff recommends that the Planning Commission direct staff to work with the applicant to refine a unique, signature exterior color palette that reflects the prominence of the Site D location and location adjacent to an active port on a prominent waterfront.

F2 Site Embarcadero Frontage

In general, the F2 building is well-articulated and responsive to the conditions on each side of the proposed building. The applicant has revised the façade facing Embarcadero to reduce mass and monotony. Staff recognizes that to address the noise condition adjacent to the very active railroad right-of-way adjacent to the Embarcadero building elevation is difficult without minimizing the articulation and openings on that side of the building. Staff believes that the current design iteration maximizes articulation and openness while ensuring compliance with building codes. However, the ground floor of the building is imposing and uninviting to passersby. Staff recommends that the Planning Commission direct staff to work with the applicant to reduce the appearance of the parking garage along the ground floor, and to soften the pedestrian experience with lighting, landscaping and visually interesting experiences that could include storefront display windows, murals or other forms of public art.

Parking Variance

The applicant is requesting a minor variance to allow reduced parking for the residential uses in the PUD. Specifically, the proposal is for .25 parking space per unit to be provided between the two residential buildings. The City of Oakland PUD regulations allow for project parking to be provided anywhere on-site and not specifically on the parcel served by the use requiring parking. As such, the PUD includes ample project parking in the Site G garage (located on Second Street, between Harrison and Alice Streets) to comply with the PUD and underlying zoning requirements. The Site G garage is currently under a long-term lease to the project applicant and, as such, can provide additional parking to satisfy the parking requirement of one space per residential unit included in the Oakland Planning Code. However, the Site G garage is owned by the Port of Oakland (under jurisdiction of State Lands), which cannot commit to dedicating parking spaces therein to the residential use after the lease has expired. In the future, at a minimum, parking in the Site G garage would be available to residential users on a first-come, first-served basis. There is currently ample parking availability in the Jack London District (particularly in the Washington Street and Site G garages), and there is likely to be even more availability in the future with evolving modes of travel. Staff believes that there is ample available public parking in the vicinity to comfortably serve the residential (and all PUD) uses into the future.

RECOMMENDATION

Staff finds both development proposals for Sites D and F2 to be well-designed, sensitive to their respective settings, and unique enough to work in synergy with the Jack London location to contribute to the regional destination quality of the area. Staff recommends the following:

- Approve the Site D FDP (case file PUD13170-PUDF01), subject to the attached findings; and
- Approve the Site F2 FDP (case file PUD13170-PUDF02), subject to the attached findings;
- Direct staff to work with the applicant prior to issuance of construction-related permits to ensure delivery of the following items:
    - Water Street improvements to be consistent with design intention indicated on plans;
    - Provision of high-quality exterior materials, minimizing the use and appearance of stucco as an exterior material, particularly on Site D;
    - Refinement of a unique, signature exterior color palette that reflects the prominence of the Site D location and location adjacent to an active port on an internationally significant waterfront;
    - On Site F2, reduce the appearance of the parking garage along the ground floor, and soften the pedestrian experience with lighting, landscaping and visually interesting experiences that could include storefront display windows, murals or other forms of public art; and
    - Ensure provision of adequate bike parking and recycling space to comply with applicable zoning regulations.

- Approve the parking variance to allow .25 parking space per residential unit to be provided between the Sites D and F2 sites.

Prepared by:

CATHERINE PAYNE
Planner IV

Approved by:

Robert D. Merkamp
Development Planning Manager

Approved for forwarding to the Planning Commission:

Darin Ranelletti, Interim Director
Planning and Building Department

Attachments:
A. Proposed Project Plans

NOTE: The EIR and Addendum was published and made publically available on May 9, 2014. The EIR and Addendum are provided under separate cover to the Planning Commission and are available to the public at 250 Frank Ogawa Plaza, Suite 3315, Oakland CA 94612 during normal business hours and at http://www2.oaklandnet.com/government/o/PBN/OurServices/Application/DOWD009158 (see line item #19).

FINDINGS FOR APPROVAL

The Jack London District PUD Sites D and F2 FDPs meet the required findings for compliance with:

- California Environmental Quality Act; and
- Oakland Planning Code Sections:
    - 17.140.040 (Final Development Plan Criteria)
    - 17.136.050 (Regular Design Review Criteria)
    - 17.148.050 (Variance Findings)

Required findings are shown in **bold** type; explanations as to why these findings can be made are in normal type. The project's conformance with the following findings is not limited to the discussion below, but is also included in all discussions in this report, the CEQA Analysis Document, and elsewhere in the record.

CEQA FINDINGS

The planned Jack London District PUD, approved in 2004, fully evaluated environmental impacts of the project, in accordance with all applicable requirements. Specifically, the City of Oakland prepared and certified an Environmental Impact Report (EIR) under the California Environmental Quality Act (CEQA). The City Council certified the EIR for the existing project approvals on June 15, 2004. The EIR considered an envelope of development of up to 960,700 square feet of commercial uses.

In 2014, the City approved revisions to the PUD. At that time, in accordance with CEQA, the City reviewed and analyzed the proposed project changes and other relevant information to determine whether circumstances requiring the preparation of a subsequent or supplemental EIR exist. The project approved in 2014 allows development of up to 1,287,700 net new gross square feet (gsf) of commercial and residential uses (including up to 665 dwelling units and a "Maximum Commercial Scenario" that could develop up to 960,700 net new gsf of commercial uses--similar to the project analyzed in the 2004 EIR). Based upon substantial information, the City determined that none of those circumstances were present. Therefore, the appropriate CEQA documentation was an Addendum. An Addendum is appropriate when none of the circumstances that require a supplemental or subsequent EIR pursuant to CEQA Guidelines Section 15162 have occurred, specifically:

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Planned Unit Development Findings

Section 17.140.040 Final Development Plan Finding
The final plan shall conform in all major respects with the approved preliminary development plan.

The FDPs conform in all respects to the preliminary development plan.
The Site D project is within the allowable residential and commercial densities, height and massing envelope included for that site in the PUD.
The Site F2 project is also within the allowable residential and commercial densities, height and massing envelope included for that site in the PUD

Regular Design Review

17.136.050--Regular design review criteria.
Regular design review approval may be granted only if the proposal conforms to all of the following general design review criteria, as well as to any and all other applicable design review criteria:

A. For Residential Facilities.

1. That the proposed design will create a building or set of buildings that are well related to the surrounding area in their setting, scale, bulk, height, materials, and textures:

   The proposed FDPS are both within the four- to eight-story height context of the surrounding area. The two buildings support the unique character of the Jack London District waterfront by incorporating references to the working Port and warehouse district in terms of design character, scale and massing. The Site D project is referential to Port activities, including cantilevered and overhang elements, offset stacked grids, and colors inspired by Port cranes, stacked containers, and the container colors on the trains that pass by immediately adjacent to the site. The F2 site includes large openings and vertical patterns that play off of the nearby Warehouse District.

2. That the proposed design will protect, preserve, or enhance desirable neighborhood characteristics;

   As noted above, the proposed FDPs are both within the four- to eight-story height context of the surrounding area. The two buildings support the unique character of the Jack London District waterfront by incorporating references to the working Port and warehouse district in terms of design character, scale and massing. The Site D project is referential to Port activities, including cantilevered and overhang elements, offset stacked grids, and colors inspired by Port cranes, stacked containers, and the container colors on the trains that pass by immediately adjacent to the site. The F2 site includes large openings and vertical patterns that play off of the nearby Warehouse District.

3. That the proposed design will be sensitive to the topography and landscape.

   As noted above, the proposed FDPs are both within the four- to eight-story height context of the surrounding area. As such, the projects do not result in a visual or physical barrier to the waterfront on which they are located and blend into the surrounding working port, warehousing and loft area.

4. That, if situated on a hill, the design and massing of the proposed building relates to the grade of the hill;

NA

5. That the proposed design conforms in all significant respects with the Oakland General Plan and with any applicable design review guidelines or criteria, district plan, or development control map which have been adopted by the Planning Commission or City Council.

   The revision to the FDP does not significantly alter the scale size or exterior treatment of any building compared to the approved project. The design continues to comply with the DA, the PUD and the design guidelines for the MacArthur Transit Village and supports the moderately dense, mixed-use scale and character of the development and surrounding neighborhood.

B. For Nonresidential Facilities and Signs.

1. That the proposal will help achieve or maintain a group of facilities which are well related to one another and which, when taken together, will result in a well-composed design, with consideration given to site, landscape, bulk, height, arrangement, texture, materials, colors, and appurtenances; the relation of these factors to other facilities in the vicinity; and the relation of the proposal to the total setting as seen from key points in the surrounding area. Only elements of design which have some significant relationship to outside appearance shall be considered, except as otherwise provided in Section 17.136.060;

   NA

2. That the proposed design will be of a quality and character which harmonizes with, and serves to protect the value of, private and public investments in the area;

   NA

3. That the proposed design conforms in all significant respects with the Oakland General Plan and with any applicable design review guidelines or criteria, district plan, or development control map which have been adopted by the Planning Commission or City Council.

   NA

C. For Local Register Properties that are not Landmarks or located in the S-7 or S-20 Zone:

1. That for additions or alterations, the proposal will not substantially impair the visual, architectural, or historic value of the affected site or facility. Consideration shall he given to design, form, scale, materials, texture, lighting, landscaping, Signs, and any other relevant design element or effect, and, where applicable, the relation of the above to the original design of the affected facility.

   NA

D. For Potential Designated Historic Properties that are not Local Register Properties: That for additions or alterations,

1. The design matches or is compatible with, but not necessarily identical to, the property's existing or historical design; or
2. The proposed design comprehensively modifies and is at least equal in quality to the existing design and is compatible with the character of the neighborhood; or
3. The existing design is undistinguished and does not warrant retention and the proposed design is compatible with the character of the neighborhood.

NA

E. For Retaining Walls:

1. That the retaining wall is consistent with the overall building and site design and respects the natural landscape and topography of the site and surrounding areas;

NA

2. That the retaining wall is responsive to human scale, avoiding large, blank, uninterrupted or undesigned vertical surfaces;

NA

3. That the retaining wall respects the natural topography, avoiding obvious scars on the land;

NA

4. That the proposed design conforms in all significant respects with the Oakland General Plan and with any applicable design review guidelines or criteria, district plan, or development control map which have been adopted by the Planning Commission or City Council.

NA

Minor Variance

17.148.050 Findings required.
A. With the exception of variances for Adult Entertainment Activities or Sign Facilities, a variance may be granted only upon determination that all of the following conditions are present:

1. That strict compliance with the specified regulation would result in practical difficulty or unnecessary hardship inconsistent with the purposes of the zoning regulations, due to unique physical or topographic circumstances or conditions of design; or, as an alternative in the case of a minor variance, that such strict compliance would preclude an effective design solution improving livability, operational efficiency, or appearance.

The project includes ample parking designed to meet the requirements and demand for on-site parking, while being located just outside of the waterfront itself. In fact, the Site G parking garage was constructed by the applicant specifically to meet the parking needs of the adopted PUD, and is currently under long-term lease to the applicant. However, the underlying land owner, the Port of Oakland, cannot commit to reserving any of the available parking specifically for the residential land use in the future. Should the current long-term lease of the garage not be renewed, parking would be available to the general public, including residential tenants, on a first-come/first-served basis. That said, the amount of existing on-site parking more than accommodates the required parking for the project and parking demand in the future. Providing additional on-site parking would result in an inordinate amount of prime waterfront land to be occupied by parking instead of desirable active uses. In addition, providing less parking on the waterfront residential sites results in more active uses on the ground floor, supporting the desired lively character of the Jack London waterfront.

2. That strict compliance with the regulations would deprive the applicant of privileges enjoyed by owners of similarly zoned property; or, as an alternative in the case of a minor variance, that such strict compliance would preclude an effective design solution fulfilling the basic intent of the applicable regulation.

The PUD provides an ample amount of parking for all project uses, in compliance with the City of Oakland parking regulations. The variance would acknowledge that, in the future, only a small portion of that existing, available parking could be reserved specifically for the residential uses. Constructing additional parking to meet the specific requirements of the underlying parking regulations would result in an over-parked, less active waterfront contrary to the City's goals and objectives. Additional parking would reduce the ability to provide active ground floor uses that support the desirable regional destination quality of the Jack London District.

3. That the variance, if granted, will not adversely affect the character, livability, or appropriate development of abutting properties or the surrounding area, and will not be detrimental to the public welfare or contrary to adopted plans or development policy.

Reducing the amount of parking reserved specifically for residential uses will not affect the availability of on-site parking. In addition, it is likely that car ownership will decrease throughout the life of the project due to new modes of travel, including ride sharing and driverless cars and increased use of mass transit; this will result in less demand for parking over time.

4. That the variance will not constitute a grant of special privilege inconsistent with limitations imposed on similarly zoned properties or inconsistent with the purposes of the zoning regulations.

The City of Oakland recently reduced the amount of required on-site parking throughout much of Oakland. This request is consistent with the trend towards providing less on-site parking. In addition, the project includes ample parking that, although not dedicated to the residential uses, is available to residential users as well as to the general public.

5. That the elements of the proposal requiring the variance (e.g., elements such as buildings, walls, fences, driveways, garages and carports, etc.) conform with the regular design review criteria set forth in the design review procedure at Section 17.136.050

The parking variance would support design review criteria by allowing the project to better achieve General Plan goals for providing a retail entertainment and dining destination by incorporating plentiful active ground floor uses. Extensive on-site parking would result in a significant amount of unactivated space, both horizontally and vertically.

6. That the proposal conforms in all significant respects with the Oakland General Plan and with any other applicable guidelines or criteria, district plan, or development control map which have been adopted by the Planning Commission or City Council.

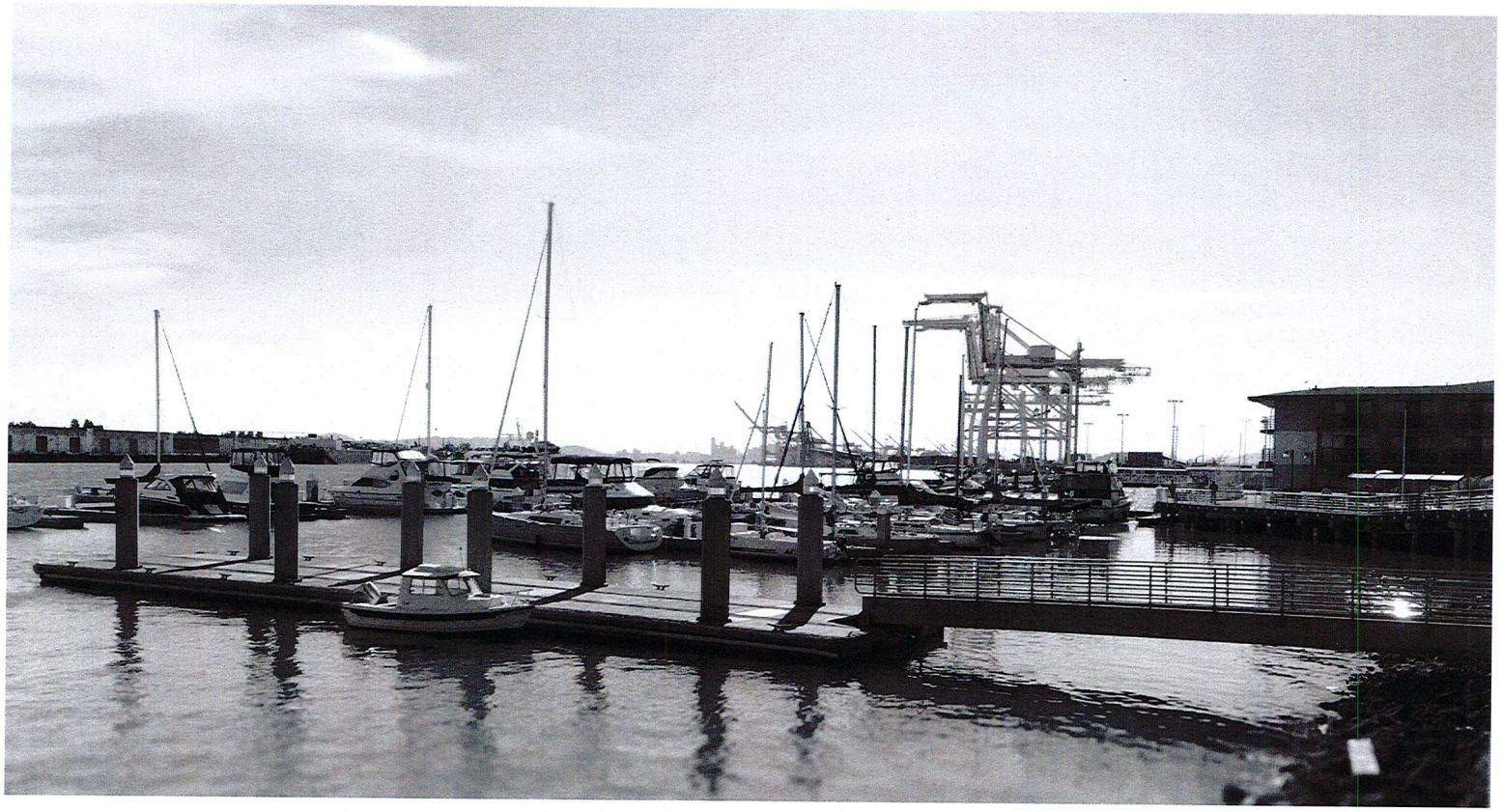
The parking variance would allow the project to better achieve General Plan goals for providing a retail entertainment and dining destination by incorporating plentiful active ground floor uses. Extensive on-site parking would result in a significant amount of unactivated space, both horizontally and vertically. In addition, Oakland recently reduced on-site parking requirements in many zoning districts; the parking variance would be in the spirit of these recent Planning Code updates.

7. For proposals involving one (1) or two (2) residential dwelling units on a lot: That, if the variance would relax a regulation governing maximum height, minimum yards, maximum lot coverage or maximum floor area ratio, the proposal also conforms with at least one of the following additional criteria:

a. The proposal when viewed in its entirety will not adversely impact abutting residences to the side, rear, or directly across the street with respect to solar access, view blockage and privacy to a degree greater than that which would be possible if the residence were built according to the applicable regulation and, for height variances, the proposal provides detailing, articulation or other design treatments that mitigate any bulk created by the additional height; or

b. Over sixty percent (60%) of the lots in the immediate vicinity are already developed and the proposal does not exceed the corresponding as-built condition on these lots and, for height variances, the proposal provides detailing, articulation or other design treatments that mitigate any bulk created by the additional height. The immediate context shall consist of the five (5) closest lots on each side of the project site plus the ten (10) closest lots on the opposite side of the street (see illustration I-4b); however, the Director of City Planning may make an alternative determination of immediate context based on specific site conditions. Such determination shall be in writing and included as part of any decision on any variance.

NA



JACK LONDON PARCEL D

PLANNING COMMISSION HEARING

OAKLAND, CALIFORNIA

DEVELOPER

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Attachment A

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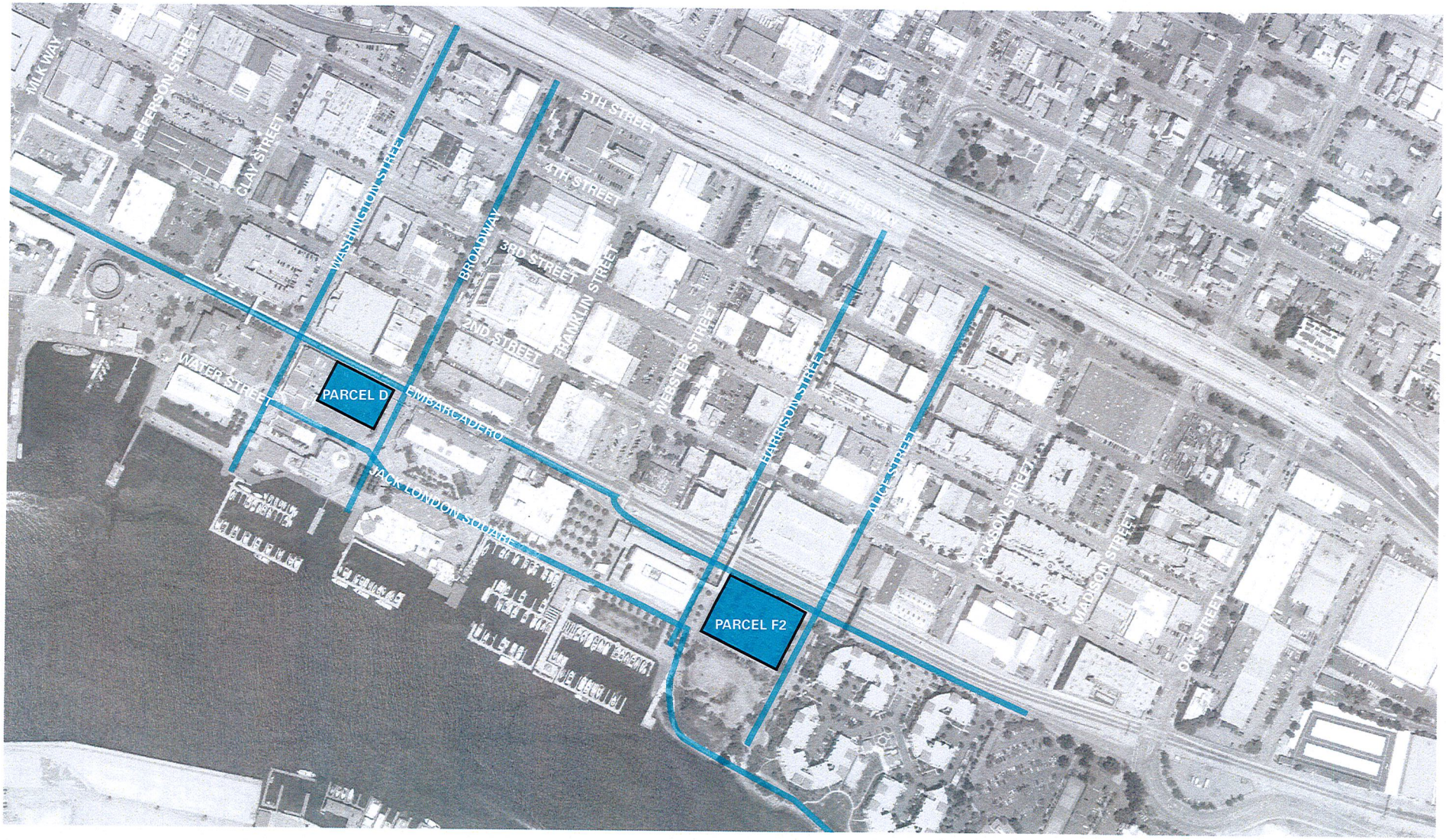

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OVERALL SITE PLAN - MAP OF CONTEXTUAL VIEWS

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Jack London Square - Parcel D

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G-2.1

Embarcadero (Facing North)

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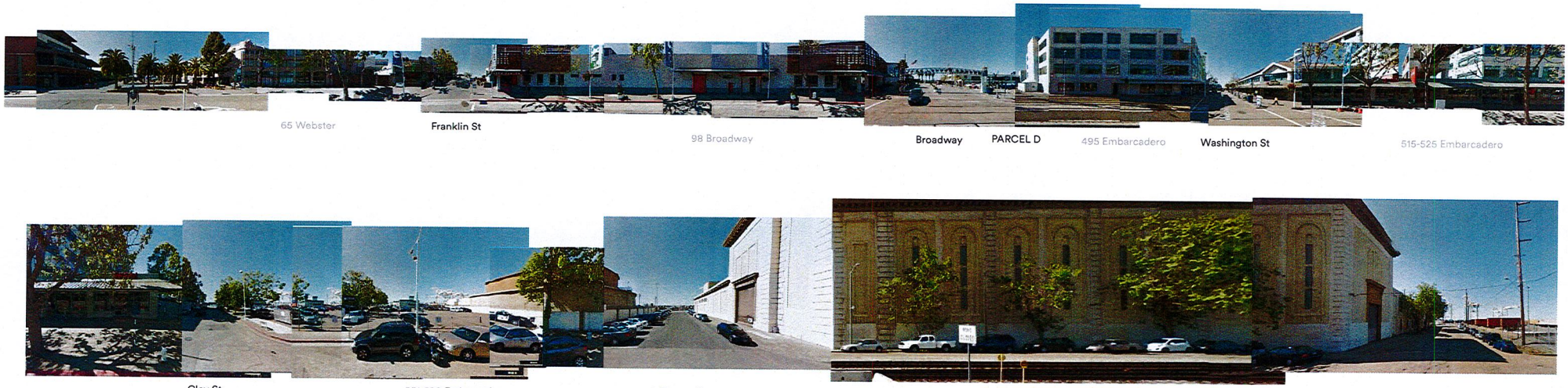
Embarcadero (Facing South)



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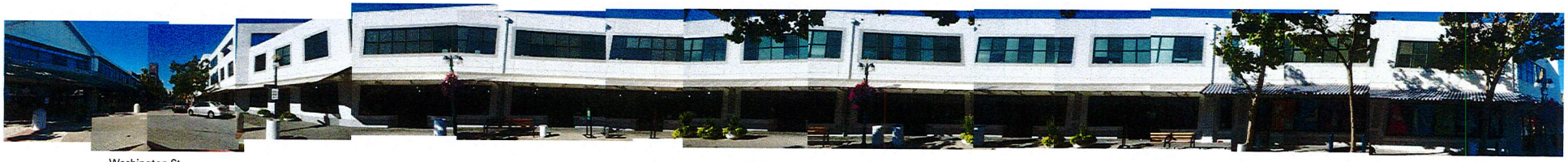
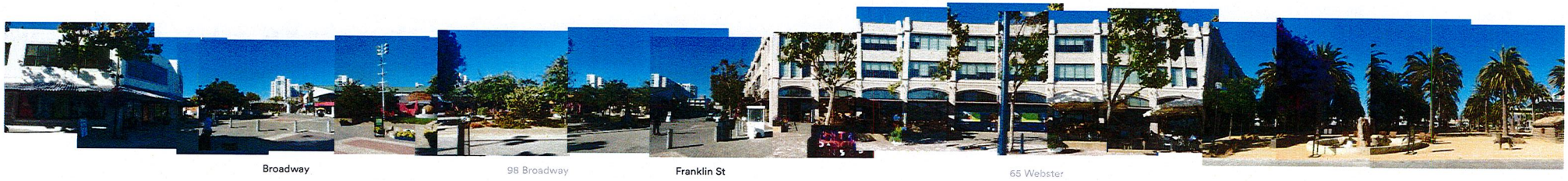
Water St & SF Bay Trail (Facing North)





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Water St & SF Bay Trail (Facing South)

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G-2.5

Washington St (Facing West)

Waterfront | 501-599 Water | Water St | 505 Embarcadero | Embarcadero | 101 Washington

2nd St | 201 Washington | 215 Washington | 221 Washington | 3rd St | 510 3rd St | 317 Washington | 4th St | 5th St | I880 Nimitz Fwy

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Broadway (Facing East)

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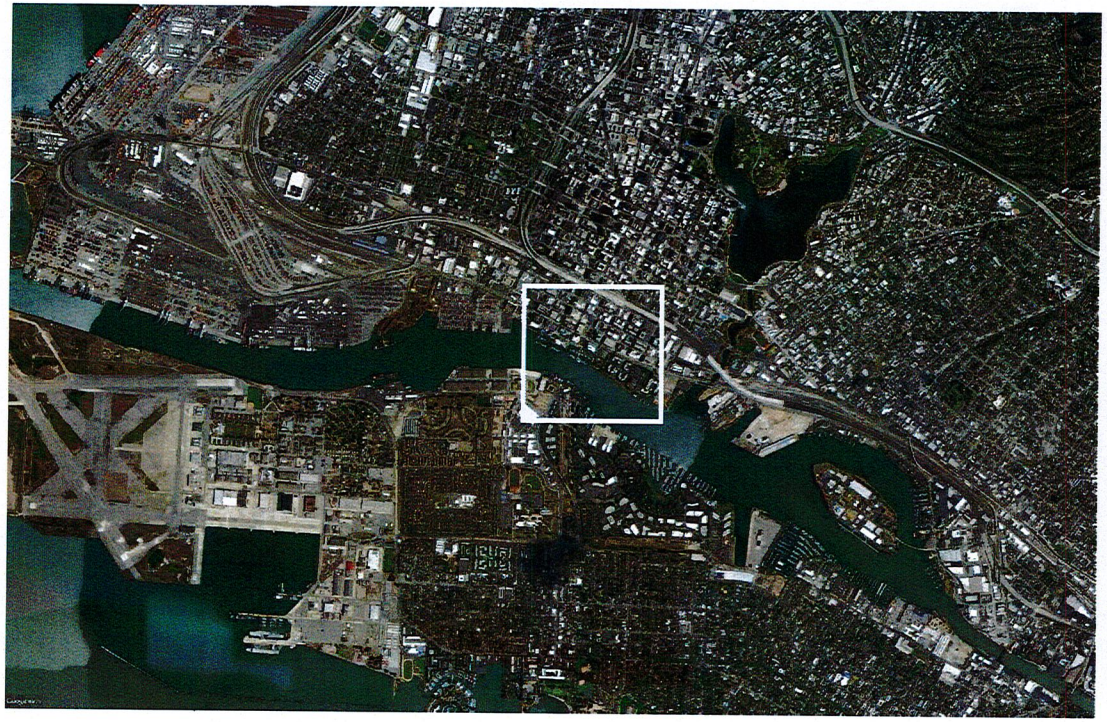

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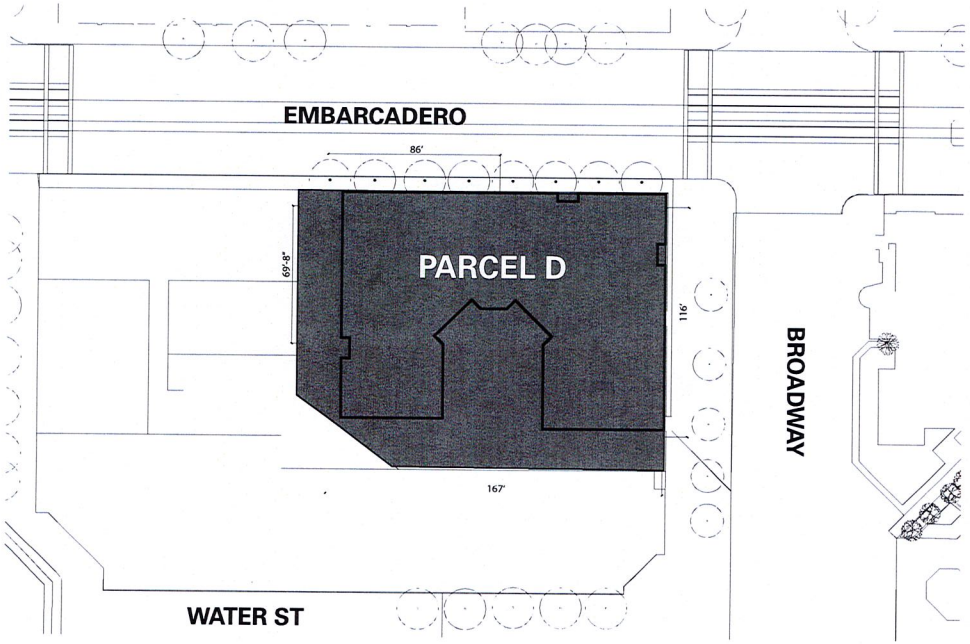
D-3.0



SITE PLAN

Item	Codes	FDP - Proposed
Regulating Agency	City of Oakland	
Governing Regulations	Approved Entitlements Case #ER03004, PUD13170, DA13171 (Most recently amended 9/23/14) Oakland Planning Code (Revised through May 2015)	
Zoning Classification	C-45; PUD	
General Plan Classification	RDE-2	
Land Use	Mixed Use (PDP high-rise scheme- 250,775 sf)	132,484 sf
Density (Unit Count)	Up to 665 Units to be allocated between Sites D & F2 (PDP high-rise scheme- 167 units Mid-rise scheme- xxx units)	135 Units
FAR	7:1, set by PUD, can be applied in an additive manner w/ Parcel F2	103,952 gsf
Building Height	193', 17 stories Allowed (PDP high-rise scheme- 179', 17 stories)	85' (8 stories)
Open Space	150 sf group space/unit (private space reduces requirement by 50%)	Level 02: Private usable open space=1,800 sf Total open space= 1,800 sf Ground Floor Street Improvements=6,900sf Code required open space has been provided via public dedication of lands within the PUD
Parking	1:1 parking on-site (C-45 Zoning) Up to 50% of spaces may be compact or intermediate. Regular Space = 18'x 8.5'(10.5' at wall) Intermediate Space = 16.5'x 8'(10' at wall) Compact Space = 15'x 7.5'(9.5' at wall) Maneuvering Aisle for 90 deg parking = 21' 1 space / 1ksf retail	.5:1 parking 68 spaces for resi. 2,500 / 1k = 3 retail 71 spaces total 25% on-site parking PLUS 25% mechanical parking OR 25% district-wide parking including site G
Loading	No on-site loading required per approved loading variance	1 off-site space
Bicycle Parking (Long Term)	1 space / 4 units & 1 space / 12ksf retail	135 / 4 = 31 2,500 /12k = 1 31+1 = 32 LT
Bicycle Parking (Short Term)	1 space/ 20 units & 1 space / 2ksf retail	135 / 20 = 7 2,500 / 2k = 2 7+2 = 9 ST
Recycling Space	2 CF space per unit Retail – 2 CF / 1ksf	135x2 = 270 CF 2,500/1k = 3CF 270 + 3 = 273 CF

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ZONING ANALYSIS
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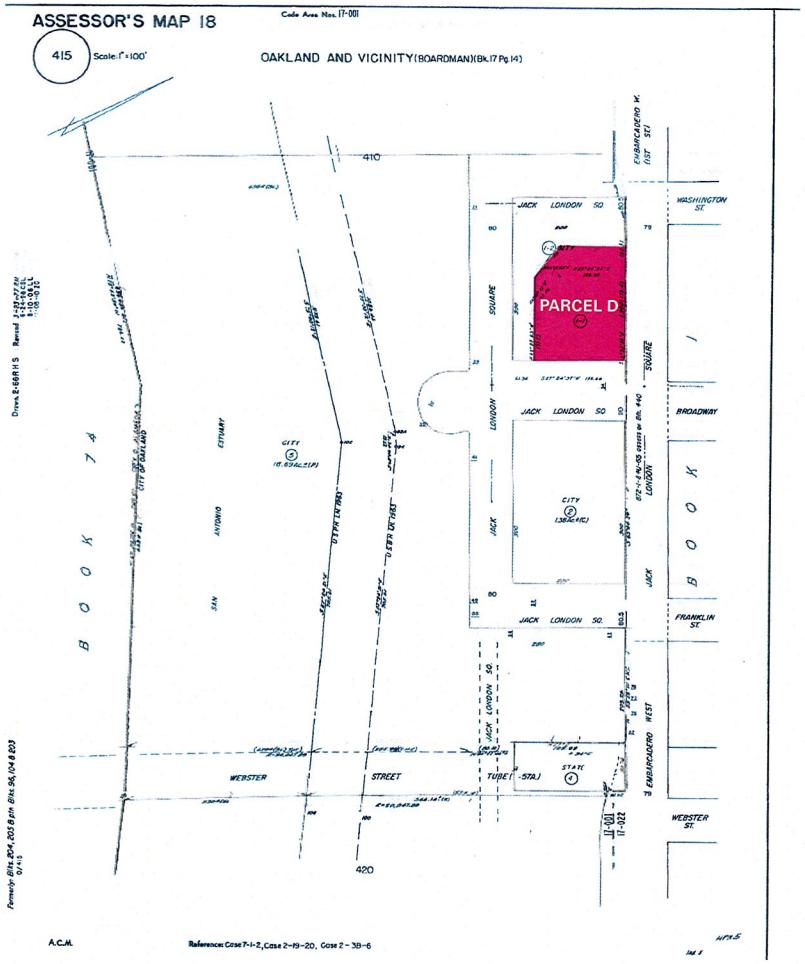

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ASSESSOR'S MAP
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D-3.2

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AERIAL VIEWS
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1: View of Parcel D looking west

2: View of Parcel D looking east

3: View of Parcel D looking southwest

4: View of Parcel D looking west

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SITE PHOTOS
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LEED-NC 2009 Checklist

Jack London Square

10.11.2016

Target = Silver

Certified 40-49 points
Silver 50-59 points
Gold 60-79 points
Platinum 80 points and above

Project Totals	Yes	?	No
	52	20	38

Checklist Certification Level: (Does not include ? credits)
Silver

Category	Credit	Credit Name	Yes	?	No	Phase	Status	Comments	Responsibility
Sustainable Sites	Prereq 1	Construction Activity Pollution Prevention	Y			C	-	Create and implement an erosion and sedimentation control plan-code requirement	G.C.
	Credit 1	Site Selection	1			D	-	Prohibits hardscape on certain site conditions (Prime farmland, wetland, etc.)	SCB
	Credit 2	Development Density & Community Connectivity	5			D	-	Will easily qualify because of urban location	SCB
	Credit 3	Brownfield Redevelopment		1		-	-	Is there any remediation required of existing site?	G.C./Owner
	Credit 4.1	Alternative Transportation, Public Transportation Access	6			D	-	Meets through adjacency to bus lines (+1pt for Regional Priority) (+1pt for Exemplary Performance Likely)	SCB
	Credit 4.2	Alt. Transportation, Bicycle Storage & Changing Rooms	1			D	-	Current unit mix requires covered spaces. Retail requires spaces and access to shower	SCB
	Credit 4.3	Alt. Transportation, Low-Emitting & Fuel-Efficient Vehicles	3			D	-	Several options available - including charging stations, car share, preferred parking for Low-Emitting & Fuel Efficient Vehicles	SCB
	Credit 4.4	Alt. Transportation, Parking Capacity	2			D	-	Credit requires one of the following: a) car pool parking b) shared zipcar or similar service c) ride share program residents	SCB
	Credit 5.1	Site Development, Protect or Restore Habitat		1		D	-	Requires landscape areas or green roofs with native or adapted vegetation for 20% of the site area	Landscape
	Credit 5.2	Site Development, Maximize Open Space	1			D	-	Pedestrian hardscape, green roof or ground level plantings total 20% of site area (+1pt for Exemplary Performance Likely)	SCB
	Credit 6.1	Stormwater Design, Quantity Control		1		D	-	Might be possible with green roof, etc.	Civil
	Credit 6.2	Stormwater Design, Quality Control		1		D	-	Permeable areas and adequate landscaping best way to meet requirements	Civil
	Credit 7.1	Heat Island Effect, Non-Roof	1			D	-	100% parking under cover (+1pt for Exemplary Performance Likely)	SCB
	Credit 7.2	Heat Island Effect, Roof	1			D	-	White roofing membrane, green roof or some combination (+1pt for Regional Priority)	SCB
	~~Credit 8~~	~~Light Pollution Reduction~~			1	n/a	-	Would require lighting to be reduced or windows shielded between 11 pm and 5 am public areas	Lighting
Water Efficiency	Prereq 1	Water Use Reduction	Y			D	-	Requires 20% reduction from baseline case	-
	Credit 1	Water Efficient Landscaping	2	2		D	-	50% reduction of potable water use for irrigation	Owner/SCB
	~~Credit 2~~	~~Innovative Wastewater Technologies~~			2	n/a	-	Low flow toilets and/or water re-use (+1pt for Regional Priority)	Landscape
	Credit 3	Water Use Reduction, 30-40% Reduction	2	2		D	-	30% savings over baseline (+1pt for Regional Priority for 40%)	Owner/SCB
Energy and Atmosphere	Prereq 1	Fund. Commissioning of Building Energy Systems	Y			C	-	Basic Commissioning	Owner/SCB
	Prereq 2	Minimum Energy Performance	Y			D	-	18% energy improvement to baseline case per ASHRAE 90.1 2007	Cx
	Prereq 3	Fundamental Refrigerant Management	Y			D	-	No or low costs. Zero CFC use required	Energy
	Credit 1	Optimize Energy Performance	4		15	D	-	18% better than baseline for 3 points - is now the prerequisite but still get points	MEP
	~~Credit 2~~	~~On-Site Renewable Energy~~			7	n/a	-	1% on site renewable energy (+1pt for Regional Priority)	Energy
	Credit 3	Enhanced Commissioning	2			C	-	Additonal consulting fees - recommended to get max. value from commissioning	Owner
	Credit 4	Enhanced Refrigerant Management		2		D	-	All mechanical units must comply	Cx
	Credit 5	Measurement & Verification		1	2	C	-	1 pt for registering project with EnergyStar portfolio manager tool and share project file with USGBC master account	MEP
	Credit 6	Green Power		2		C	-	2 year contract to purchase green power - low cost and decision can be deferred	Owner
Materials and Resources	Prereq 1	Storage & Collection of Recyclables	Y			D	-	Required to provide a strategy for the collection of recyclables with sufficient space dedicated for storage.	Owner/SCB
	~~Credit 1.1~~	~~Building Reuse, Maintain Existing Walls, Floors & Roof~~			3	n/a	-	Not applicable	-
	~~Credit 1.2~~	~~Building Reuse, Maintain Interior Non-Structural Elements~~			1	n/a	-	Not applicable	-
	Credit 2	Construction Waste Management	2			C	-	75% construction waste recycled - 95% feasible for Exemplary Performance	G.C.
	~~Credit 3~~	~~Materials Reuse~~			2	n/a	-	Typically too costly	-
	Credit 4	Recycled Content,	2			C	-	Requires 20% recycled content (by cost)	G.C./SCB
	Credit 5	Regional Materials	2			C	-	Requires 20% regional content (by cost)	G.C./SCB
	~~Credit 6~~	~~Rapidly Renewable Materials~~			1	n/a	-	If only option is for finish materials usually is expensive	-
	~~Credit 7~~	~~Certified Wood~~		1		n/a	-	50% wood by value is FSC certified	G.C.
Indoor Environmental Quality	Prereq 1	Minimum IAQ Performance	Y			D	-	Meet ASHRAE 62.1-2007 sections 4-7 or applicable local code, kitchen exhaust will be major issue	MEP
	Prereq 2	Environmental Tobacco Smoke (ETS) Control	Y			D	-	No smoking in public spaces and within 25' of building openings. • Requires signage to be posted at all buildng entrances (public, private / Ground, Amenity)	Owner
	~~Credit 1~~	~~Outdoor Air Delivery Monitoring~~			1	n/a	-	Outdoor airflow monitors required	MEP
	~~Credit 2~~	~~Increased Ventilation~~			1	n/a	-	Increase ventilation to at least 30% above ASHRAE 62.1-2007	MEP
	Credit 3.1	Construction IAQ Management Plan, During Construction	1			C	-	Protect/clean mechanical system during construction - Follow SMACNA 2007 IAQ Plan guidelines	G.C.
	~~Credit 3.2~~	~~Construction IAQ Management Plan, Before Occupancy~~			1	n/a	-	Conduct building flush-out prior to occupancy or conduct IAQ tests	G.C./Owner
	Credit 4.1	Low-Emitting Materials, Adhesives & Sealants	1			C	-	Must meet VOC requirements for material type	G.C./SCB
	Credit 4.2	Low-Emitting Materials, Paints & Coatings	1			C	-	Must meet VOC requirements for material type	G.C./SCB
	Credit 4.3	Low-Emitting Materials, Flooring Systems	1			C	-	Must meet VOC requirements for material type	G.C./SCB
	Credit 4.4	Low-Emitting Materials, Composite Wood & Agrifiber Prod.	1			C	-	Must meet VOC requirements for material type	G.C./SCB
	Credit 5	Indoor Chemical & Pollutant Source Control		1		D	-	Walkoff mats required at all entrances and MERV-13 filtration.	Owner/SCB
	Credit 6.1	Controllability of Systems, Lighting	1			D	-	Switches in each room of units and switches in common facilities	Lighting
	Credit 6.2	Controllability of Systems, Thermal Comfort	1			D	-	Need thermostats for common spaces and units.	MEP
	Credit 7.1	Thermal Comfort, Design	1			D	-	HVAC design ust meet ASHRAE 55-2004 - usually standard practice	MEP
	~~Credit 7.2~~	~~Thermal Comfort, Verification~~			1	n/a	-	Not applicable for residential projects	-
	Credit 8.1	Daylight & Views, Daylight		1		C	-	Daylight for 75% of regularly occupied areas of buildings - High glass percentage: likely too bright (+1pt for Regional Priority)	SCB
	Credit 8.2	Daylight & Views, Views	1			D	-	Direct outdoor line of sight view for 90% of all regularly occupied spaces.	SCB
Innovation in Design	Credit 1.1	Innovation in Design: Exemplary Performance - TBD	1			D	-	SCB to review - SSc4.1	SCB
	Credit 1.2	Innovation in Design: Exemplary Performance - TBD	1			D	-	SCB to review - SSc5.2	SCB
	Credit 1.3	Innovation in Design: Exemplary Performance - TBD	1			D	-	SCB to review - SSc7.1	SCB
	Credit 1.4	Innovation in Design: TBD		1		C	-	Green Cleaning / Integrated Pest Management / Pilot Credit	Owner/SCB
	Credit 1.5	Innovation in Design: TBD		1		C	-	Green Cleaning / Integrated Pest Management / Pilot Credit	Owner/SCB
	Credit 2	LEED® Accredited Professional	1			D	-		SCB
Regional Credit Priorities	Credit 1.1	Regional Priority Credit -SSc4.1	1			D	-	Meet requirements of SSc4.1	SCB
	Credit 1.2	Regional Priority Credit - SSc7.1	1			D	-	Meet requirements of SSc7.1	SCB
	Credit 1.3	Regional Priority Credit - WEc2 or WEc3		1		D	-	Meet requirements of WEc2 or Achieve 40% potable water reduction on WEc3	Owner/SCB
	Credit 1.4	Regional Priority Credit - EAc2 or EQc8.1		1		D	-	Meet requirements of EAc2 or IEQc8.1	Owner/SCB

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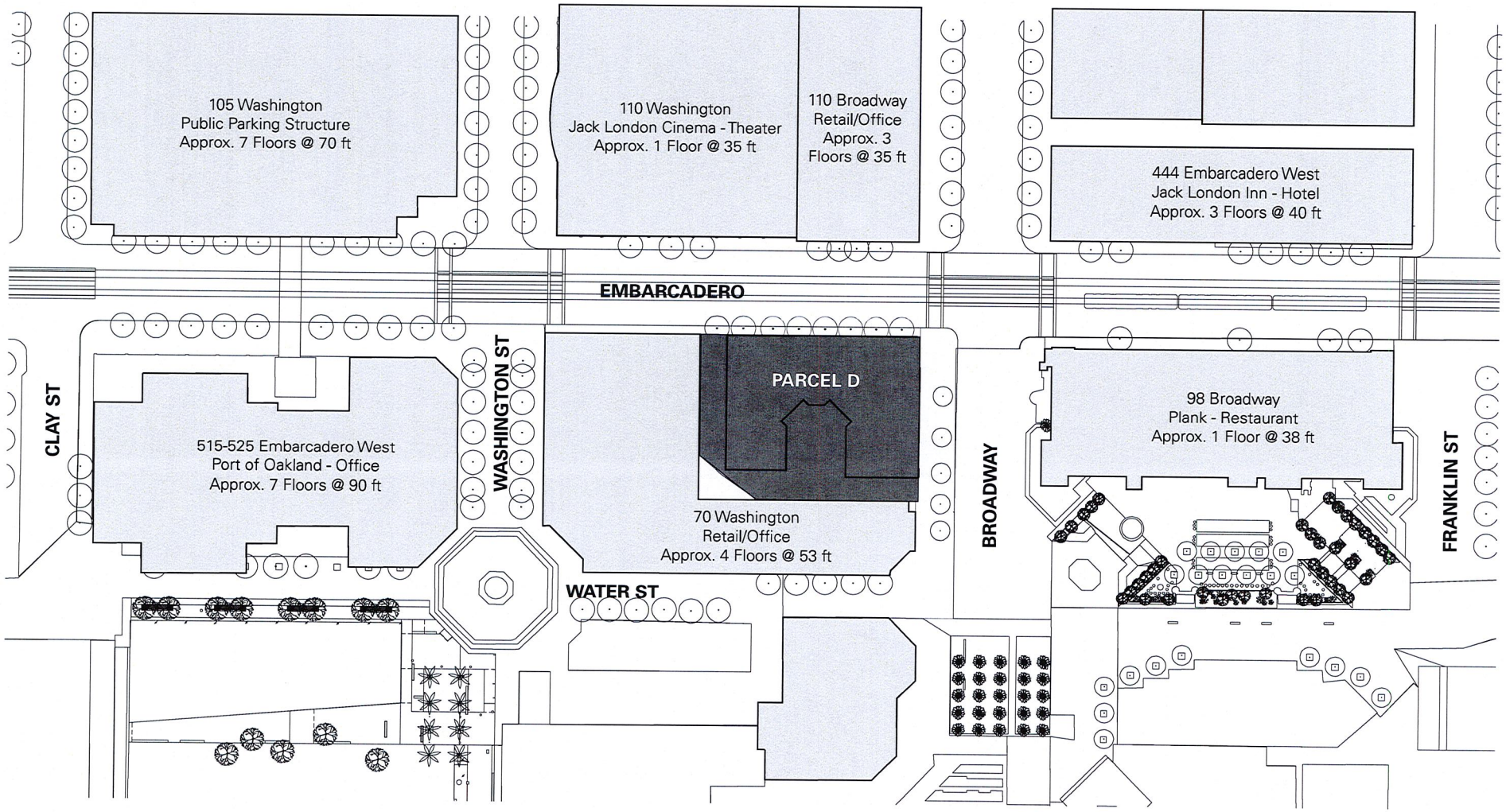
CIM

LEED SCORE CARD
PLANNING COMMISSION HEARING
Jack London Square - Parcel D
CIM Group

03 - 16 - 2017

2016041.000



0 50' 100'

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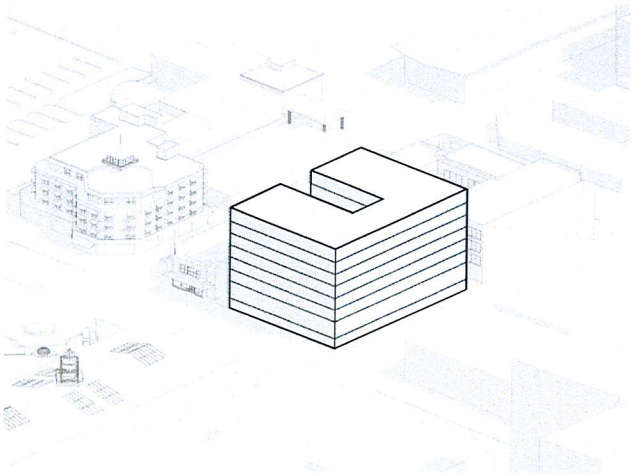
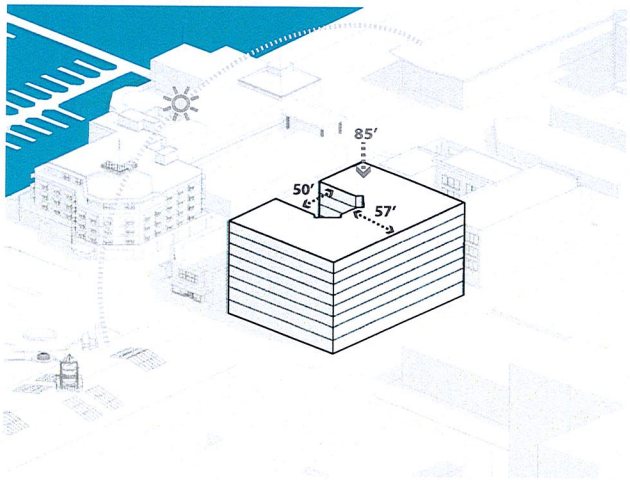
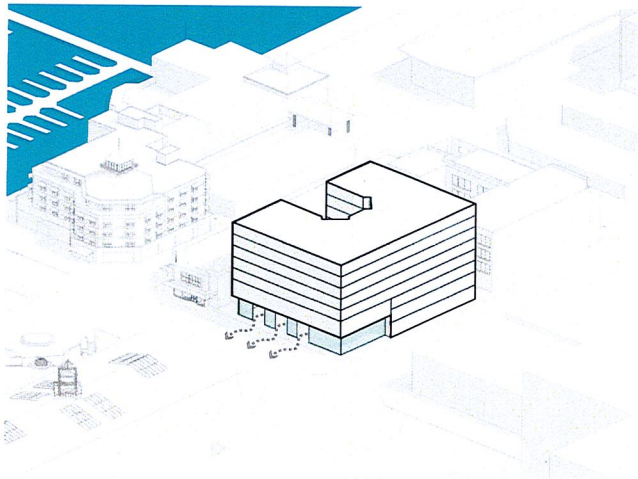
SITE MAP
PLANNING COMMISSION HEARING
Jack London Square - Parcel D
CIM Group

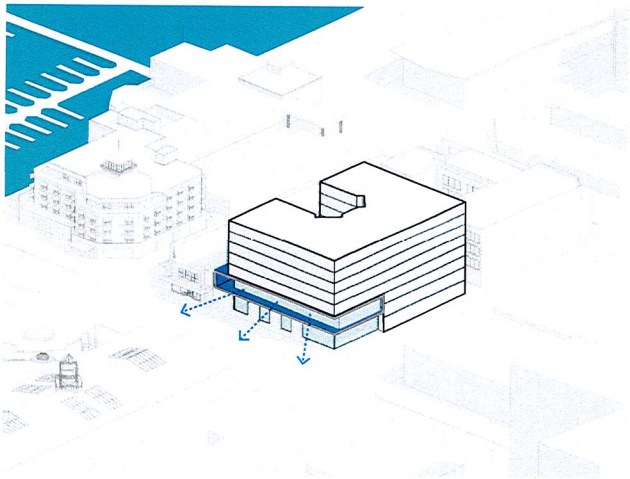
03 - 16 - 2017

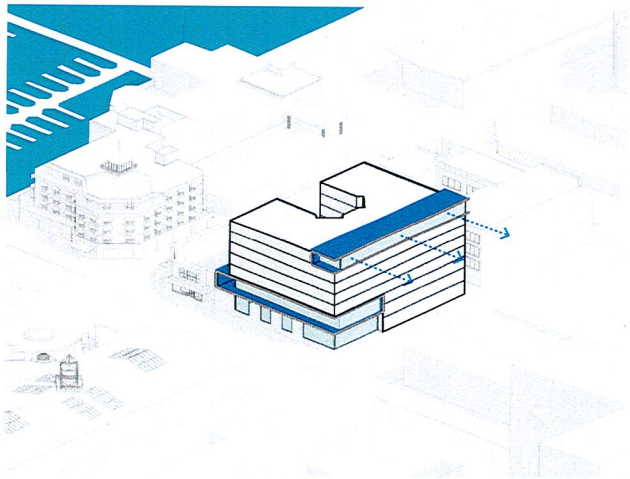
2016041.000











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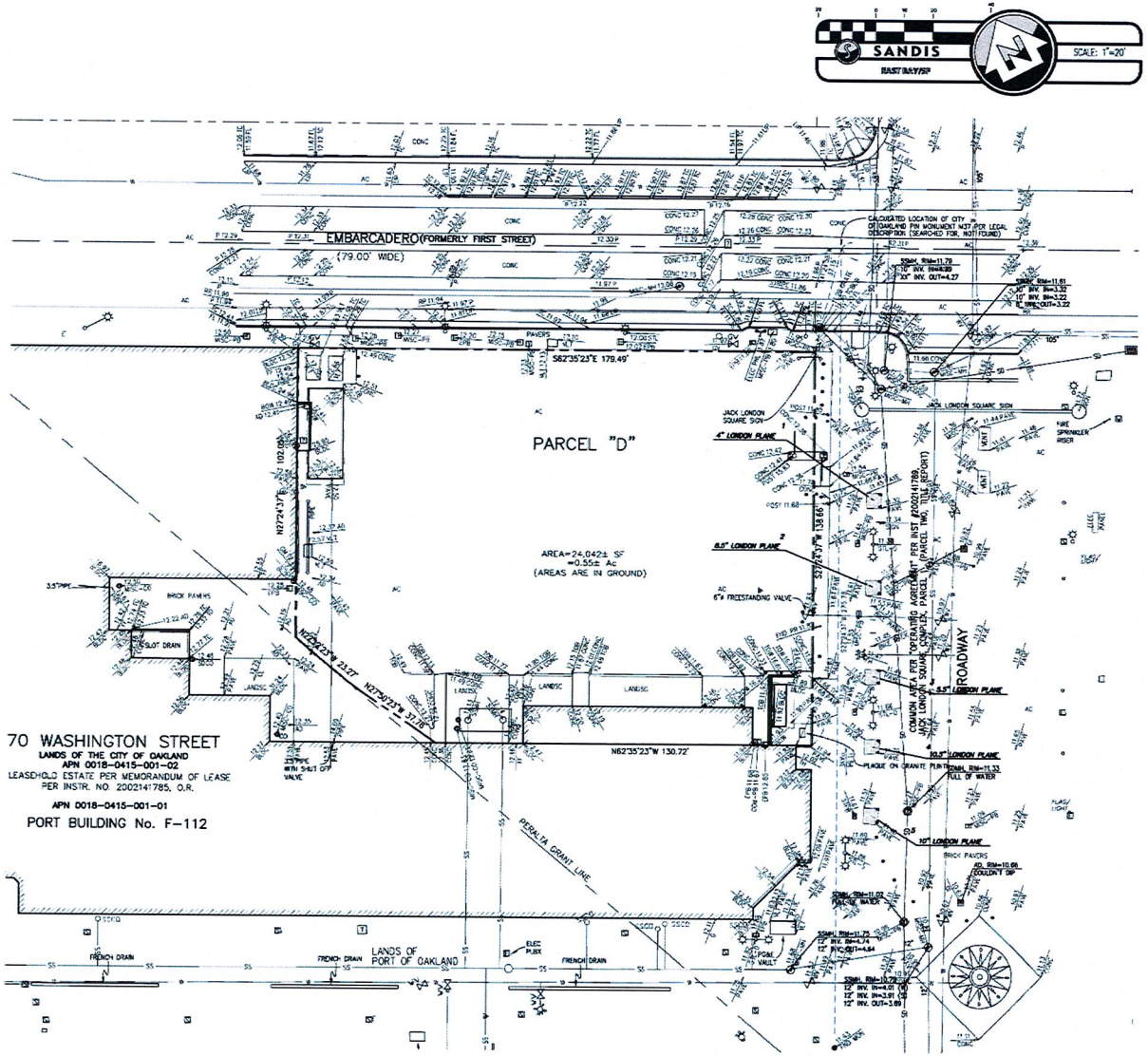
MASSING ANALYSIS
PLANNING COMMISSION HEARING
Jack London Square - Parcel D
CIM Group

03 - 16 - 2017

2016041.000



TREE NOTE

TREE SPECIES AND SIZE PER ARBORIST REPORT BY SBCA TREE CONSULTING DATED SEPTEMBER 11, 2015.

SURVEY NOTE

SURVEY INFORMATION SHOWN INCLUDES POINTS FROM SURVEYS CONDUCTED IN 2011 AND 2006. SOME INFORMATION SHOWN MAY REPRESENT OBJECTS WHICH HAVE SINCE BEEN DEMOLISHED.

BENCHMARK

THE BASIS OF ELEVATION FOR THIS SURVEY IS THE PORT OF OAKLAND MONUMENT No. H016, AN ALUMINUM DISK STAMPED "PORT OF OAKLAND SURVEY CONTROL" ON EMBARCADERO AT WEBSTER STREET.

ELEVATION = 11.838 FEET (PORT OF OAKLAND DATUM)

SURVEY UTILITY NOTE

THE TYPES, LOCATIONS, SIZES AND /OR DEPTHS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS TOPOGRAPHIC SURVEY ARE APPROXIMATE AND WERE OBTAINED FROM SOURCES OF VARYING RELIABILITY. ONLY ACTUAL EXCAVATION WILL REVEAL THE TYPES, EXTENT, SIZES, LOCATIONS AND DEPTHS OF SUCH UNDERGROUND UTILITIES. A REASONABLE EFFORT HAS BEEN MADE TO LOCATE AND DELINEATE ALL KNOWN UNDERGROUND UTILITIES. HOWEVER, THE ENGINEER CAN ASSUME NO RESPONSIBILITY FOR THE COMPLETENESS OR ACCURACY OF ITS DELINEATION OF SUCH UNDERGROUND UTILITIES WHICH MAY BE ENCOUNTERED, BUT WHICH ARE NOT SHOWN ON THIS SURVEY.

ABBREVIATIONS

AD	– AREA DRAIN
BLDC	– BUILDING CORNER
BLDL	– BUILDING LINE
BOLL	– BOLLARD
BOW	– BOTTOM OF WALL
CHKSHT	– SURVEY CHECK SHOT
CL	– CENTER LINE OR CLASS
CNPT	– SURVEY CONTROL POINT
COM–PB	– COMMUNICATIONS PULLBOX
CONC	– CONCRETE
DI	– DRAIN INLET
ELEC–MH	– ELECTRICAL MANHOLE
ELEC PNL	– ELECTRICAL PANEL
EP	– EDGE OF PAVEMENT
EPB	– ELECTRICAL PULLBOX
FDC	– FIRE DEPARTMENT CONNECTION
FF	– FINISHED FLOOR
FH	– FIRE HYDRANT
FL	– FLOW LINE
FND MON	– FOUND SURVEY MONUMENT
FND RR	– FOUND RAILROAD SPIKE (SURVEY BENCHMARK)
FNDCNPT	– FOUND SURVEY CONTROL POINT
G	– GROUND
GM	– GAS METER
GRATE	– DRAIN INLET GRATE
GV	– GAS VALVE
LIP	– LIP OF GUTTER
MISC–CO	– MISCELLANEOUS CLEANOUT
MISC–MH	– MISCELLANEOUS MANHOLE
MISC–PB	– MISCELLANEOUS PULLBOX
MISC–V	– MISCELLANEOUS VALVE
OH	– BUILDING OVERHANG
P	– PAVEMENT ELEVATION
PAVE	– PAVER ELEVATION
RR	– RAILROAD TRACK CENTERLINE
SDCO	– STORM DRAIN CLEANOUT
SSMH	– SANITARY SEWER MANHOLE
STL	– STREET LIGHT
STL–D	– DOUBLE–ARM STREET LIGHT
STL	– SINGLE–ARM STREET LIGHT
STPB	– STREET LIGHT PULLBOX
SW	– SIDEWALK
TC	– TOP OF CURB
TC@CB	– TOP OF CURB AT CATCH BASIN
TD	– TRENCH DRAIN
TH	– DOOR THRESHOLD
TOB	– TOP OF BANK
TOW	– TOP OF WALL
TWP	– SURVEY TEMPORARY WORK POINT
VLT	– VAULT
WV	– WATER VALVE

LEGEND

ALL DISTANCES AND DIMENSIONS ARE SHOWN IN FEET AND DECIMALS THEREOF.

	BUILDING FACE
	EDGE OF PAVEMENT
	BUILDING OVERHANG
	CURB LINE
SS	SANITARY SEWER
SD	STORM DRAIN
T	TELECOMMUNICATIONS LINE
W	WATER LINE
UGE	UNDERGROUND ELECTRIC LINE
X	FENCE
101 / 100	CONTOURS
HWS	HOT WATER SUPPLY
HWR	HOT WATER RETURN
CWS	CHILLED WATER SUPPLY
CWR	CHILLED WATER SUPPLY
TV / FA	TV / FIRE ALARM
TEL	TELECOM
480V	ELECTRIC
FG	(UNKNOWN UTILITY)
A	(UNKNOWN UTILITY)
	FOUND STANDARD CITY MONUMENT
SDMH	STORM DRAIN MANHOLE
SSMH	SANITARY SEWER MANHOLE
SSCO	SANITARY SEWER CLEANOUT
AD	AREA DRAIN
	ELECTRIC UTILITY BOX
	TELEPHONE PULL BOX
	CABLE TELEVISION BOX
	POWER POLE
	WATER VALVE
	GAS VALVE
	WATER METER
	ELECTROLIER
	ELECTRIC VALVE
	SIGN
	GUY WIRE
	FIRE HYDRANT
x 95.94	SPOT ELEVATION
	TREE WITH DRIPLINE, TYPE AND DIAMETER AS INDICATED

0 20' 40'

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CIM
INVESTING IN URBAN COMMUNITIES

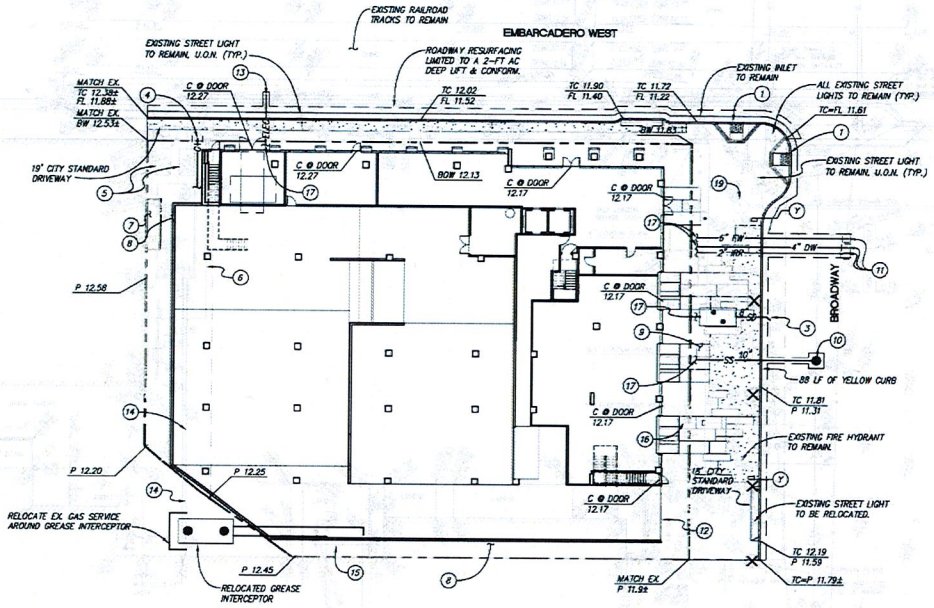
BOUNDARY AND TOPOGRAPHIC SURVEY
FINAL DEVELOPMENT PLAN
Jack London Square - Parcel D
CIM Group

PROFESSIONAL LAND SURVEYOR DAVID W. BOHAM No. 7047 Exp. 12/31/16 STATE OF CALIFORNIA

10-19-2016
1" = 20'
F2: 2016013.000
D: 2016041.000

C0.1

SANDIS
EAST BAY/SF
SCALE: 1"=20'



SHEET NOTES

1. CURB RAMP.
2. CONNECT TO EXISTING STORM DRAIN MANHOLE.
3. CONNECT TO EXISTING STORM DRAIN MAIN.
4. CONNECT TO EXISTING GAS LATERAL.
5. EXISTING ELECTRIC TRANSFORMER TO REMAIN. INSTALL TRAFFIC RATED COVER.
6. EXISTING GAS LATERAL TO BE REMOVED.
7. EXISTING BUILDING TO BE REMOVED.
8. EXISTING ONE-STORY STRUCTURE TO BE REMOVED.
9. REPLACE EXISTING PAVERS WITH NEW SIDEWALK.
10. CONNECT TO EXISTING SANITARY SEWER.
11. CONNECT TO EXISTING WATER MAIN.
12. ATM TO BE RELOCATED.
13. CONNECT TO EXISTING ELECTRICAL.
14. EXISTING UTILITY TO REMAIN. PROTECT IN PLACE.
15. EXISTING GREASE INTERCEPTOR TO BE RELOCATED.
16. EXISTING VALVE TO BE REMOVED. CUT AND CAP 5-FT FROM RIGHT-OF-WAY.
17. SEE MEP PLAN FOR CONTINUATION.
18. GAS METER. SEE MEP PLAN FOR DETAIL AND CONTINUATION.
19. EXISTING SIGN POST TO REMAIN.

LEGEND

- PROPERTY LINE
- SAWCUT LINE AT ASPHALT OR APPROXIMATE LIMIT OF PAVER REMOVAL
- EXISTING TREE TO BE REMOVED
- CURB PAINT CALLOUT. FLAG INDICATES DIRECTION OF CURB PAINT

0 20' 40'

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CIM
INVESTING IN URBAN COMMUNITIES

GRADING AND UTILITY PLAN
FINAL DEVELOPMENT PLAN
Jack London Square - Parcel D
CIM Group

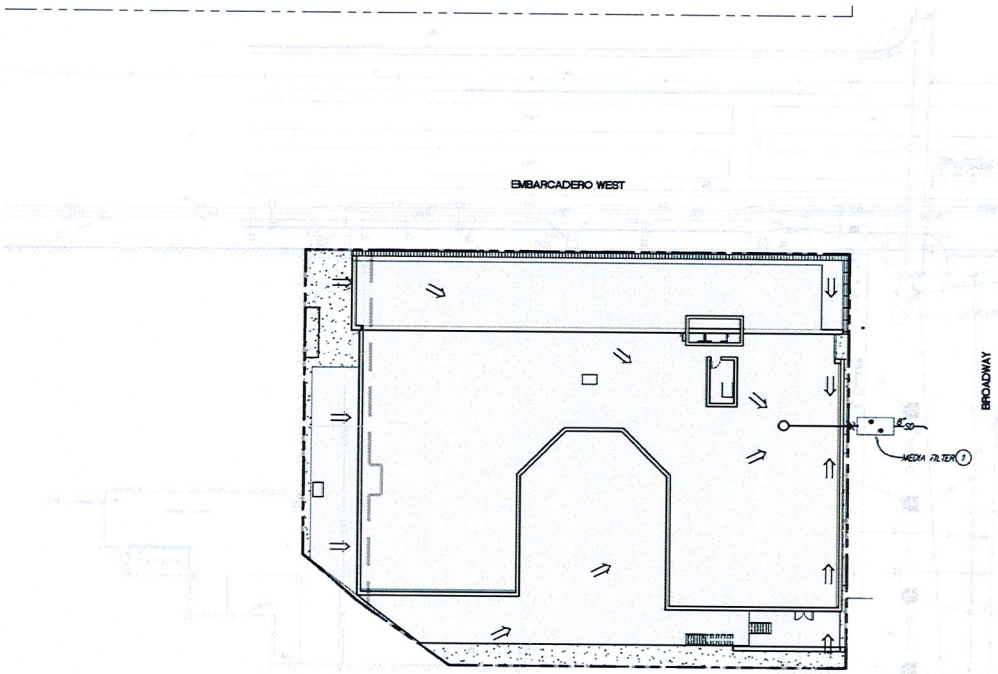
REGISTERED PROFESSIONAL ENGINEER MICHAEL A. KUTKUHELL No. 72870 Exp. 6/30/17 CIVIL STATE OF CALIFORNIA

10-19-2016
1" = 20'
F2: 2016013.00C
D: 2016041.00C

C3.0

SANDIS
EASTMAYER
SCALE: 1"=20'

PRELIMINARY STORMWATER TEATMENT MEASURES			
NEW IMPERVIOUS AREA (SF)			
AREA ID	ROOF	IMPERVIOUS	TOTAL
A1	21,588	0	21,588
A2	0	2,454	2,454



STORMWATER MANAGEMENT PLAN NOTES

1. THE PROPOSED PROJECT WILL INCLUDE MORE THAN 10,000 SQUARE FEET OF IMPERVIOUS SURFACE AND HAS THEREBY BEEN PLANNED TO COMPLY WITH THE PROVISION C.3 – NEW DEVELOPMENT AND REDEVELOPMENT OF THE MUNICIPAL REGIONAL STORMWATER PERMIT (ORDER NO. R2-2009-0074).
2. GENERAL STORMWATER QUALITY APPROACH – STORMWATER QUALITY FEATURES WILL INCLUDE MINIMIZING IMPERVIOUS SURFACES AND FILTERING SITE STORMWATER RUNOFF BY DRAINING ALL IMPERVIOUS SURFACES TO MEDIA FILTERS PRIOR TO BEING CONVEYED OFFSITE.

TRANSIT ORIENTED DEVELOPMENT NOTE

THIS PROJECT QUALIFIES AS A TRANSIT ORIENTED DEVELOPMENT PROJECT AND IS CONSIDERED A CATEGORY B SPECIAL PROJECT. BASED ON THE ALAMEDA COUNTY SPECIAL PROJECTS WORKSHEET, THIS PROJECT COULD BE ALLOWED TO USE NON-LID TREATMENT METHODS FOR UP TO 100% OF THE PROJECT'S REQUIRED STORMWATER TREATMENT.

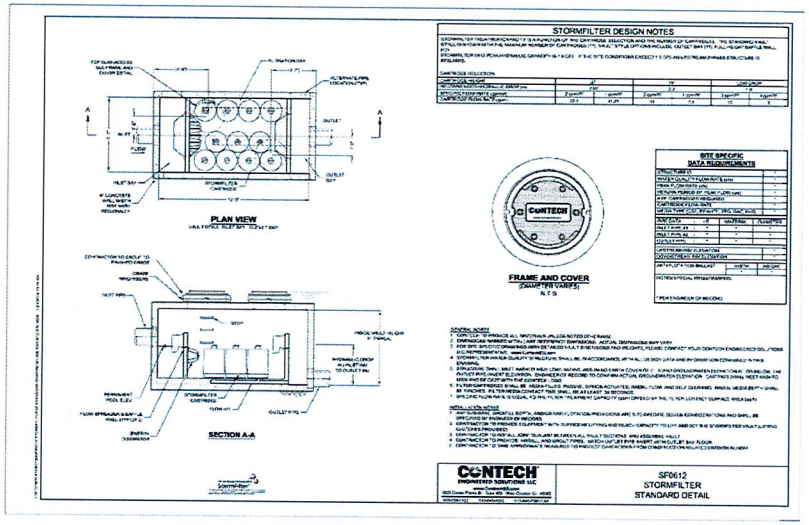
TOTAL PROJECT AREA: 24,042 S.F.
AREA ALLOWED TO BE TREATED BY NON-LID METHODS: 24,042 SF
AREA PROPOSED TO BE TREATED BY NON-LID METHODS: 24,042 SF
TRANSIT ORIENTED DEVELOPMENT: 100%

SHEET NOTES

1. MEDIA FILTER LOCATED UNDER 2ND FLOOR OVERHANG

LEGEND

- PROPERTY LINE
- WATERSHED BOUNDARY
- DIRECTION OF RUNOFF
- ROOF (IMPERVIOUS)
- PAVING (IMPERVIOUS)



0 20' 40'

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INVESTING IN URBAN COMMUNITIES

STORMWATER MANAGEMENT PLAN
FINAL DEVELOPMENT PLAN
Jack London Square - Parcel D
CIM Group

10-19-2016
T = 20
T2 2016012 000
D 2016041 000

C4.0

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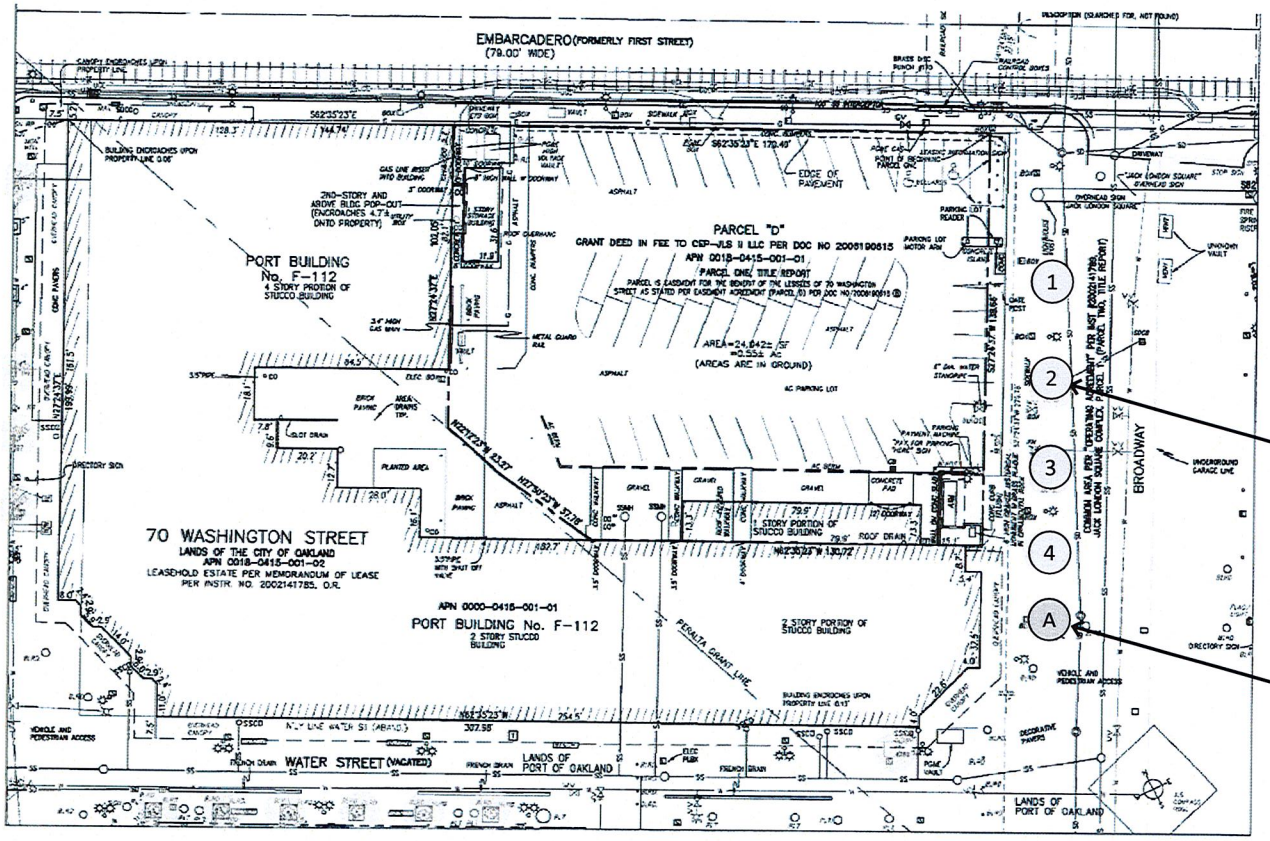

Trees to be Removed

Trees to be Remain

COLUMN HEADING DESCRIPTIONS 9/11/2015

Tag# - Cooresponds to Tree Location Map

Species - Scientific name

Common Name - Vernacular name

DBH - Diameter measured in inches at 4.5 feet above soil grade

Spread - In feet

Notes - Pertinant Arborist comments

Tree #	Species	Common Name	DBH	Spread	Notes
1	*Platanus x hispanica 'Yarwood'*	Yarwood London Plane	4	10	
2	*Platanus x hispanica 'Yarwood'*	Yarwood London Plane	8.5	25	Lean
3	*Platanus x hispanica 'Yarwood'*	Yarwood London Plane	5.5	20	In contact with metal grate, Lean
4	*Platanus x hispanica 'Yarwood'*	Yarwood London Plane	10.5	30	Lean
A	*Platanus x hispanica 'Yarwood'*	Yarwood London Plane	10	25	In contact with metal grate, Lean

General Note: The City of Oakland defines Trees as Protected if they have a 9" or greater DBH and/or are located in the public right of way. All of the trees in this survey fall under this designation.

LEGEND AND NOTES

ALL DISTANCES AND DIMENSIONS ARE SHOWN IN FEET AND DECIMALS THEREOF.

- CENTER LINE
- DISTINCTIVE BOUNDARY LINE
- TRACT SUBDIVISION LINE
- ADJACENT PARCEL LINE
- EXISTING EASEMENT LINE
- EXISTING CHAIN LINK FENCE LINE
- EXISTING CURB LINE
- EXISTING LIP LINE
- EXISTING FLOW LINE
- EXISTING BACK FLOW PREVENTOR
- EXISTING STORM DRAIN CLEAN-OUT
- EXISTING WATER VAULT
- EXISTING STANDARD CITY MONUMENT
- CONTROL POINT
- BOLLARD
- FLAG POLE
- STREET LIGHT PULL BOX
- FIRE DEPARTMENT CONNECTION
- GAS VALVE

SANDIS SUNNYVALE SCALE: 1"=20'

SBCA Tree Consulting
1534 Rose St. Crockett, CA 94525
steve@sbcatree.com

Phone (510) 787-3075
Fax (510) 787-3065
www.sbcatree.com

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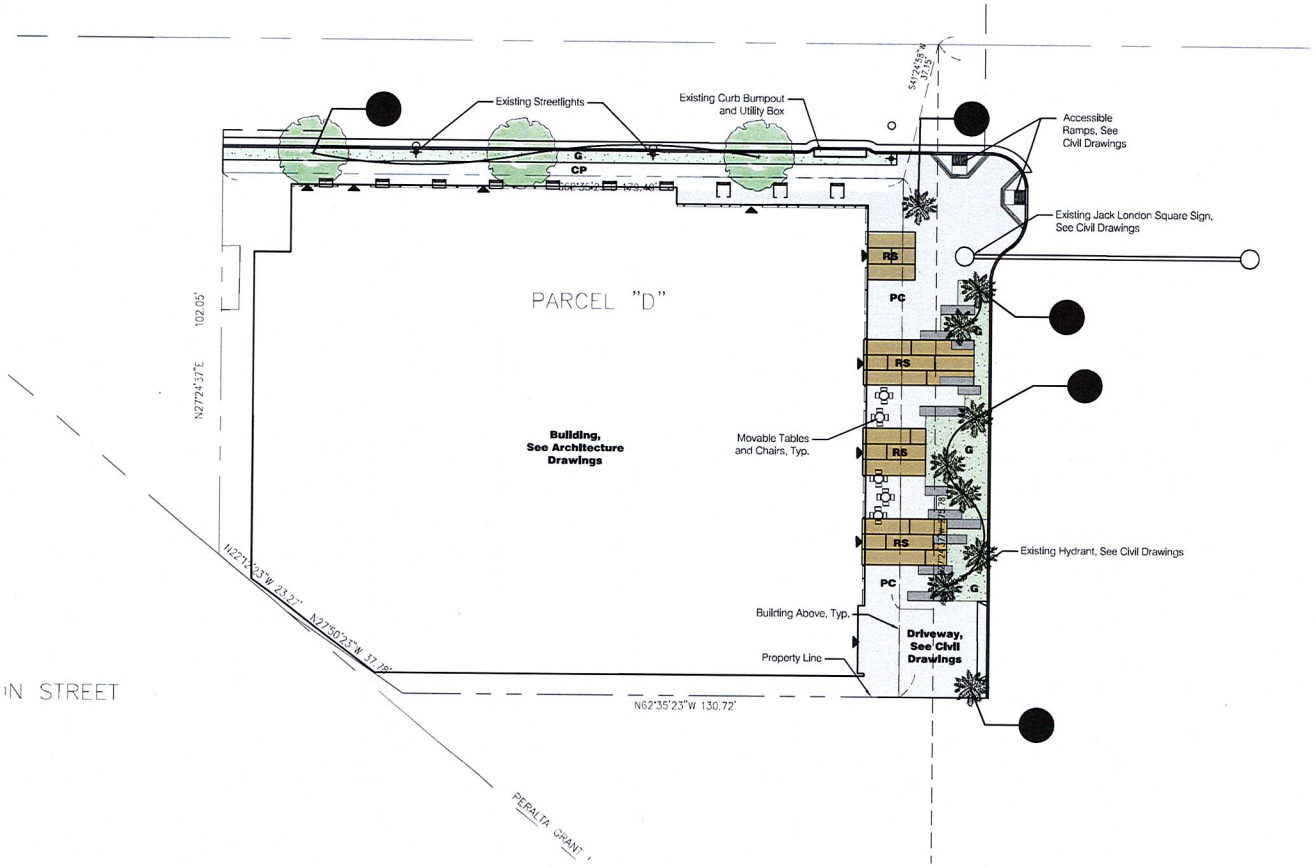
TREE SURVEY
FINAL DEVELOPMENT PLAN
Jack London Square - Parcel D
CIM Group

10 - 19 - 2016

F2: 2016013.000
D: 2016041.000

0 16' 32'

D-TS.1



Landscape Architectural Layout Notes

1. Use dimensional information given. Do not scale drawings. Direct questions about dimensions to the Landscape Architect in writing.
2. All dimensions are parallel or perpendicular to the lines from which they are measured unless noted otherwise.
3. All dimensions shown are taken to the face or centerline of elements unless otherwise noted.

Landscape Architectural Materials Notes

1. The Contractor shall refer any questions on materials, finishes, labor and/or products not specified herein to the Landscape Architect, prior to ordering materials or starting work.
2. All lines and dimensions are parallel or perpendicular to the lines from which they are measured unless otherwise shown.
3. Field Measurements: Take accurate field measurements before preparation of Shop Drawings and/or fabrication. Do not delay job progress.

Landscape Architectural Irrigation Notes

1. The irrigation system will be designed to distribute a minimum amount of water in order t promote active and healthy growth of all proposed plantings
2. The irrigation system shall be designed and installed in conformance with all applicable state and local codes and ordinances, By licensed contractors and experienced workmen.
3. The irrigation controller shall have an automatic timer with battery backup and rain shutoff
4. All valves shall have separate pressure regulators filters and shut off as necessary.
5. The system shall have a shut-off and reduced pressure backflow assembly.
6. The irrigation system shall be comprised of all drip or bubblers

LEGEND

- LOW — Limit of Work
- Centerline
- Structure Above
- Property Line
- Egress
- PC — Precast Concrete Pavers - Pedestrian
- CP — Concrete Paving per City Standards
- RS — Reclaimed Steel Plate Paving
- G — Groundcover
- Custom Timber Bench
- Proposed Tree (Platanus acerifolia 'Columbia') See Schedule
- Proposed Tree (Washingtonia robusta) See Schedule
- Existing Trees
- Street Light, See Civil Drawings
- Fire Hydrant, See Civil Drawings

Landscape Architectural Planting Notes

1. The Contractor shall locate and verify all existing and new utility line locations prior to planting, and shall report any utility conflicts to the Construction Manager.
2. Contractor shall receive approval from Landscape Architect of plant layout prior to installation.
3. The trunk flare (at the base of the tree) shall be properly exposed for all plantings.
4. Keep root balls intact prior to and during planting operations. Plants with broken or damaged root balls shall be rejected and immediately removed from the site. Keep root balls damp and protected from damage due to sun and wind. Do not shave root balls.
5. Temporary irrigation shall be provided for plant establishment and maintenance period.
6. 1 year landscape maintenance and warranty period.

Planting Schedule

Key	Qty	Scientific Name	Common Name	Size	Rootball	Root	Notes:
Trees							
PA	3	*Platanus x acerifolia var. Columbia*	London Plane	3.5" Cal.	48"	Box	--
WR	9	*Washingtonia robusta*	Mexican Fan Palm	15-20' Tall	--	B&B	--
Groundcover							
CT	50%	*Carex testacea*	Orange Sedge	1 Gal.	--	Cont.	18" O.C.
CA	50%	*Carex tumulicola*	Foothill Sedge	1 Gal.	--	Cont.	18" O.C.

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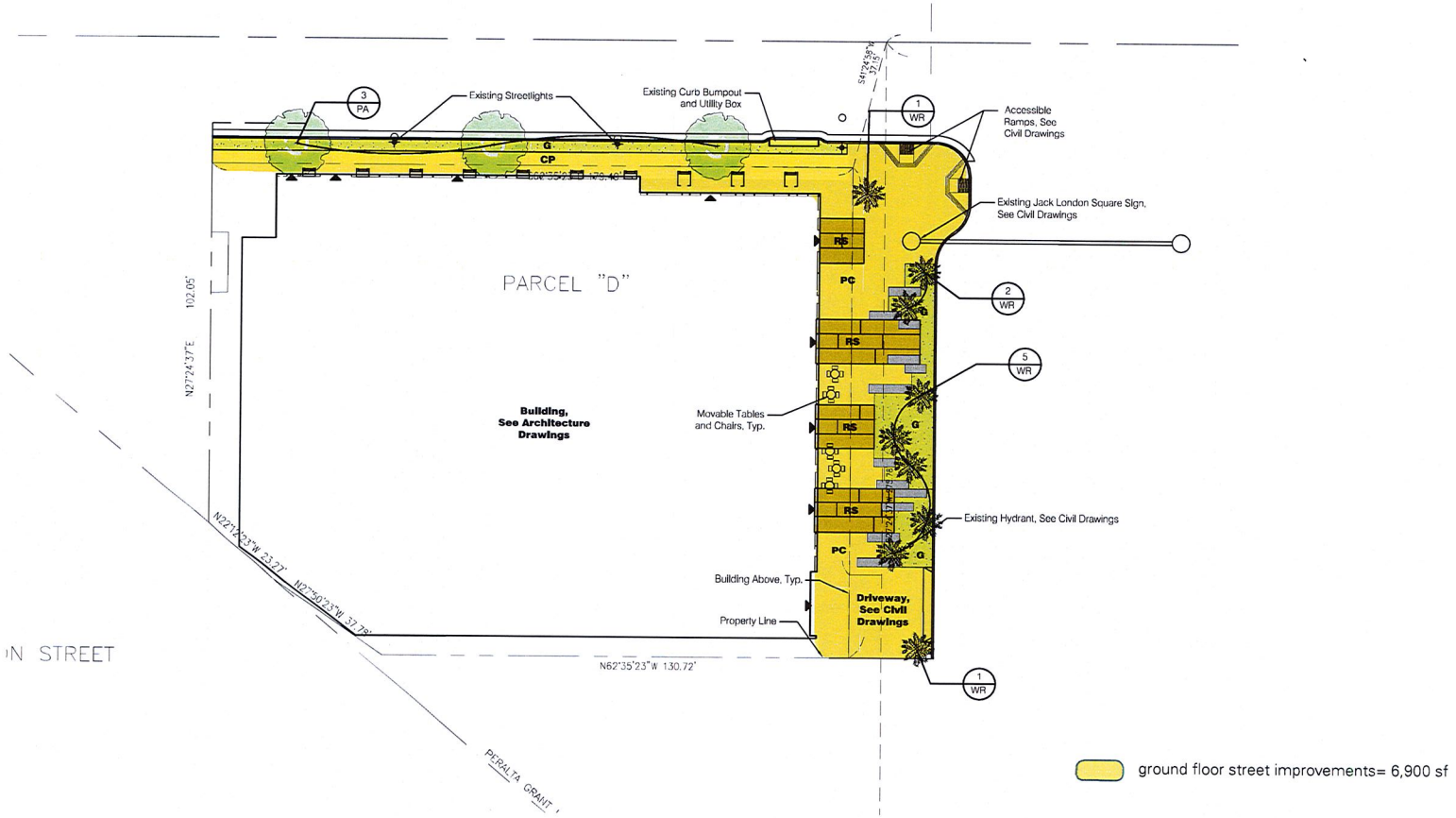
LANDSCAPE PLAN - GROUND LEVEL
FINAL DEVELOPMENT PLAN
Jack London Square - Parcel D
CIM Group

10 - 19 - 2016

F2: 2016013.000
D: 2016041.000

0 16' 32'

D - L1.01



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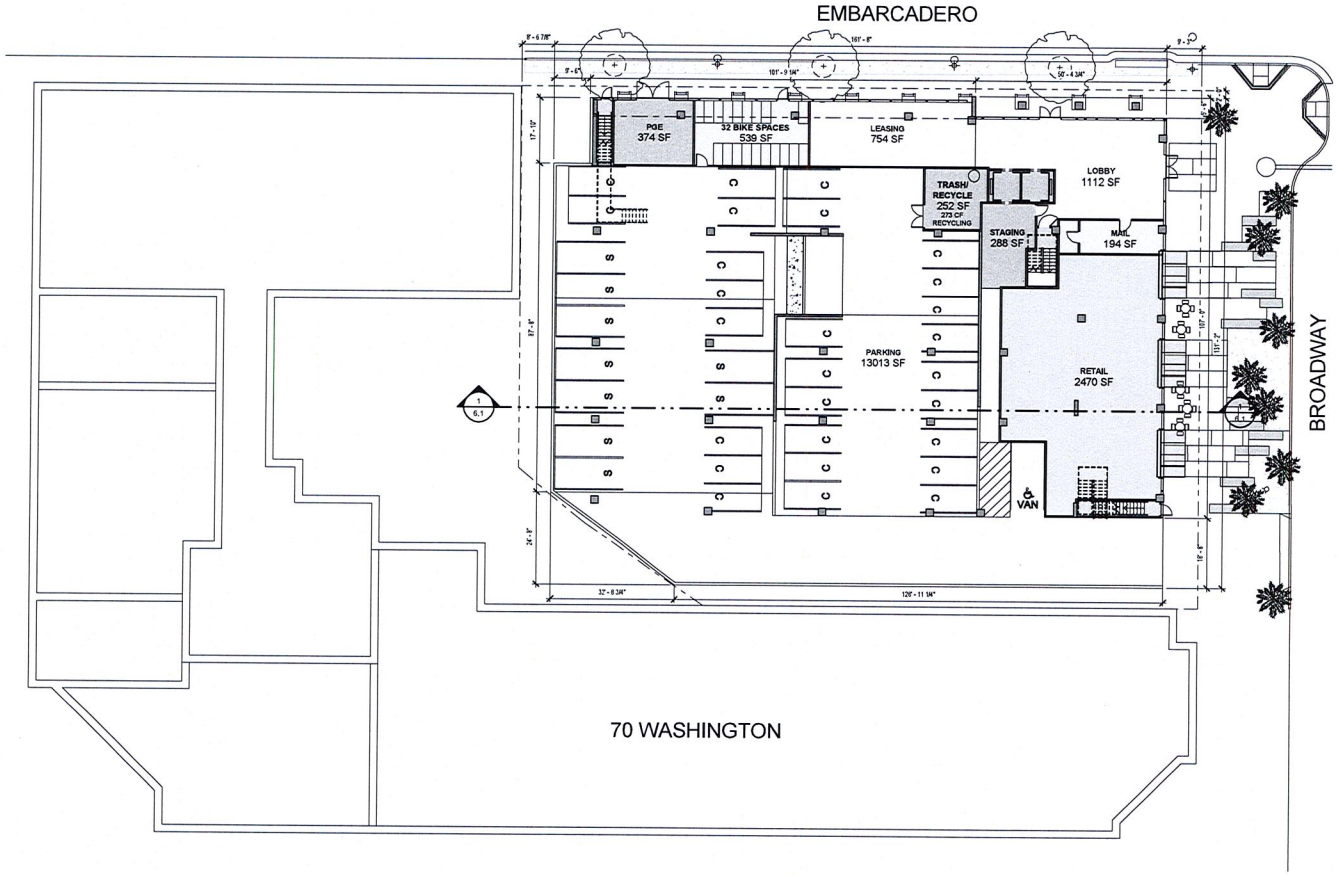
GROUND FLOOR IMPROVEMENT DIAGRAM
PLANNING COMMISSION HEARING
Jack London Square - Parcel D
CIM Group

03 - 16 - 2017
2016041.000

D-4.0



0 16' 32'

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GROUND FLOOR PLAN

PLANNING COMMISSION HEARING

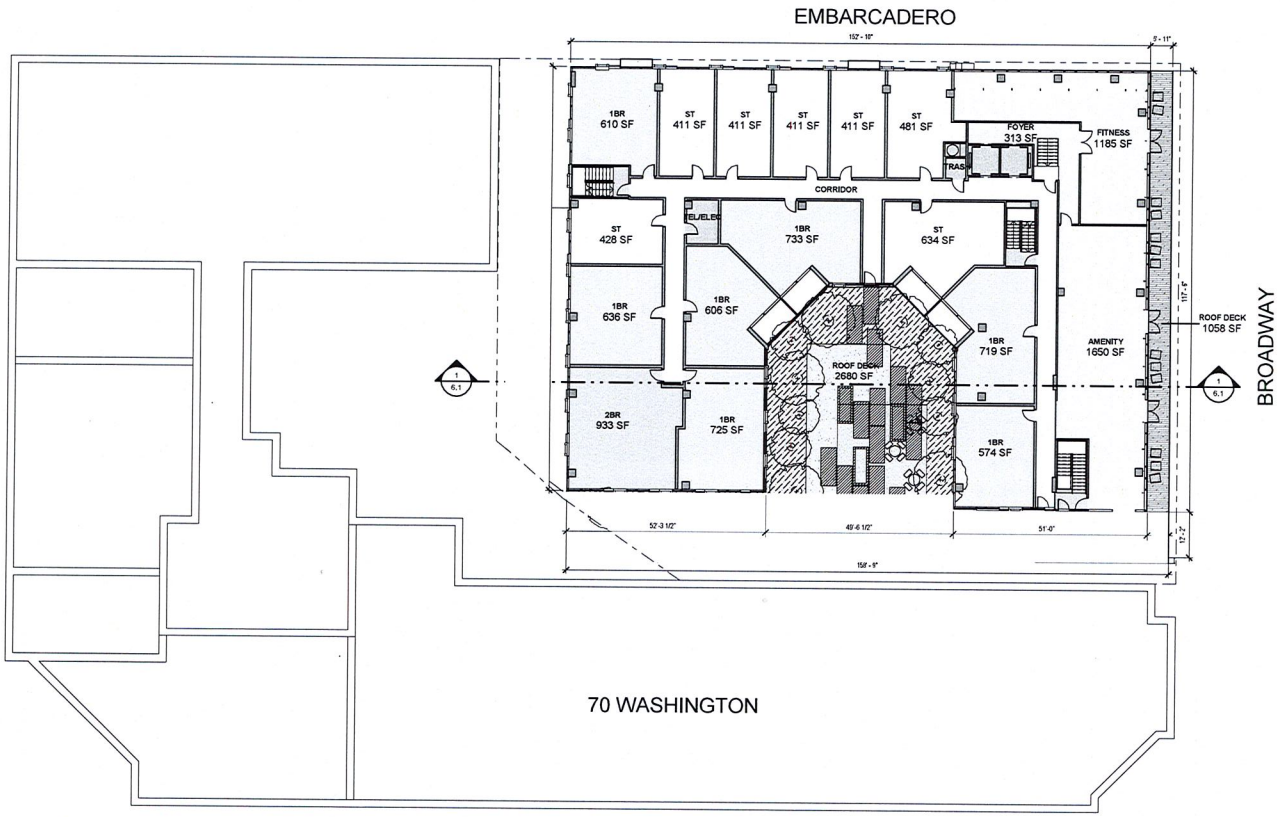
Jack London Square - Parcel D

CIM Group

03 - 16 - 2017

2016041.000

D-4.1



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LEVEL 2

PLANNING COMMISSION HEARING

Jack London Square - Parcel D

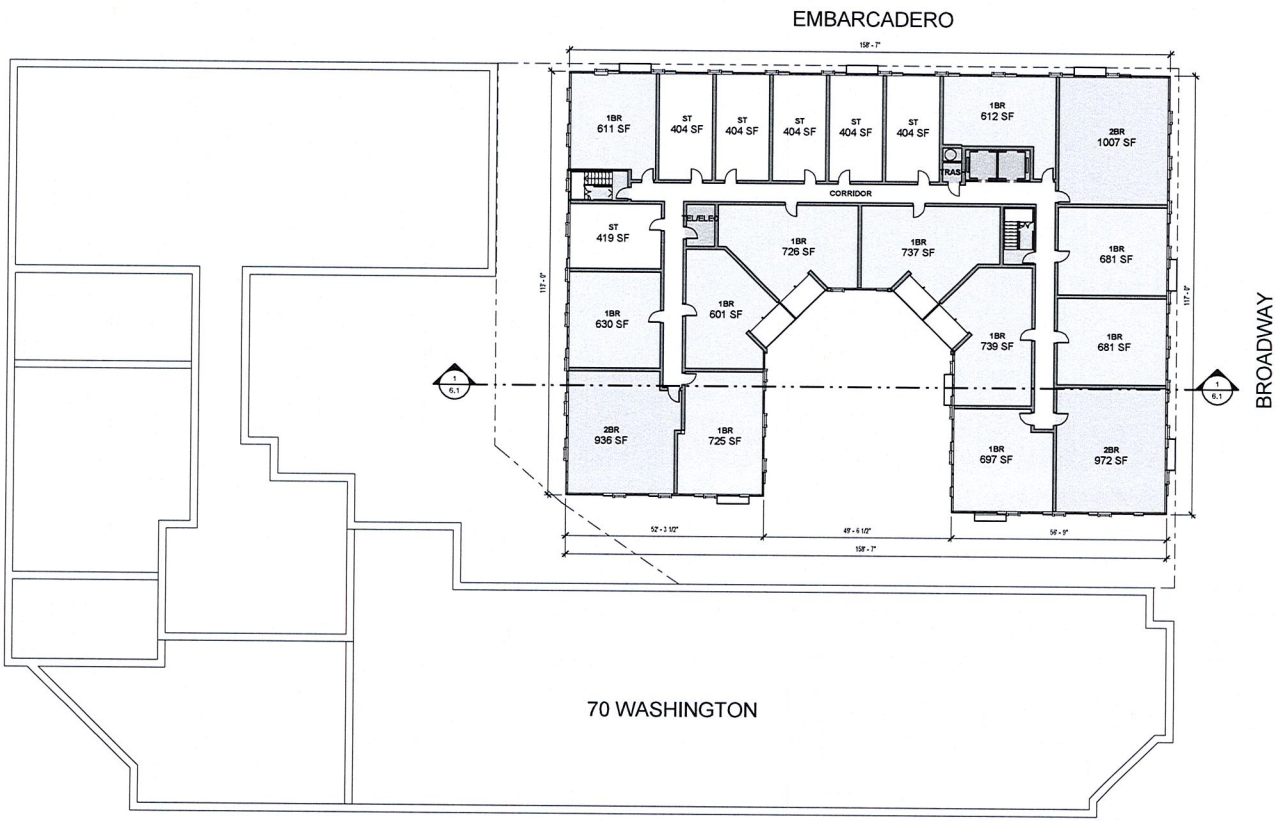
CIM Group

03 - 16 - 2017

2016041.000

0 16' 32'

D-4.2



0 16' 32'

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LEVELS 3-8

PLANNING COMMISSION HEARING

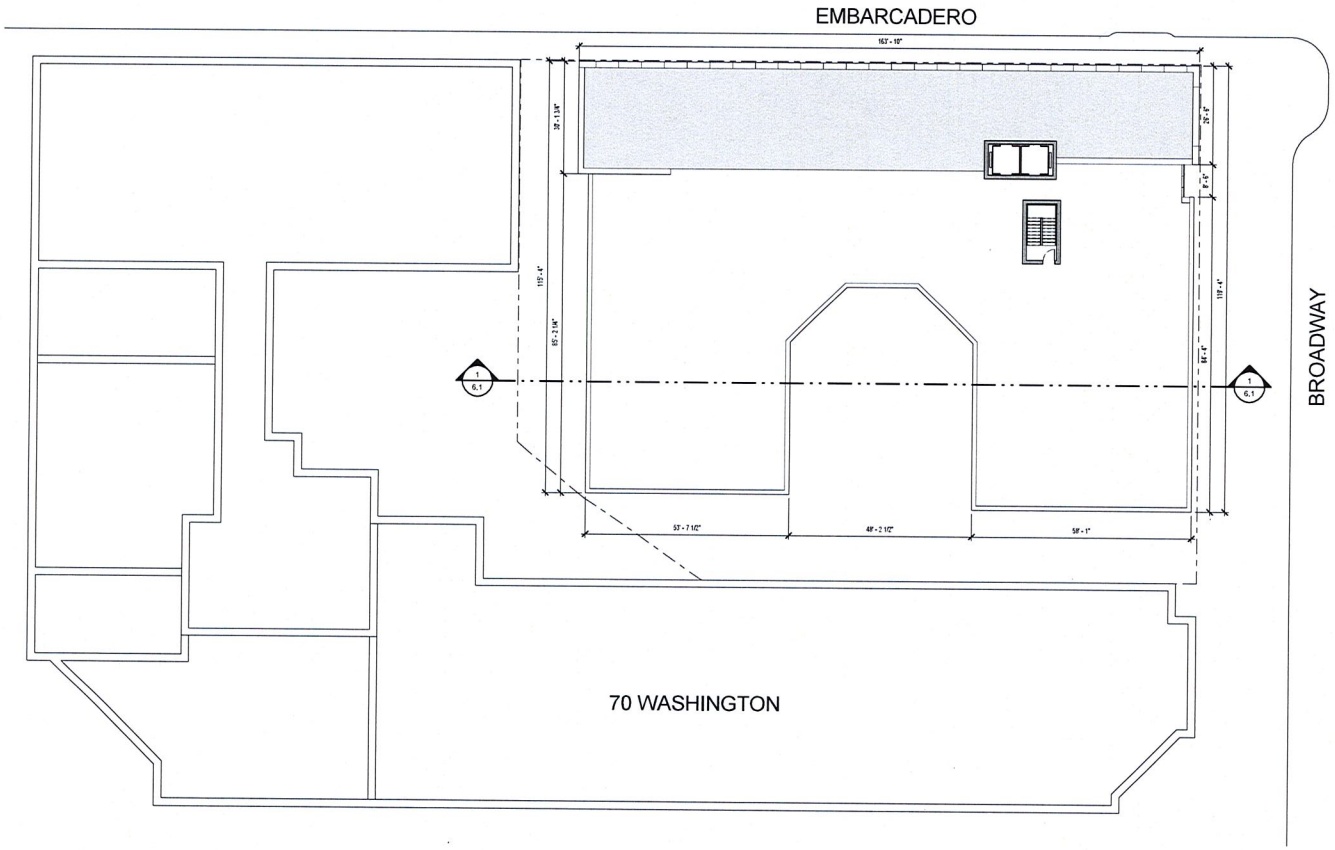
Jack London Square - Parcel D

CIM Group

03 - 16 - 2017

2016041.000

D-4.3



0 16' 32'

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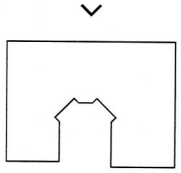
ROOF PLAN
PLANNING COMMISSION HEARING
Jack London Square - Parcel D
CIM Group

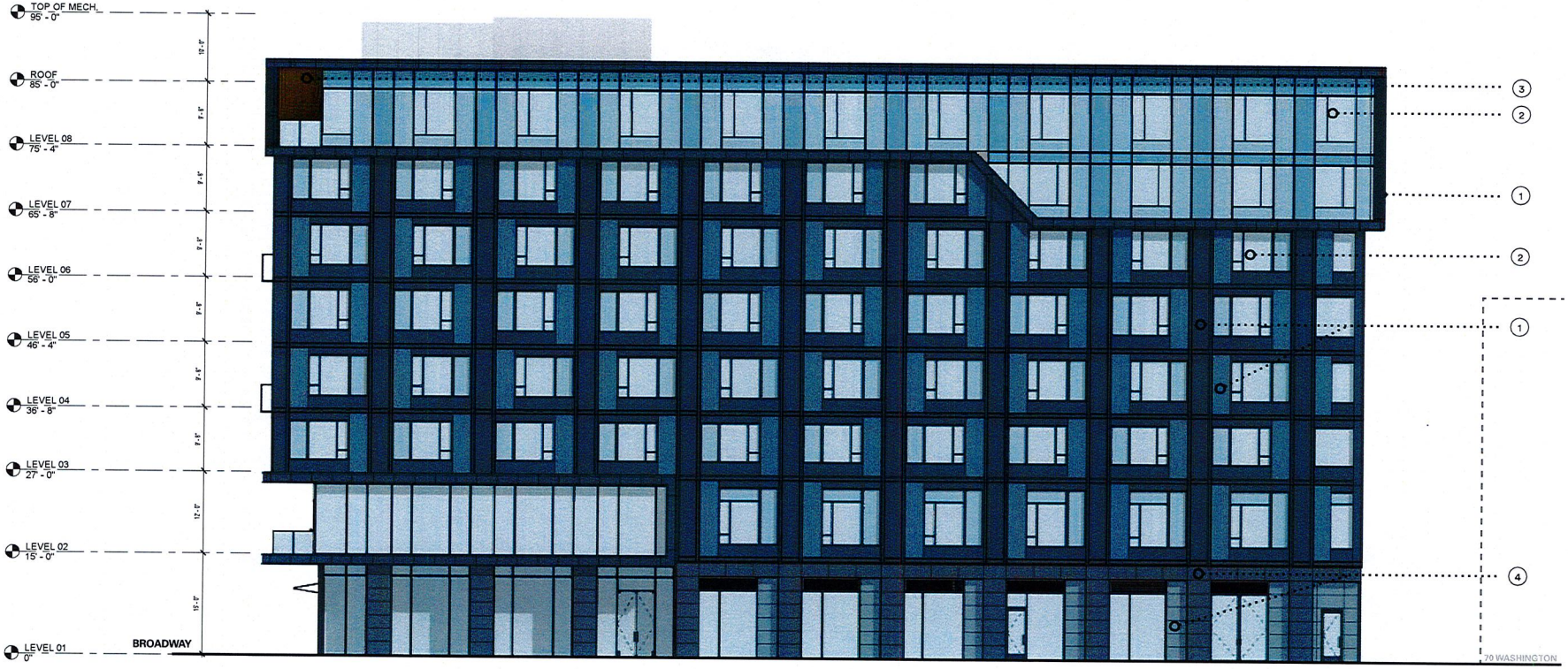
03 - 16 - 2017

2016041.000

D-4.4

1. PAINTED CEMENT BOARD RAIN SCREEN
2. HIGH PERFORMANCE INSULATED GLAZING WITH LOW-E COATING
3. PREFINISHED ALUMINUM ACCENT PANEL
4. LARGE FORMAT PORCELAIN TILE





0 8' 16'

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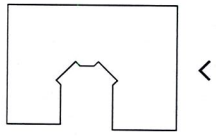
NORTH (EMBARCADERO) ELEVATION
PLANNING COMMISSION HEARING
Jack London Square - Parcel D
CIM Group

03 - 16 - 2017

2016041.000

D-5.1

1. PAINTED CEMENT BOARD RAIN SCREEN
2. HIGH PERFORMANCE INSULATED GLAZING WITH LOW-E COATING
3. PREFINISHED ALUMINUM ACCENT PANEL
4. LARGE FORMAT PORCELAIN TILE



0 8' 16'

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EAST (BROADWAY) ELEVATION
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CIM Group

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D-5.2

1. PAINTED CEMENT BOARD RAIN SCREEN
2. HIGH PERFORMANCE INSULATED GLAZING WITH LOW-E COATING
3. PREFINISHED ALUMINUM ACCENT PANEL
4. LARGE FORMAT PORCELAIN TILE

0 8' 16'

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SOUTH ELEVATION
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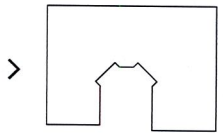
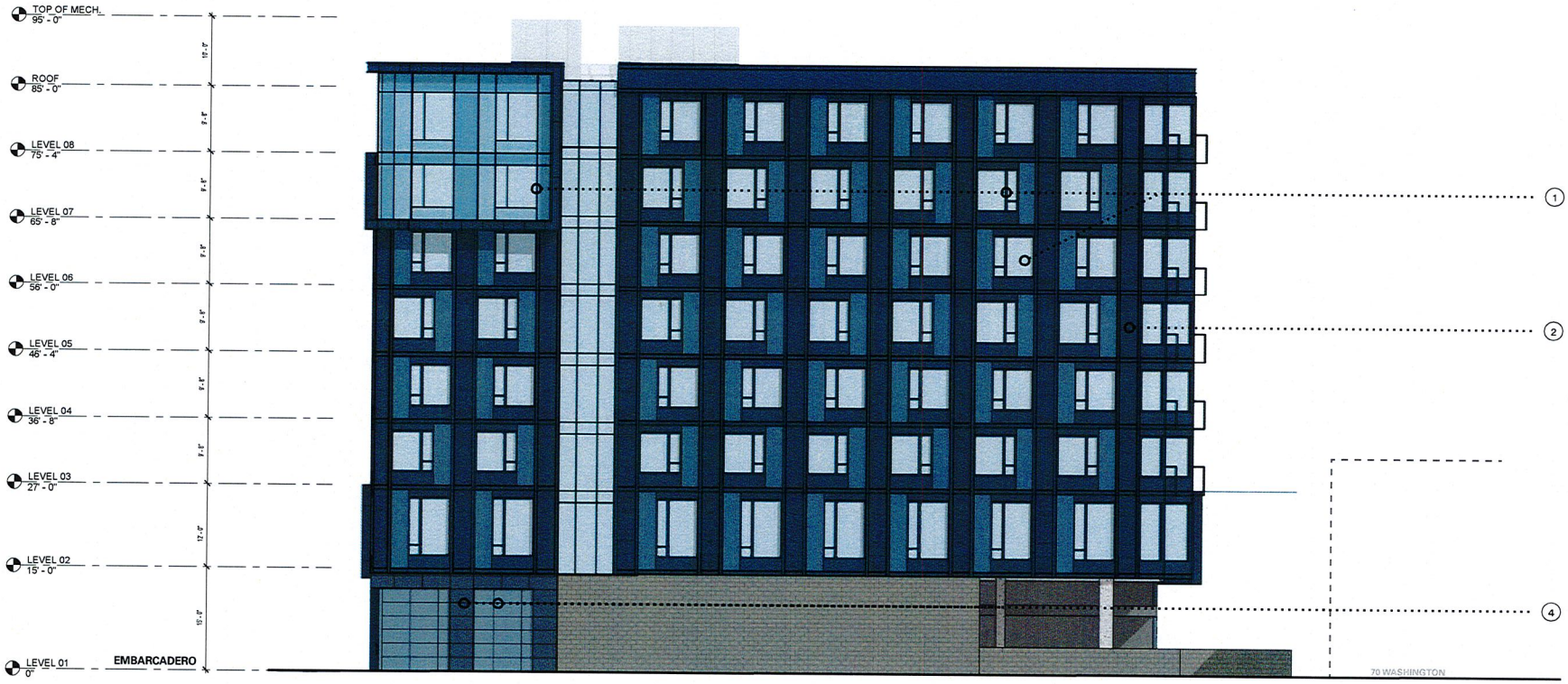
D-5.3

1. PAINTED CEMENT BOARD RAIN SCREEN
2. HIGH PERFORMANCE INSULATED GLAZING WITH LOW-E COATING
3. PREFINISHED ALUMINUM ACCENT PANEL
4. LARGE FORMAT PORCELAIN TILE





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WEST ELEVATION
PLANNING COMMISSION HEARING
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D-5.4

1. PAINTED CEMENT BOARD RAIN SCREEN
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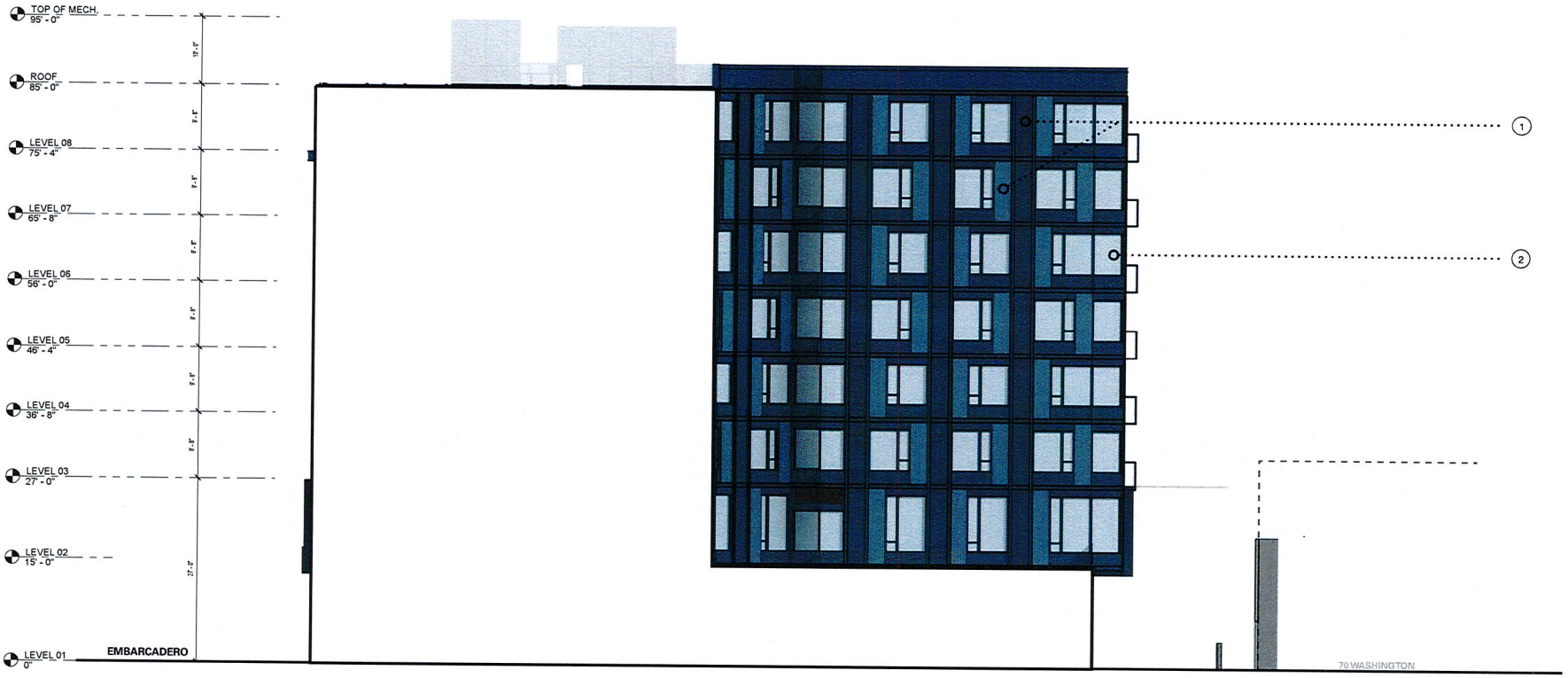
3. PREFINISHED ALUMINUM ACCENT PANEL



0 8' 16'

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COURTYARD WEST ELEVATION
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D-5.5

1. PAINTED CEMENT BOARD RAIN SCREEN
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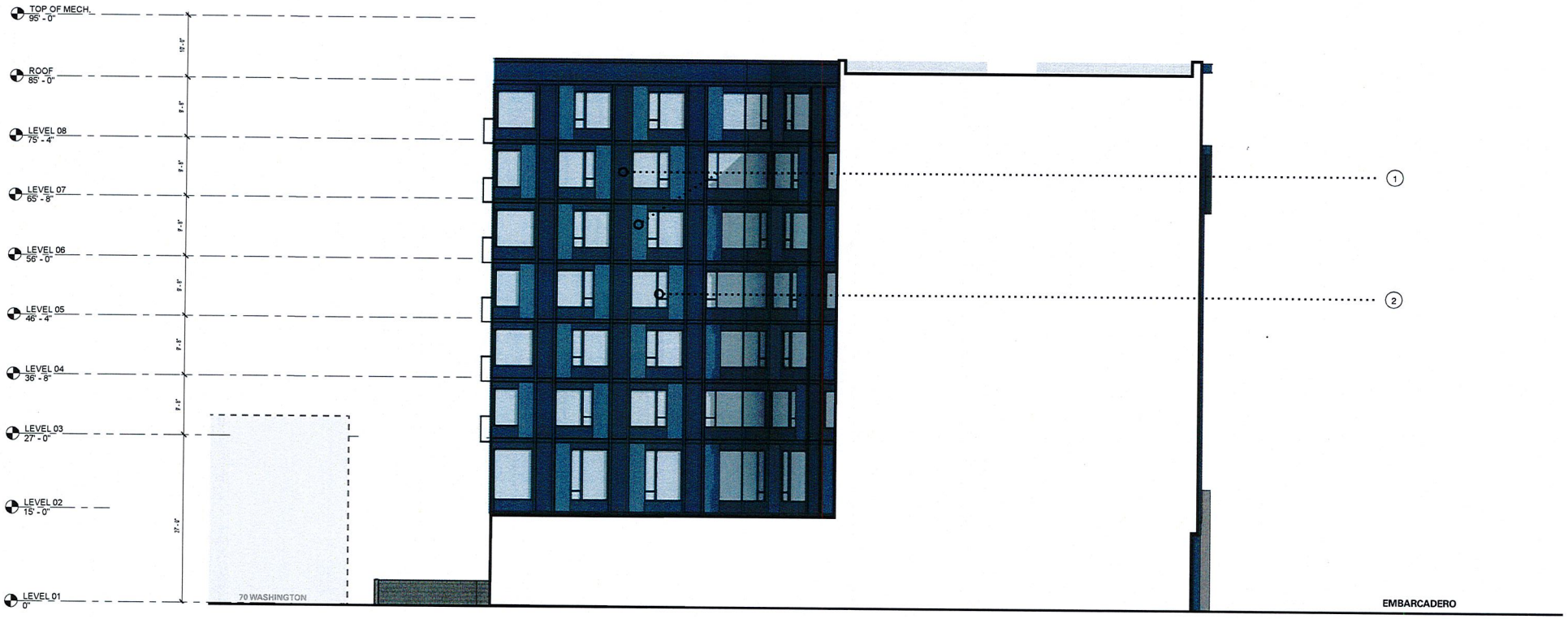

0 8' 16'

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COURTYARD EAST ELEVATION
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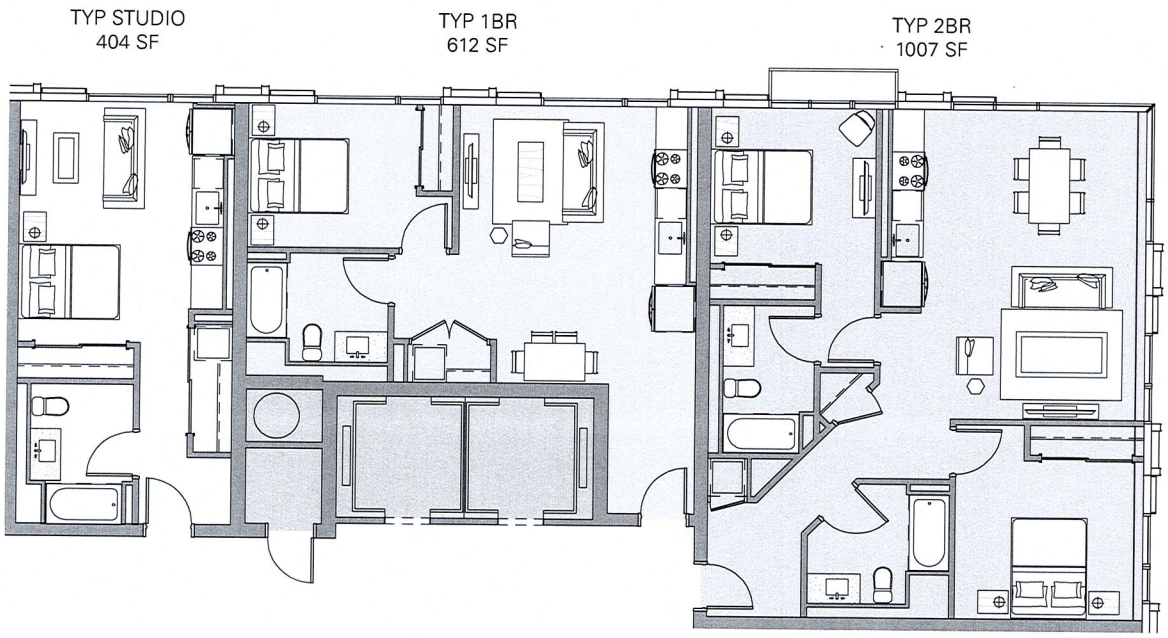
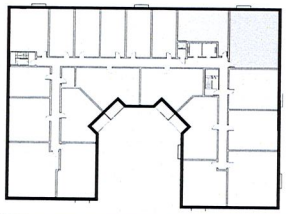
D-5.6





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UNIT PLANS
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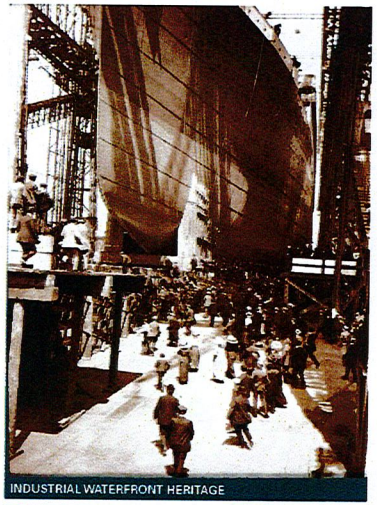
D-7.1



INDUSTRIAL WATERFRONT HERITAGE

PORT ACTIVITY

1. PAINTED CEMENT BOARD RAIN SCREEN
2. HIGH PERFORMANCE INSULATED GLAZING WITH LOW-E COATING
3. PREFINISHED ALUMINUM ACCENT PANEL
4. LARGE FORMAT PORCELAIN TILE

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BUILDING MATERIALS
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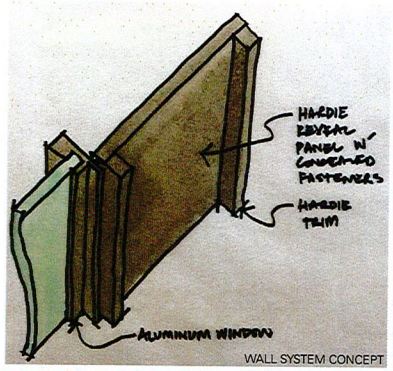
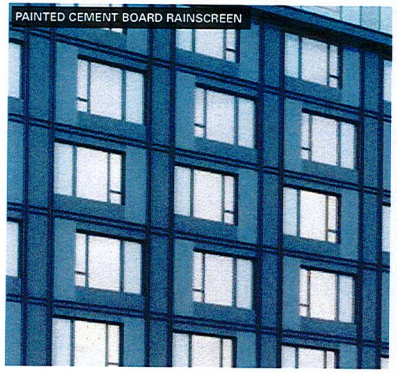
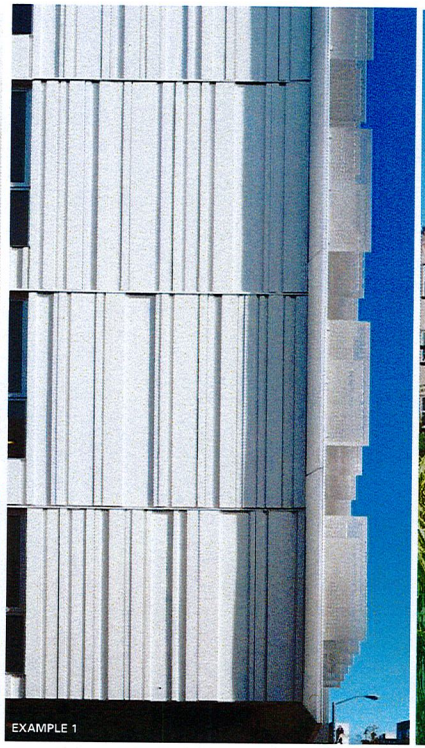


WALL SYSTEM CONCEPT


PAINTED CEMENT BOARD RAINSCREEN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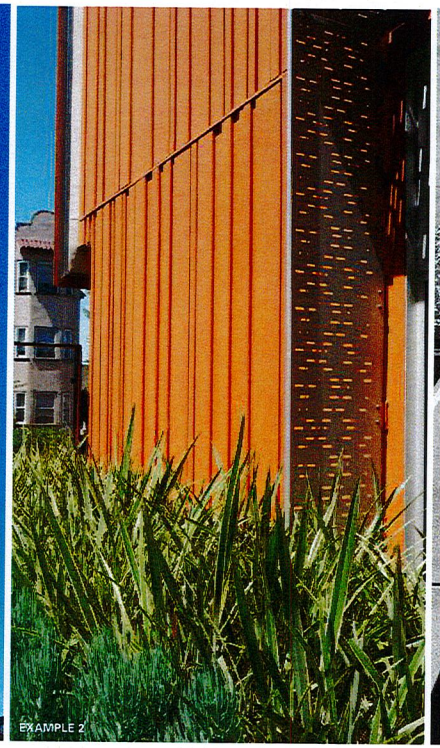
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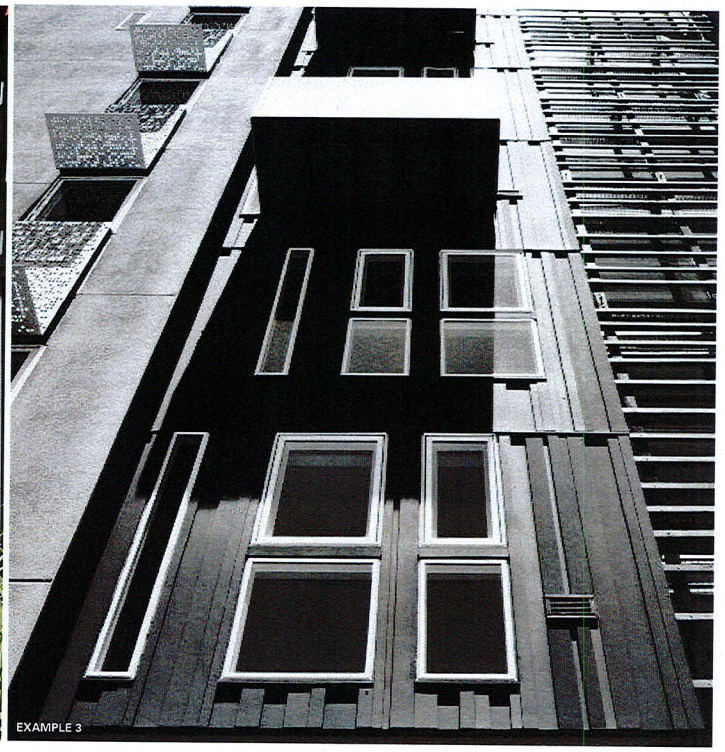
EXAMPLE 3

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PAINTED CEMENT BOARD RAIN SCREEN
PLANNING COMMISSION HEARING
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RENDERING - EAST FACADE
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RENDERING - SOUTHEAST CORNER
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RENDERING - NORTH FACADE
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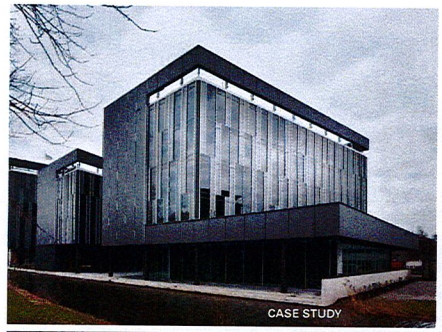

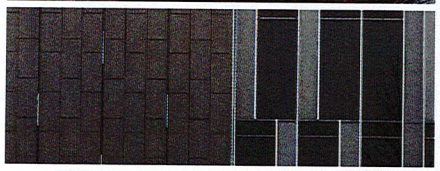

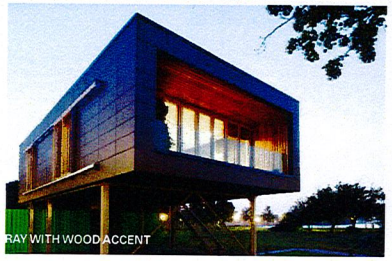

(1) PAINTED CEMENT BOARD RAIN SCREEN

(4) LARGE FORMAT PORCELAIN TILE

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COLOR SCHEME VARIANT - GRAY
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COLOR SCHEME COMPARISON
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RENDERING - NORTH FACADE
PLANNING COMMISSION HEARING
Jack London Square - Parcel D
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JACK LONDON SQUARE F-2

PLANNING COMMISSION HEARING

OAKLAND, CALIFORNIA

DEVELOPER

CIM Group

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LOS ANGELES, CA 90010
T 323.860.4900

ARCHITECT

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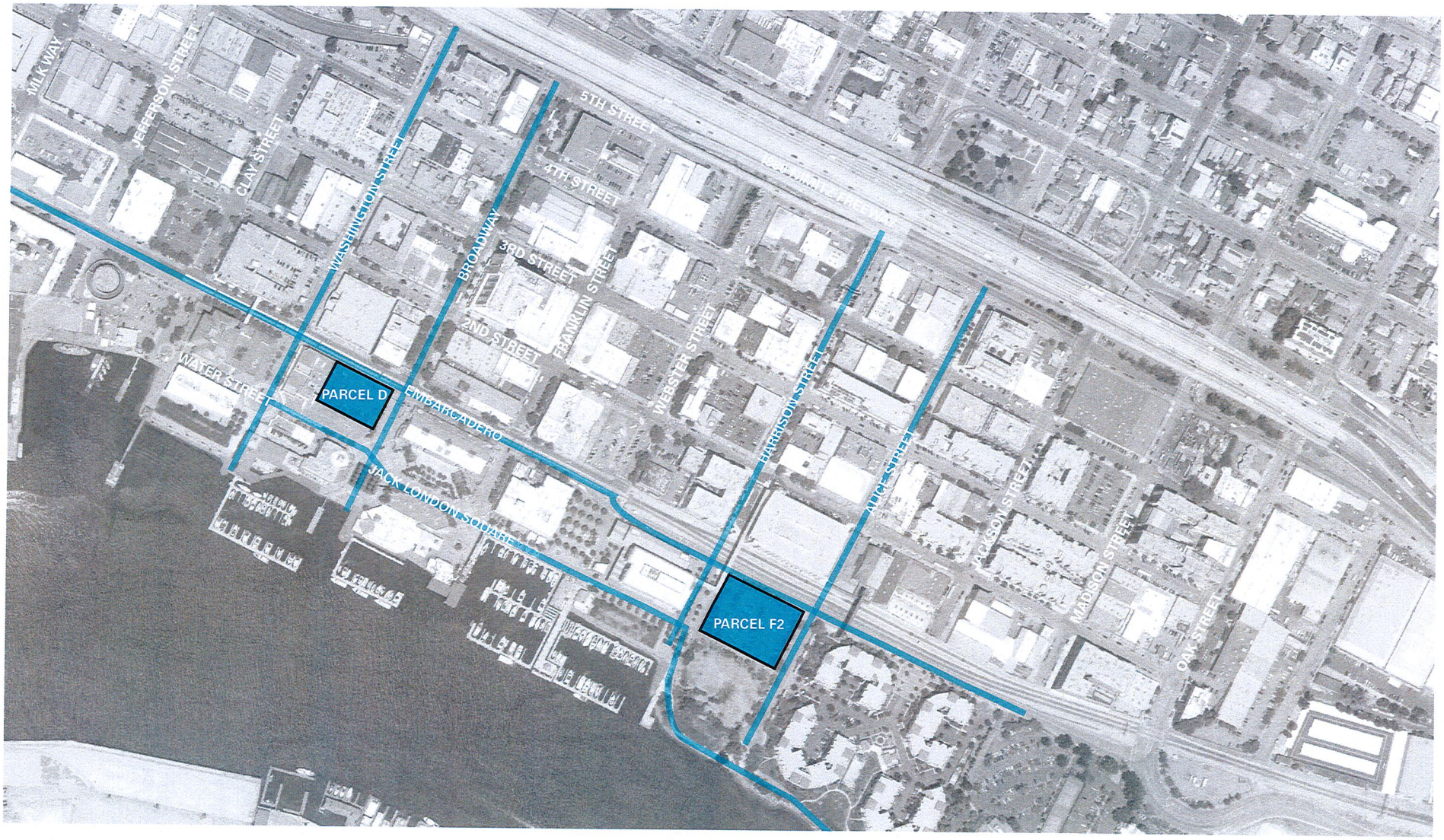

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NEIGHBORING PROPERTY VIEWS
PLANNING COMMISSION HEARING
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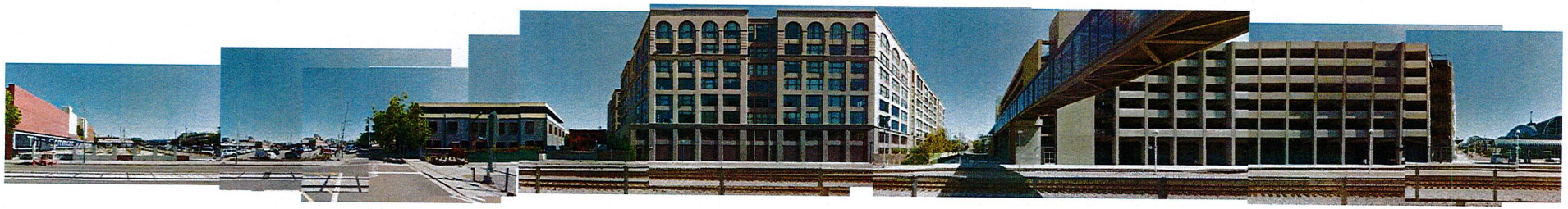
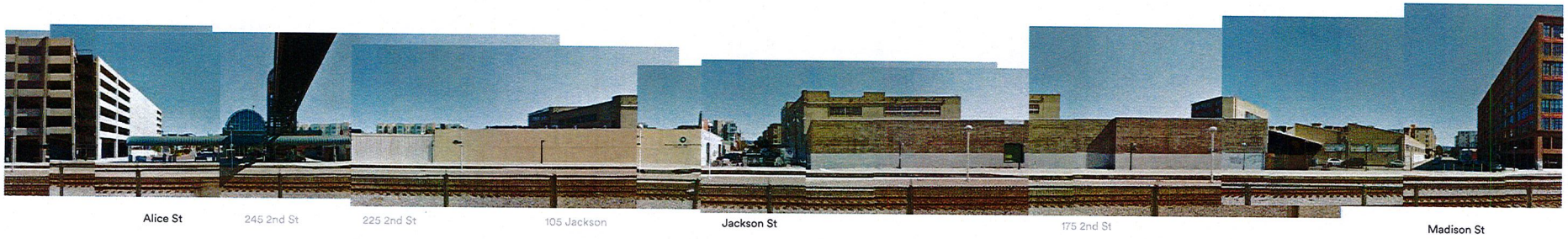
F2 2016013.000

G - 2.1

Embarcadero (Facing North)





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VIEWS - EMBARCADERO FACING NORTH
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G - 2.2

Embarcadero (Facing South)

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VIEWS - EMBARACERO FACING SOUTH
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Water St & SF Bay Trail (Facing North)

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VIEWS- WATER ST FACING NORTH
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G - 2.4

Water St & SF Bay Trail (Facing South)

PARCEL F2

Webster St

31 Webster

1 Franklin

Franklin St

409 Water

Broadway

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WATER ST FACING SOUTH
PLANNING COMMISSION HEARING
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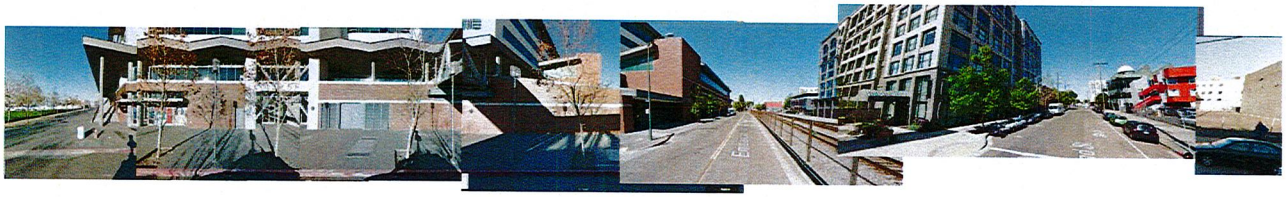
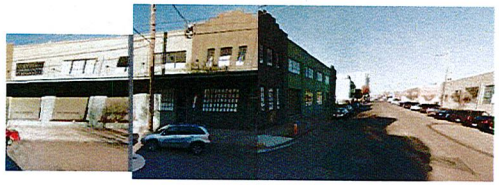
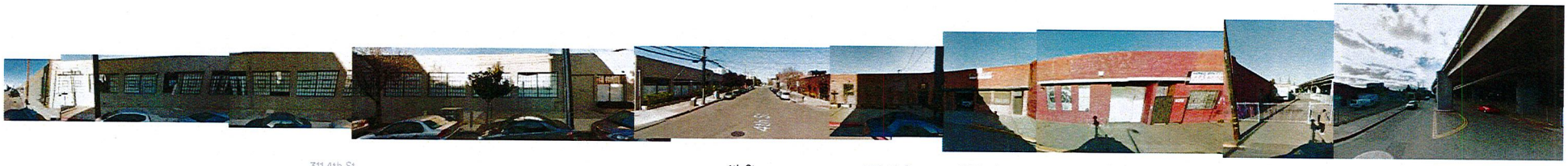
Harrison St (Facing West)



55 Harrison | Embarcadero | 311 2nd St | 2nd St



229 Harrison | 3rd St



311 4th St | 4th St | 300 4th St | 417 Harrison | 425 Harrison | 5th St

Alice St (Facing East)

5th St | 428 Alice | 4th St | 247 4th St | 248 3rd St | 3rd St | 220-200 Alice | 2nd St

Embarcadero | 101 Embarcadero | 2-58 Alice

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VIEWS - HARRISON ST & ALICE ST
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
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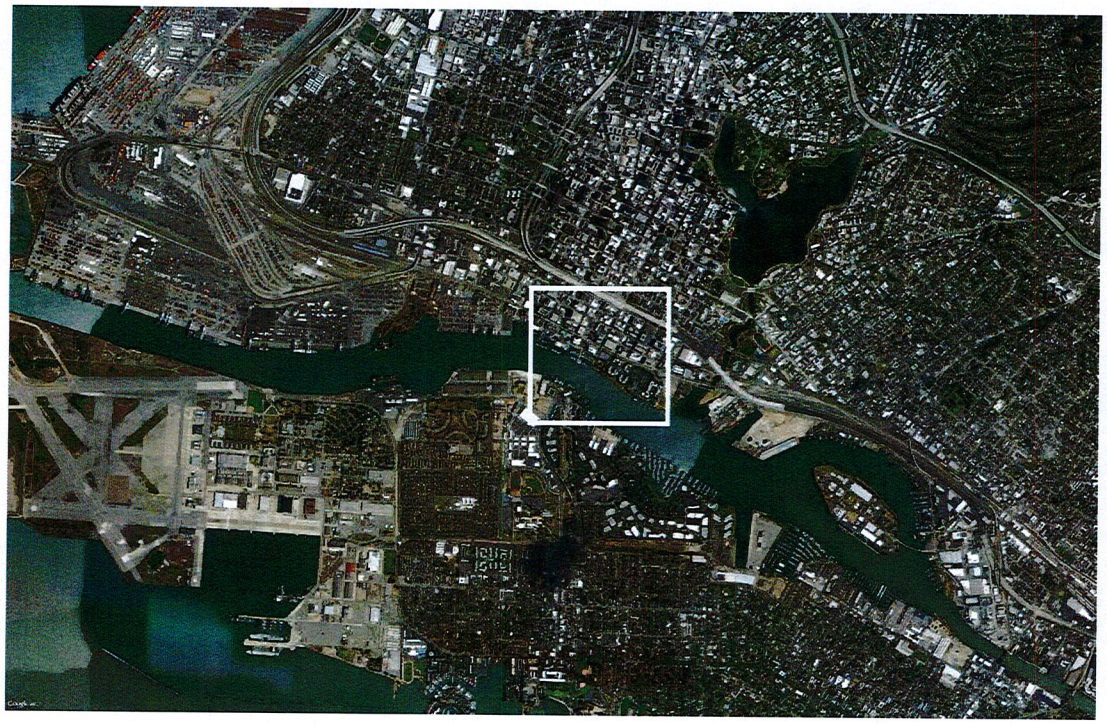

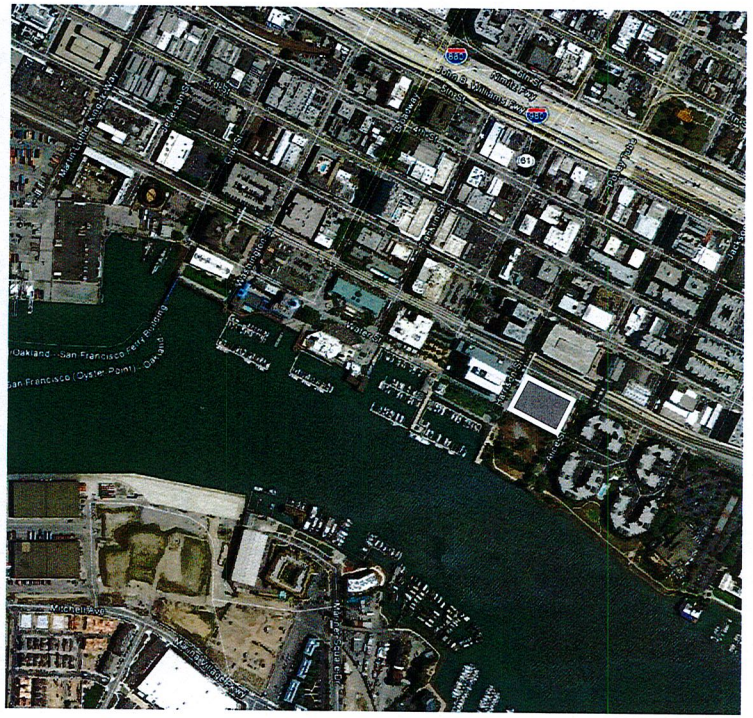

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CONTEXT MAP
PLANNING COMMISSION HEARING
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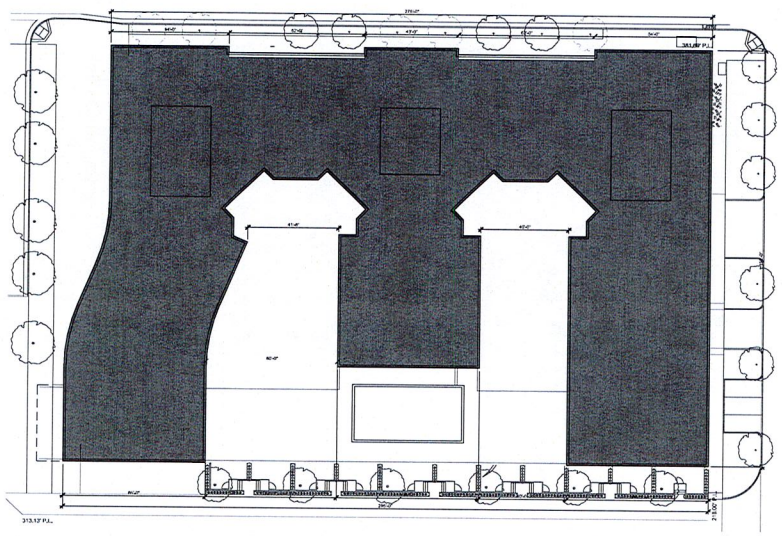
F2 - 3.0

EMBARCADERO



HARRISON ST

ALICE ST

SITE PLAN

Item	Notes	FDP - Proposed
Regulating Agency	City of Oakland	
Governing Regulations	Approved Entitlements Case #ER03004, PUD13170, DA13171 (Most recently amended 9/23/14) Oakland Planning Code (Revised through May 2015)	
Parcel Area	73,847 sf	
Zoning Classification	C-45; PUD	
General Plan Classification	MUD	
Land Use	Mixed Use (Multi-Family Residential)	Mixed Use
Density (Unit Count)	Up to 665 Units to be allocated between Sites D & F2 370 per PDP high-rise and 282 per mid-rise scheme	336 Units
FAR	5:1, set by PUD, can be applied in an additive manner	303,100 sf
Building Height	293' (26 stories Max.) 92'-4" per PDP mid-rise scheme	85' (8 stories)
Open Space	150 sf group space/unit (private space reduces requirement by 50%)	OPEN SPACE Ground Floor: Private Usable Open Space = 3,300 sf Level 02: Private Usable Open Space = 2,400 sf Group Usable Open Space = 8,800 sf Level 04: Group Usable Open Space = 6,100 sf Total Open Space = 20,600 sf Ground Floor Improvements within Property Line = 23,210 sf Ground Floor Improvements beyond Property Line = 6,230 sf Code required open space has been provided via public dedication of lands within the PUD
Parking	1:1 Parking on-site (C-45 Zoning) Up to 50% of spaces may be compact or intermediate. Regular Space = 18'x 8.5'(10.5' at wall) Intermediate Space = 16.5'x 8'(10' at wall) Compact Space = 15'x 7.5'(9.5' at wall) Maneuvering Aisle for 90 deg parking = 21' 1 space/ 1ksf retail	.5:1 parking 168 spaces for resi. 2,400/ 1k = 3 retail 171 spaces total 25% on-site parking PLUS 25% on-site mechanical parking OR 25% district-wide parking in existing structures including Site G
Loading	1 on-site loading space required per approved loading variance	1 space provided
Bicycle Parking (Long Term)	1 space / 4 units & 1 space / 12ksf retail (§17.117)	336 / 4 = 84 2400 /12k = 1 84+1 = 85 LT SEE PAGE F2-4.1
Bicycle Parking (Short Term)	1 space/ 20 units & 1 space / 2ksf retail (§17.117)	336 / 20 = 17 2400 / 2k = 2 17+2 = 19 ST SEE PAGE F2-4.1
Recycling Space	Residential: 2 CF per unit Retail: 2 CF / 1ksf	336x2 = 672 CF = 25 CY 2400 /1k = 3 CF = 1 CY 25+1 = 26 CY SEE PAGE F2-4.1

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ZONING ANALYSIS
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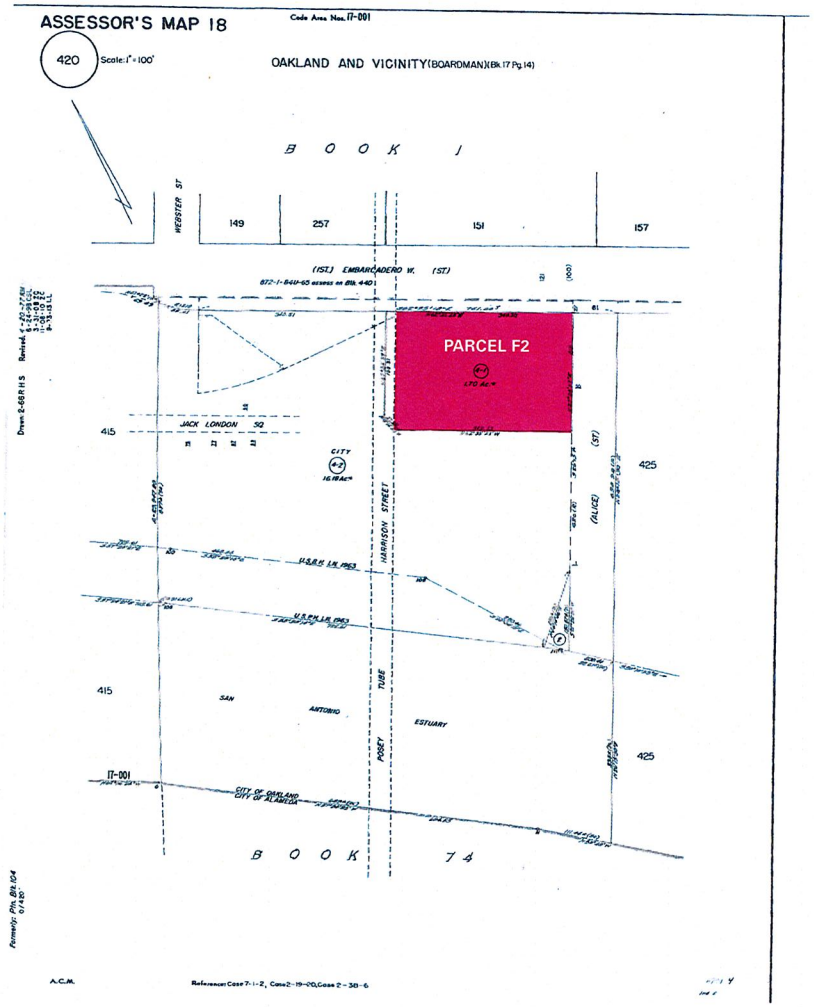

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ASSESSOR'S MAP
PLANNING COMMISSION HEARING
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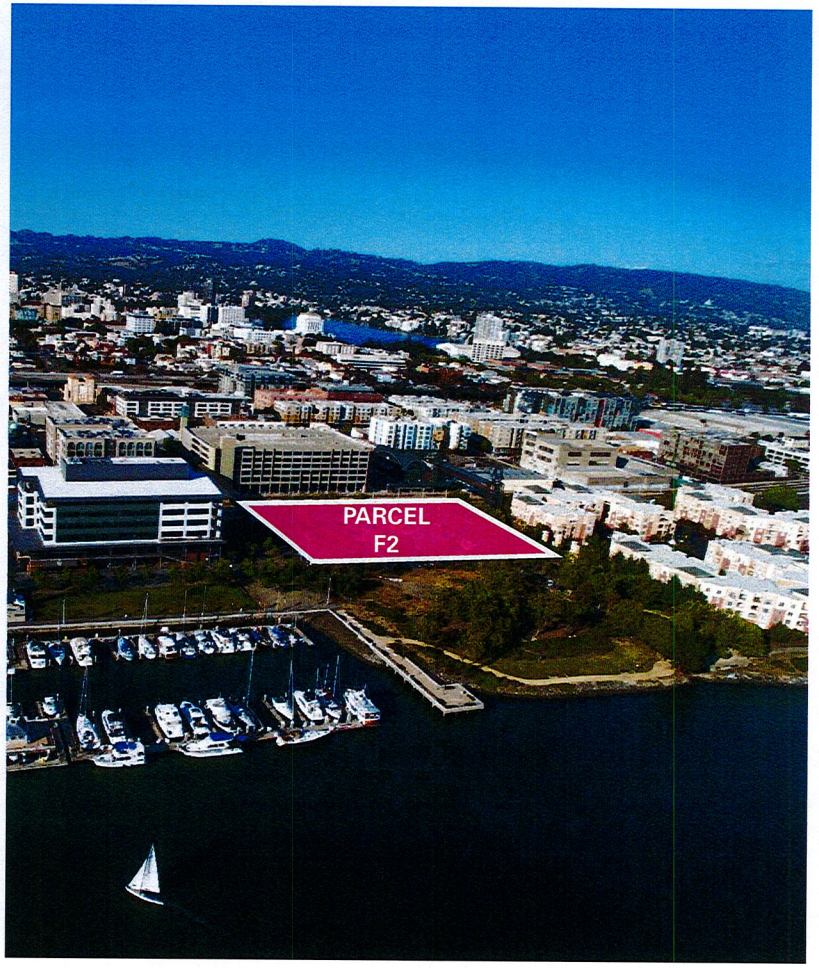
F2: 2016013.000

0 50" 100'

F2 - 3.2



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AERIAL VIEWS
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
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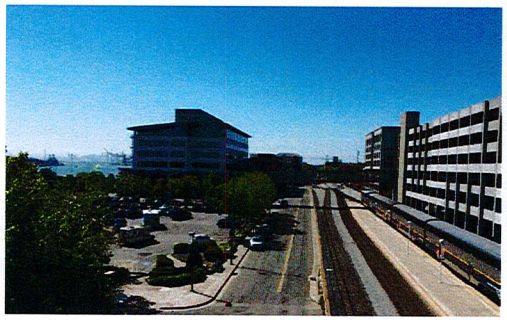
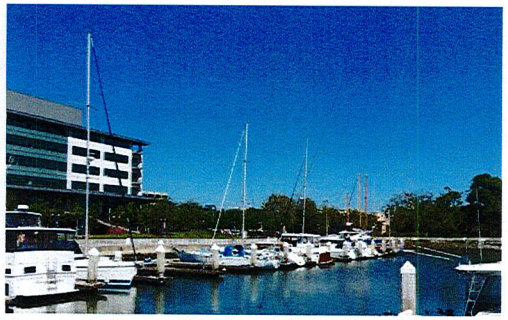
03 - 16 - 2017

F2: 2016013.000

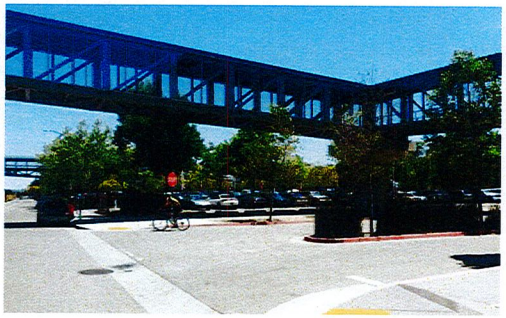
F2 - 3.3



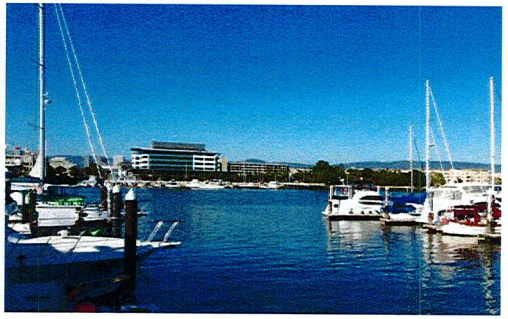
1: View of Parcel F2 from the Amtrak Station



2: View of Parcel F2 from Waterfront



3: View on Harrison Street looking South



4: View of Parcel F2 from Alameda Island

5: View from SF Bay Trail of Parcel F2

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SITE PHOTOS
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
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F2. 2016013.000

LEED 2009 for New Construction and Major Renovations

Project Scorecard Jack London Square Parcel F2

Date: 9/9/2015

INTEGRAL GROUP

Sustainable Sites — Possible Points: 26

Y	?	N				
20	3	3				
Y			c	Prereq 1	Construction Activity Pollution Prevention	
1			d	Credit 1	Site Selection	1
5			d	Credit 2	Development Density and Community Connectivity	5
	1		d	Credit 3	Brownfield Redevelopment	1
6			d	Credit 4.1	Alternative Transportation—Public Transportation Access	6
1			d	Credit 4.2	Alternative Transportation—Bicycle Storage and Changing Rooms	1
3			d	Credit 4.3	Alternative Transportation—Low-Emitting, Fuel-Efficient Vehicles	3
2			d	Credit 4.4	Alternative Transportation—Parking Capacity	2
		1	c	Credit 5.1	Site Development—Protect or Restore Habitat	1
	1		d	Credit 5.2	Site Development—Maximize Open Space	1
		1	d	Credit 6.1	Stormwater Design—Quantity Control	1
		1	d	Credit 6.2	Stormwater Design—Quality Control	1
1			c	Credit 7.1	Heat Island Effect—Non-roof	1
1			d	Credit 7.2	Heat Island Effect—Roof	1
	1		d	Credit 8	Light Pollution Reduction	1

Water Efficiency — Possible Points: 10

Y	?	N				
4	3	3				
Y			d	Prereq 1	Water Use Reduction—20% Reduction	
2	2		d	Credit 1	Water Efficient Landscaping	2 to 4
		2	d	Credit 2	Innovative Wastewater Technologies	2
2	1	1	d	Credit 3	Water Use Reduction	2 to 4

Energy and Atmosphere — Possible Points: 35

Y	?	N				
4	17	14				
Y			c	Prereq 1	Fundamental Commissioning of Building Energy Systems	
Y			d	Prereq 2	Minimum Energy Performance	
Y			d	Prereq 3	Fundamental Refrigerant Management	
2	10	7	d	Credit 1	Optimize Energy Performance	1 to 19
	3	4	d	Credit 2	On-Site Renewable Energy	1 to 7
2			c	Credit 3	Enhanced Commissioning	2
	2		d	Credit 4	Enhanced Refrigerant Management	2
		3	c	Credit 5	Measurement and Verification	3
	2		c	Credit 6	Green Power	2

Materials and Resources — Possible Points: 14

Y	?	N				
6	1	7				
Y			d	Prereq 1	Storage and Collection of Recyclables	
		3	c	Credit 1.1	Building Reuse—Maintain Existing Walls, Floors, and Roof	1 to 3
		1	c	Credit 1.2	Building Reuse—Maintain 50% of Interior Non-Structural Elements	1
2			c	Credit 2	Construction Waste Management	1 to 2
		2	c	Credit 3	Materials Reuse	1 to 2

Materials and Resources, Continued

Y	?	N				
2			c	Credit 4	Recycled Content	1 to 2
2			c	Credit 5	Regional Materials	1 to 2
		1	c	Credit 6	Rapidly Renewable Materials	1
	1		c	Credit 7	Certified Wood	1

Indoor Environmental Quality — Possible Points: 15

Y	?	N				
11	3	1				
Y			d	Prereq 1	Minimum Indoor Air Quality Performance	
Y			d	Prereq 2	Environmental Tobacco Smoke (ETS) Control	
1			d	Credit 1	Outdoor Air Delivery Monitoring	1
	1		d	Credit 2	Increased Ventilation	1
1			c	Credit 3.1	Construction IAQ Management Plan—During Construction	1
		1	c	Credit 3.2	Construction IAQ Management Plan—Before Occupancy	1
1			c	Credit 4.1	Low-Emitting Materials—Adhesives and Sealants	1
1			c	Credit 4.2	Low-Emitting Materials—Paints and Coatings	1
1			c	Credit 4.3	Low-Emitting Materials—Flooring Systems	1
1			c	Credit 4.4	Low-Emitting Materials—Composite Wood and Agrifiber Products	1
	1		d	Credit 5	Indoor Chemical and Pollutant Source Control	1
1			d	Credit 6.1	Controllability of Systems—Lighting	1
1			d	Credit 6.2	Controllability of Systems—Thermal Comfort	1
1			d	Credit 7.1	Thermal Comfort—Design	1
	1		d	Credit 7.2	Thermal Comfort—Verification	1
1			d	Credit 8.1	Daylight and Views—Daylight	1
1			d	Credit 8.2	Daylight and Views—Views	1

Innovation and Design Process — Possible Points: 6

Y	?	N				
6						
1			d/c	Credit 1.1	Innovation in Design/ Exemplary Performance	1
1			d/c	Credit 1.2	Innovation in Design/ Exemplary Performance	1
1			d/c	Credit 1.3	Innovation in Design/ Exemplary Performance	1
1			d/c	Credit 1.4	Innovation in Design	1
1			d/c	Credit 1.5	Innovation in Design	1
1			d	Credit 2	LEED Accredited Professional	1

Regional Priority Credits — Possible Points: 4

EAc2 (1%), EQc81, SSc4.1, SSc7.1, WEc2, WEc3 (40%)

Y	?	N				
3	1					
1				Credit 1.1	Regional Priority: EQc8.1	1
1				Credit 1.2	Regional Priority: SSc4.1	1
1				Credit 1.3	Regional Priority: SSc7.1	1
	1			Credit 1.4	Regional Priority: EAc2, WEc2, WEc3	1

Project Total — Possible Points: 110

Y	?	N	
54	28	28	

Certified 40 to 49 points Silver 50 to 59 points Gold 60 to 79 points Platinum 80 to 110

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LEED SCORECARD
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
CIM Group

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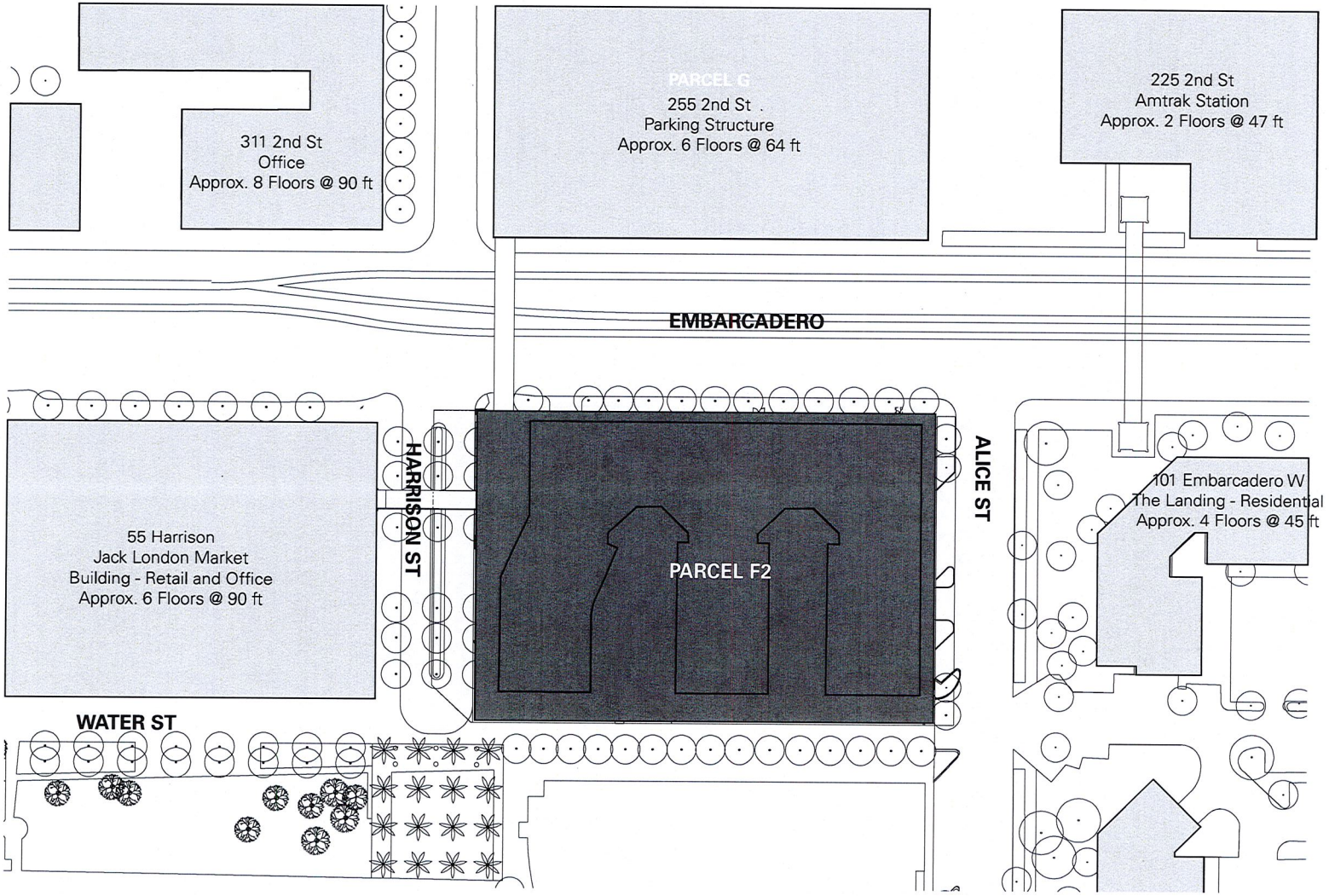
F2: 2016013.000



0 25' 50'

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SITE MAP
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
CIM Group

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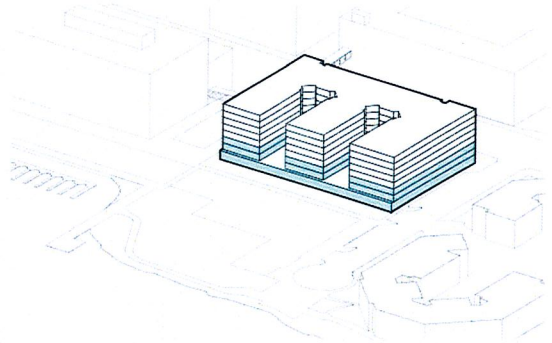
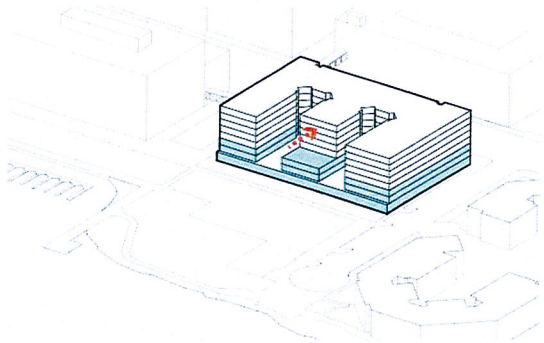
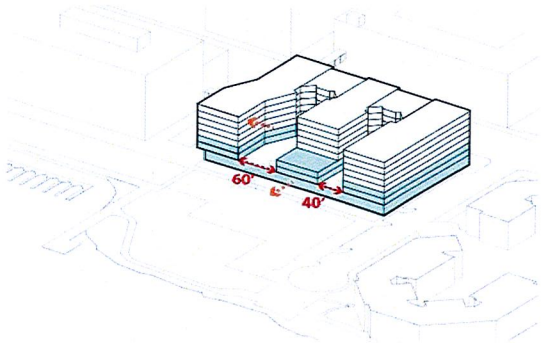
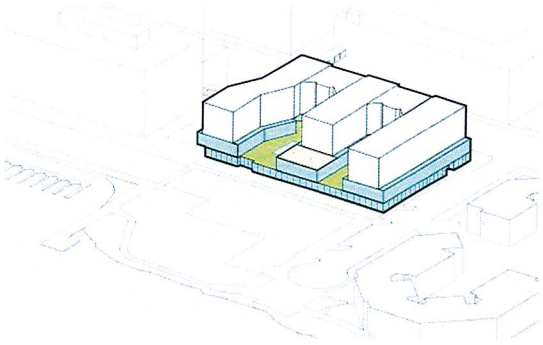
F2. 2016013.000

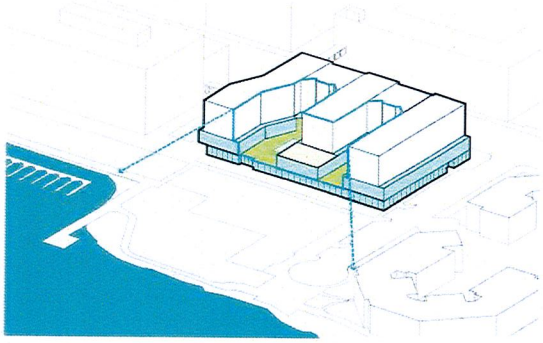
F2 - 3.6

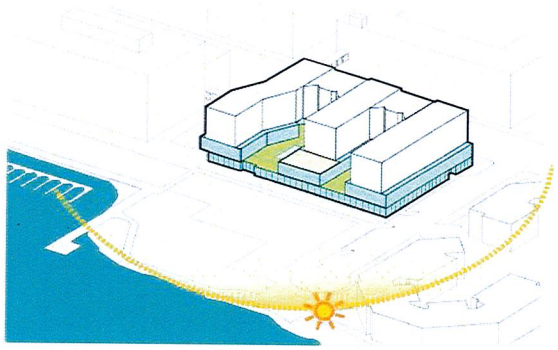












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MASSING ANALYSIS
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
CIM Group

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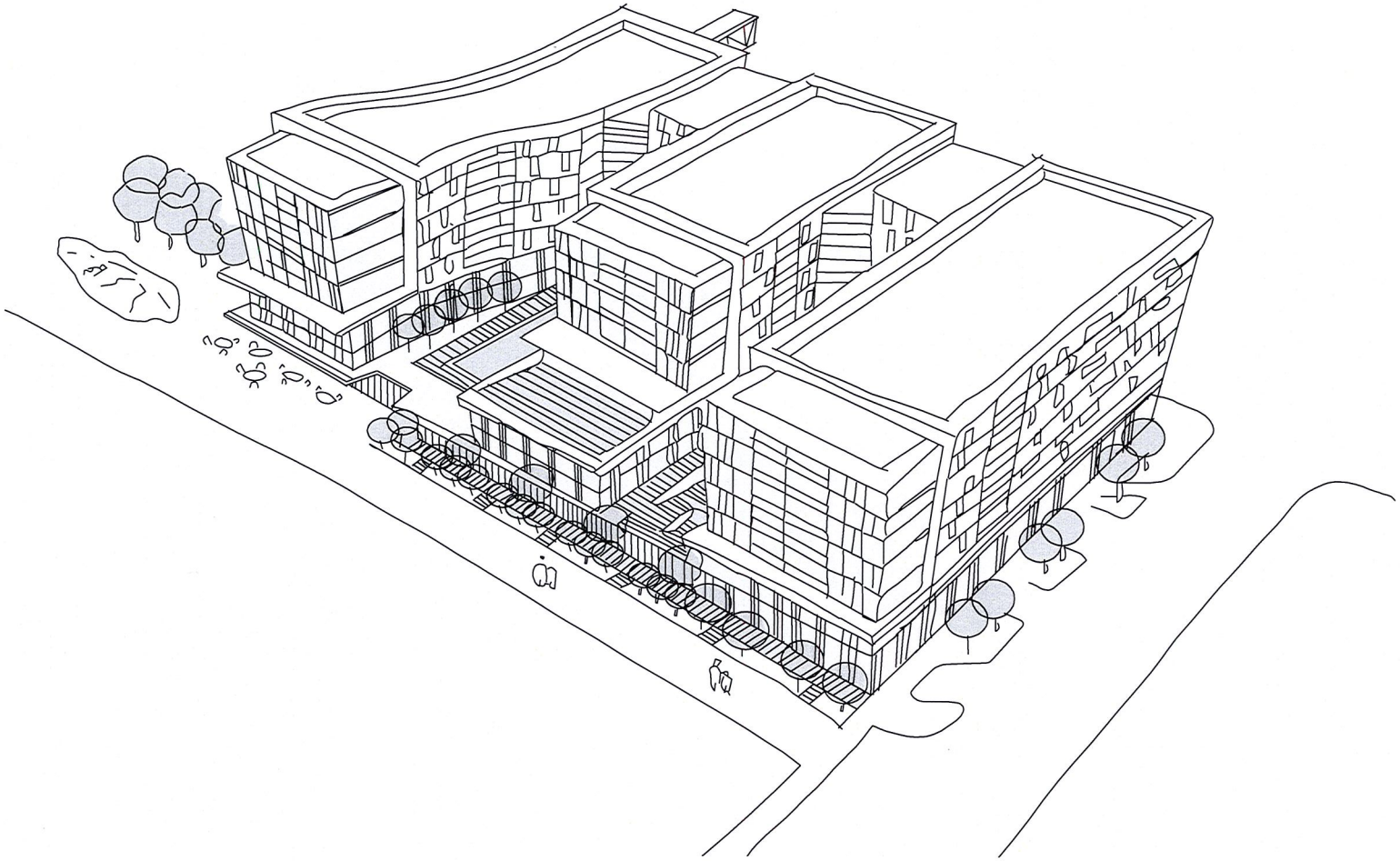
F2: 2016013.000



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MASSING SKETCH

PLANNING COMMISSION HEARING

Jack London Square - Parcel F2

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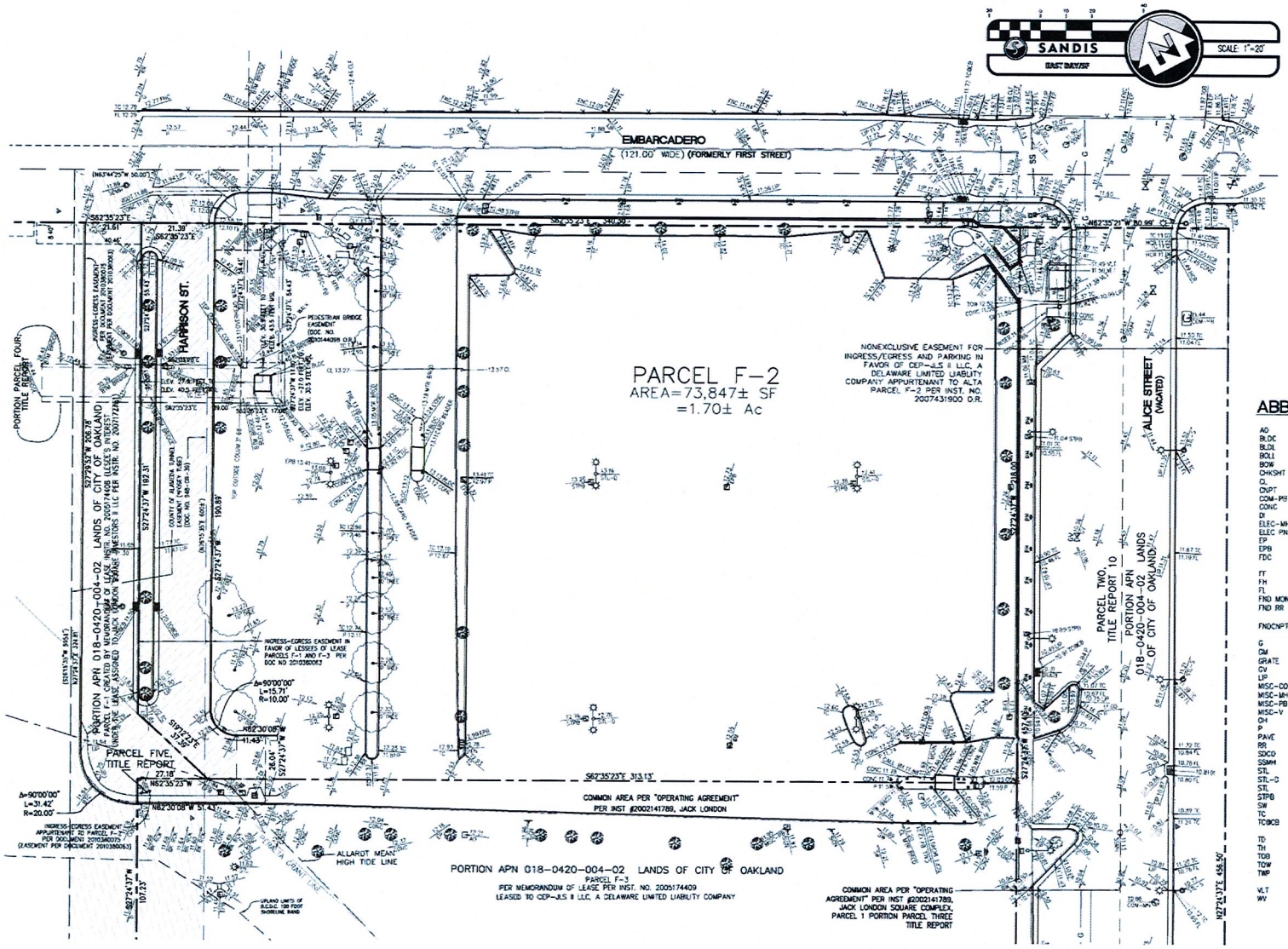
F2: 2016013.000

F2 - 3.8



TREE NOTE

TREE SPECIES AND SIZE PER ARBORIST REPORT BY SBCA TREE CONSULTING DATED SEPTEMBER 11, 2015.

SURVEY NOTE

SURVEY INFORMATION SHOWN INCLUDES POINTS FROM SURVEYS CONDUCTED IN 2011 AND 2006. SOME INFORMATION SHOWN MAY REPRESENT OBJECTS WHICH HAVE SINCE BEEN DEMOLISHED.

BENCHMARK

THE BASIS OF ELEVATION FOR THIS SURVEY IS THE PORT OF OAKLAND MONUMENT No. H016, AN ALUMINUM DISK STAMPED "PORT OF OAKLAND SURVEY CONTROL" ON EMBARCADERO AT WEBSTER STREET.

ELEVATION = 11.838 FEET (PORT OF OAKLAND DATUM)

SURVEY UTILITY NOTE

THE TYPES, LOCATIONS, SIZES AND /OR DEPTHS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS TOPOGRAPHIC SURVEY ARE APPROXIMATE AND WERE OBTAINED FROM SOURCES OF VARYING RELIABILITY. ONLY ACTUAL EXCAVATION WILL REVEAL THE TYPES, EXTENT, SIZES, LOCATIONS AND DEPTHS OF SUCH UNDERGROUND UTILITIES. A REASONABLE EFFORT HAS BEEN MADE TO LOCATE AND DELINEATE ALL KNOWN UNDERGROUND UTILITIES. HOWEVER, THE ENGINEER CAN ASSUME NO RESPONSIBILITY FOR THE COMPLETENESS OR ACCURACY OF ITS DELINEATION OF SUCH UNDERGROUND UTILITIES WHICH MAY BE ENCOUNTERED, BUT WHICH ARE NOT SHOWN ON THIS SURVEY.

ABBREVIATIONS

AD	– AREA DRAIN
BLDC	– BUILDING CORNER
BLDL	– BUILDING LINE
BOLL	– BOLLARD
BOW	– BOTTOM OF WALL
CHKSHT	– SURVEY CHECK SHOT
CL	– CENTER LINE OR CLASS
CNPT	– SURVEY CONTROL POINT
COM-PB	– COMMUNICATIONS PULLBOX
CONC	– CONCRETE
DI	– DRAIN INLET
ELEC-MH	– ELECTRICAL MANHOLE
ELEC PNL	– ELECTRICAL PANEL
EP	– EDGE OF PAVEMENT
EPB	– ELECTRICAL PULLBOX
FDC	– FIRE DEPARTMENT CONNECTION
FF	– FINISHED FLOOR
FH	– FIRE HYDRANT
FL	– FLOW LINE
FND MON	– FOUND SURVEY MONUMENT
FND RR	– FOUND RAILROAD SPIKE (SURVEY BENCHMARK)
FNDCNPT	– FOUND SURVEY CONTROL POINT
G	– GROUND
GM	– GAS METER
GRATE	– DRAIN INLET GRATE
GV	– GAS VALVE
LIP	– LIP OF GUTTER
MISC-CO	– MISCELLANEOUS CLEANOUT
MISC-MH	– MISCELLANEOUS MANHOLE
MISC-PB	– MISCELLANEOUS PULLBOX
MISC-V	– MISCELLANEOUS VALVE
OH	– BUILDING OVERHANG
P	– PAVEMENT ELEVATION
PAVE	– PAVER ELEVATION
RR	– RAILROAD TRACK CENTERLINE
SDCO	– STORM DRAIN CLEANOUT
SSMH	– SANITARY SEWER MANHOLE
STL	– STREET LIGHT
STL-D	– DOUBLE-ARM STREET LIGHT
STL	– SINGLE-ARM STREET LIGHT
STPB	– STREET LIGHT PULLBOX
SW	– SIDEWALK
TC	– TOP OF CURB
TCBCB	– TOP OF CURB AT CATCH BASIN
TD	– TRENCH DRAIN
TH	– DOOR THRESHHOLD
TOB	– TOP OF BANK
TOW	– TOP OF WALL
TWP	– SURVEY TEMPORARY WORK POINT
VLT	– VAULT
WV	– WATER VALVE

LEGEND

ALL DISTANCES AND DIMENSIONS ARE SHOWN IN FEET AND DECIMALS THEREOF.

	BUILDING FACE
	EDGE OF PAVEMENT
	BUILDING OVERHANG
	CURB LINE
SS	SANITARY SEWER
SD	STORM DRAIN
T	TELECOMMUNICATIONS LINE
W	WATER LINE
UGE	UNDERGROUND ELECTRIC LINE
X	FENCE
101 / 100	CONTOURS
HWS	HOT WATER SUPPLY
HWR	HOT WATER RETURN
CWS	CHILLED WATER SUPPLY
CWR	CHILLED WATER SUPPLY
TV/FA	TV / FIRE ALARM
TEL	TELECOM
460V	ELECTRIC
FG	(UNKNOWN UTILITY)
A	(UNKNOWN UTILITY)
	FOUND STANDARD CITY MONUMENT
SDMH	STORM DRAIN MANHOLE
SSMH	SANITARY SEWER MANHOLE
SSCO	SANITARY SEWER CLEANOUT
AD	AREA DRAIN
	ELECTRIC UTILITY BOX
	TELEPHONE PULL BOX
	CABLE TELEVISION BOX
	POWER POLE
	WATER VALVE
	GAS VALVE
	WATER METER
	ELECTROLIER
	ELECTRIC VALVE
	SIGN
	GUY WIRE
	FIRE HYDRANT
x 95.94	SPOT ELEVATION
	TREE WITH DRIPLINE, TYPE AND DIAMETER AS INDICATED

CIM

BOUNDARY AND TOPOGRAPHIC SURVEY
DESIGN REVIEW COMMITTEE
Jack London Square - Parcel F2
CIM Group

02 - 22 - 2017

C0.1

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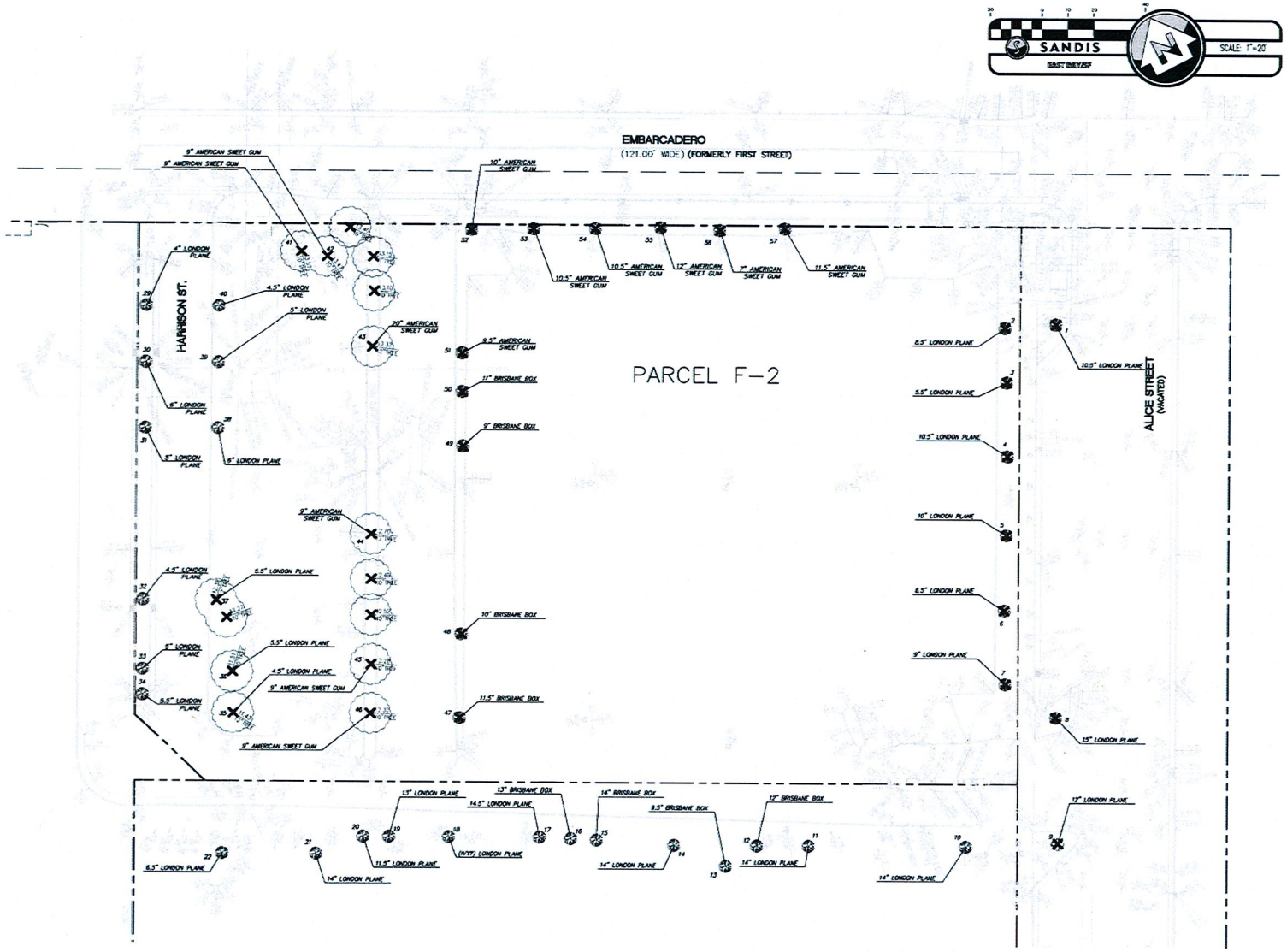

SURVEY NOTE

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ELEVATION = 11.838 FEET (PORT OF OAKLAND DATUM)

LEGEND

TREE WITH DRIPLINE, TYPE AND DIAMETER AS INDICATED

TREE WITH DIAMETER AS INDICATED

DEMO AND REMOVE EX. TREE

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TREE PLAN
DESIGN REVIEW COMMITTEE
Jack London Square - Parcel F2
CIM Group

02 - 22 - 2017

C1.1

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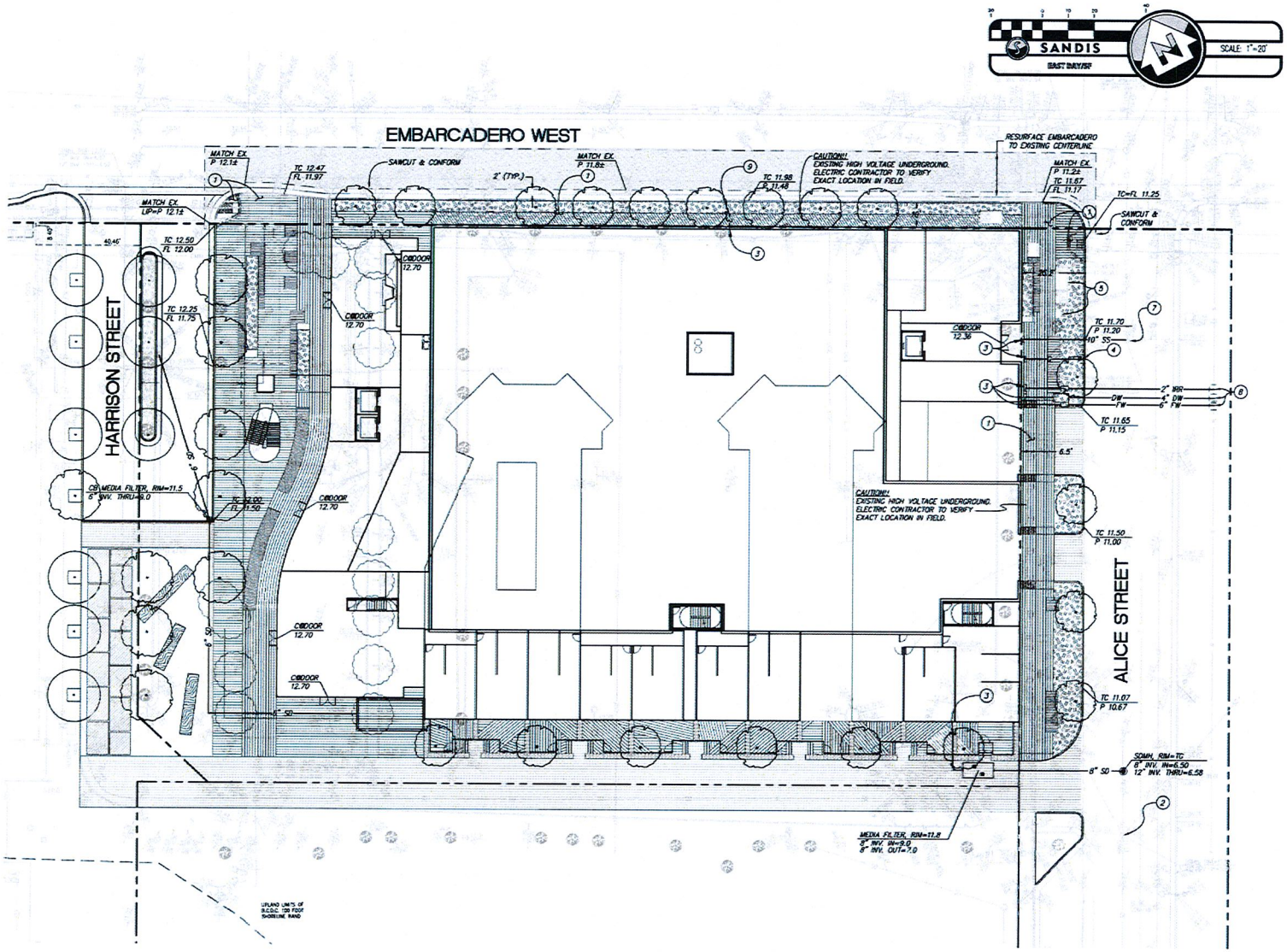

GENERAL NOTES

1. INFORMATION REGARDING EXISTING SUBSURFACE IMPROVEMENTS AND UTILITIES SHOWN ON THESE PLANS WAS TAKEN FROM RECORD DATA KNOWN TO THE DESIGN ENGINEER AND IS NOT MEANT TO BE A FULL CATALOG OF EXISTING CONDITIONS. CONTRACTOR SHALL CONDUCT FIELD INVESTIGATIONS AS REQUIRED TO VERIFY THE LOCATION AND ELEVATION OF EXISTING SUBSURFACE IMPROVEMENTS AND UTILITIES (WHETHER SHOWN ON THESE PLANS OR NOT) PRIOR TO THE COMMENCEMENT OF WORK. CONTRACTOR SHALL NOTIFY THE CIVIL DESIGN ENGINEER IMMEDIATELY UPON DISCOVERY OF ANY DISCREPANCIES BETWEEN EXISTING CONDITIONS IN THE FIELD AND INFORMATION SHOWN ON THESE PLANS.
2. ELEVATIONS AND LOCATIONS OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO START OF CONSTRUCTION AFFECTING SAID LINES.

SHEET NOTES

1. REPLACE EXISTING SIDEWALK AND CURB RAMP IN-KIND.
2. CONNECT TO EXISTING STORM DRAIN MANHOLE.
3. CONNECT TO EXISTING STORM DRAIN MAIN.
4. CONNECT TO EXISTING GAS MAIN.
5. EXISTING ELECTRIC VAULT/STRUCTURE TO REMAIN.
6. CONNECT TO EXISTING SANITARY SEWER MAIN.
7. CONNECT TO EXISTING SSMH
8. CONNECT TO WATER MAIN
9. CONNECT TO EXISTING ELECTRIC SERVICE.
10. UTILITY SERVICE POINT OF CONNECTION. SEE MEP PLANS FOR CONTINUATION.

LEGEND

— — — —	PROPERTY LINE
— — — —	SAWCUT LINE
—SS—	SANITARY SEWER LINE
—SD—	STORM DRAIN LINE
—FW—	FIRE WATER LINE
—WTR—	WATER LINE

EARTHWORK QUANTITIES:

FILL = 923 CY
CUT = 9227 CY
(IF ZERO, INDICATE 0)

NOTE:
THE EARTHWORK QUANTITIES SHOWN ARE PROVIDED FOR THE PURPOSE OF GRADING PERMIT APPROVAL ONLY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CARRY OUT THE CUT/FILL, IMPORT/EXPORT AS NECESSARY TO MEET THE DESIGN GRADES AS SHOWN ON THE PLANS REGARDLESS OF THE ESTIMATED EARTHWORK QUANTITIES AS INDICATED. SIGNIFICANT REVISIONS TO THE QUANTITIES NEED REVIEW BY THE CITY. FILL SHORTAGE IS ANTICIPATED TO COME FROM ON-SITE SPOILS ACQUIRED FROM UTILITY TRENCHES AND FOOTING SPOILS.

0 20' 40'

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GRADING AND UTILITY PLAN
DESIGN REVIEW COMMITTEE
Jack London Square - Parcel F2
CIM Group

02 - 22 - 2017

C3.0

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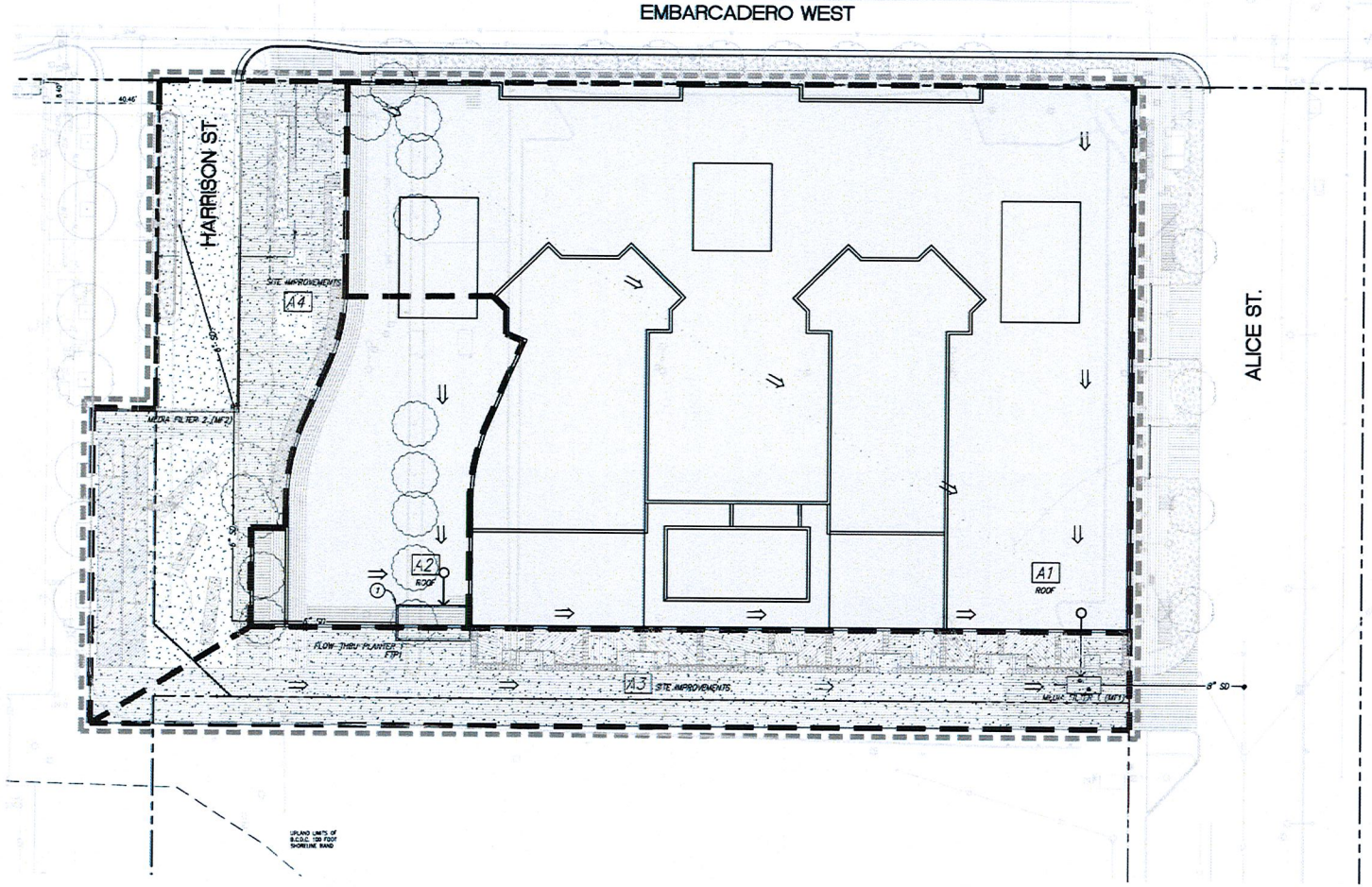

SANDIS
EAST BAYSIDE
SCALE: 1"=20'

PRELIMINARY STORMWATER TREATMENT MEASURES

NEW IMPERVIOUS AREA (SF)

AREA ID	ROOF	IMPERVIOUS	TOTAL	TREATED VIA
A1	47,536	0	47,536	MF1
A2	7,649	0	7,649	FTP1
A3	0	11,293	11,293	MF1
A4	0	10,013	10,013	MF2

SHEET NOTES

(1) FLOW-THRU PLANTER (306 SF). LOCATIONS AND NUMBER OF PLANTERS TO BE DETERMINED

STORMWATER MANAGEMENT PLAN NOTES

1. THE PROPOSED PROJECT WILL INCLUDE MORE THAN 10,000 SQUARE FEET OF IMPERVIOUS SURFACE AND HAS THEREBY BEEN PLANNED TO COMPLY WITH THE PROVISION C.3 – NEW DEVELOPMENT AND REDEVELOPMENT OF THE MUNICIPAL REGIONAL STORMWATER PERMIT (ORDER NO. R2-2009-0074).
2. GENERAL STORMWATER QUALITY APPROACH – STORMWATER QUALITY FEATURES WILL INCLUDE MINIMIZING IMPERVIOUS SURFACES AND FILTERING STORMWATER RUNOFF BY DRAINING IMPERVIOUS SURFACES TO MEDIA FILTERS PRIOR TO BEING CONVEYED OFFSITE.

TRANSIT ORIENTED DEVELOPMENT NOTE

THIS PROJECT QUALIFIES AS A TRANSIT ORIENTED DEVELOPMENT PROJECT AND IS CONSIDERED A CATEGORY C SPECIAL PROJECT. BASED ON THE ALAMEDA COUNTY SPECIAL PROJECTS WORKSHEET, THIS PROJECT COULD BE ALLOWED TO USE NON-LID TREATMENT METHODS FOR UP TO 100% OF THE PROJECT'S REQUIRED STORMWATER TREATMENT.

TOTAL PROJECT IMPERVIOUS AREA: 76,491 S.F.
AREA ALLOWED TO BE TREATED BY NON-LID METHODS: 68,842 SF
AREA PROPOSED TO BE TREATED BY NON-LID METHODS: 7,649 SF
TRANSIT ORIENTED DEVELOPMENT: 90%

LEGEND

- PROPERTY LINE
- LIMIT OF SITE WORK
- WATERSHED BOUNDARY
- DIRECTION OF RUNOFF
- ROOF (IMPERVIOUS)
- PAVING (IMPERVIOUS)

0 20' 40'

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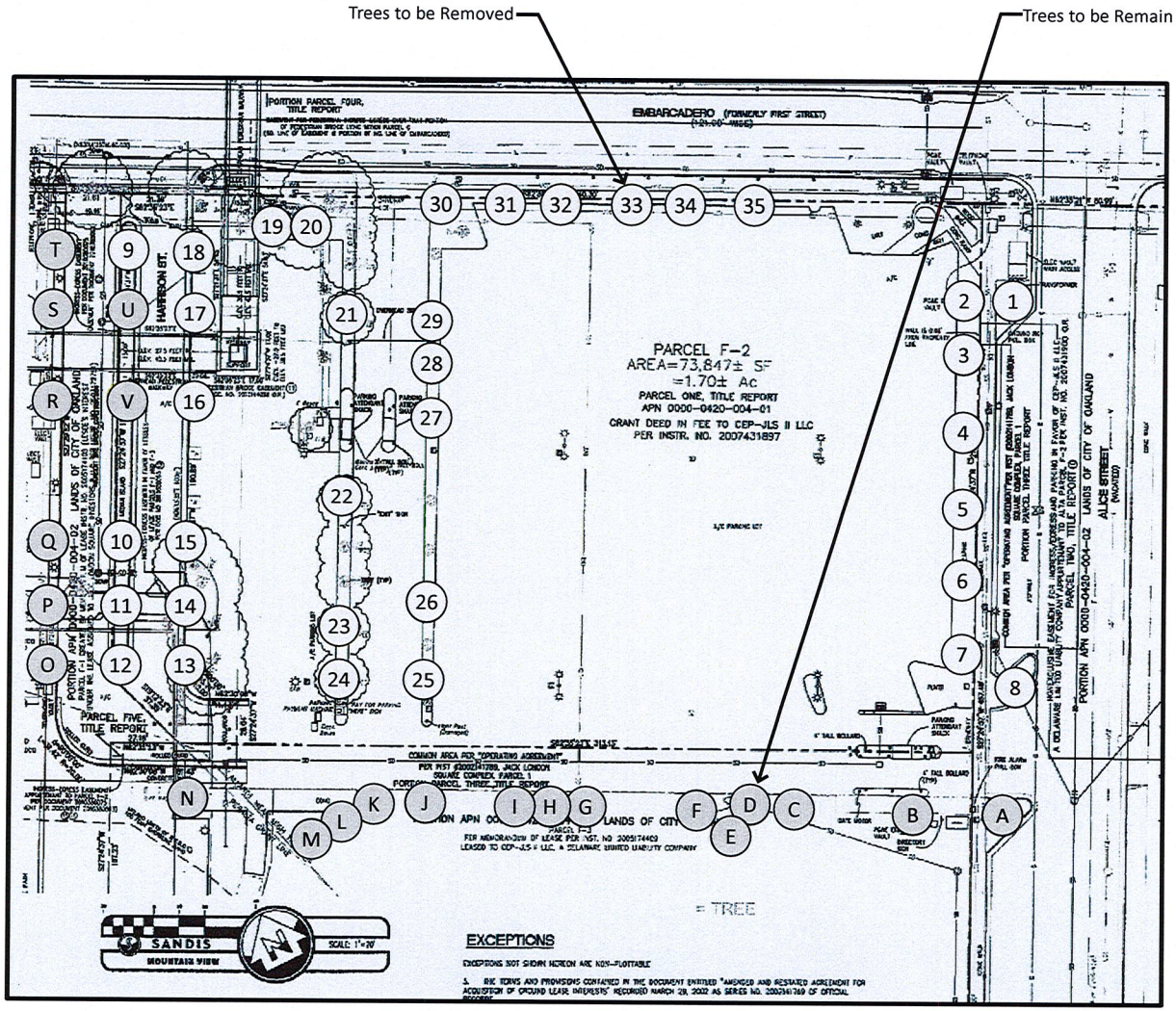
STORMWATER MANAGEMENT PLAN
DESIGN REVIEW COMMITTEE
Jack London Square - Parcel F2
CIM Group

02 - 22 - 2017

C4.0



COLUMN HEADING DESCRIPTIONS 9/13/2015

Tag# - Corresponds to Tree Location Map
Species - Scientific name
Common Name - Vernacular name
DBH - Diameter measured in inches at 4.5 feet above soil grade, unless otherwise indicated
Spread - In feet
Tree Location - Designated location
Notes - Pertinant Arborist comments

Tag #	Species	Common name	DBH	Spread	Tree Location	Notes
1	Platanus x hispanica	London Plane	10.5	35	Public Right Of Way	Anthracnose
2	Platanus x hispanica	London Plane	6.5	20	Public Right Of Way	Lean, Anthracnose
3	Platanus x hispanica	London Plane	5.5	-	Public Right Of Way	Dead
4	Platanus x hispanica	London Plane	10.5	35	Public Right Of Way	Anthracnose
5	Platanus x hispanica	London Plane	10	30	Public Right Of Way	Anthracnose
6	Platanus x hispanica	London Plane	6.5	25	Public Right Of Way	Anthracnose
7	Platanus x hispanica	London Plane	9	30	Public Right Of Way	Anthracnose
8	Platanus x hispanica	London Plane	15	45	Public Right Of Way	Anthracnose
9	Platanus x hispanica	London Plane	4	-	Public Right Of Way	Dead
10	Platanus x hispanica	London Plane	4.5	10	Public Right Of Way	Powdery mildew, Anthracnose
11	Platanus x hispanica	London Plane	5	15	Public Right Of Way	Powdery mildew, Anthracnose
12	Platanus x hispanica	London Plane	5.5	15	Public Right Of Way	Powdery mildew, Anthracnose
13	Platanus x hispanica	London Plane	4.5	15	Public Right Of Way	Powdery mildew, Anthracnose
14	Platanus x hispanica	London Plane	5.5	20	Public Right Of Way	Powdery mildew, Anthracnose
15	Platanus x hispanica	London Plane	5.5	15	Public Right Of Way	Powdery mildew, Anthracnose
16	Platanus x hispanica	London Plane	4	20	Public Right Of Way	Powdery mildew, Anthracnose
17	Platanus x hispanica	London Plane	3	20	Public Right Of Way	Powdery mildew, Anthracnose
18	Platanus x hispanica	London Plane	4.5	15	Public Right Of Way	Powdery mildew, Anthracnose
19	Liquidambar styraciflua	American Sweet Gum	9	20	Public Right Of Way	Hole in trunk, Girdling roots, Branches in contact with pedestrian
20	Liquidambar styraciflua	American Sweet Gum	9	20	Public Right Of Way	Girdling roots
21	Liquidambar styraciflua	American Sweet Gum	20	25	Parcel F-2	Surface roots
22	Liquidambar styraciflua	American Sweet Gum	9	15	Parcel F-2	Internal decay, Structurally compromised
23	Liquidambar styraciflua	American Sweet Gum	9	20	Parcel F-2	Girdling roots, Surface roots
24	Liquidambar styraciflua	American Sweet Gum	9	20	Parcel F-2	Girdling roots, Surface roots
25	Lophostemon confertus	Brisbane Box	13.5	20	Parcel F-2	Curb displacement, Surface roots
26	Lophostemon confertus	Brisbane Box	10	15	Parcel F-2	Surface roots
27	Lophostemon confertus	Brisbane Box	9	15	Parcel F-2	Lean
28	Lophostemon confertus	Brisbane Box	11	15	Parcel F-2	
29	Liquidambar styraciflua	American Sweet Gum	9.5	15	Parcel F-2	Girdling roots, Surface roots
30	Liquidambar styraciflua	American Sweet Gum	10	25	Public Right Of Way	Surface roots
31	Liquidambar styraciflua	American Sweet Gum	10.5	30	Public Right Of Way	Girdling roots, Curb displacement
32	Liquidambar styraciflua	American Sweet Gum	10.5	30	Public Right Of Way	Surface roots, Girdling roots, Curb displacement
33	Liquidambar styraciflua	American Sweet Gum	12	30	Public Right Of Way	Surface roots, Curb displacement
34	Liquidambar styraciflua	American Sweet Gum	7	20	Public Right Of Way	Surface roots, Curb displacement
35	Liquidambar styraciflua	American Sweet Gum	11.5	25	Public Right Of Way	Curb displacement
A	Platanus x hispanica	London Plane	12	15	Public Right Of Way	Anthracnose
B	Platanus x hispanica	London Plane	14	35	Adjacent Parcel F-3	Anthracnose
C	Platanus x hispanica	London Plane	14	30	Adjacent Parcel F-3	Almost dead
D	Lophostemon confertus	Brisbane Box	12	25	Adjacent Parcel F-3	Wire girdling trunk
E	Lophostemon confertus	Brisbane Box	9.5	15	Adjacent Parcel F-3	Almost dead
F	Platanus x hispanica	London Plane	12.5	30	Adjacent Parcel F-3	Anthracnose
G	Lophostemon confertus	Brisbane Box	14	30	Adjacent Parcel F-3	
H	Lophostemon confertus	Brisbane Box	13	20	Adjacent Parcel F-3	Dieback
I	Platanus x hispanica	London Plane	14.5	35	Adjacent Parcel F-3	Severly stressed
J	Platanus x hispanica	London Plane	7 (avg)	30	Adjacent Parcel F-3	Severly stressed
K	Platanus x hispanica	London Plane	13	35	Adjacent Parcel F-3	Severly stressed
L	Platanus x hispanica	London Plane	11.5		Adjacent Parcel F-3	Dead
M	Platanus x hispanica	London Plane	14	35	Adjacent Parcel F-3	Almost dead
N	Platanus x hispanica	London Plane	6.5	15	Public Right Of Way	Powdery mildew, Anthracnose
O	Platanus x hispanica	London Plane	5	15	Public Right Of Way	Powdery mildew, Anthracnose
P	Platanus x hispanica	London Plane	5	15	Public Right Of Way	Powdery mildew, Anthracnose
Q	Platanus x hispanica	London Plane	5	15	Public Right Of Way	Powdery mildew, Anthracnose
R	Platanus x hispanica	London Plane	5	15	Public Right Of Way	Powdery mildew, Anthracnose
S	Platanus x hispanica	London Plane	4.5	15	Public Right Of Way	Powdery mildew, Anthracnose
T	Platanus x hispanica	London Plane	5.5	15	Public Right Of Way	Powdery mildew, Anthracnose
U	Platanus x hispanica	London Plane	6	15	Public Right Of Way	Powdery mildew, Anthracnose
V	Platanus x hispanica	London Plane	5	15	Public Right Of Way	Powdery mildew, Anthracnose

General Note: The City of Oakland defines Trees as Protected if they have a 9" or greater DBH or are located in the public right of way. All of the trees in this survey fall under this designation.

SBCA Tree Consulting
1534 Rose St. Crockett, CA 94525
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Phone (510) 787-3075
Fax (510) 787-3065
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0 16' 32'

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TREE SURVEY
DESIGN REVIEW COMMITTEE
Jack London Square - Parcel F2
CIM Group

02 - 22 - 2017

F2: 2016013.000
D: 2016041.000

F2 -TS1.01

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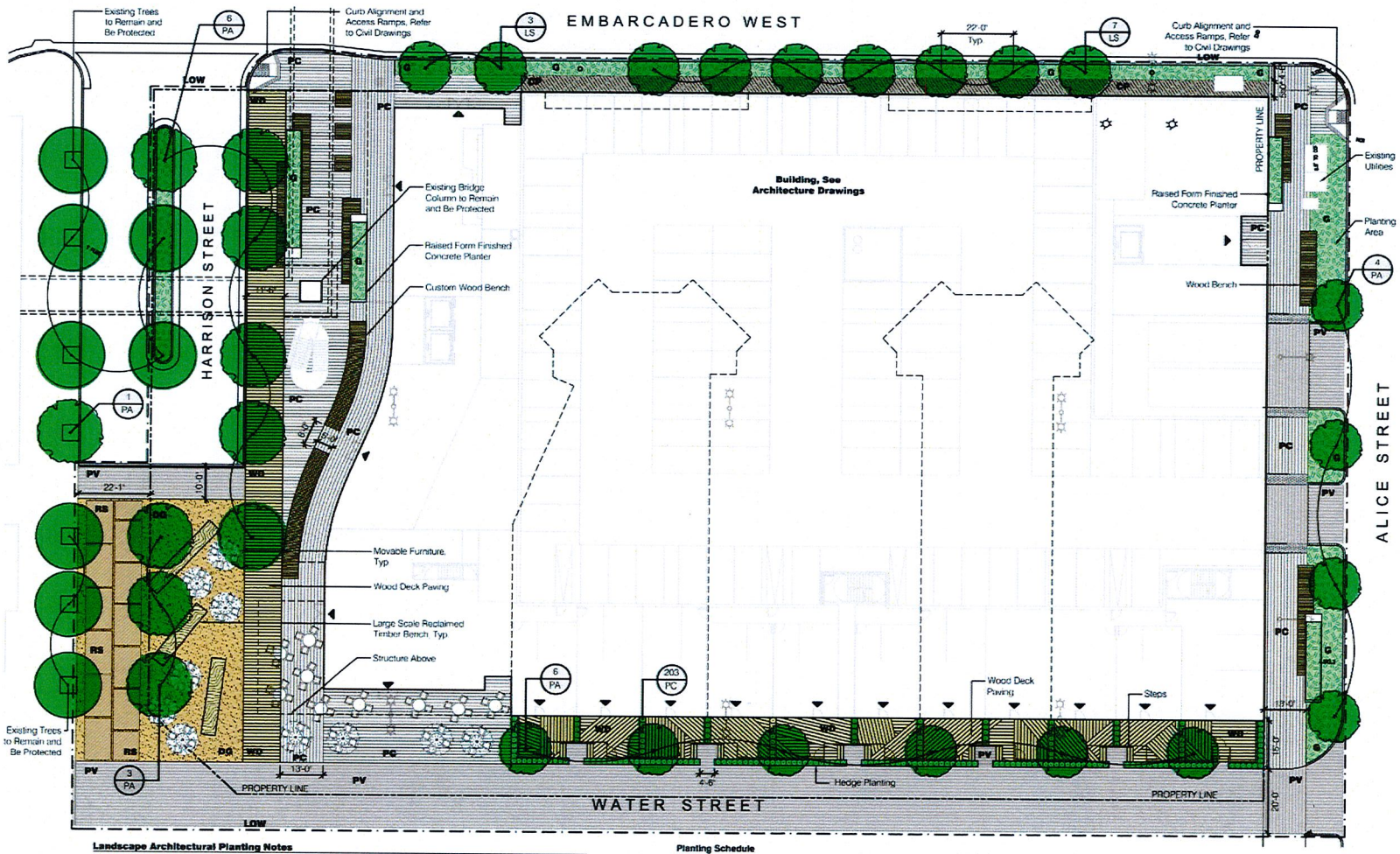

Landscape Architectural Planting Notes

1. The Contractor shall locate and verify all existing and new utility line locations prior to planting, and shall report any utility conflicts to the Construction Manager.
2. Contractor shall receive approval from Landscape Architect of plant layout prior to installation.
3. The trunk flare (at the base of the tree) shall be properly exposed for all plantings.
4. Keep root balls intact prior to and during planting operations. Plants with broken or damaged root balls shall be rejected and immediately removed from the site. Keep root balls damp and protected from damage due to sun and wind. Do not shave root balls.
5. Temporary irrigation shall be provided for plant establishment and maintenance period.
6. 1 year landscape maintenance and warranty period.

Planting Schedule

Key	Qty	Scientific Name	Common Name	Size	Rootball	Root	Notes
Trees							
LS	9	*Liquidamber straisaflolia 'Festival'*	Sweetgum	3.5' Cal	48"	Box	--
PA	20	*Platanus x acerifolia 'Columbia'*	London Plane	3.5' Cal	48"	Box	--
Shrubs							
PC	203	*Prunus carolniana*	Carolina Laurelcherry	3' Cal	15Gal	Cont.	24" O.C.
Groundcover Planting - 3,315 sq. ft. Total							
CT	33%	*Carex testacea*	Orange Sedge	1 Gal.	--	Cont.	18" O.C.
CA	33%	*Carex tumulicola*	Foothill Sedge	1 Gal.	--	Cont.	18" O.C.
PE	33%	*Pennisetum alopecuroides 'Little Bunny'*	Dwarf Fountain Grass	1 Gal.	--	Cont.	18" O.C.

Landscape Architectural Layout Notes

1. Use dimensional information given. Do not scale drawings. Direct questions about dimensions to the Landscape Architect in writing.
2. All dimensions are parallel or perpendicular to the lines from which they are measured unless noted otherwise.
3. All dimensions shown are taken to the face or centerline of elements unless otherwise noted.

Landscape Architectural Materials Notes

1. The Contractor shall refer any questions on materials, finishes, labor and/or products not specified herein to the Landscape Architect, prior to ordering materials or starting work.
2. All lines and dimensions are parallel or perpendicular to the lines from which they are measured unless otherwise shown.
3. Field Measurements: Take accurate field measurements before preparation of Shop Drawings and/or fabrication. Do not delay job progress.

Landscape Architectural Irrigation Notes

1. The irrigation system will be designed to distribute a minimum amount of water in order t promote active and healthy growth of all proposed plantings.
2. The irrigation system shall be designed and installed in conformance with all applicable state and local codes and ordinances. By licensed contractors and experienced workmen.
3. The irrigation controller shall have an automatic timer with battery backup and rain shutoff.
4. All valves shall have separate pressure regulators filters and shut off as necessary.
5. The system shall have a shut-off and reduced pressure backflow assembly.
6. The irrigation system shall be comprised of all drip or bubblers.

LEGEND

- LOW — Limit of Work
- Structure Above
- Property Line
- Egress
- PC — Precast Concrete Pavers - Pedestrian
- PV — Precast Concrete Pavers - Vehicular
- DG — Decomposed Granite Paving
- CP — Concrete Paving per City Standards
- WD — Wood Deck Paving
- RS — Reclaimed Steel Plate Paving
- G — Groundcover
- Custom Wood Bench
- Hedge Planting
- Movable Furniture
- Existing Trees
- Proposed Tree

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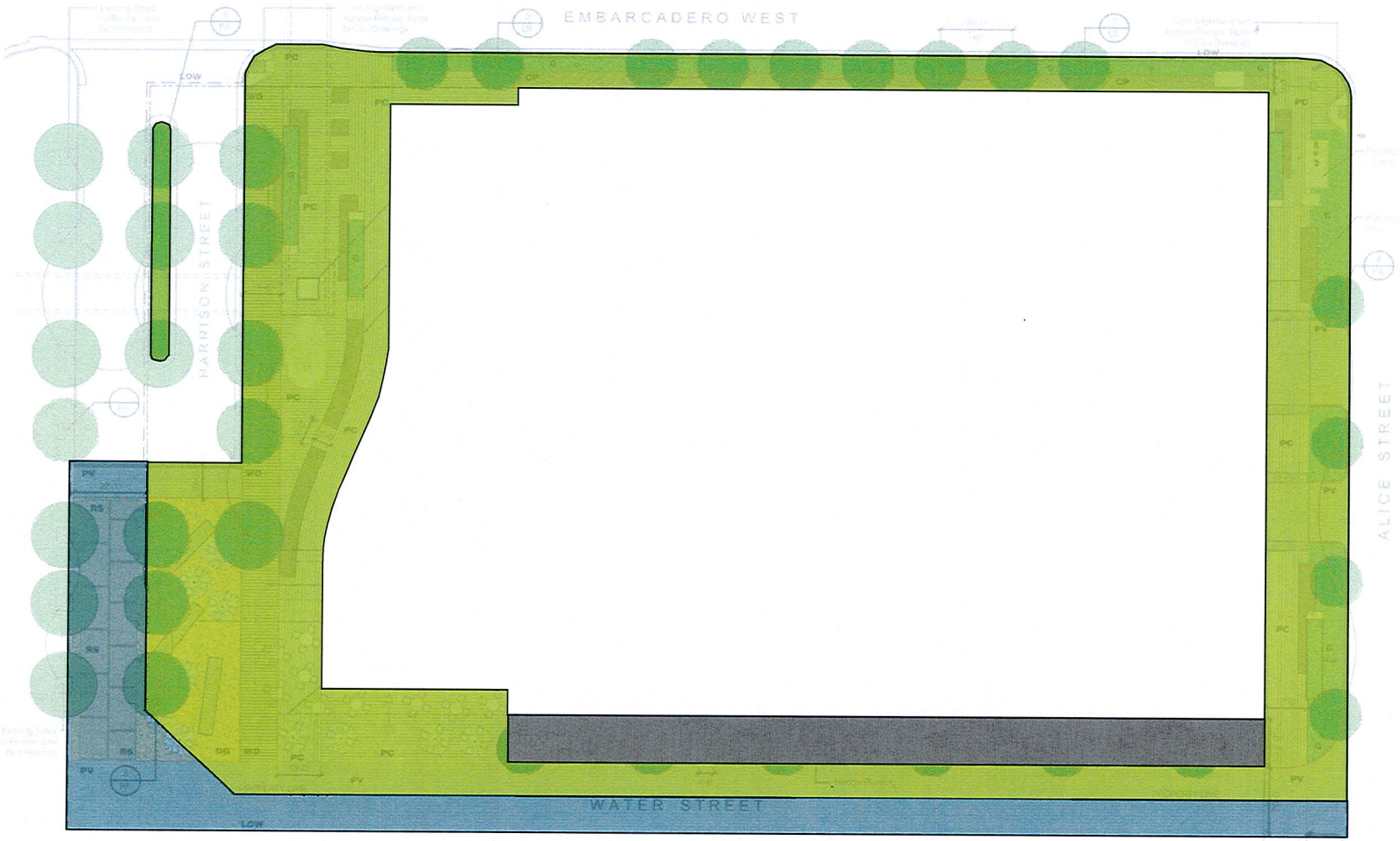
LANDSCAPE PLAN - GROUND LEVEL
DESIGN REVIEW COMMITTEE
Jack London Square - Parcel F2
CIM Group

02 - 22 - 2017

F2: 2016013.000
D: 2016041.000

F2 - L1.01



	PRIVATE USABLE OPEN SPACE ON THE GROUND FLOOR	11 UNITS x (150 x 2) = 3,300 sf
	GROUND FLOOR IMPROVEMENTS_ F2	23,210 sf
	GROUND FLOOR IMPROVEMENTS_ BEYOND F2	6,230 sf

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SITE IMPROVEMENTS - GROUND LEVEL
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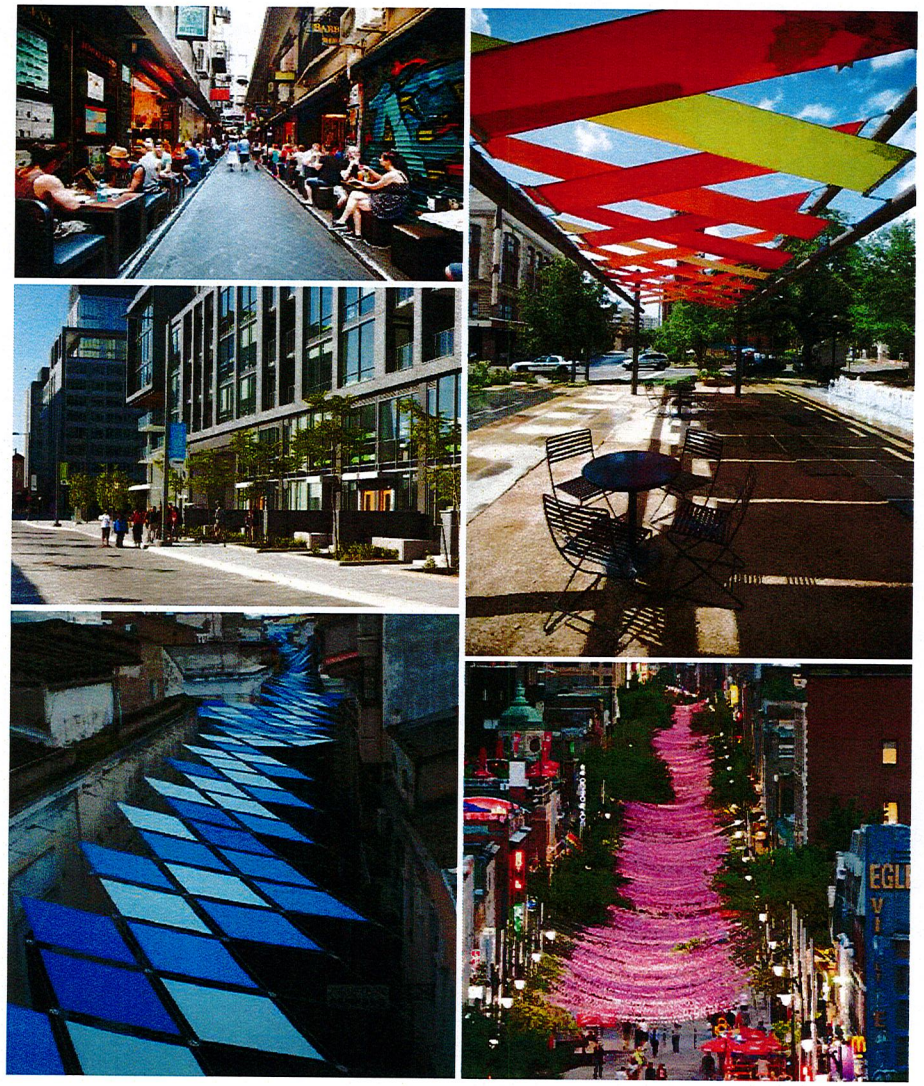
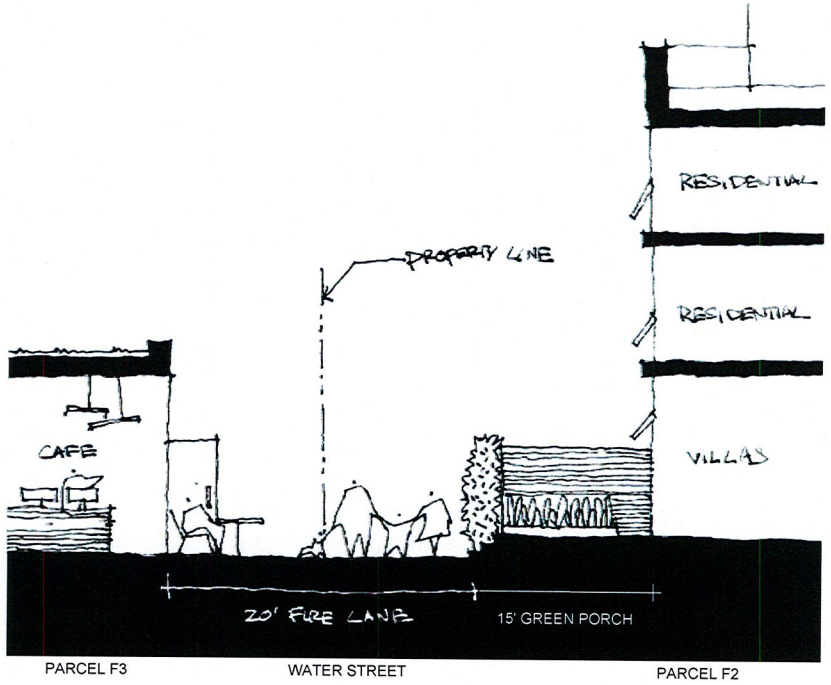
F2: 2016013.000
D: 2016041.000

F2 - L1.02





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WATER STREET SECTION
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
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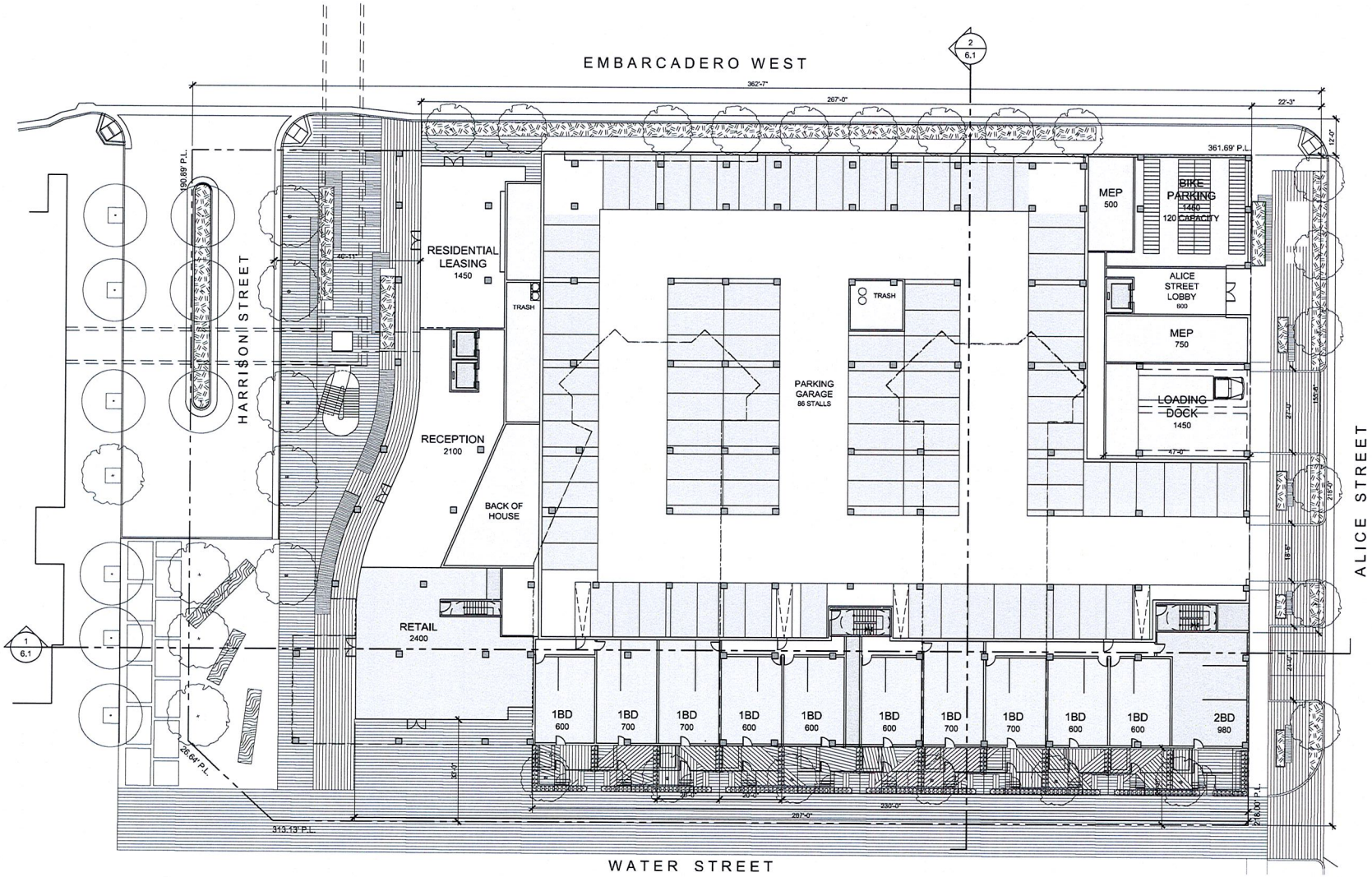
03 - 16 - 2017

F2: 2016013.000

F2 - L1.03



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GROUND FLOOR PLAN
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
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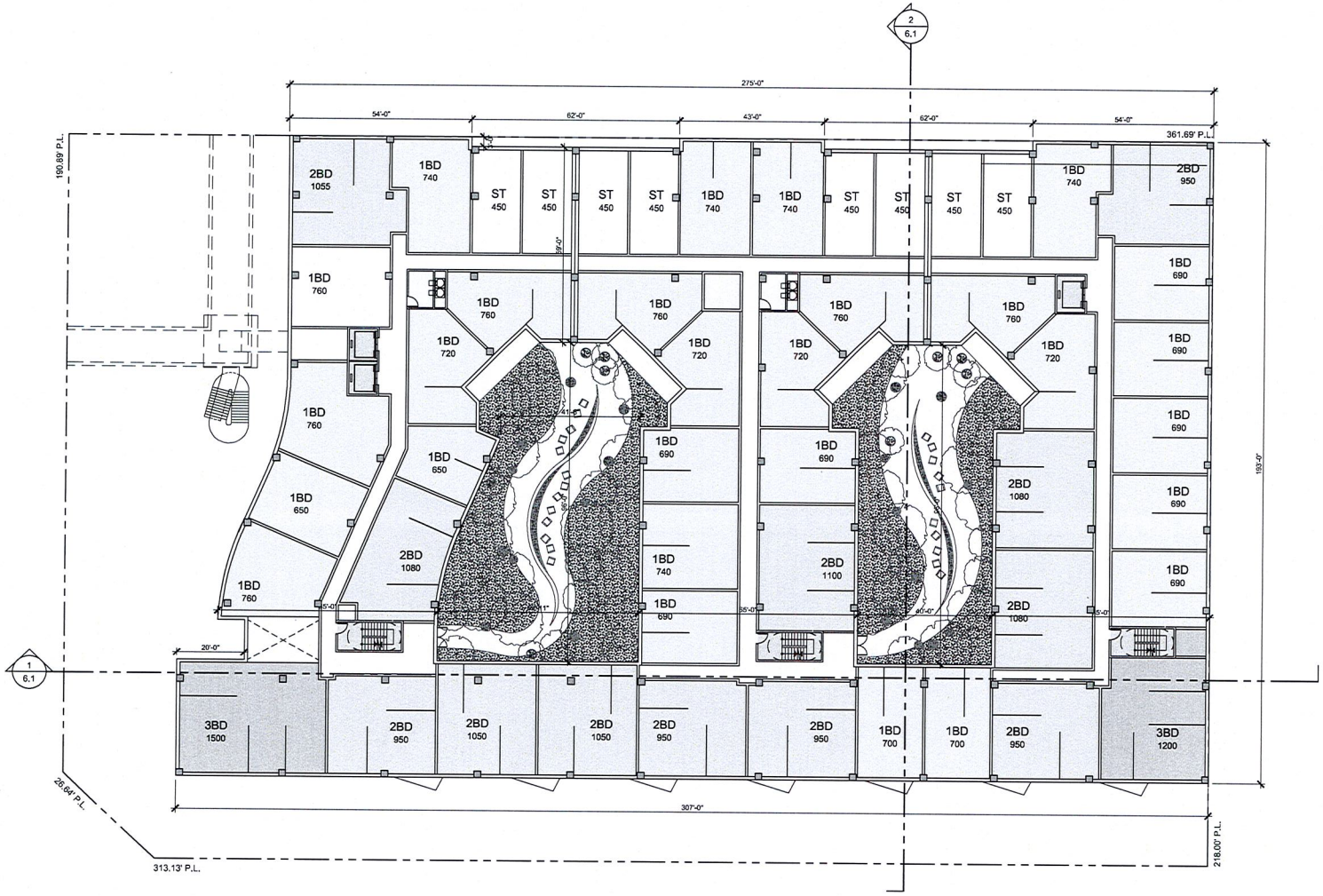
F2: 2016013.000

F2 - 4.1



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LEVEL 2 FLOOR PLAN

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F2: 2016013.000

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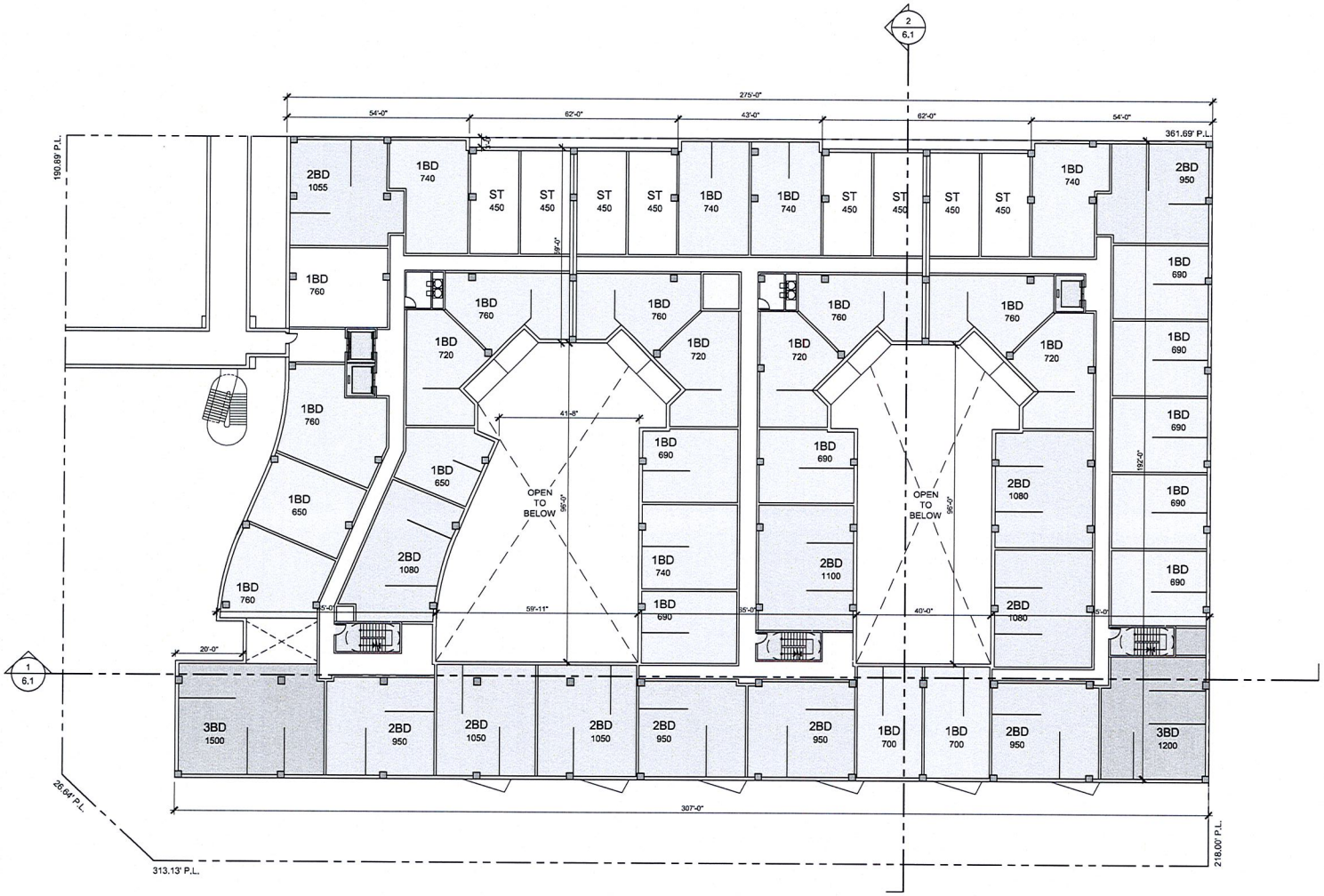
F2 - 4.2



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LEVEL 3 FLOOR PLAN

PLANNING COMMISSION HEARING

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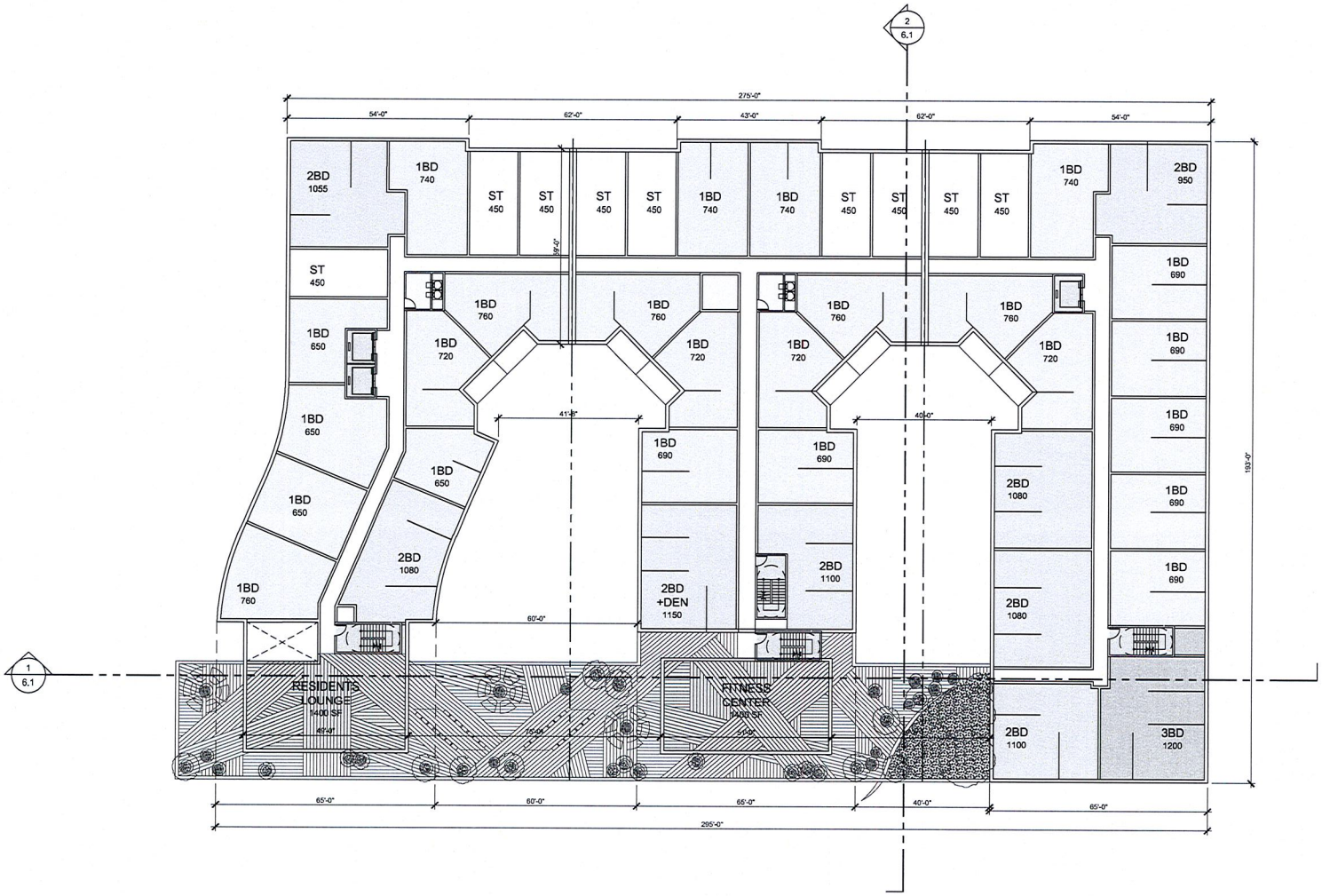
F2: 2016013.000

F2 - 4.3



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LEVEL 4 FLOOR PLAN

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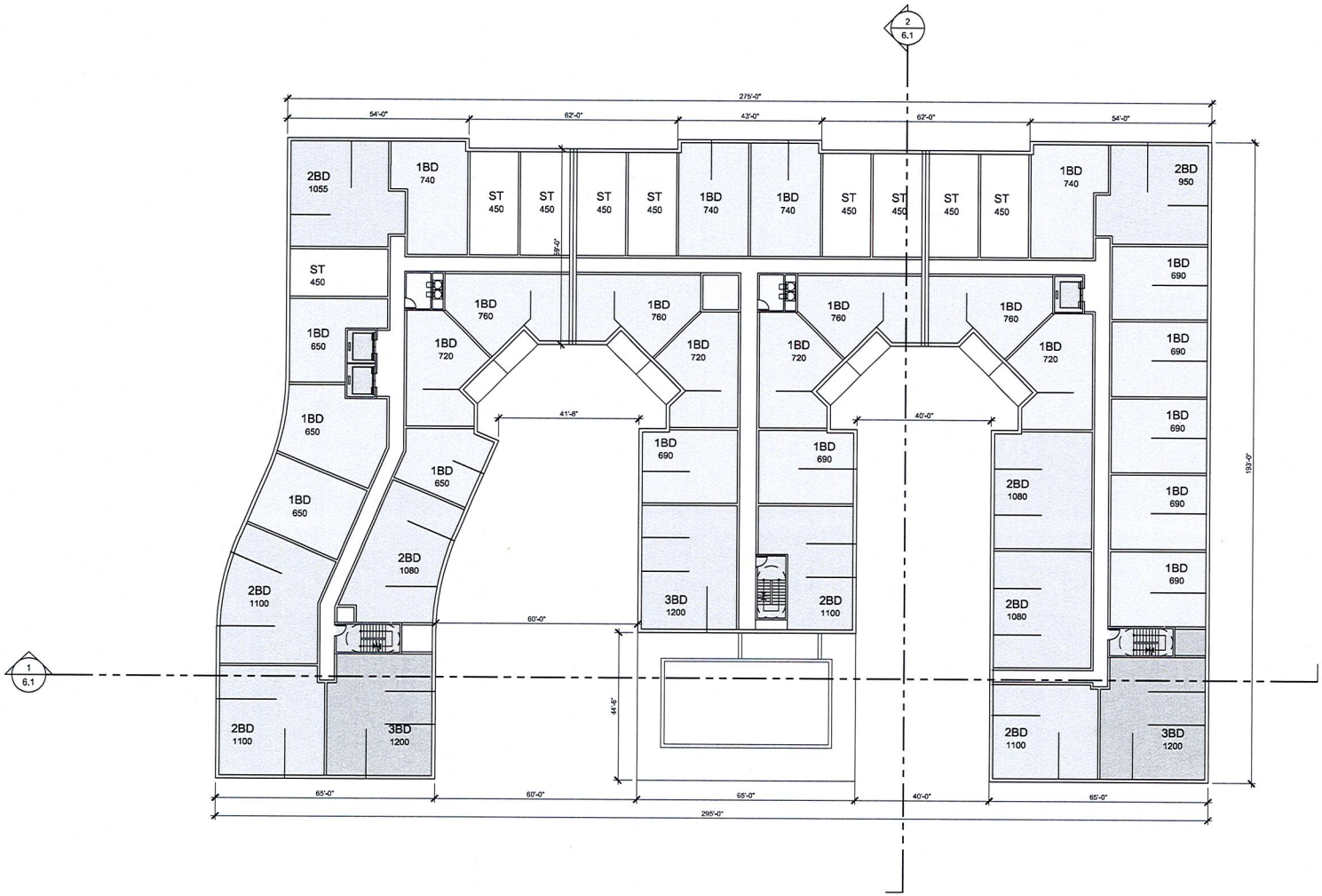
F2: 2016013.000

F2 - 4.4



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LEVEL 5-8 FLOOR PLAN

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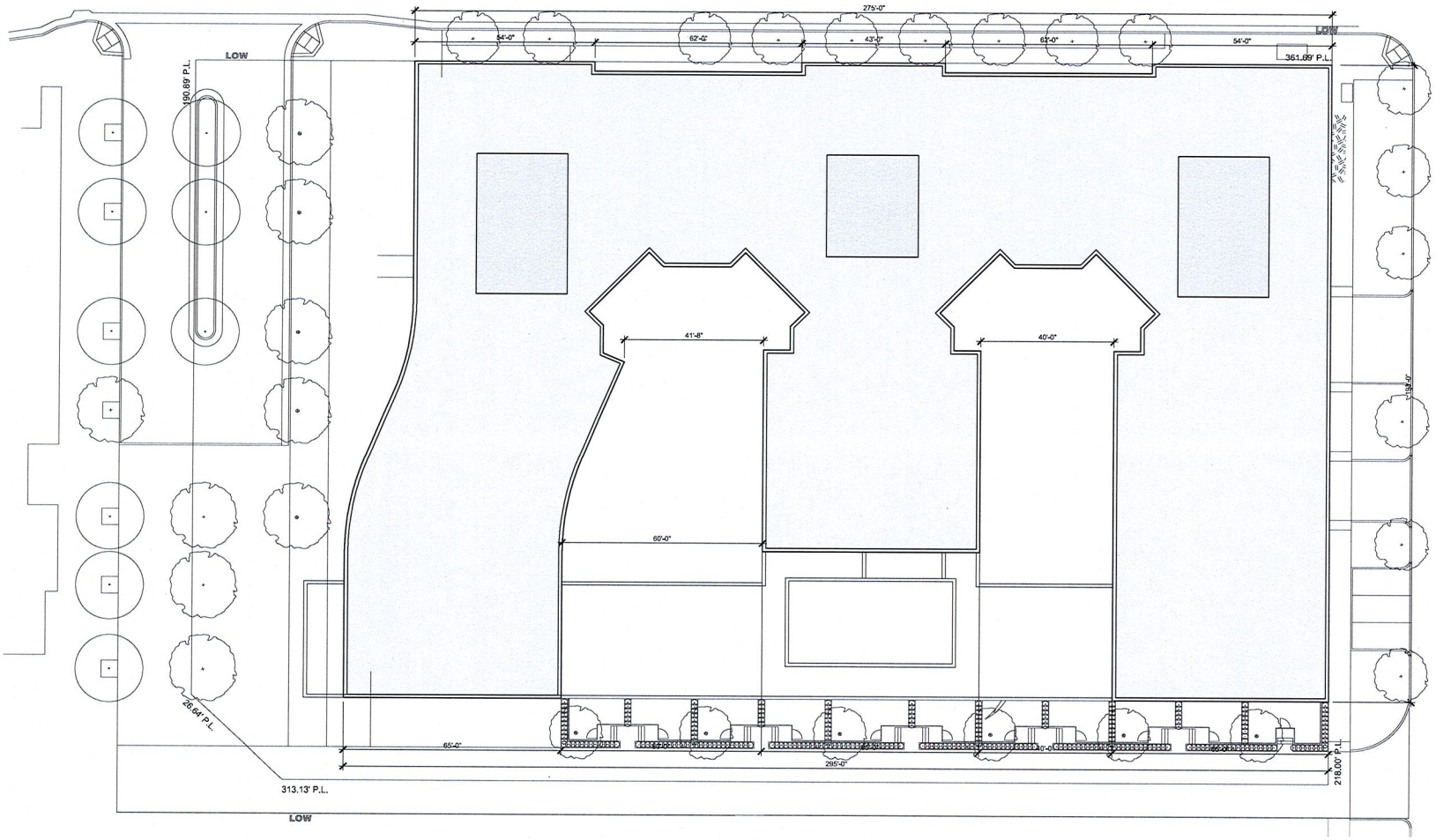
F2 - 4.5



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ROOF PLAN

PLANNING COMMISSION HEARING

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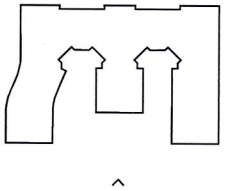
F2 : 2016013.000

F2 - 4.6

1. EXTERIOR INSULATION AND FINISH SYSTEM
2. PRE-FINISHED ALUMINUM WINDOW FRAME (DARK)
3. MASONRY CLADDING
4. HIGH PERFORMANCE INSULATED GLAZING (LOW-E COATING)
5. WOOD PANEL / SOFFIT DETAIL
6. PERFORATED METAL PANEL



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SOUTH (WATER STREET) ELEVATION
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
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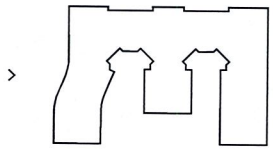
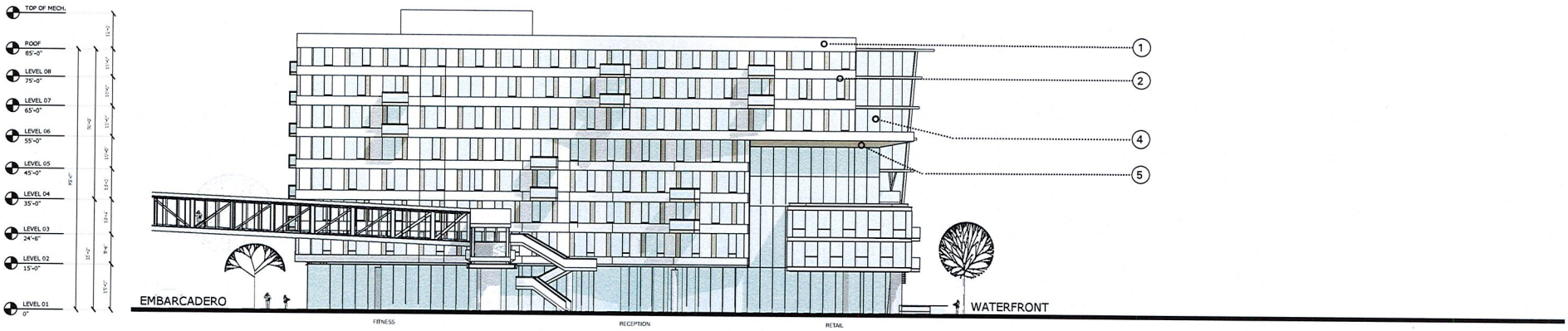
F2 : 2016013.000

F2 - 5.1

1. EXTERIOR INSULATION AND FINISH SYSTEM
2. PRE-FINISHED ALUMINUM WINDOW FRAME (DARK)
3. MASONRY CLADDING
4. HIGH PERFORMANCE INSULATED GLAZING (LOW-E COATING)
5. WOOD PANEL / SOFFIT DETAIL
6. PERFORATED METAL PANEL





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WEST (HARRISON STREET) ELEVATION
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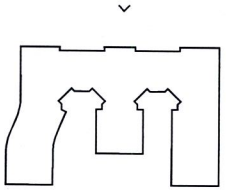
03 - 16 - 2017

F2: 2016013.000

F2 - 5.2

1. EXTERIOR INSULATION AND FINISH SYSTEM
2. PRE-FINISHED ALUMINUM WINDOW FRAME (DARK)
3. MASONRY CLADDING
4. HIGH PERFORMANCE INSULATED GLAZING (LOW-E COATING)
5. WOOD PANEL / SOFFIT DETAIL
6. PERFORATED METAL PANEL



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NORTH (EMBARCADERO) ELEVATION
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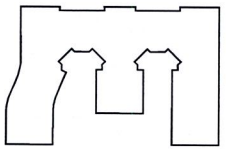
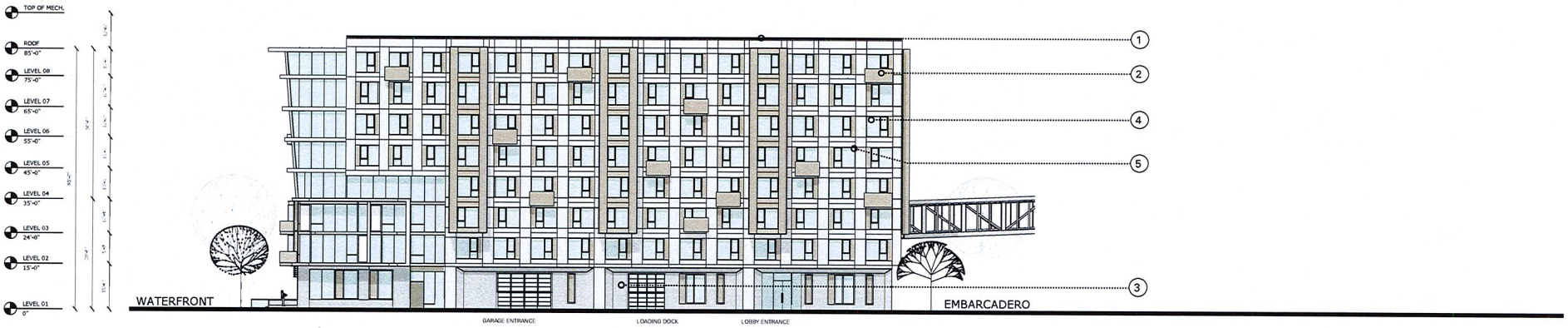
F2 - 5.3

1. EXTERIOR INSULATION AND FINISH SYSTEM
2. PRE-FINISHED ALUMINUM WINDOW FRAME (DARK)
3. MASONRY CLADDING
4. HIGH PERFORMANCE INSULATED GLAZING (LOW-E COATING)
5. WOOD PANEL / SOFFIT DETAIL
6. PERFORATED METAL PANEL





0 16' 32'

CIM

EAST (ALICE STREET) ELEVATION
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
CIM Group

03 - 16 - 2017

F2 : 2016013.000

F2 - 5.4

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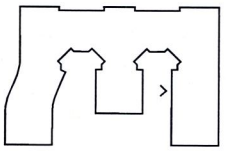
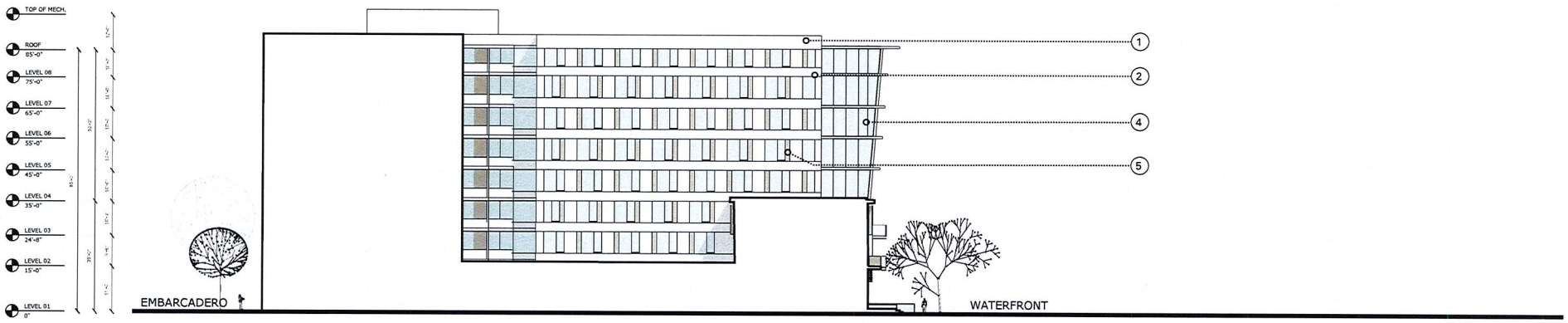
1. EXTERIOR INSULATION AND FINISH SYSTEM
2. PRE-FINISHED ALUMINUM WINDOW FRAME (DARK)
3. MASONRY CLADDING
4. HIGH PERFORMANCE INSULATED GLAZING (LOW-E COATING)
5. WOOD PANEL / SOFFIT DETAIL
6. PERFORATED METAL PANEL





0 16' 32'

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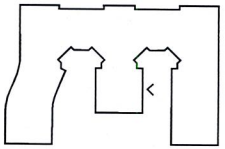
EAST COURTYARD - EAST ELEVATION
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
CIM Group

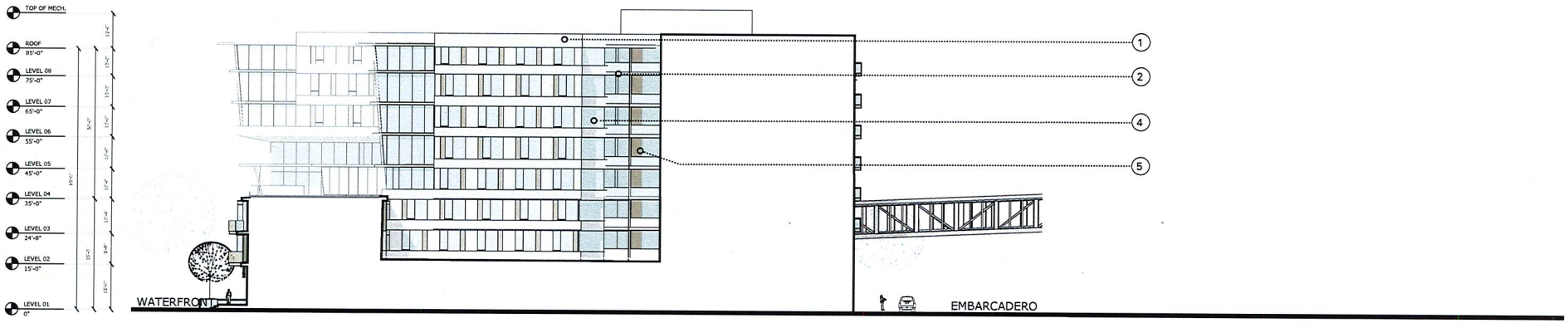
03 - 16 - 2017

F2: 2016013.000

F2 - 5.5

1. EXTERIOR INSULATION AND FINISH SYSTEM
2. PRE-FINISHED ALUMINUM WINDOW FRAME (DARK)
3. MASONRY CLADDING
4. HIGH PERFORMANCE INSULATED GLAZING (LOW-E COATING)
5. WOOD PANEL / SOFFIT DETAIL
6. PERFORATED METAL PANEL





0 16' 32'

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EAST COURTYARD - WEST ELEVATION
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
CIM Group

03 - 16 - 2017

F2: 2016013.000

F2 - 5.6

1. EXTERIOR INSULATION AND FINISH SYSTEM
2. PRE-FINISHED ALUMINUM WINDOW FRAME (DARK)
3. MASONRY CLADDING
4. HIGH PERFORMANCE INSULATED GLAZING (LOW-E COATING)
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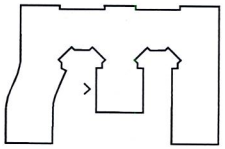

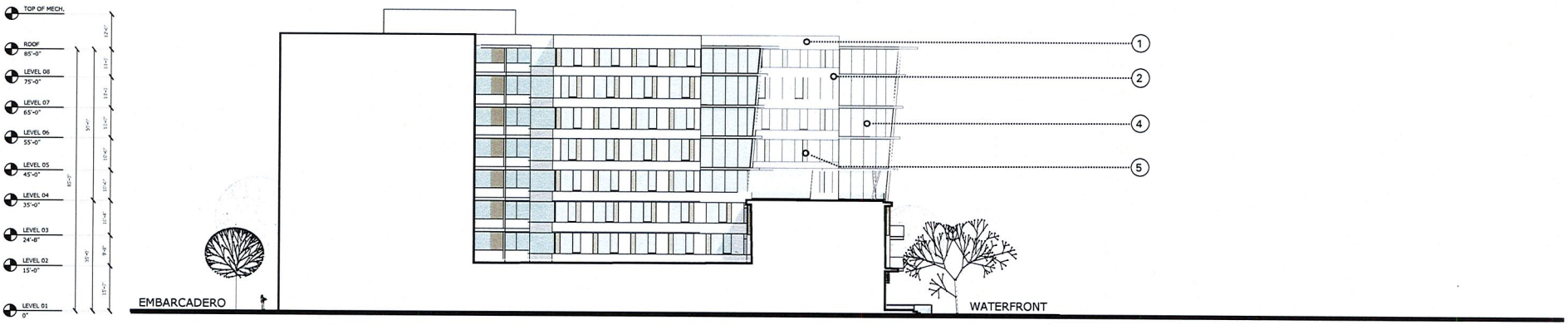

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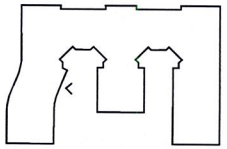
WEST COURTYARD - EAST ELEVATION
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
CIM Group

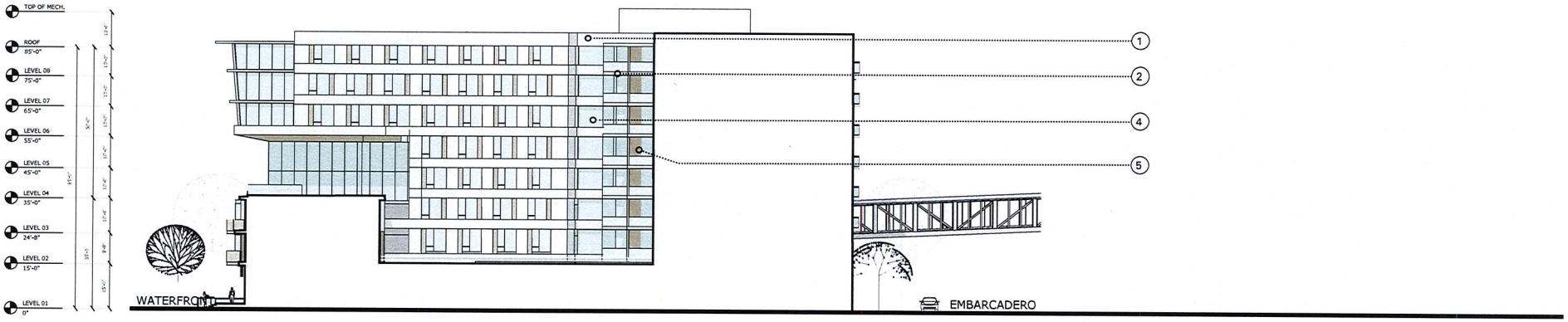
03 - 16 - 2017

F2: 2016013.000

F2 - 5.7

1. EXTERIOR INSULATION AND FINISH SYSTEM
2. PRE-FINISHED ALUMINUM WINDOW FRAME (DARK)
3. MASONRY CLADDING
4. HIGH PERFORMANCE INSULATED GLAZING (LOW-E COATING)
5. WOOD PANEL / SOFFIT DETAIL
6. PERFORATED METAL PANEL





0 16' 32'

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WEST COURTYARD - WEST ELEVATION
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
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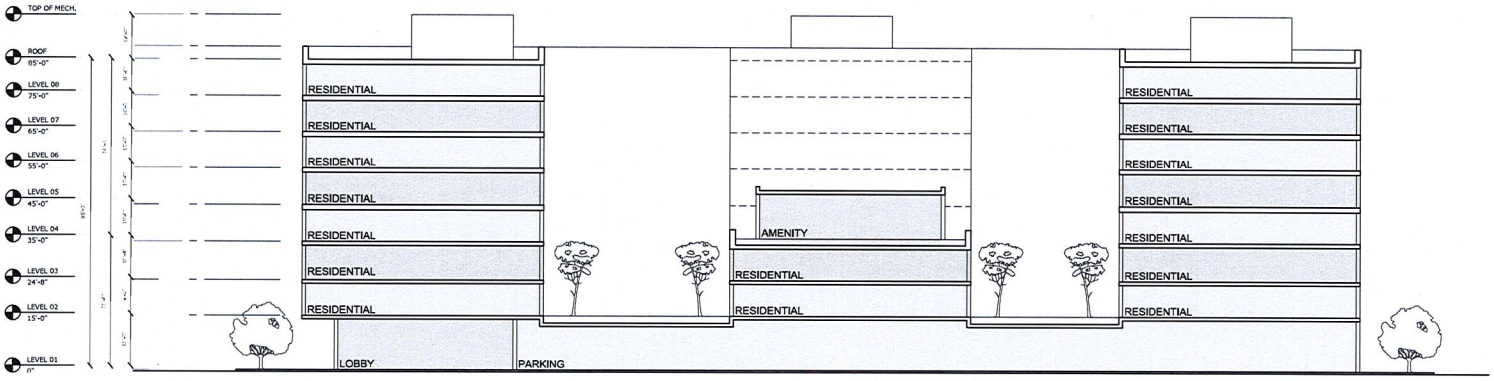
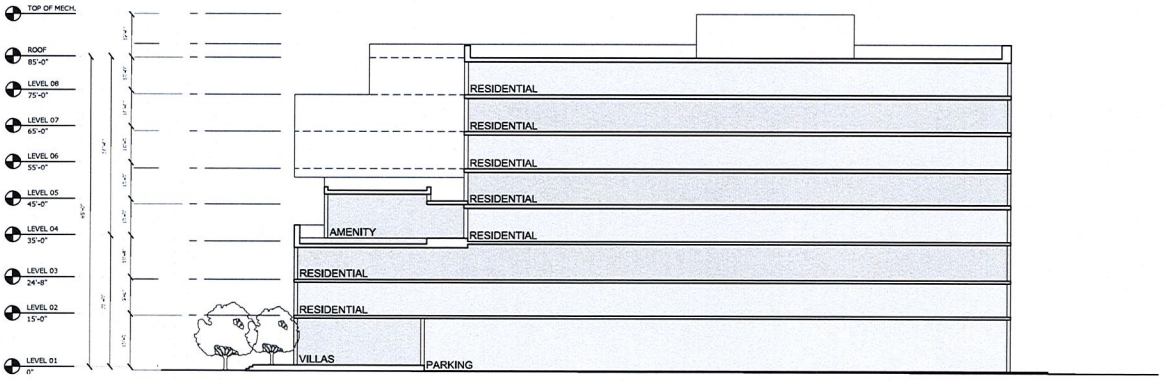
F2 - 5.8





0 16' 32'

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SECTIONS

PLANNING COMMISSION HEARING

Jack London Square - Parcel F2

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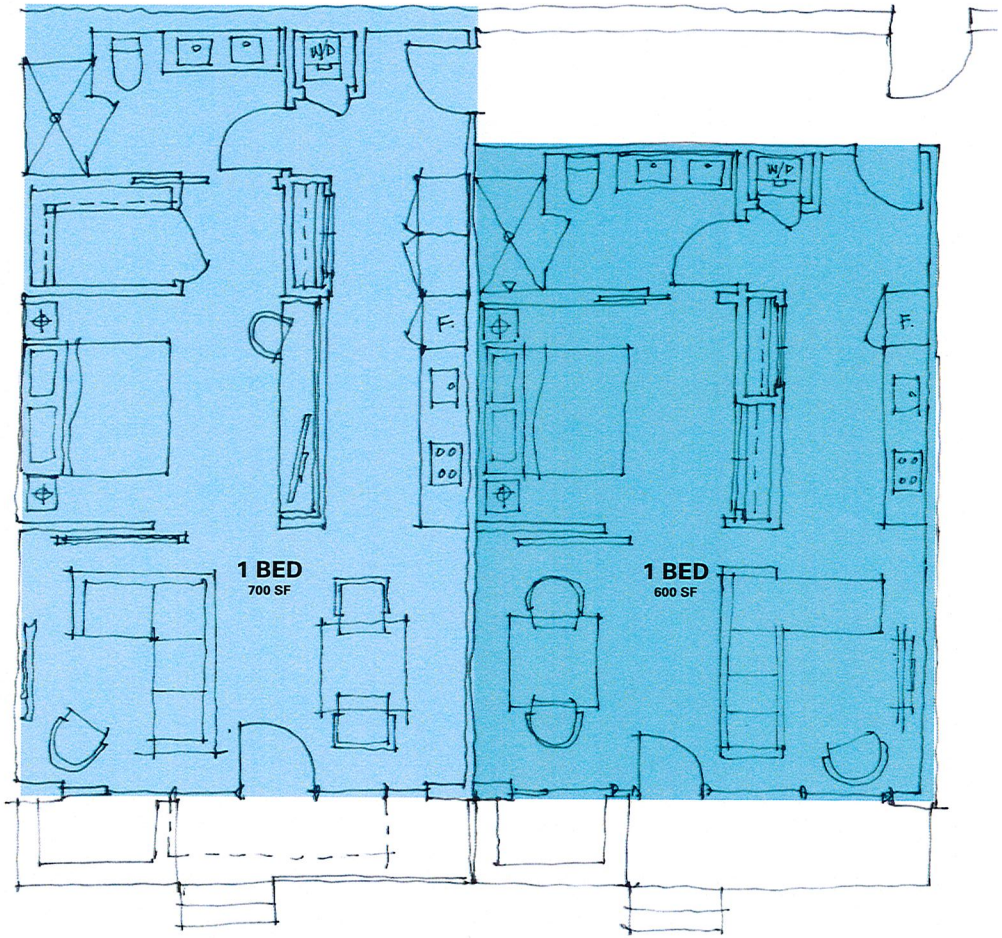
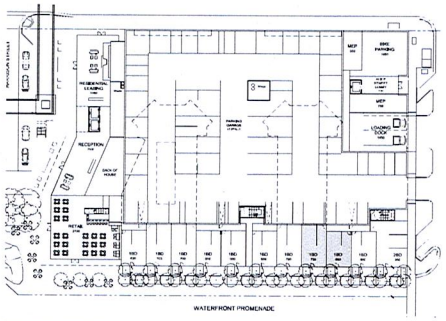
F2: 2016013.000

F2 - 6.1





GROUND FLOOR

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UNIT PLANS
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
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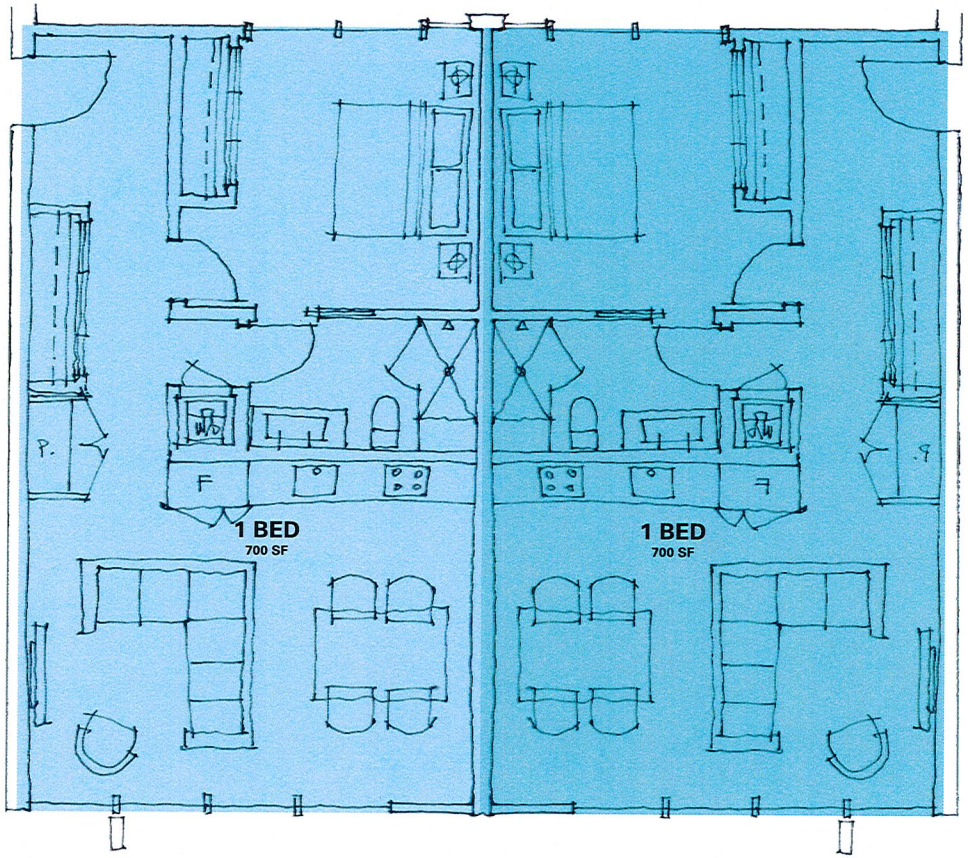
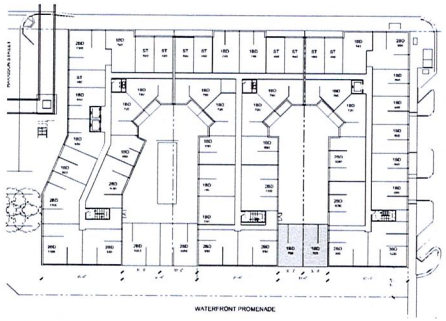
F2: 2016013.000





LEVELS 2 AND 3

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UNIT PLANS
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
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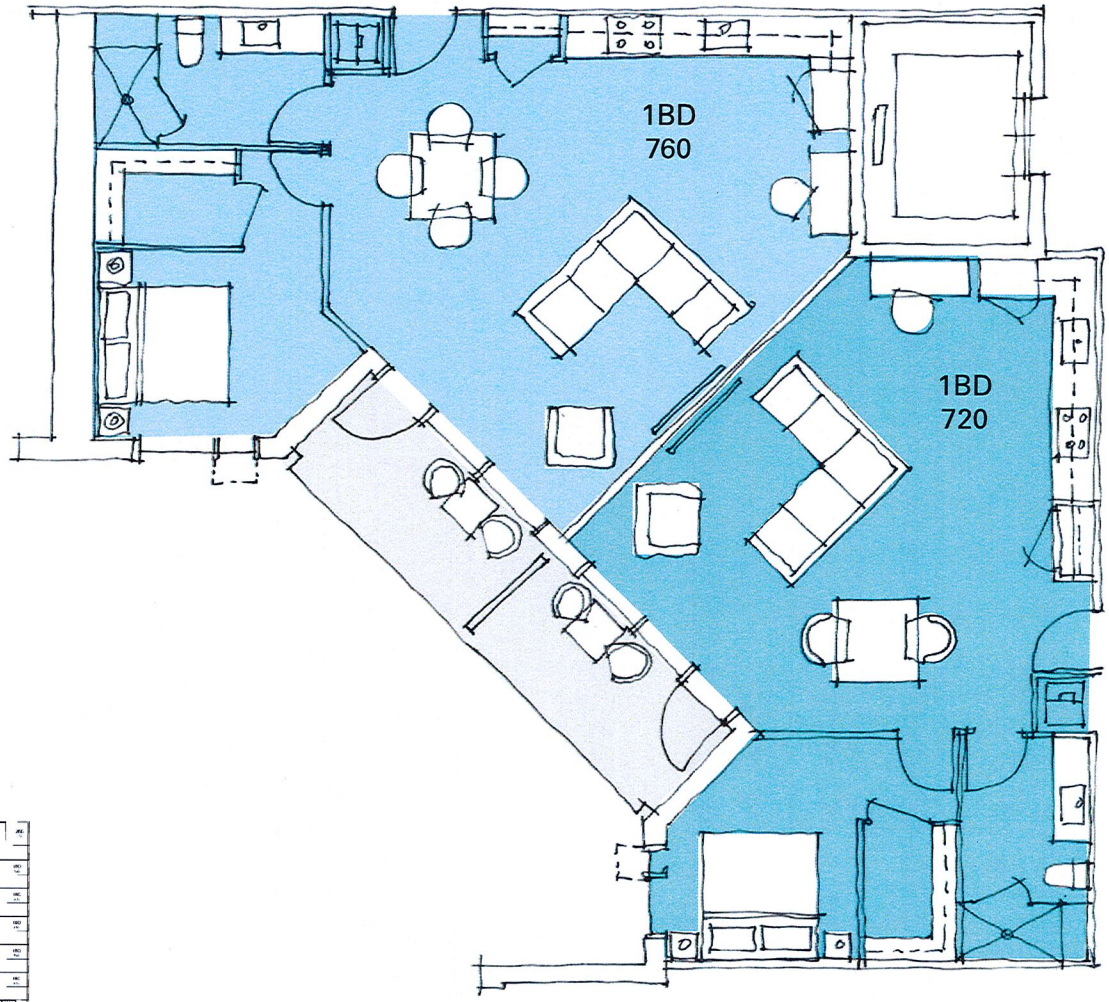
03 - 16 - 2017

F2: 2016013.000



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UNIT PLANS
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
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03 - 16 - 2017

F2: 2016013.000

1 EXTERIOR INSULATION AND FINISH SYSTEM

2 PRE-FINISHED ALUMINUM WINDOW FRAME (DARK)

3 MASONRY CLADDING

4 HIGH PERFORMANCE INSULATED GLAZING (LOW-E)

5 WOOD PANEL / SOFFIT DETAIL

6 PERFORATED METAL PANEL

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MATERIAL SELECTION
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F2 - 8.0

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RENDERING - SOUTHWEST AERIAL VIEW
PLANNING COMMISSION HEARING
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F2 - 8.1

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RENDERING - SOUTHWEST AERIAL VIEW ALTERNATE
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F2 - 8.2

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RENDERING - WATERFRONT
PLANNING COMMISSION HEARING
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F2 - 8.3

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RENDERING - VIEW FROM HARRISON
PLANNING COMMISSION HEARING
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CIM Group

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F2: 2016013.000

F2 - 8.4

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RENDERING - VIEW FROM EMBARCADERO-HARRISON ST.
PLANNING COMMISSION HEARING
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F2: 2016013.000

F2 - 8.5

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RENDERING - VIEW FROM EMBARCADERO-ALICE ST.
PLANNING COMMISSION HEARING
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CIM Group

03 - 16 - 2017

F2: 2016013.000

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RENDERING - VIEW FROM HARRISON (TWILIGHT)
PLANNING COMMISSION HEARING
Jack London Square - Parcel F2
CIM Group

03 - 16 - 2017

F2: 2016013.000